



## AGENDA

### MEETING OF THE MINTURN PLANNING COMMISSION Minturn, CO 81645 • (970) 827-5645

**The public is welcome to join in person at 302 Pine Street or using the following methods:**

**<https://us02web.zoom.us/j/87566873957>**

**Phone:**

+1 651 372 8299 US

+1 301 715 8592 US

**Meeting ID: 875 6687 3957**

**OR 302 Pine Street Council Chambers for In-Person Option**

**Wednesday, August 11, 2021  
Regular Session – 6:30 PM**

**CHAIR – Lynn Teach**

**COMMISSION MEMBERS:**

Jeff Armistead

Elliot Hovey

Tom Priest

Christopher Manning

Jena Skinner

When addressing the Commission, please state your name and your address for the record prior to providing your comments. Please address the Commission as a whole through the Chair. All supporting documents are available for public review in the Town Offices – located at 302 Pine Street, Minturn CO 81645 – during regular business hours between 8:00 a.m. and 5:00 p.m., Monday through Friday, excluding holidays.

### **Regular Session – 6:30pm**

**1. Call to Order**

- Roll Call
- Pledge of Allegiance

**2. Approval of Agenda**

- Items to be Pulled or Added
- Declaration of Conflicts of Interest

**3. Approval of Minutes**

- July 28, 2021

**4. Public comments on items, which are NOT on the agenda (5min time limit per person)**

**5. Planning Commission Comments**

**DESIGN REVIEW AND LAND USE PUBLIC HEARINGS**

**6. Dowd Lift Station 4 Replacement**

Review of the Dowd Lift Station 4 Replacement.

**Recommendation:** Approval

**7. 832 Main Street – Jones Residence Alteration of Approved Plans for New Single Family Residence**

Review and approval of minor alterations to plans originally approved March 24, 2021.

**Recommendation:** Approval

**8. 151 Main Street – Conceptual Review of a Mikvah**

Conceptual review of a mikvah at 151 Main Street.

**Recommendation:** N/A

**PROJECTS AND UPDATES**

**9. Project Updates**

- Community Plan Update

**10. Planning Director Report & Minor DRB Approvals by Director**

- 105 Williams Street – Sign for RDC Architecture
- 152 Main Street – Sign for Helen’s House
- 195 Pine Street – Flag Pole for Firehouse
- 223 Boulder Street – Fence Approval
- 511 Main Street – Fence Approval
- 160 Railroad Avenue – Agaves Temporary Sign

**11. Future Meetings**

- July 28, 2021
- August 11, 2021

**12. Adjournment**



## OFFICIAL MINUTES

### MEETING OF THE MINTURN PLANNING COMMISSION Minturn, CO 81645 • (970) 827-5645

**The public is welcome to join in person at 302 Pine Street or using the following methods:**

**<https://us02web.zoom.us/j/85194015667>**

**Phone:**

+1 651 372 8299 US

+1 301 715 8592 US

**Meeting ID: 851 9401 5667**

Due to an anticipated large viewing, and restrictions within the virtual meeting platform, the Town is making additional efforts during the COVID pandemic to make these meetings as readily accessible to all. If you do not intend to actively participate, please view the meeting on the Town of Minturn's [YouTube](#) channel where it will be live streamed and archived. For members of the public without internet access, there is availability at the Town Hall for meeting viewing, reservations are required

**Wednesday, July 28, 2021  
Regular Session – 6:30 PM**

**CHAIR – Lynn Teach**

**COMMISSION MEMBERS:**

Jeff Armistead

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### **Regular Session – 6:30pm**

**1. Call to Order**

- Roll Call

Lynn T. called the meeting to order at 6:30 pm.

Those present at roll call: Lynn T., Tom P., Chris M., and Jeff A.

Staff Members Present: Town Planner Scot Hunn, Town Manager Michelle Metteer, Town Attorney Richard Peterson-Cremer, Town Engineer Jeff Spanel and Planner I

Madison Harris.

- Pledge of Allegiance

## 2. Approval of Agenda

- Items to be Pulled or Added  
Motion by Jeff A., second by Tom P., to approve the agenda as presented.  
Motion passed 4-0.  
*Note: Elliot H. and Jena S. are excused absent.*
- Declaration of Conflicts of Interest  
No Conflicts of Interest

## 3. Approval of Minutes

- July 14, 2021

Lynn T. had a few minor corrections. Madison H. had a few minor corrections on the conditions associated with 261 Main Street.

Motion by Jeff A., second by Chris M, to approve the minutes of July 14, 2021 as amended. Motion passed 4-0.

*Note: Elliot H. and Jena S. are excused absent.*

## 4. Public comments on items, which are NOT on the agenda (5min time limit per person)

No Public Comment.

## 5. Planning Commission Comments

No Planning Commission Comment.

# DESIGN REVIEW AND LAND USE PUBLIC HEARINGS

## 6. Minturn North PUD – Railroad Planned Unit Development Preliminary Plan Review

**Recommendation:** Approval with Conditions

Scot H. introduced the public hearing. He summarized the criteria that the PUD should be reviewed by, and gave a brief summary of what is being proposed. He listed the variation requests that the Applicant is asking for, as well as, the areas of conformance and the remaining outstanding issues.

Greg Sparhawk, 245 Pine Street, Minturn, CO 81645.

The intent is to seek Phase 1 approval. There are outstanding studies that need to be completed for Phase 2 approval. This project is an extension of Town. There is a diversity in size and scale of the units and thus the attainability. People will come in and build what they can afford, not every lot will be maximized. There are two pocket parks in Phase 1 and another park in Phase 2 along with the trailhead extension and parking

for Game Creek Trail. Some amenities have changed from the original iteration. The ski-way has been removed. Will deed restrict 21% units (cottage lots). There will be a 1% additional transfer fee. 95% of deposits are from locals. Will remove building height calculation variation, will instead rely on the MMC. There has been 63,000 square feet identified for snow storage along with windrows along the road. A new stormwater system will be put in. Will be straightening, re-paving, provide parking, curb and gutter, and sidewalks or ECO-trail along Taylor and Minturn Road. Will be putting in a deceleration and left hand turn lane at the Highway 23 and County Road intersection. Will be cleaning up signage and the intersection at the Highway 24 and Bellm Bridge intersection.

**Public Comment Opened.**

Hany Malek, 498 Taylor Street.

Been opposed since 2019. Many people feel like the opposition has been ignored. Have a petition of about 60 people opposing this development. He then read the description of the petition. Had comments on driveways, straightening Taylor Street, turning Taylor Street into a one way. Doesn't like the construction time.

Chris Rieder, 332 Taylor Street.

Had concerns about actual affordability. This plan doesn't address housing issues, just provides small vacation homes. This is being compared to Taylor's density which already isn't ideal.

Rob Gosiewski, 560 Taylor Street.

Thinks that alleys between the backs of lots is important for access. Thinks they should try to stick to the rules. Minturn Road isn't usable during mud season so everyone has to go by the Saloon. This is too much traffic using one way in and out. Density should be dialed back. Small lots are tricky to build on. Everyone should be happy with the end product.

Karl Kruger, 362 Taylor Street.

Had a house in Minturn for 16 years. Doesn't buy the locals part of the housing program. Doesn't believe in experimental zoning within PUDs. Roofs need reflectivity regulations. Don't want Taylor Street to look down on two tiers of roofs. Don't want to allow gravel drives. Surprised ADUs are allowed. Surprised the 13% upzoning has been allowed. Wants to make sure that the dirt from Phase 1 will not be allowed to be piled onto Phase 2.

Anthony Martinez, 232 Taylor Street.

Concerned about roof heights. Have wildlife concerns. Snow storage concerns. Wants to know what will happen on the west side of Taylor Street.

Candace Jenkins, 103 Lions Lane.

Has concerns about the legal access. Thinks the Minturn Towne Homes have an implied easement on the property. Bringing all of this traffic behind the Saloon is not feasible. There are too many variances being asked for.

David Clapp, 392 Taylor Street.

Thinks this project is an extension of the Minturn Towne Homes. Thinks it is too dense. Thinks it could be done without variances and by the code.

Michael Gottino, 110 Taylor Street President of the Minturn Towne Homes.

The property is seeking tremendous variances. Why do we have rules if we are just going to break them? He showed a three inch binder which is 1 of 16 in a lawsuit against the developer. Has safety concerns at the S-Turn. The core issues of this development has not been addressed. Asked the Planning Commission not to ignore what happened at the Minturn Towne Homes. He reference and submitted the Bornengineering Construction Compliance Evaluation Report on the Minturn Townhomes generated on September 8, 2008.

Justine LaCoss, 322 Taylor Street.

The Minturn Towne Homes drainage has not been fixed. Thinks it isn't affordable. Works from home. Thinks it is irresponsible to be building this development with the water shortages.

Michael Ganarche, 1010 Main.

This development seems to match the Town. Fixes an eyesore. Minturn is not a street, it's a Town, there are other people here. First time that he thought that there was a chance that he could own a home. Supports this project. If something like this doesn't get developed, then it will just be the same houses that are already on Taylor or it will become a resort community.

Kristen Pinner, 177 Hummingbird Trail, Homestead.

Does have financial gain if this project is approved. She is born and raised here. Never been able to afford Minturn. To be able to buy a lot here and finance it for what works for your family is a dream. The idea of this is to be organic growth.

Stevenson Smith, 112 Taylor Street.

Would encourage people to think about what is important about Minturn. The reality of the Minturn Town Homes, even with normal traffic, pulling out of the driveway is incredibly dangerous.

Trent Schaffler, 105 Lions Lane.

By breaking the code (allowing the variations), there will be a haphazard development. The Planning Commission should take a look at what the Town represents and how we want to see development in this area. Project density is too dense. One way street proposal is absurd.

*Note: Public comment submitted after the packet went out, but before the meeting will be included as an appendix to these minutes.*

*Note: 5 minute recess at 9:00 pm.*

Tom P. thanked Staff and Applicant for all the work that has been done. Asked about access to the property.

- Richard P-C said that there are still minor issues to work out and that it is getting close, but there are still a couple of outstanding issues.
- Tom P. believes that everyone is in agreement that the Town needs to grow. The fundamental problem is that the only thing being considered by UP is what is currently on offer. The infrastructure doesn't support it. Railroad Ave. is not a walkable, safe pedestrian corridor with vehicle traffic. Based on the package and comments there is Minturn Road and Taylor Ave. The S-curve is a big issue. The infrastructure to support the max is not appropriate. The one-way will generate aversion. The southern lot needs to be squared to make a safe intersection. Need to minimize the impacts to the neighboring property. Had questions about water. Would like a long warranty period.

Jeff A. agrees with Tom P. about the warranty period. A problem is that there is still issues about what the common elements will be. Need to keep in mind that this parcel has no code associated with it. This is the first time that rules will be on it. Rules vary by zone. There can't be blanket statements on density. There is a historic record of people having smaller lots than 5,000 square feet. Generally supportive of the variance requests. Doesn't agree with the one-way on Taylor Street. The impact to Taylor Street is undeniable. Since the Applicant is trying to get vesting for both phases, it is important that we look down the road to any potential problems with Phase 2. There is more room for discussion.

Lynn T. pointed out the discrepancy between irrigation numbers.

- Mr. Sparhawk said that there are specific square footages that is allotted due to lot size.
- Lynn T. thinks that short term rentals shouldn't be allowed. Would like to see public bathrooms added back in.
- Mr. Sparhawk said they are open to it, just trying to figure out potential liability and the ownership/management.
- Michelle M. said that the bathroom would be for trailhead users. There are complications that come with public restrooms. Staff is looking into it, but not wanting to put more burden on the public for the upkeep.

Chris M. asked if the public could use the parks.

- Mr. Sparhawk said that it is under discussion with the Town.
- Chris M. wants to see families in Town. Likes the plan, but has concerns with what everyone brought up.
- Chris M. asked about the cash-in-lieu fee for the School District
- Mr. Sparhawk said they were not planning on submitting one as it isn't required.
- Chris M. does not like the one way. Thinks people will maximize the lots where they can.

Lynn T. would like to address the concerns about the noise. Would like to see the hours of construction limited to something like 7-5 and not weekends.

Jeff A. thinks that the HOA needs to provide someone to go around and make sure that the covenants and code is being adhered too.

Tom P. would like confirmation of access and stated that the s-curve and the infrastructure is a problem for him

Jeff S. said that the intersection of Taylor Street and Minturn Road has been a concern the whole time. Need clarification from the traffic engineer. There is no physical barrier forcing it into a one way street.

Motion by Jeff A., second by Tom P., to continue to August 25, 2021. Motion passed 4-0.

*Note: Elliot H. and Jena S. are excused absent.*

## PROJECTS AND UPDATES

### 7. Project Updates

- Community Plan Update
  - Will start interviews soon.

### 8. Planning Director Report & Minor DRB Approvals by Director

- None

### 9. Future Meetings

- August 11, 2021
  - Tom P. will not be here.
- August 25, 2021

### 10. Adjournment

Motion by Chris M., second by Tom P., to adjourn the regular meeting of July 28, 2021 at 10:37 pm. Motion passed 4-0.

*Note: Elliot H. and Jena S. are excused absent.*

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Lynn Teach, Commission Chair

ATTEST:

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Scot Hunn, Planning Director



## Madison Harris

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**From:** Andrew Cryer <acryer13@gmail.com>  
**Sent:** Tuesday, July 27, 2021 11:17 AM  
**To:** Madison Harris  
**Subject:** Minturn North Comments

Hi Madison,

I'm writing to show our support for the Minturn North PUD approval tomorrow night as we have a deposit in for one of the lots. Thank you for reading.

We are a young local family and had our first daughter in Minturn. After, we moved to Eagle-Vail because we needed more space for our growing family. We recently had our 2nd child, a boy.

During the Covid real estate price boom in our area, many of our friends (young families) in our age group have moved from the area. They simply can't make sense of the finances of living here anymore.

We are very excited about the opportunity to move back to Minturn and raise our family there. Minturn North seems like it may be our only opportunity to stay in the area at a reasonable price tag. We think this new community will bring vibrancy to Minturn but will also be an asset to the many aspects of Eagle County that will be impacted by its residents.

Kind regards,

--

Andrew James Cryer  
970-376-1029

## Madison Harris

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**From:** Chris Rieder <crieder588@gmail.com>  
**Sent:** Tuesday, July 27, 2021 5:37 PM  
**To:** Madison Harris  
**Cc:** helen rieder  
**Subject:** Minturn North PUD

I plan to be in person for the meeting tomorrow to share my perspectives first hand, but thought it prudent to submit an email as well.

Dear Planning Commission,

I am writing in advance of the commission meeting on the 28th, with regards to the review of the Minturn North PUD application. As a resident of Minturn, specifically Taylor St, for over a decade; this development is of significant interest and concern to myself and my family. My wife and I are raising our two young children in the neighborhood and have come to love the quiet and friendly atmosphere here on Taylor St. This project brings concerns regarding alignment with town vision, quality of life for existing residents, and safety for the community.

I understand that development in general, and of this particular parcel, is inevitable. This project, as proposed, however, is unacceptable; based not just on subjective opinions, but on the very Minturn Town Codes created to guide this commissions decisions on proposed projects. I will do my best to keep this brief, but know my concerns span well beyond the few following written points.

*Minturn Town Code 16-15-10 (b)*

*The proposed PUD is consistent with the Community Plan and the character of the town and (12) establishes incentives for applicants to encourage the provision of long term affordable housing.*

In their narrative they state that the PUD will provide an ‘opportunity for Minturn and Eagle County residents that does not currently exist’. They are saying that by selling a variety of lots and allowing for the over development of each of the lot sizes, through duplexes and ADUs, that they will create ‘affordable options’. There are no deed restricted options, lots will be going at market rate, and the cost of construction is astronomical. No average local resident will be able to purchase a lot and build a home on it. We will see a huge increase in largely vacant second homes, short term vacation rentals, and developers buying up multiple lots to cash in on the opportunity.

I know the vision of the community is to maintain its small town ‘local’ character and this development will likely become another large congregation of properties owned by out of towners and the wealthy. Some of them may rent places to locals at stupidly high market rates, but options that benefit long term affordable housing options for locals are not actually provided through this proposal. The picture they are trying to paint is that through variety of lots they are addressing it, but they are not willing to sacrifice any profit to make lots actually affordable to locals.

*Minturn Town Code 16-15-160 (1c)*

*The adjunct and nearby neighborhoods will not be detrimentally affected by the proposed PUD.*

The developer has largely neglected to consider what the impact of adding 30-50% of the town’s population will have on Taylor Street and Minturn residents. I will speak more about traffic and safety in the next section, but adding that many dwellings in a previously undeveloped area is very detrimental to the neighborhood. They

are utilizing 'potential density' of Taylor Street to justify the obscene number of allowable units. If any of you have walked down Taylor Street, I think we can agree that the density is already not ideal and they are utilizing a density rate higher than what it already is. Why determine density for a new development based on an unideal situation? Because it allows them to make more profit.

The other hugely detrimental affect on current residents is the construction timeline. (*Minturn Town Code 16-15-140 (a) The PUD can be **completed within a reasonable amount of time.***) The developer is selling this as an opportunity for each owner to build a unique home. They speak to time frames in taking care of their infrastructure projects, but not with regards to the time frame of construction in the development. The home construction will occur over 3-10 years. At that rate my 2nd grader will be off to college by the time our front yard construction project is complete. Though I do appreciate the proposal trying to avoid a bunch of cookie cutter, mass constructed boxes, I also do not want my family to have to endure construction traffic, noise, and pollution during the remainder of their childhood. They are once again punting responsibility for time frame once they sell their lots and make their money.

*Minturn Town Code 16-15-70 (3)*

*The design and construction of the PUD shall include **adequate, safe, and convenient** arrangements for pedestrian and vehicular circulation, off street parking and loading space with access adequate to support anticipated traffic, on and off site, including the emergency and utility vehicles.*

*Minturn Town Code 16-15-160 (j)*

*The streets are adequate to support anticipated traffic and the development **will not overload streets outside the planned area.***

Out of all of my concerns regarding this project, as a father, safety out ranks the rest. From the beginning of this proposal the traffic and the access/egress of vehicles had been a frequently discussed point. The main issues have been the S-turn, the intersection of Minturn Rd and Taylor, and the inability for County Road to handle the increased traffic. At this point the developer said that there's nothing that can be done with the S-turn due to the railroad, that their only solution for the intersection was to turn Taylor into a one-way (unacceptable), and that they are no longer improving County Rd (only putting in turn lane on Hwy 24). If these were the major sticking points and there is still no solution or real progress, then why are we proceeding with approval? At this point there are no effective or safe plans for how the increased traffic will enter or leave the area.

The traffic study was done during Covid on a weekday, so I consider it arbitrary. Taylor Street is busy and adding nearly 30 driveways accessing it is a huge spike in traffic. We have many young families on this street and the increased traffic without dedicated access through the development is a safety hazard. I am not even going to get into the safety hazard that is presented with some of the gradients that they are proposing on new intersections.

As stated before, though those are my chosen points, they only begin to encompass my concerns with this development. The planning commission is tasked with ensuring that proposals are consistent with Town code and appropriate for the community. This project, as it stands is still not either of those things. When this proposal was postponed to allow the developer to address issues that existed it was discussed that it could only be approved, approved with conditions, or denied. I am asking you to consider if there was any real progress made on the issues of alignment with town vision, quality of life/impact on current residents, or safety of roads and access. I very much understand that the developer has a bottom line and that some things are beyond their control, however, if they are unable to address issues and create reasonable solutions to clear problems in their project, then maybe they need to go back to the drawing board. It is not acceptable to approve an application that does not abide by Town codes and is detrimental on many levels to Town residents just to help a developer meet their budget or because they have been working really hard to try to make it work. I trust you will consider Town codes and your common sense judgement when considering this proposal on the 28th. Thank you very much for taking the time to hear my concerns.

Sincerely,

Chris & Helen Rieder  
332 Taylor St

## Madison Harris

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**From:** Duncan Robinson <duncanrobi@gmail.com>  
**Sent:** Wednesday, July 28, 2021 2:55 PM  
**To:** Madison Harris  
**Subject:** Minturn North PUD public comment

Duncan Robinson  
531 Taylor St

RE: Minturn North PUD

Town of Minturn Planning and Zoning Commission,

Please record and read this as public comment on the Minturn North Development at the July 28 2021 meeting.

Thank you for hearing comments and allowing this important, democratic process to occur. It allows the remaining developable land to be shaped the way the community approves.

From proposal materials related to the Minturn North Development my concerns fall under the following categories.

1. Density: To allow development that is denser than the town code and standard will adversely impact the neighborhood creating problems with parking, traffic flow, snow removal, and snow storage. The solution to these problems is space and the town's 5,000 ft<sup>2</sup> minimum lot size has proven to be a sustainable minimum.
2. Roads and Bridges: The developer has not provided a sufficient impact analysis and solution for the load the development will have on the supporting infrastructure of Minturn Road, the Saloon Bridge, Bellm Bridge, and Highway 24. I fear the town will be saddled financially with these improvements.

Minturn Road, one of the two egresses for the neighborhood, is a county maintained gravel road crossing railroad land. Referencing a conversation I had with Eagle County Road and Bridge regarding the degradation of road conditions this spring, they want to give the road control back to Minturn and mentioned the possibility of the road being closed by the railroad. Now imagine all of Taylor St, Minturn North, and the Railroad commercial users using the saloon bridge access to Hwy 24.

Bellm Bridge at the north end of Minturn Road is an ancient bridge that was widened before my time to allow for more traffic, but is not sufficiently wide for the traffic increase from Minturn North. Who will pay for a major project like a bridge upgrade/ replacement? This access to Hwy 24 also needs acceleration and deceleration lanes, another big ticket item.

Saloon Bridge is also a problematic intersection to hwy 24 that lacks any space for improvement.

3. Water: who shoulders the cost of the needed upgrades and new piping? Minturn a few years ago in a vote said no to the interconnect water project with Eagle River Water and Sanitation, and the current water infrastructure cannot handle the added load of Minturn North.

Minturn North's financial success requires maximizing the number of lots through density variances and avoiding the concerns adjacent to the property lines like roads and water. The road and water improvements to our town and surrounding area needed to support this bloated proposal make the development financially unattractive to a developer or would bankrupt our town.

Thank you for your time

Duncan Robinson

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Duncan Robinson  
[duncanrobi@gmail.com](mailto:duncanrobi@gmail.com)

## Madison Harris

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**From:** hany malek <hany.a.malek@gmail.com>  
**Sent:** Tuesday, July 27, 2021 4:34 PM  
**To:** Madison Harris  
**Subject:** Minturn North PUD  
**Attachments:** Hany Malek PUD Review Letter 12-11-2019.pdf

### To Minturn Planning, Town Council, and Staff (please include this as written public comments)

The purpose of this letter is to express my deep concerns and opposition to the proposed Minturn North PUD.

**Background:** My wife and I have owned our home on 498 Taylor Street for over a decade and have retired in this community. We have been part time residents in the valley for over three decades. We are both Professional Engineers (Civil/Geotechnical and Structural) with over 35 years of experience. We owned a design-build company in Denver that specialized in federal projects west of the Mississippi, and had a staff of engineers, architects, and construction crews.

This project is directly in front of our home and will have a significant impact on us. I took the time to browse through most of the documents posted online including drawings, reports, etc. and reviewed some of them in detail. I believe this land is a prime development opportunity and appreciate the positive impact it could have on the town's future. Thus, we recognize and accept some reasonable impact and changes.

I had looked at the Conceptual Plan of this PUD that was put forward in 2019 and submitted comments to the planning commission in a letter dated December 11, 2019 which I am attaching and would like to be included in the record. Since then, I also had various discussions with the town staff and even met with the developer and Scot Hunn in early 2020.

**The Problem:** I have quickly reviewed the planning commission packet for the 7-28-2021 hearing which includes the staff report on the Preliminary Development Plan for this PUD. I appreciate the significant effort that Staff has put into this project given the size of the application and its complexity. I understand that the package submitted to you by the staff may be overwhelming. There is a lot to absorb. As Staff pointed out, there are still outstanding issues with this application. Based on my review, some of the technical issues are more significant than represented. Furthermore, the impact on Taylor Street residents (brought up since 2019) are not properly addressed and even ignored by the Applicant. I believe a major redesign of the plan and application would be necessary to properly address those deficiencies. This proposed development will have a significant impact on the town and an overwhelming negative impact on Taylor Street residents. If in doubt, I urge you to take your time with this application, meet with Taylor Street residents, and dive into the details. I believe you will find that this application is deficient and should not be approved.

### Arguments:

Many concerns and opposing comments have been relayed to the staff, Planning Commission, and Town Council since 2019 by myself and by other concerned Taylor residents. Many concerned Citizens have taken the time to submit comments and come to hearings. Most of those comments are still valid and have not been properly addressed. Many citizens, including myself, feel that we are not being heard, or are being ignored. All public input related to this PUB, from as far back as 2019, should be brought forward and included, as they are pertinent for the review of this application, and should be properly addressed.

I will not repeat my entire letter dated December 11, 2019 or go in detail on points brought and explained by others such as by Chris Rieder and Karl Krueger, in your package. I will generally limit my comments to the most critical I have identified so far, that relate to the impact on Taylor Street Residents. My fellow neighbors and I would be more than happy to meet with you, staff or consultants onsite, to discuss those or other issues.

This PUB is within the Game Creek PUD Holding Zone which falls under Article 12, Section 16.12 of the code. Section 16-12-30 b) of the code specifically addresses the necessity of public input and in addition states: “...***Development in this area needs to incorporate appropriate residential and low-impact land uses along Taylor Avenue to minimize impacts to the existing neighborhood***...” (Added underline for emphasis)

This language would not apply to all PUDs but was written specifically to any PUD in this Character Area, and consequently to this proposed PUD. Dictionaries define “minimized” “as to reduce, decrease or lessen the smallest possible amount.”. The definition according to the law are even more restrictive.

This Application and development is very far from meeting this requirement. On the contrary, it creates unnecessary and totally avoidable impacts on the existing neighborhood. The Applicant has ignored feedback and comments and has blatantly pursued an approach to maximize lots by externalizing impacts and costs on the Neighborhood and the Town. This assessment is consistent with Inter-Mountain Engineering’s observation: “***The current design looks like density has been maximized ignoring good street design***” (February 8, 2021, Page 2, bullet 2 bottom of page)

Some key point where the Applicant has failed to minimize impact to residents are:

- Development and home construction on 95 individual lots will likely subject the existing Taylor residents to 10 or more years of continuous construction and is an unacceptable time frame. Other development models could easily “minimize” the impact to the existing residents.
- It is more difficult for people to afford to buy land and then get a loan to build on it. It is a wrong approach for addressing affordable housing and will set people up for failure.
- Existing lots on Taylor Street are on average 71% larger than the average proposed ones.
- The smaller lots will be difficult to develop, they are too small.
- Turning Taylor Street to one way, which has a significant impact on existing residents, instead of looking for a real solution to addressing the hazardous intersection, is unacceptable.
- Adding 28 new driveways on Taylor Street and 29 on Minturn Road is a pure example of externalization and pushing many problems onto the community and town instead of trying to minimize them. This design approach to maximize land use is the primary contributor to most of the existing concerns including low parking counts, limited snow storage, loss of usable sidewalks, increased hazards to pedestrians, children, and vehicles, and impact on traffic flow. A layout which eliminates most driveways along Taylor Street and Minturn Road and make use of alleys and new roads to access all properties would significantly “minimize” the impact to the existing neighborhood. This was rejected by the Developer during 2019 hearings and a subsequent meeting.
- Eliminating some existing parking (and landscape) on both sides of Taylor Street, and not having a real and workable solution for the parking, even after it has been brought up multiple times for over a year. (Parking is separately addressed below.)
- Snow storage is grossly underestimated and not properly addressed. (This is addressed separately below.)
- Not properly addressing how to manage the “Milers” skier issues. The path parallel to Taylor Street, to be used for “Milers” to ski to town has been removed because of some town concerns. This was a key point of discussions during previous hearings both for safety and to prevent skiers from going down Taylor Street. Instead of addressing the issue and coming up with a reasonable solution, the applicant plans now is to send them to Minturn Road which has private residences on one side and snow storage (piles) on the other. Having “Milers” go straight to Minturn Road and then walk all the way to town will only encourage them to keep skiing down on Taylor Street or walk on Minturn Road with the traffic. Both are hazardous and worse solutions than the current condition.
- Not improving and paving County Road. Or considering it as part of Phase 2.
- Proposing to use parks for snow storage.



The Applicant has praised this development for helping the affordable housing crisis in the county. This is unrealistic. It is much more difficult to qualify for two separate loans, one for the land and another one for a construction loan, than to buy a finished dwelling with one loan. If someone can pay for both loans and still pay for 2 or 3 years of rent while their house is being built, they would not likely qualify for affordable housing. When interest rates go up, as predicted, they may not be able to complete the home or afford to refinance to combine the two loans into one at a higher interest loan. Without proper financial resources, homes will take longer to be built and some may be left partially finished for a while. This would lengthen the construction period and be a nuisance to the neighbors.

This development has no provisions to accommodate people with disabilities or the elderly. The proposed Belden Place development is a much more sensible solution on this issue.

This approach for affordable housing is flawed; it does not help with the affordable housing crises and can even set people up for failure. It will not achieve the Town's goal. This is purely a way to justify a high-density development and make this development more appealing to the town and public and skirt the need to minimize impact on the existing neighborhood.

### **Lot Sizes**

The average existing individual lot on Taylor Street is 7,039 sf is 71% larger than the average proposed ones (from Sheets A11 and A12). In addition, this development adds the equivalent of approximately 40% of all of Minturn population into an area, a fraction of the size of the entire town. The proposed density does not fit the existing character of the neighborhood, and certainly does not show any attempt to minimize the impact on the existing Taylor Street neighborhood.

Lots on Taylor Street have been priced similarly but have taken a while to sell and develop. Lot sizes of 2,500 and 4,000 sf make it harder to fit a home within the setbacks, limit onsite parking and amenities, and result in a much higher cost per square foot. It does aggravate parking and snow storage and externalizes those issues onto the adjacent neighbors.

### **Parking**

The Taylor Street realignment will impact existing parking along the east side of Taylor Street. The full impact is very difficult to assess from the drawings. (example: in front of 564, 530, 492, 292 Taylor). In addition, it looks like some of the existing landscaping and features, some property owners have installed will be taken out. It is very difficult to see what will actually be done and its impact from the provided drawings, even for me. This needs to be clearly shown in the field so that the existing homeowners can properly understand the impact prior to any approval. Losing existing parking is a significant impact to existing neighbors. Taylor street realignment has some minor benefits but is not necessary. It should be performed without any reduction of parking along the east side and its impact fully disclosed to current residents.

As noted by staff the proposed parking count is deficient. Parking has been a significant concern and voiced very often by Taylor Street residents as insufficient and unrealistic since 2019. It is unbelievable that we still have a proposed similar and unworkable parking plan. The parking cannot be addressed without changes to the layout and/or reduction of the number of lots, something has to be given and it should not be by providing less parking. The Applicant is still ignoring the issue and hoping he can externalize the problem onto the existing residents, the town, and future owners. This is unacceptable.

The proposed 19 parking spots along the west side of Taylor Street (Sheets A13 and A14) is less than is currently available. Not only is the Applicant eliminating existing parking spots but expects those spots to be shared with 28 new lots and up to 53 new Units west of Taylor Street. The 23 parking spots along Minturn Road are also less than are used during Minturn Market days. These are to be shared with 29 new residences. Half of the lots on Minturn Road and Taylor Street do not have parking near them, some are even half a block away. This does not match the character of the town and is a safety hazard. It is unacceptable.

There is currently parking along the south side of Taylor Street (greater than 60) and along Minturn Road (at least 20). Those are enforced by the town and used by homeowners and guests. Those are existing parking spots and are not the developers. We should not allow the Applicant to appropriate them and take credit for the existing parking. The developer needs to provide additional parking beyond the existing ones. Residents have repeatedly brought up the high number of cars being parked at the trailhead only to be dismissed by the Applicant. I have seen over 40 cars parked from about 4<sup>th</sup> street to the trail head during the summer. Those numbers are real. Existing parking on both sides of Taylor and Minturn Road should be fully evaluated and analyzed and determined with current residents' input. The applicant should not be allowed to pick numbers that are convenient to him.

In 2019 Darin Tucholke of 530 Taylor Street and I had pointed out that we have steeper driveways that ice up in the winter, are slippery, and could be a hazard. Since they proposed to shift the road closer to our property and made the area narrower, we had asked to eliminate parking in front of our driveways. The applicant agreed to it but is now showing parking at that location. Another example of ignoring public comments. If the parking spaces cannot be removed, then the road location should stay as is.

### **Snow Storage:**

Snow storage is also an issue that has been expressed by staff and has been repeatedly brought up as a significant concern by the current Taylor Street residences since 2019.

Historically, during larger snowstorms, front end loaders have been used to store snow continuously along the roads in linear piles. Those piles have often been over my head (6 to 10 foot high), approximately 20 to 40 foot wide (can vary based on slopes) and were continuously on both sides of Minturn Road, on both sides of 4<sup>th</sup> avenue, and along the west side of Taylor Street. Some areas, such as near the intersection of Minturn Road and Taylor Street, the piles could be bigger. The historical snow piles have been where the applicant is proposing new parking, driveways, sidewalks, and where homes are proposed. This is a significant amount of existing snow storage that will have to be consistently moved in the winter. The snow storage areas proposed by the Applicant on Sheet A11 and A12 appeared to be grossly undersized and placed in areas that may be convenient for the Applicant but are difficult to use but not practical. The snow removal will likely be performed with front end loaders, will require long hauls, difficult pile stacking due to the geometry, resulting in a very inefficient process (even if it was possible to achieve). This will be a hazard to people, block or stop traffic, be noisy, and will be difficult to complete early in the morning. The impact to existing and new residents will be high. This will also be very costly to the town and will have a detrimental effect on the pavement. This is another example where the Applicant has taken over a year and still not changed his flawed approach and ignored the current neighborhood concerns. He has again maximized his lots and pushed the future impact on the current and future residents and the Town. This is a significant issue, that it would be wise to have a specialized consultant accurately evaluate existing conditions and volumes and comment on the approach and long-term costs. Since this is a long-term recurring cost that the town will be taking on, it would be best for the Town to hire this consultant directly and not leave it to the Applicant.

### **Conclusion:**

The Applicant pursuit of much higher than allowed density while maximizing the number of lots has created a development that is in opposition to the requirements of Section 16-12 and maximizes the impact on the current neighborhood, instead of minimizing it. It is a flawed attempt to address the affordable housing issue. It is purely a disguise to justify a high-density development and make this development more appealing to the town and public and skirt the need to minimize impact on the existing neighborhood. It only benefits the Applicant, not the existing neighborhood or the Town.

From the start, the public has expressed concerns of the developer's record of maximizing his interest at the expense of others. Insisting on cramming this many lots on this site, has created more problems and impacts on the existing neighborhood than necessary and has limited the Applicant's ability to address and solve the many issues brought up by citizens, consultants, and staff since 2019. This Applicant was given an opportunity to change his approach and provide

the Town with an acceptable plan and failed. His project does not meet a fundamental requirement of the code and will not likely meet it without a significant redesign and significantly more time. It is time to end it, listen to the residents of Taylor Street and do not let it drag on any longer: reject this application.

Thank you,

Hany Malek

# HANY MALEK

498 Taylor Street, Minturn, CO 81645 · 303-638-6329

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December 11, 2019

Minturn Planning Commission  
Town of Minturn  
302 Pine Street  
Minturn, CO 81645

RE: Comments on Railroad PUD Conceptual Plan

Dear Planning Commission:

The purpose of this letter is to comment on the Railroad PUD Conceptual Plan.

My wife and I have owned our home on Taylor Avenue for approximately 10 years. We chose to purchase our home in Minturn, and specifically Taylor Avenue, because of its location within the Vail Valley and the small town feel of Minturn. In addition, Taylor Avenue felt as a low-density residential area which was adjacent to open areas, including to forest service land and the railroad property, but was still within walking distance from old town.

Unfortunately, I will be unable to attend the meeting on December 11, 2019 but wanted to communicate some of my comments and concerns related to the proposed Railroad PUD Conceptual Plan.

The residential nature of the plan, proposed parks, improvements along Taylor Avenue, along with the attempt to address the Game Creek Trail parking and skier access to town are all positive proposals that need further development. However, the overall plan falls short in many areas including *"minimizing the impact to the existing neighborhood"* on Taylor Avenue.

I have many comments related to the proposed plan and have relayed some to Scot Hunn, however due to time limitations, I am listing only critical ones in this letter and will resume my review following the outcome of the meeting.

- Based on the staff report and discussion with Scott Hunn, this development would result in 114 to 185 new residences which would add approximately 296 to 481 new residents to the town. This would result in a net population increase to Minturn of approximately 30% to 50%. Those new residences would be concentrated in less than 19 acres which is a fraction of the overall size of Minturn, resulting in an extremely densely populated development. Both the population increase, and its density will have a tremendous

impact on Minturn and an unreasonable impact on to the existing Taylor Avenue neighborhood. This is a detriment to the town for many reasons. It would be worthwhile if staff could provide population density calculations for the various areas in Minturn for comparison.

- This high-density development does not meet the requirement as stated in the MCC *“Development in this area needs to incorporate appropriate residential and low-impact land uses along Taylor Avenue to minimize impacts to the existing neighborhood.”* Adding a population equivalent of 30% to 50% to the entire town, adjacent to Taylor Avenue, is not “appropriate” and does not “minimize” the impact on the existing neighborhood.
- Lot sizes should not be below 5,000 sf. The density should not exceed the current density of the Residential Zone within Game Creek Character Area. Average lot size would need to be adjusted and density decreased based on impact. Thus a “minimum” lot size of 5,000 sf in existing zones should not become the accepted standard for a new PUD. A higher minimum “average” lot size should be considered in addition to a larger minimum lot size.
- The current requirement for ADU and Duplex should not be altered. ADU should only be allowed on lots larger than 5,000 sf and Duplexes conditionally allowed only on much larger lots. Eliminating ADU and Duplex would reduce the impact on the existing neighborhood.
- Lot sizes for Old Downtown should not apply or be considered as a comparison for the Game Creek Character Area. Those are two completely different areas with different characters and impacts.
- High density residential developments provide a short-lived infusion of revenue to a town but is an unsustainable model in the long term. I feel the town has put too much emphasis on higher density residential developments which will have detrimental long-term impact on the financial stability of the town.

#### Traffic & Parking:

- This plan addresses the parking challenges at the extreme north end of Taylor Avenue, but additional measures will be needed to control unauthorized parking in the area.
- The improvements along Taylor Avenue will improve parking, however it does not appear to sufficiently address the potential significant increase of parking needs created by the high-density development.
- Replacing 4<sup>th</sup> and 6<sup>th</sup> street with only one, indirect route, will change the traffic flow and have a negative impact on Taylor residents. Many of the residences take 4<sup>th</sup> Street and then Minturn Road to go north. Since traffic will be increased on Taylor street, two streets should be maintained with at least one with direct access to Minturn Road.
- Traffic impact along Minturn Road will have to be addressed along with egress to HW24 in both directions. It should address the following MMC requirement *“The rail corridor should be maintained and improved access to and across the Eagle River should be incorporated into proposed development plans.”*
- Skiers pose a significant hazard in the winter along Taylor Avenue and Minturn Road. Skiers are constantly zipping along in the middle of Taylor Ave as well as Minturn Road, sometimes at very high speed. The proposed “run” may be a solution, however

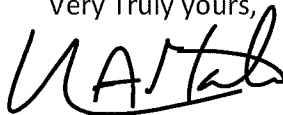
crossing of 4<sup>th</sup>, 5<sup>th</sup>, or 6<sup>th</sup> street could become a higher hazard due to limited visibility and would have to be addressed. In addition, the needs of both pedestrians and skiers would have to be addressed, as snowy and icy paths are welcome by skiers but could be hazard for pedestrians.

Municipal Water:

- Water, water, water! Where is the water coming from for this development? I had been told in the past that existing water taps will be saved for infill lots and this development will have to provide its own water. This application implies that water taps will be provided by Minturn. Please clarify.
- Sheet A1, Note 4: How is air and water quality maintained or improved? The answer addresses municipal water which is a different issue. Surface water as well as air quality will be impacted by the development.
- Sheet A1, Note 10: How is surface and groundwater quality and quantity maintained and enhanced? I understand that this is a conceptual design but both surface and groundwater have not been addressed in this application even on a conceptual level. A significant amount of impermeable surfaces will be created and will have a significant impact on surface water. No storm drains are shown on A14, no detention ponds or other surface water mitigation measures are shown. Impermeable surfaces will reduce infiltration and thus reduce groundwater quantity unless other mitigation measures are planned.
- Light Pollution: Will it be addressed and how?
- Will the proposed high voltage power line have an impact on this development?

I feel this lot provides a great opportunity for the town. Unfortunately, I think this proposed plan has significant short comings and would need a significant reduction of the lot density in order to meet the stated goals of the Town and minimize impact on Taylor residences. A mixed-use development on the south end along with a low density residential on the remaining site may be a more appropriate option for both the town and the developer.

Very Truly yours,

A handwritten signature in black ink, appearing to read 'HANY MALEK', written in a cursive style.

Hany Malek

## Madison Harris

---

**From:** Jason Hutto <jhutto@biadvisors.com>  
**Sent:** Tuesday, July 27, 2021 1:13 PM  
**To:** Madison Harris  
**Subject:** Minturn North

To whom it may concern:

I won't be able to join the town hall but wanted to express my support and excitement over Minturn North. My family and I have been interested in Minturn for a few years. However, we have two young children and have never been able to get comfortable with thinking of Minturn as a good town for families. We know a few families who are on the list and believe that development will be a great addition and we are excited to live there.

Jason

C. Jason Hutto, CFA  
(617) 448-5920

**Disclaimer:**

This message is intended only for the personal and confidential use of the designated recipient or recipients named above. Any review, retransmission, dissemination or other use of, or taking of any action in reliance upon, this information by persons or entities other than the intended recipient is prohibited. If you have received this e-mail in error, please immediately notify the sender by replying to this e-mail and delete the message and any attachments from your system.

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July 26, 2021

To: Minturn Planning- planner1@minturn.org

Dear Planning Commission:

I oppose the Minturn North Development currently in application review before the Planning Commission. The proposed density of the development is simply too great for this property. The current population of Minturn is approximately 1100 residents. The proposed development would increase the population by 300 to 500 residents. An approximate  $\frac{1}{3}$  to  $\frac{1}{2}$  increase in the town population in an area less than 1% of the area of the town of Minturn (0.03 square miles versus 7.8 square miles) is unconscionable. This development will have an unreasonable impact on me and other Taylor Street residents. Please do not allow this assault on the character of our town, the quality of life of current residents, and the infrastructure and resources available to our town.

Sincerely,

**Kim Malek**



**ROB GOSIEWSKI**  
 560 Taylor Street ✱  
 Minturn, CO 81645

**Minturn North: PUD Comments**  
 July 25, 2021

Density has been the primary concern for existing residents since the parcel was listed for sale. It was the concern most voiced at the “conceptual review” for the PUD, and continues to be the largest concern for Taylor Street neighbors.

The developer claims that proposed density is similar to what can be found in Minturn, and that it matches the existing Taylor Street neighborhood. While there are non-conforming lots on Taylor Street and Old Town, past exceptions should not be used as part of a modern standard. The developer justifies the proposed plan by citing similarities to Old Town, but Taylor Street is the “outskirts”, and a different type of neighborhood. I've been told that this side of town is oddly developed (and perhaps underutilized) because historical train and railyard operations made it a very noisy part of town. There were often long waits at the rail crossings.

| <b>Requested variations from the current code include:</b>         | <b>Town of Minturn Requirement:</b> | <b>Minturn North Proposal:</b> | <b>% Change</b> |
|--|-------------------------------------|--------------------------------|-----------------|
| Reduction of minimal square footage standards (for “compact lots”) | 5,000 sq. ft.                       | 4,000 sq. ft.                  | 20%             |
| Reduction of minimal square footage standards (for “cottage lots”) | 5,000 sq. ft.                       | 2,500 sq. ft.                  | 50%             |
| Reduction of minimal front setback distances (cottage lot)         | 20 ft. front / 10 ft. rear          | 10 ft. Front / 5 ft. rear      | 50%             |
| Reduction of minimal front setback distances (multifamily lot)     | 20 ft. front / 10 ft. rear          | 10 ft. front / 10 ft. rear     | 50%             |
| Increase to maximum lot coverage                                   | 40%                                 | 45%                            | 13%             |
| Increase of maximum building height (multifamily lots)             | 28 ft.                              | 32 ft.                         | 14%             |
| Reduction to minimum sidewalk widths                               | 5 ft.                               | 4 ft.                          | 20%             |
| Increase to minimum snow storage (how is parking affected?)        | 5%                                  | 20%                            | 300%            |

With density being a large concern the above variations should not be granted. Each request from the developer serves to maximize density beyond established code. If granted, the variances would certainly help the developer maximize their profitability, but the existing neighborhood would be impacted unfairly. The majority of existing lots in the area are conforming, and recent projects have adhered to current code. The planning commission will “disappoint” one side or the other with the decisions made as a board. The commission should make the choice to deny the developer's deviations from code in order to prevent unfair impacts to the Taylor Street neighborhood.

There are several key issues with the proposed PUD which can all be adjusted by regulating density:

**Traffic**

As proposed, I am weary of long waits at the Bellm Bridge; even worse if the train were to re-open. For Old Town, I counted (19) town roads that access Highway 24. Taylor Street only has (2) access points to Highway 24. If the proposed plan was fully built-out the area would only have (1) access point per ~350 residents. Old Town has an access point per ~58 residents (and that number is inclusive of many properties that can access Highway 24 direct from their driveways)

**Parking**

Small lots will force cars onto the streets. Each new lot will bring 2-5 cars, plus friends and visitors. Alleys would help relieve parking pressure. As proposed there is not enough area to park all vehicles, trailers, snowmobiles, boats, etc.

### Reduced Setbacks/Increased Lot Coverage

Reducing setbacks will also reduce available parking, snow storage, and potential for nice landscapes. Views would be limited by buildings creeping towards right-of ways. Reduced setbacks would make construction more complicated and impact privacy between neighbors.

### Snow Storage

The wind blows fiercely on Taylor Street and creates drifts. Public Works needs more places to stockpile snow. Small lots would lead to snow storage conflicts between neighbors (and extra work for the town).

### Drainage

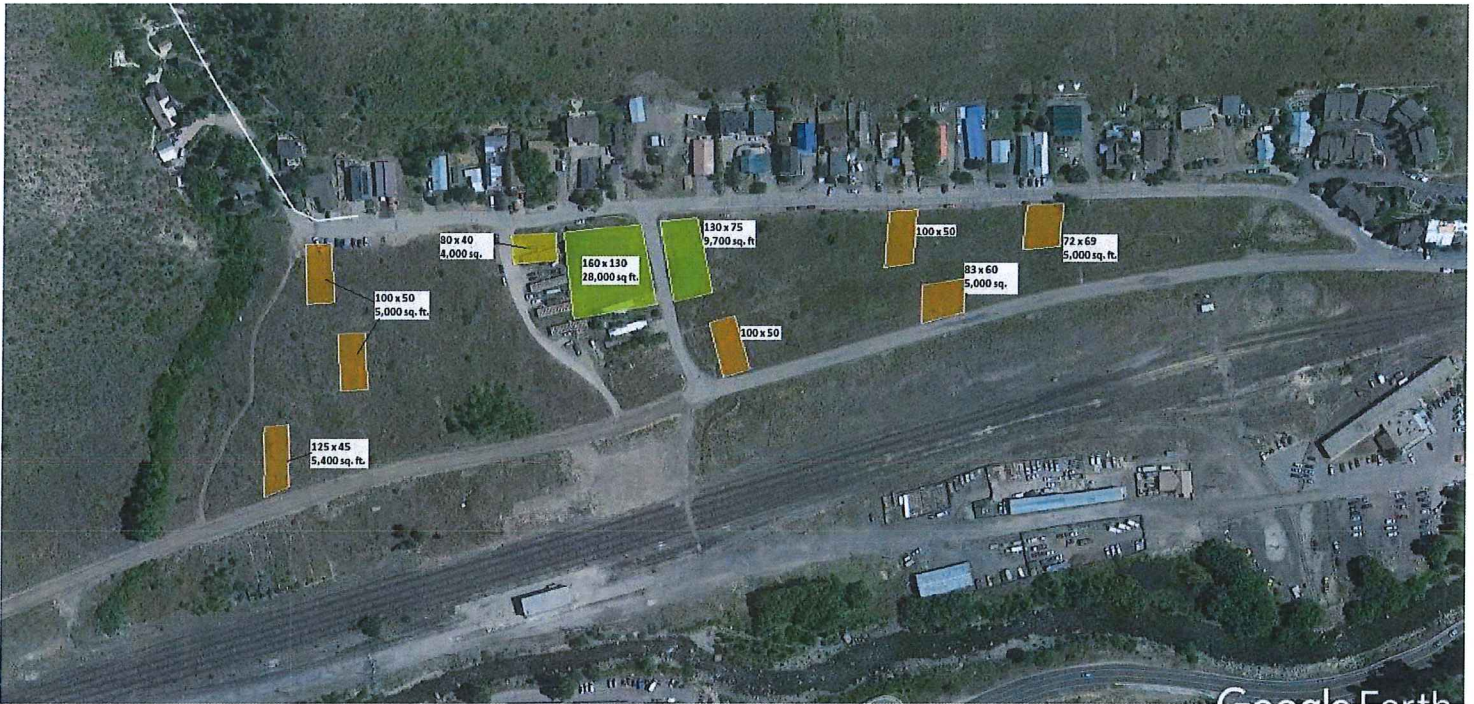
As proposed, there are some lots that drain downhill directly into other lots. Adding alleys could provide common drainage routes .

### Re-Alignment of Taylor Street

The project proposes a realignment of Taylor Street, which would disrupt existing driveways, retaining structures, and landscaping. This re-alignment would eliminate currently utilized parking spaces on the east side of Taylor Street.

I was curious what the parcel would look like if subdivided into lots that meet the current town requirements, so I used google earth and a basic drawing program to make a sketch. How do properly-sized lots would affect the developer's profitability? I was also interested to see if alleys could be added, and if a more cohesive park area could be created. I used Google Earth to determine scale and tried to match the existing neighborhood.





I then counted the lots and made a basic revenue table. The alternate layout - that presented itself when drawing lots to meet current town requirements - shows reduced lot density, as expected. I included (8) "compact" lots on the plan to honor the developer's intention to create some "attainable" parcels that could be deed-restricted. "Cottage lots" were not included in my sketch.

**Minturn North Proposal:**

|              |           |         |                   |
|--------------|-----------|---------|-------------------|
| Multi-Family | 3         | 800,000 | 2,400,000         |
| Estate       | 8         | 600,000 | 4,800,000         |
| Standard     | 24        | 400,000 | 9,600,000         |
| Compact      | 36        | 300,000 | 10,800,000        |
| Cottage      | 24        | 200,000 | 4,800,000         |
|              | <b>95</b> |         | <b>32,400,000</b> |

**Alternative:**

|              |           |         |                   |
|--------------|-----------|---------|-------------------|
| Multi-Family | 3         | 800,000 | 2,400,000         |
| Estate       | 8         | 600,000 | 4,800,000         |
| Standard     | 44        | 400,000 | 17,600,000        |
| Compact      | 8         | 300,000 | 2,400,000         |
| Cottage      | 0         | 200,000 | 0                 |
|              | <b>63</b> |         | <b>27,200,000</b> |

*chg. from MiNo proposal:*                      -34%    -16%

The developer's drawings are based-on alterations to existing code which could increase density by ~34%, compared to my sketch that adheres to current regulations. This reflects the developer's requested lot size and setback variations ranging from 13%-50%

While I do appreciate the developer's approach of offering small lots to keep structure footprints "modest", the realities of building, especially in this area, are such that even small lots can not realistically be considered affordable/attainable. The developers have packaged their design with attractive terms, and they promote a target market of working-class folk in our community.

However, with an estimated 4,000 sq.ft. "compact lot" price of \$300,000, and a conservative building cost of \$750,000 (for a 2,500 sq. ft. home) the numbers quickly add up... to \$1,050,000... not something that most working families can afford.

The marketing presented by the developers is an approach to ask for unreasonable variations from current building code. They are requesting exceptions from precedents that are already established in the Taylor Street neighborhood, and the developers are even going so far as to alter existing conditions on the east side of Taylor Street

This PUD will affect property values of existing properties on Taylor Street. Proposed project density needs to be corrected to avoid overcrowding; the character of the area should not be eroded as a result of aggressive subdividing. Our town code exists to prevent issues between properties and neighbors. I am asking the planning commission to adhere to the current town building requirements so development in this area can proceed equitably to the existing residents.

It is not the community's responsibility to ensure that the developer's project achieves maximum profitability. However, it is stated in the code that any proposed development minimize the impacts to the existing neighborhood.

Thank you for you time & consideration,

Rob Gosiewski Jr.  
560 Taylor Street



Coolidge



## Madison Harris

---

**From:** Contact form at Minturn CO <cmsmailer@civicplus.com>  
**Sent:** Wednesday, July 28, 2021 4:01 PM  
**To:** Madison Harris  
**Subject:** [Minturn CO] Minturn North PUD (Sent by Justine LaCross, justinefurseth@yahoo.com)

Hello mharris,

Justine LaCross ([justinefurseth@yahoo.com](mailto:justinefurseth@yahoo.com)) has sent you a message via your contact form (<https://www.minturn.org/user/353/contact>) at Minturn CO.

If you don't want to receive such e-mails, you can change your settings at <https://www.minturn.org/user/353/edit>.

Message:

Hello, My family and I would like to be sure you know that we are OPPOSED to the Minturn North PUD. The developers are trying to portray that a lot of people are supportive of this development, however, this is not true.

We have lived in Minturn for 25 years. This development is too big and will create a great impact on our community. The traffic will increase, the noise will increase and this development is not keeping Minturn in the "small town character" which is important to why our family lives here. We feel like we are being squeezed out of our fabulous small town. Adding 100's of vehicles (without sufficient parking) to our street is NOT safe for our children.

Thank You,

Nathan, Justine, Bodie and Macie LaCross

**Minturn Planning Department**  
Minturn Town Center  
302 Pine Street  
Minturn, Colorado 81645



**Minturn Planning Commission**  
Chair – Lynn Teach  
Jeff Armistead  
Elliot Hovey  
Chris Manning  
Tom Priest  
Jena Skinner

**To:** Planning Commission  
**From:** Scot Hunn, Planning Director  
Madison Harris, Planner I  
**Date:** August 5, 2021  
**Re:** Dowd Lift Station 4 Replacement

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Eagle River Water and Sanitation District (ERWSD) requests review of the Dowd Lift Station 4 Replacement. Per the Applicant's request:

*“ERWSD is replacing the lift station that conveys wastewater flow from the entire Town of Minturn to the collection system for eventual treatment in Avon. The existing station lacks capacity, reliability, and redundancy.”*

The Town of Minturn granted a 1041 permit exemption on September 16, 2019 “for the purpose of completing the Dowd Junction Collection System Improvements project” as “staff has determined that such maintenance, repair and replacement of existing facilities *will not* constitute a material change; *will not* cause negative impacts different from the existing facility; and, *will not* otherwise exacerbate existing impacts.” (Letter to ERWSD regarding 1041 permit exemption dated September 16, 2019)

Staff has been meeting with the District for over a year as we help them through the process. The plans associated with the application have been submitted for the Design Review Board's consideration. This is mostly a technical formality.

Staff is recommending **approval without conditions**.





# DESIGN REVIEW APPLICATION

**TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT**  
 P.O. Box 309 302 Pine Street Minturn, Colorado 81649-0309  
 Phone: 970-827-5645 Fax: 970-827-5545 Email: planner@minturn.org

**Project Name:**

**Project Location**

Street Address:

Zoning:

Parcel Number(s):

**Application Request:**

**Applicant:**

Name:

Mailing Address:

Phone:

Email:

**Property Owner:**

Name:

Mailing Address:

Phone:

Email:

**Required Information:**

|               |  |                           |                                 |
|---------------|--|---------------------------|---------------------------------|
| Lot Size:     | Type of Residence (Single Family, ADU, Duplex) | # of Bedrooms             | # On-site Parking Spaces        |
| # of Stories: | Snow storage sq ft:                            | Building Footprint sq ft: | Total sq ft Impervious Surface: |

**Signature:**

Fee Paid: \_\_\_\_\_

Date Received: \_\_\_\_\_

Planner: \_\_\_\_\_



July 13, 2021

Madison Harris/Scot Hunn  
Town of Minturn Planning Department  
P.O. Box 309  
302 Pine Street  
Minturn, CO 81645

Re: Letter of Intent: Dowd Junction Lift Station Replacement Design Review

Dear Madison and Scot:

Please accept this letter and attached documentation as the Letter of Intent required by the Application Process Guide. Eagle River Water and Sanitation District (ERWSD or the District) is in the process of capital investment in wastewater collection system infrastructure serving the Town of Minturn. The project is known as the Dowd Junction Collection System Improvements of which there are four components. Three of the components are either partially or entirely located within the Town of Minturn. Those components include:

- Replacement of the Minturn Interceptor Aerial Crossing (currently under construction)
- Replacement of Lift Station 4, the pump station conveying wastewater from the town to the downstream treatment facility in Avon. Also included in this scope is new collection system piping from the new aerial crossing to the new lift station.
- Replacement of the force main piping, the pressurized discharge piping leaving the lift station

This correspondence is strictly related to design review of the replacement lift station, as the Town of Minturn granted ERWSD a 1041 Permit Exemption on September 16, 2019 for the project as a whole. That correspondence is attached as Exhibit A. The lift station will be located immediately south of the existing lift station/boat ramp parking lot and is currently at the 90 percent design level. We are evaluating some design changes that would decrease the size of the building, but the overall appearance and location will remain the same. The station is designed to accommodate wastewater flows from the current population of Minturn as well as eventual build out.

The project is located entirely on lands owned by the US Forest Service (USFS). An updated USFS Special Use Permit was acquired for the work and is attached to this correspondence as Exhibit B. The site is located adjacent to the regional ECO Trail and the project will re-align a small portion of the trail. The site is close to the Eagle River but is located entirely outside of the 100-year floodplain and has no impacts to wetlands. The existing pump station will be demolished once the replacement project is complete and will be reclaimed to add 1-2 additional recreational parking spaces, resulting in a net increase in public parking upon completion of the project.

The proposed infrastructure at the site includes a new 18-inch gravity sewer main along US Highway 24 upstream of the station and a force main to be installed along the Highway 24 corridor heading west. There is no potable water infrastructure available at the site and none proposed. The facility will be powered by electricity from Xcel Energy and will be heated from natural gas also from Xcel energy. Of note, the station is equipped with an emergency standby generator and a fuel-driven fully redundant pump system also powered by natural gas.

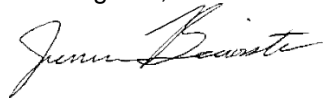
The pump station has an above-ground superstructure in addition to below-ground wastewater 'wet well.' The building is proposed to be approximately 763 square feet and 12 feet tall on the highway 24 side and 18 feet tall on the Eagle River Side. The roof will be equipped with solar panels to offset a small portion of the site's electrical usage and advance ERWSD's sustainability goals. The building will have a cast in place concrete foundation and a masonry superstructure, with exterior building materials consisting of concrete paneling around the base of the building, corrugated metal siding, and a standing seam metal roof. The north and south elevations of the building are equipped with translucent wall panels rather than windows to provide natural light to the facility and louvers for HVAC. The south elevation is to have 'wood-look' paneling different than the corrugated metal paneling. Adjacent to the building is a small fenced-in area housing an emergency generator and electrical equipment. The fence is proposed to be 8-foot tall chain link with privacy slats and will be accessible via a locked gate for routine maintenance activities. Exhibit C includes select plan sheets and Exhibit D includes renderings of the proposed lift station building.

There are certain requirements of Minturn Town Code related to development applications from which ERWSD is requesting relief. These items were discussed with your office during the pre-application process in December 2020, but are referenced here for your reference. Those items are summarized in the table below:

| <b>Code Section</b> | <b>Requirement</b>          | <b>ERWSD Response</b>   |
|---------------------|-----------------------------|---|
| 16.21.170 (3)       | Disclosure of ownership     | In lieu of a title commitment, ERWSD will provide the updated USFS Special Use Permit Authorization and accompanying Plan of Development (POD).   |
| 16.21.170 (6)       | Environmental impact report | The location is in an already developed/impacted area. ERWSD requests relief from preparation of a separate Environmental Impact Report and instead utilize the USFS POD.   |
| 16.21.170 (7)       | Adjacent Property Owners    | No information is available on adjacent parcels from the Eagle County Assessors data. The adjacent property owners include the US Forest Service and Union Pacific Railroad. ERWSD requests that this requirement be waived.  |
| 16.21.615 (c)(2)    | Boundary Survey             | A full boundary survey was not completed since this is part of vast White River National Forest land. ERWSD requests relief from this requirement and that the USFS SUP and the localized topographic survey prepared for the project be deemed sufficient. In addition, we have been coordinating tightly with CDOT on access and improvements within and adjacent to US 24 and will not proceed with construction without all applicable permits secured. |
| 16.21.615 (c)(7)    | Landscaping Plan            | This location is on USFS property and surrounded by native vegetation. ERWSD requests a waiver of this submittal requirement as no formal landscaping is proposed beyond revegetation of disturbed areas.   |
| 16.21.615 (c)(11)   | Property corner staking     | Staking the corners of the entire USFS property is impractical, ERWSD requests a waiver from this requirement and is providing the USFS SUP in lieu.  |

We are excited to present this critical infrastructure project to the Town of Minturn for design review and welcome any feedback we can receive prior to the public hearing, as well as concurrence on the requested exceptions from code enumerated above. If you have any questions or concerns, please do not hesitate to contact me.

Best regards,



Jenna Beirsto, P.E.  
Project Engineer

List of Attachments:

|           |   |
|-----------|---|
| Exhibit A | 1041 Exemption Letter dated 9/16/2020           |
| Exhibit B | USFS Special Use Permit and Plan of Development |
| Exhibit C | Select plan sheets from 90% design              |
| Exhibit D | Colored elevations/renderings                   |

Cc: Michelle Metteer  
File

## **Exhibit A**

1041 Permit Exemption

**Minturn Planning Department**  
Minturn Town Center  
302 Pine Street  
Minturn, Colorado 81645



**Minturn Planning Commission**  
Chair – Lynn Teach  
Jeff Armistead  
Lauren Dickie  
Burke Harrington  
Greg Sparhawk

September 16, 2019

Jeffery Schneider, P.E.  
Engineering Supervisor  
Eagle River Water & Sanitation District  
846 Forest Road  
Vail, CO 81657

Via: jschneider@erwsd.org

Re: Dowd Junction Collection System Improvements – Request for 1041 Permit Exemption

Thank you for your recent submittal on behalf of the Eagle River Water and Sanitation District (ERWSD) of a letter and associated materials in support of a formal request for exemption from the Town of Minturn’s 1041 regulations for the purpose of completing the Dowd Junction Collection System Improvements project.

The Town has reviewed your submission and has determined that those portions of the project situated within the Town of Minturn municipal boundaries which include “the replacement of existing pipelines and lift station with new equipment” according to your letter dated August 30, 2019 meet the criteria for exemption.

Specifically, the project – consisting of replacement, repair and/or maintenance of Lift Station No. 4; and, the replacement of an existing Force Main and the “Minturn Aerial Crossing” – have been deemed to be maintenance, repair, or replacement of existing components of existing facilities specifically in a manner that does not constitute a material change.

In accordance with the Minturn Municipal Code, Section 16-25-30 – *Applicability*,

*(6) A municipal or industrial water project or sewage treatment system is exempt from this Article if it falls within one (1) of the following categories:*

- a. The maintenance, repair, or replacement of an existing component of a facility if it does not constitute a material change, does not cause negative impacts different from the existing facility, and does not otherwise exacerbate existing impacts.*
- b. Replacement of an existing water diversion or storage structure without change in the point of diversion, type or place of use of the water, or yield.*
- c. A proposed municipal or industrial water project with a new or increased diversion per year, or a new or increased storage capacity of less than one hundred (100) acre-feet.*

In this case, based on a review of your submission and description of the work to be performed for the Dowd Junction Collection System Improvements project, the Town has determined that those portions of

the project situated within the Town of Minturn are exempt from the Town's 1041 regulations due to the nature of the work to be performed.

Specifically, staff has determined that such maintenance, repair and replacement of existing facilities *will not* constitute a material change; *will not* cause negative impacts different from the existing facility; and, *will not* otherwise exacerbate existing impacts.

Last, as referenced in your letter requesting exemption, the Town appreciates ERWSD's proactive approach to the design of a new pedestrian bridge across the Eagle River at the existing Minturn Aerial Crossing location to accommodate the future 12" water main, as well as potential regional trail uses. We look forward to working with the District to identify and secure partnerships to fund and construct the bridge improvements.

On behalf of the Town of Minturn, we thank you and the staff at Eagle River Water and Sanitation District for your collaborative and comprehensive approach to this project.

Please contact me if you have any questions regarding the Town's review of your request or this letter confirming the exemption of the Dowd Junction Collection System Improvement project from the Town's 1041 regulations in this instance.

Kindest regards,



Scot Hunn, AICP  
Planning Director

Cc: Michelle Metteer  
Michael Sawyer  
Jeff Spanel

## **Exhibit B**

US Forest Service  
Special Use Permit



**U.S. DEPARTMENT OF AGRICULTURE  
FOREST SERVICE  
AMENDMENT  
FOR**

**SPECIAL-USE AUTHORIZATION**

**Amendment#: 1**

This amendment is attached to and made a part of the HOL369 special use authorization for SEWAGE TRANSMISSION LINE issued to EAGLE RIVER WATER & SANITATION DISTRICT on 10/22/2007 which is hereby amended as follows:

**End of life replacement of existing sewer infrastructure, including replacement of lift station at Dowds Junction, reconstruction of two aerial crossings of Eagle River, replacement of pipelines (1,015 linear feet of gravity sanitary sewer; 1,775 linear feet of dual force mains)**

**Note: Not all infrastructure authorized under HOL369 will be replaced -- only the specific components itemized above.**


This Amendment is accepted subject to the conditions set forth herein, and to the conditions outlined in the final Plan of Development attached hereto and made a part of this Amendment.

EAGLE RIVER WATER & SANITATION DISTRICT

SCOTT G. FITZWILLIAMS

  
Holder (signature)

  
Authorized Officer (signature)

  
Holder (printed name)

Forest Supervisor, White River NF  
Title

3.23.20  
Date

4/29/2020  
Date

According to the Paperwork Reduction Act of 1995, an agency may not conduct or sponsor, and a person is not required to respond to a collection of information unless it displays a valid OMB control number. The valid OMB control number for this information collection is 0596-0082. The time required to complete this information collection is estimated to average one (1) hour per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information.

The U.S. Department of Agriculture (USDA) prohibits discrimination in all its programs and activities on the basis of race, color, national origin, age, disability, and where applicable, sex, marital status, familial status, parental status, religion, sexual orientation, genetic information, political beliefs, reprisal, or because all or part of an individual's income is derived from any public assistance. (Not all prohibited bases apply to all programs.) Persons with disabilities who require alternative means for communication of program information (Braille, large print, audiotape, etc.) should contact USDA's TARGET Center at 202-720-2600 (voice and TDD).

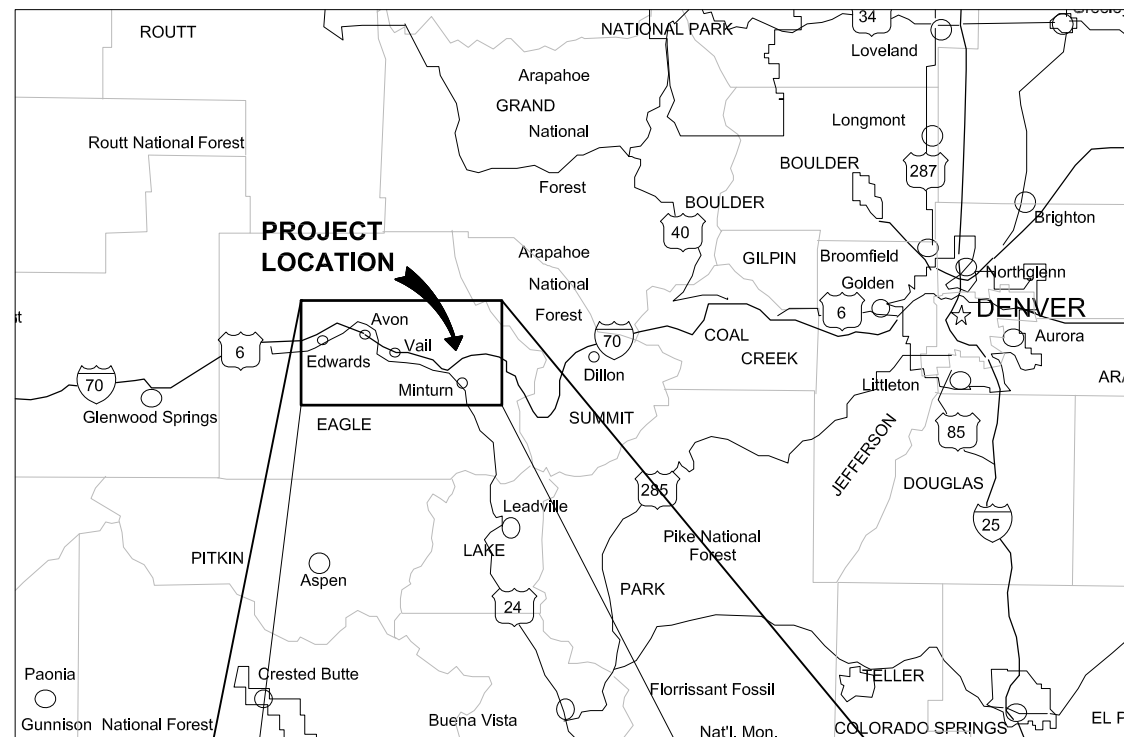
To file a complaint of discrimination, write USDA, Director, Office of Civil Rights, 1400 Independence Avenue, SW, Washington, DC 20250-9410 or call toll free (866) 632-9992 (voice). TDD users can contact USDA through local relay or the Federal relay at (800) 877-8339 (TDD) or (866) 377-8642 (relay voice). USDA is an equal opportunity provider and employer.

The Privacy Act of 1974 (5 U.S.C. 552a) and the Freedom of Information Act (5 U.S.C. 552) govern the confidentiality to be provided for information received by the Forest Service.

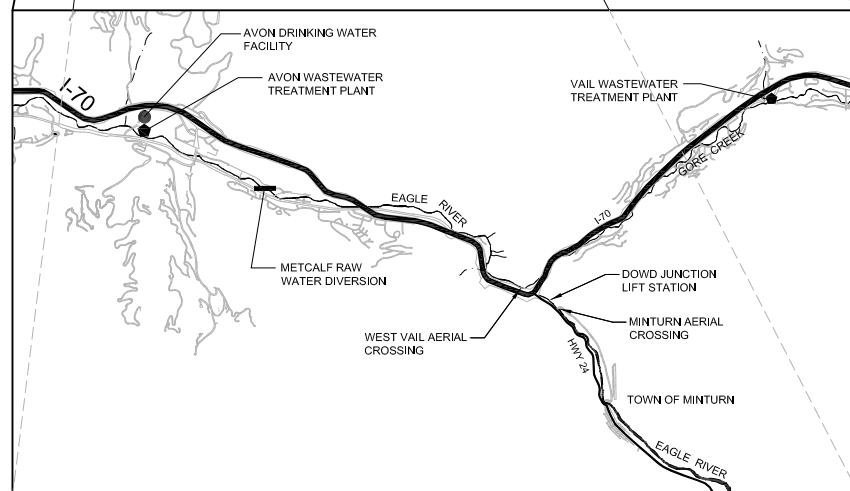
## **Exhibit C**

### Design Drawings:

- a. Cover Page – Vicinity Map
- b. Survey Control Plan
- c. Site Plan
- d. Preliminary Building Plans and Elevations



**Location Map**  
NO SCALE



Contract Drawings For

# Dowd Junction Collection System Improvements

## LIFT STATION AND SANITARY SEWER

### 90% DELIVERABLE

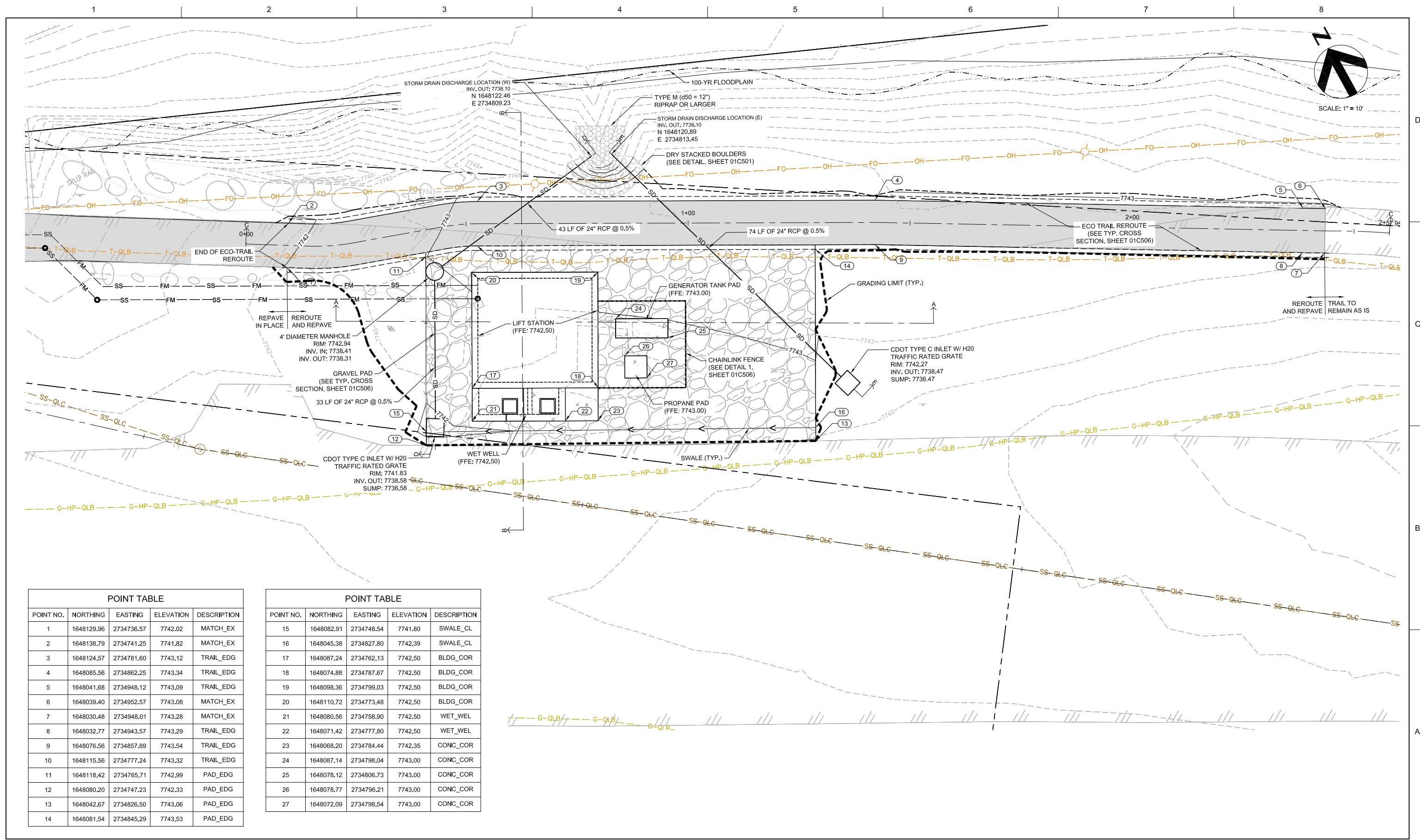
ERWSD Contract No.  
XX.XX.XXX

HDR Project No.  
10116326

Prepared for:  
Eagle River Water and Sanitation District  
Vail, CO  
October 2020

#### INDEX OF DRAWINGS

|                     |   |
|---------------------|---|
| <b>GENERAL</b>      |   |
| 00G001              | COVER SHEET   |
| 00G002              | SYMBOLS AND LEGEND  |
| 00G003              | GENERAL ABBREVIATIONS AND NOTES                                       |
| 00G004              | GENERAL CIVIL LEGEND AND NOTES  |
| 00G005              | GENERAL NOTES   |
| 00G006              | ELECTRICAL LEGEND AND NOTES   |
| 00G007              | INSTRUMENTATION LEGEND AND NOTES                                      |
| 00G008              | MECHANICAL LEGEND & SCHEDULES   |
| 00C100              | GESC NOTES, CONSTRUCTION SEQUENCE AND LEGEND                          |
| 00C501              | GESC DETAILS  |
| 00S001              | STRUCTURAL NOTES  |
| 00S501              | CONCRETE DETAILS  |
| 00S502              | CONCRETE DETAILS  |
| 00S503              | MASONRY DETAILS   |
| 00S504              | MISCELLANEOUS DETAILS   |
| 00Y001              | P&ID  |
| 00Y501              | INSTRUMENTATION - DETAILS   |
| 00Y601              | INSTRUMENTATION - NETWORK DIAGRAM                                     |
| 00Y602              | INSTRUMENTATION - RTU LAYOUT  |
| 00Y603              | INSTRUMENTATION - RTU POWER SCHEMATIC                                 |
| 00Y604              | INSTRUMENTATION - TYPICAL DI MODULE SCHEMATIC WIRING                  |
| 00Y605              | INSTRUMENTATION - TYPICAL DO MODULE SCHEMATIC WIRING                  |
| 00Y606              | INSTRUMENTATION - TYPICAL AI MODULE SCHEMATIC WIRING                  |
| 00Y607              | INSTRUMENTATION - TYPICAL AO MODULE SCHEMATIC WIRING                  |
| 00Y608              | INSTRUMENTATION - VFD DIAGRAM   |
| <b>SITE WORK</b>    |   |
| 01C001              | EXISTING UTILITY NOTES AND LEGEND                                     |
| 01C100              | OVERALL EXISTING UTILITY PLAN   |
| 01C101              | SITE GRADING PLAN AND SURVEY CONTROL PLAN                             |
| 01C102              | STORMWATER MANAGEMENT PLAN - STA. 10+00.00 TO STA. 14+00.00           |
| 01C103              | STORMWATER MANAGEMENT PLAN - STA. 14+00.00 TO STA. 18+50.00           |
| 01C104              | STORMWATER MANAGEMENT PLAN - STA. 18+50.00 TO STA. 20+12.84           |
| 01C110              | MINTURN INTERCEPTOR PLAN AND PROFILE - STA. 10+00.00 TO STA. 14+00.00 |
| 01C111              | MINTURN INTERCEPTOR PLAN AND PROFILE - STA. 14+00.00 TO STA. 18+50.00 |
| 01C112              | MINTURN INTERCEPTOR PLAN AND PROFILE - STA. 18+50.00 TO STA. 20+12.84 |
| 01C113              | FORCE MAIN PLAN AND PROFILE- STA. 1+25.00 TO STA. 3+29.00             |
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| 01C402              | ENLARGED EXISTING UTILITY PLAN  |
| 01C403              | ENLARGED EXISTING UTILITY PLAN  |
| 01C501              | DETAILS   |
| 01C502              | DETAILS   |
| 01C503              | DETAILS   |
| 01C504              | DETAILS   |
| 01C505              | PROFILES  |
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| 02C401              | ENLARGED SITE PLAN  |
| 02S101              | LOWER LEVEL PLAN  |
| 02S102              | GRADE LEVEL PLAN  |
| 02S103              | ROOF PLAN   |
| 02S301              | SECTIONS  |
| 02S302              | SECTIONS  |
| 02S501              | DETAILS   |
| 02S502              | DETAILS   |
| 02S503              | DETAILS   |
| 02A101              | FLOOR PLAN  |
| 02A201              | BUILDING ELEVATIONS   |
| 02A301              | BUILDING SECTION AND DETAILS  |
| 02A501              | DETAILS   |
| 02A601              | SCHEDULES AND DETAILS   |
| 02D101              | WETWELL LEVEL AND FIRST FLOOR PLAN                                    |
| 02D301              | SECTIONS  |
| 02D302              | SECTIONS  |
| 02D501              | DETAILS   |
| 02M101              | FLOOR PLANS - MECHANICAL  |
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| 02E601              | SINGLE LINE DIAGRAM   |
| 02E602              | MCC ELEVATION   |
| 02E603              | LOAD CALC AND SCHEDULES   |



| POINT TABLE |            |            |           |             |
|-------------|------------|------------|-----------|-------------|
| POINT NO.   | NORTHING   | EASTING    | ELEVATION | DESCRIPTION |
| 1           | 1648129.96 | 2734736.57 | 7742.02   | MATCH_EX    |
| 2           | 1648138.79 | 2734741.25 | 7741.82   | MATCH_EX    |
| 3           | 1648124.57 | 2734781.60 | 7743.12   | TRAIL_EDG   |
| 4           | 1648085.56 | 2734862.25 | 7743.34   | TRAIL_EDG   |
| 5           | 1648041.68 | 2734948.12 | 7743.09   | TRAIL_EDG   |
| 6           | 1648039.40 | 2734952.57 | 7743.08   | MATCH_EX    |
| 7           | 1648030.48 | 2734948.01 | 7743.28   | MATCH_EX    |
| 8           | 1648032.77 | 2734943.57 | 7743.29   | TRAIL_EDG   |
| 9           | 1648076.56 | 2734857.89 | 7743.54   | TRAIL_EDG   |
| 10          | 1648115.56 | 2734777.24 | 7743.32   | TRAIL_EDG   |
| 11          | 1648118.42 | 2734765.71 | 7742.99   | PAD_EDG     |
| 12          | 1648080.20 | 2734747.23 | 7742.33   | PAD_EDG     |
| 13          | 1648042.67 | 2734826.50 | 7743.06   | PAD_EDG     |
| 14          | 1648081.54 | 2734845.29 | 7743.53   | PAD_EDG     |

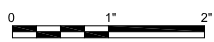
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|-------------|------------|------------|-----------|-------------|
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| 16          | 1648045.38 | 2734827.80 | 7742.39   | SWALE_CL    |
| 17          | 1648087.24 | 2734762.13 | 7742.50   | BLDG_COR    |
| 18          | 1648074.88 | 2734787.67 | 7742.50   | BLDG_COR    |
| 19          | 1648098.36 | 2734799.03 | 7742.50   | BLDG_COR    |
| 20          | 1648110.72 | 2734773.48 | 7742.50   | BLDG_COR    |
| 21          | 1648080.56 | 2734758.90 | 7742.50   | WET_WEL     |
| 22          | 1648071.42 | 2734777.80 | 7742.50   | WET_WEL     |
| 23          | 1648068.20 | 2734784.44 | 7742.35   | CONC_COR    |
| 24          | 1648087.14 | 2734798.04 | 7743.00   | CONC_COR    |
| 25          | 1648078.12 | 2734806.73 | 7743.00   | CONC_COR    |
| 26          | 1648078.77 | 2734796.21 | 7743.00   | CONC_COR    |
| 27          | 1648072.09 | 2734798.54 | 7743.00   | CONC_COR    |

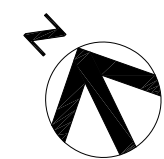
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|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
| STRUCTURAL      | J. CORONADO  |
| PROCESS         | B. REINER    |
| UTILITIES       | T. SCHARRA   |
| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | I. MARTINEZ  |
| PROJECT NUMBER  | 10116326     |

| ISSUE      | DATE              | DESCRIPTION |
|------------|-------------------|-------------|
| 10/08/2020 | 90% DESIGN REVIEW |             |
| 06/26/2020 | 60% DESIGN REVIEW |             |
| 02/21/2020 | 30% DESIGN REVIEW |             |

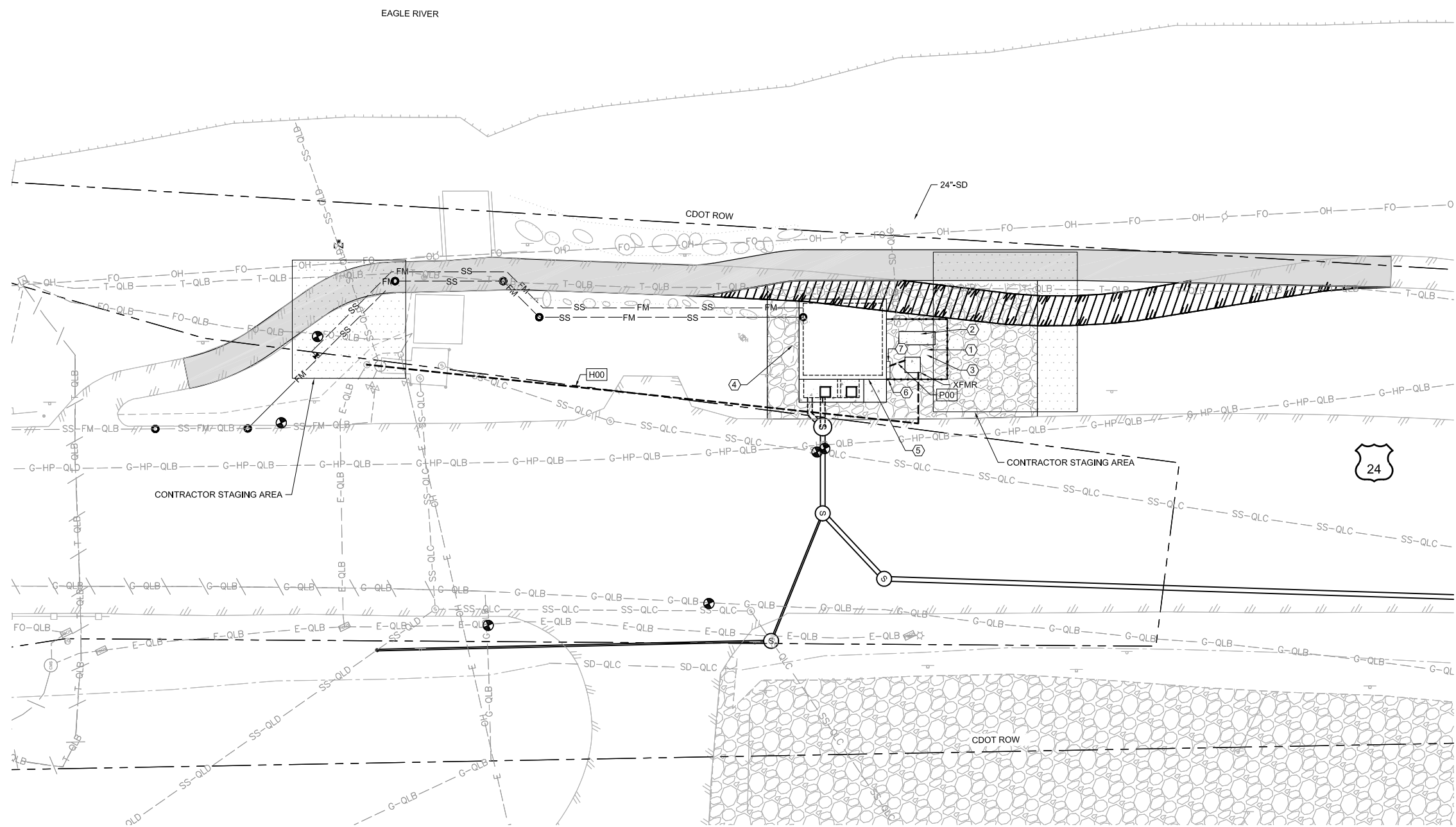


  
**EAGLE RIVER**  
 WATER & SANITATION  
 DISTRICT  
**DOWD JUNCTION COLLECTION SYSTEM**  
**IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

**SITE GRADING PLAN AND SURVEY CONTROL PLAN**  
 FILENAME | 01C101.dwg SHEET | 401C101  
 SCALE | 1" = 10'



SCALE: 1" = 20'



- KEY NOTES:**
- ① PROPOSED PROPANE TANK PAD, SEE DETAIL X/XX
  - ② PROPOSED GENERATOR PAD, SEE DETAIL X/XX
  - ③ PROPOSED FENCE, SEE DETAIL X/XX
  - ④ PROPOSED BOLLARD (TYP.), SEE DETAIL 1/01C506
  - ⑤ PROPOSED DOOR STOOP, SEE DETAIL X/XX
  - ⑥ 200A /3 METER / MAIN CB (MMS)
  - ⑦ AUTOMATIC TRANSFER SWITCH (ATS)

- NOTES:**
- 1. BOAT RAMP SHALL REMAIN OPEN TO THE PUBLIC THROUGHOUT DURATION OF CONSTRUCTION.
  - 2. MAINTAIN PUBLIC ACCESS TO EXISTING PEDESTRIAN TRAIL.



| ISSUE | DATE       | DESCRIPTION       |
|-------|------------|-------------------|
|       | 10/08/2020 | 90% DESIGN REVIEW |
|       | 06/26/2020 | 60% DESIGN REVIEW |
|       | 02/21/2020 | 30% DESIGN REVIEW |

|                 |              |
|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
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| PROCESS         | B. REINER    |
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| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | I. MARTINEZ  |
| PROJECT NUMBER  | 10116326     |



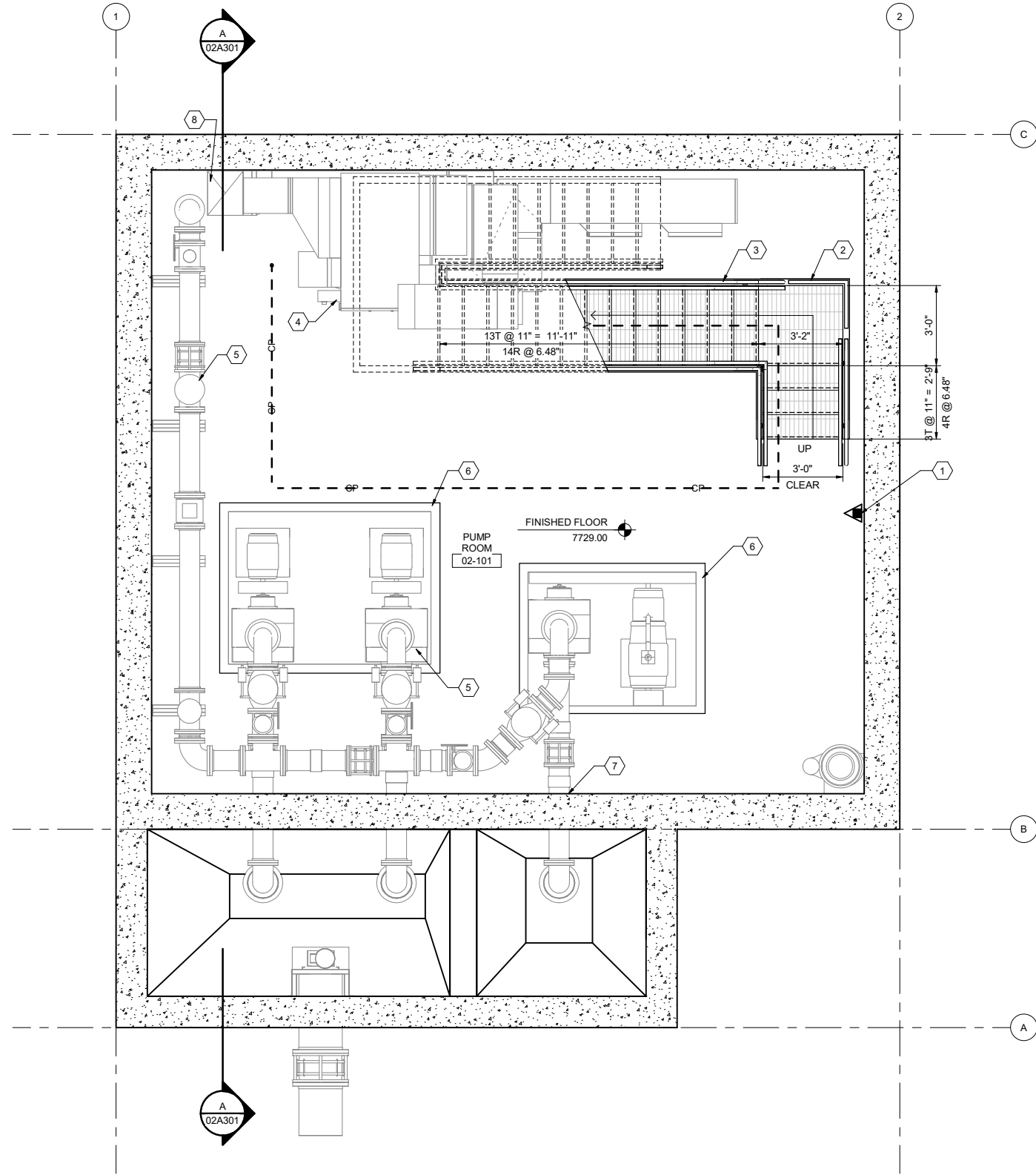
**DOWD JUNCTION COLLECTION SYSTEM IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

**ENLARGED SITE PLAN**



FILENAME | 02C401.dwg  
SCALE | 1" = 20'

SHEET  
**402C401**



**GENERAL NOTES**

1. ALL DIMENSIONS ARE TO FACE OF MASONRY UNIT
2. SEE SHEET 02A601 FOR SCHEDULES, DOOR AND FRAME TYPES

**KEYNOTES #**

- 1 GENERAL PURPOSE ABC FIRE EXTINGUISHER
- 2 ALUMINUM GUARDRAIL
- 3 ALUMINUM STAIR RAIL
- 4 MECHANICAL EQUIPMENT SEE MECHANICAL
- 5 PROCESS PIPING AND EQUIPMENT; SEE MECHANICAL/PROCESS DESIGN DOCUMENTS
- 6 EQUIPMENT MOUNTING PAD; SEE STRUCTURAL
- 7 PIPE PENETRATION; SEE STRUCTURAL AND PROCESS DRAWINGS
- 8 DUCT SEE MECHANICAL

| CODE ANALYSIS      |                      | CODE SECTION |           |
|--------------------|----------------------|--------------|-----------|
| OCCUPANCY GROUP:   | F-2                  |              |           |
| CONSTRUCTION TYPE: | IIB                  |              |           |
| BUILDING HEIGHT:   | PERMITTED:           | 55 FT        | §504.3    |
|                    | ACTUAL:              | 18 FT 6 IN   |           |
| NUMBER OF STORIES: | PERMITTED:           | 3 STORIES    | §504.4    |
|                    | ACTUAL:              | 1 STORY      |           |
| BUILDING AREA:     | PERMITTED:           | 23,000 SF    | §506.2    |
|                    | ACTUAL:              | 763 SF       |           |
| OCCUPANT LOAD:     | F-2 @ 300SF / PERSON | 3 PERSONS    | §504.3    |
| FIRE ALARM:        | NOT REQUIRED         |              | §907.2.4  |
| FIRE SPRINKLER:    | NOT REQUIRED         |              | §903.2    |
| ACCESSIBILITY:     | NOT REQUIRED         |              | §1103.2.9 |

| APPLICABLE CODES                            |
|---|
| 2015 INTERNATIONAL BUILDING CODE            |
| 2015 INTERNATIONAL FIRE CODE                |
| 2015 INTERNATIONAL FUEL GAS CODE            |
| 2015 INTERNATIONAL MECHANICAL CODE          |
| 2015 INTERNATIONAL PLUMBING CODE            |
| 2015 INTERNATIONAL ENERGY CONSERVATION CODE |
| 2017 NATIONAL ELECTRICAL CODE               |
| 2009 ICC/ANSI 117.1 ACCESSIBILITY CODE      |
| 2016 NFPA 820                               |

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| ISSUE      | DATE              | DESCRIPTION |
|------------|-------------------|-------------|
|            |                   |             |
|            |                   |             |
| 10/08/2020 | 90% DESIGN REVIEW |             |
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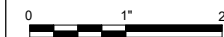
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|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
| STRUCTURAL      | J. CORONADO  |
| PROCESS         | B. REINER    |
| UTILITIES       | T. SCHARRA   |
| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | R. NELSON    |
| PROJECT NUMBER  | 1011626      |

**PRELIMINARY  
NOT FOR  
CONSTRUCTION  
OR  
RECORDING**



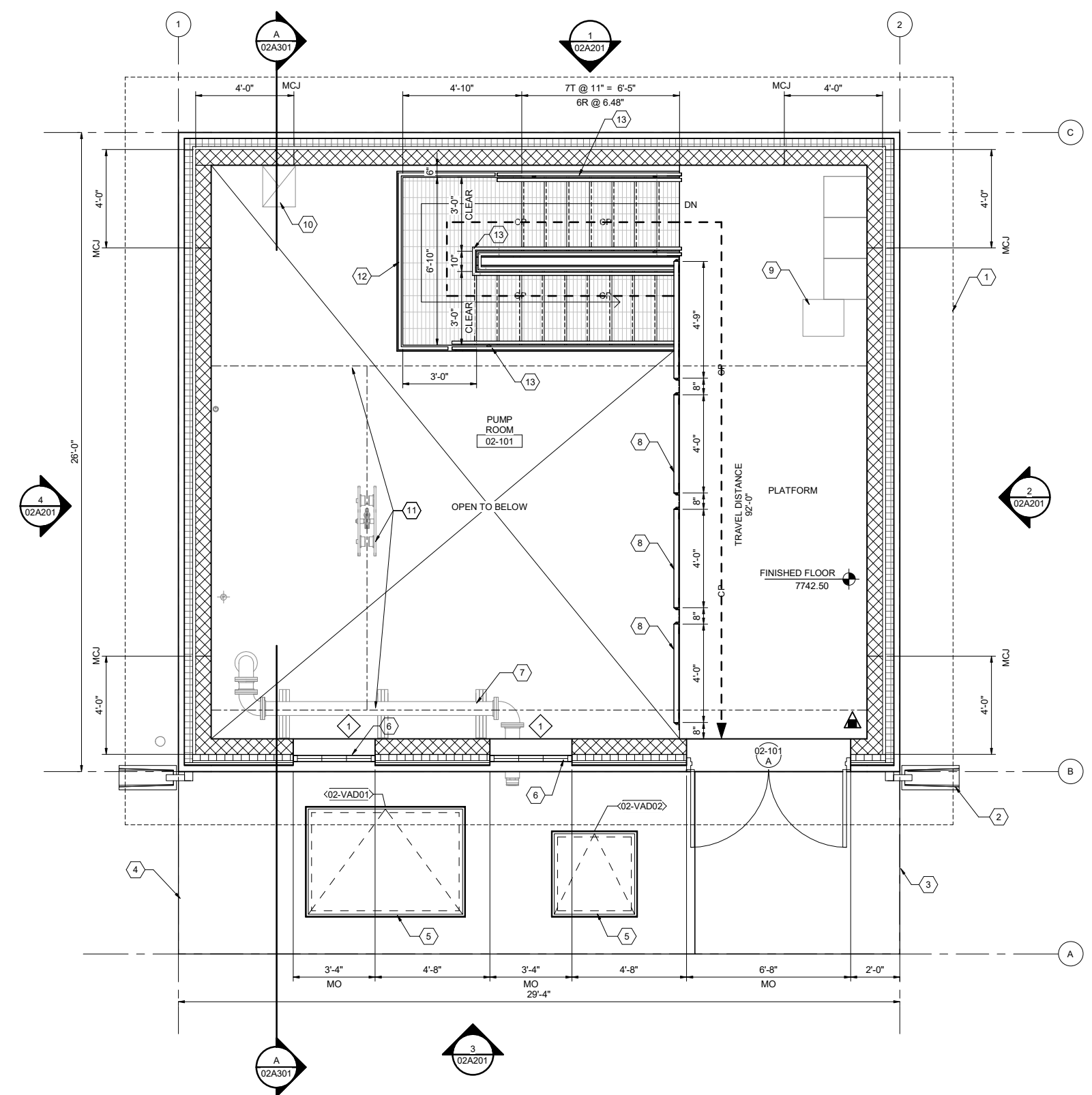
**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

**LIFT STATION  
LOWER LEVEL PLAN**



FILENAME | 10116326\_02\_Structural.rvt  
SCALE | 3/8" = 1'-0"

SHEET  
**402A101**



- GENERAL NOTES**
1. ALL DIMENSIONS ARE TO FACE OF MASONRY UNIT
  2. SEE SHEET 02A601 FOR SCHEDULES, DOOR AND FRAME TYPES

- KEYNOTES #**
- 1 LINE OF ROOF ABOVE
  - 2 DOWNSPOUT AND SPLASHBLOCK
  - 3 STOOP SEE STRUCTURAL
  - 4 CONCRETE SLAB
  - 5 VAULT ACCESS DOOR
  - 6 TRANSLUCENT PANEL WINDOW FRAMING
  - 7 PROCESS PIPING AND EQUIPMENT; SEE MECHANICAL/PROCESS DESIGN DOCUMENTS
  - 8 ALUMINUM REMOVABLE GUARDRAIL
  - 9 SEE ELECTRICAL FOR POWER LAYOUT AND DESIGN
  - 10 DUCT SEE MECHANICAL
  - 11 MONORAIL CRANE AND RAILS ABOVE
  - 12 ALUMINUM GUARDRAIL
  - 13 ALUMINUM STAIR RAIL



| ISSUE | DATE       | DESCRIPTION       |
|-------|------------|-------------------|
|       | 10/08/2020 | 90% DESIGN REVIEW |

|                        |              |
|------------------------|--------------|
| <b>PROJECT MANAGER</b> | GINA MACPHEE |
| STRUCTURAL             | J. CORONADO  |
| PROCESS                | B. REINER    |
| UTILITIES              | T. SCHARRA   |
| ARCHITECTURAL          | J. RICKERT   |
| MECHANICAL             | A. THORPE    |
| ELECTRICAL             | B. CASSITY   |
| DRAWN BY               | R. NELSON    |
| <b>PROJECT NUMBER</b>  | 1011626      |

**PRELIMINARY  
NOT FOR  
CONSTRUCTION  
OR  
RECORDING**



**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

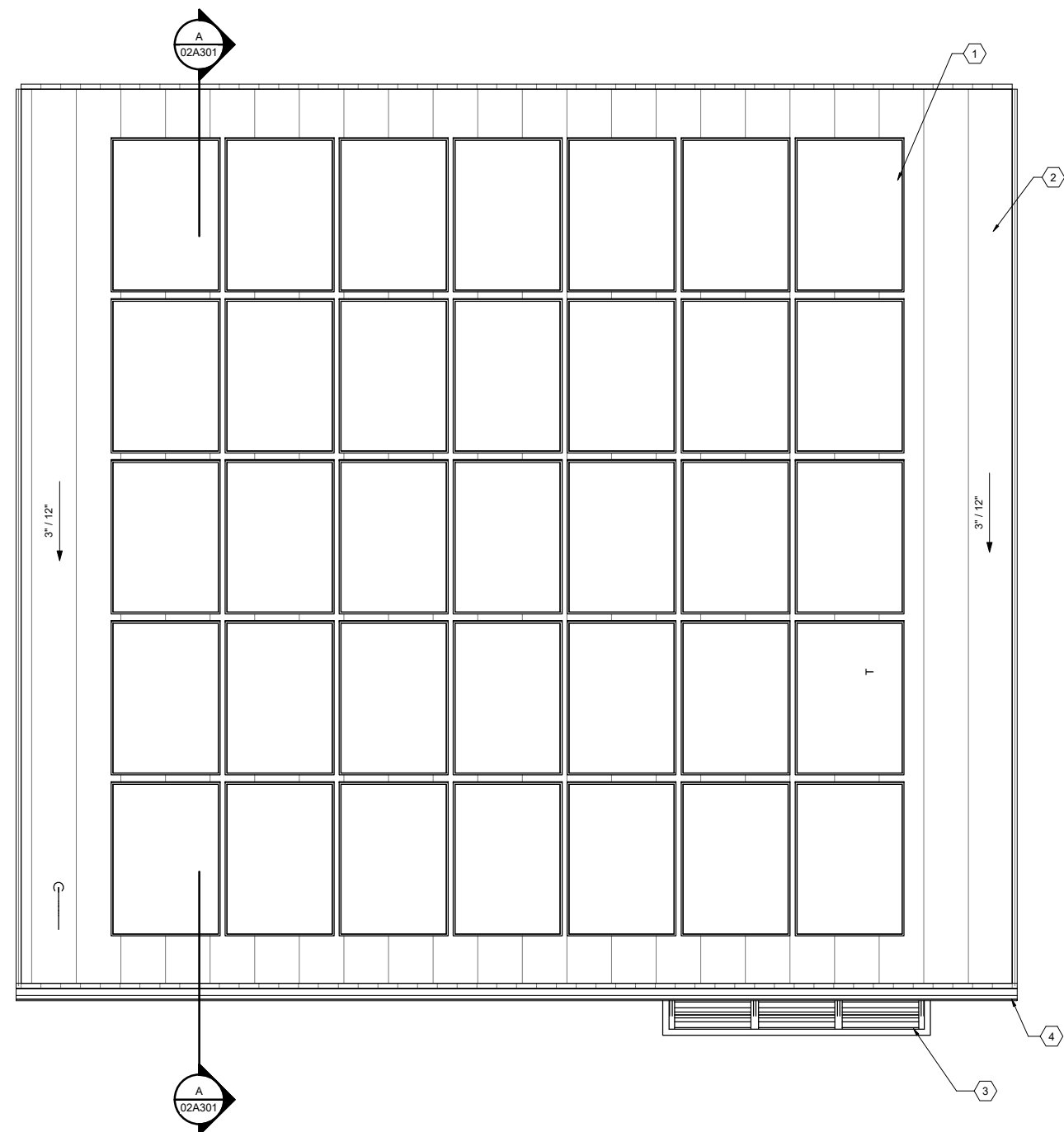


**LIFT STATION  
GRADE LEVEL PLAN**

FILENAME | 10116326\_02\_Structural.rvt  
SCALE | 3/8" = 1'-0"

SHEET  
**4702A102**

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- KEYNOTES** #
- 1 SOLAR PANEL
  - 2 STANDING SEAM METAL ROOF
  - 3 METAL CANOPY
  - 4 GUTTER



| ISSUE | DATE | DESCRIPTION |
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|       |      |             |
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|       |      |             |

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|------------------------|--------------|
| <b>PROJECT MANAGER</b> | GINA MACPHEE |
| STRUCTURAL             | J. CORONADO  |
| PROCESS                | B. REINER    |
| UTILITIES              | T. SCHARRA   |
| ARCHITECTURAL          | J. RICKERT   |
| MECHANICAL             | A. THORPE    |
| ELECTRICAL             | B. CASSITY   |
| DRAWN BY               | R. NELSON    |
| <b>PROJECT NUMBER</b>  | 1011626      |

**PRELIMINARY  
NOT FOR  
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OR  
RECORDING**



**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**



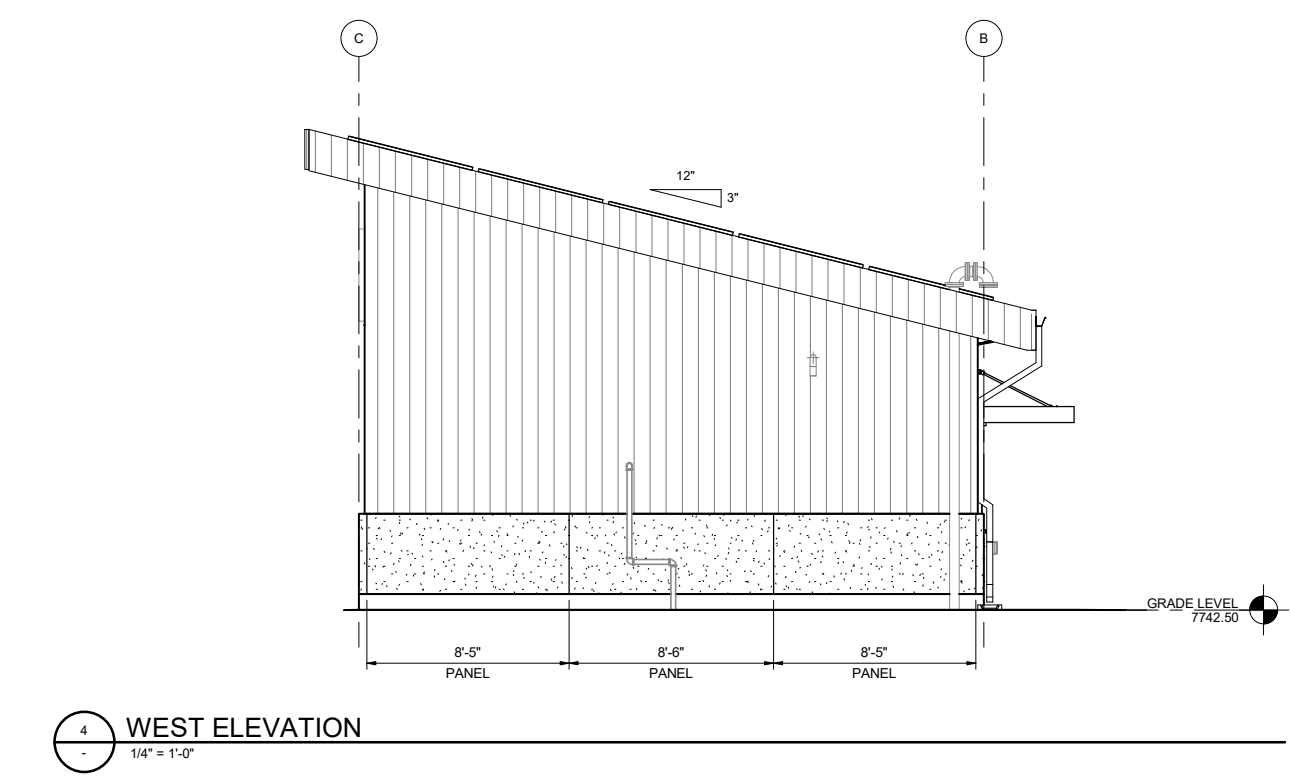
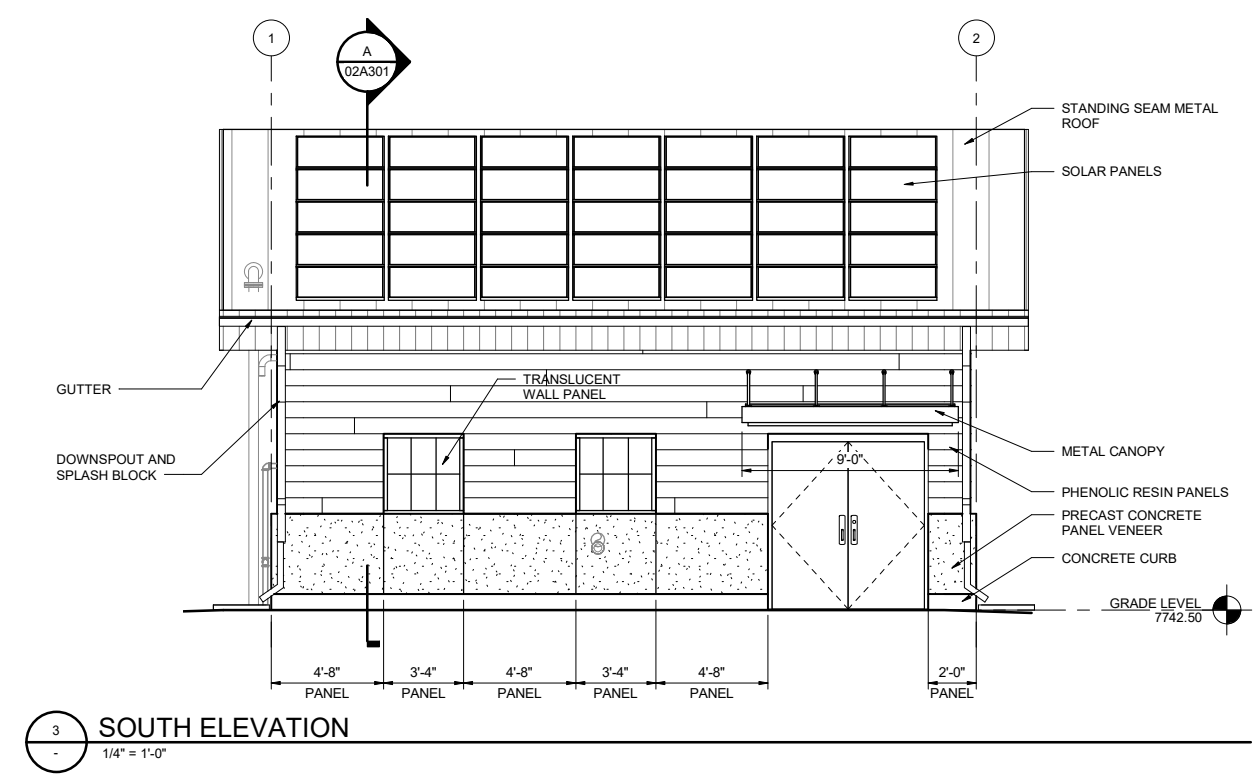
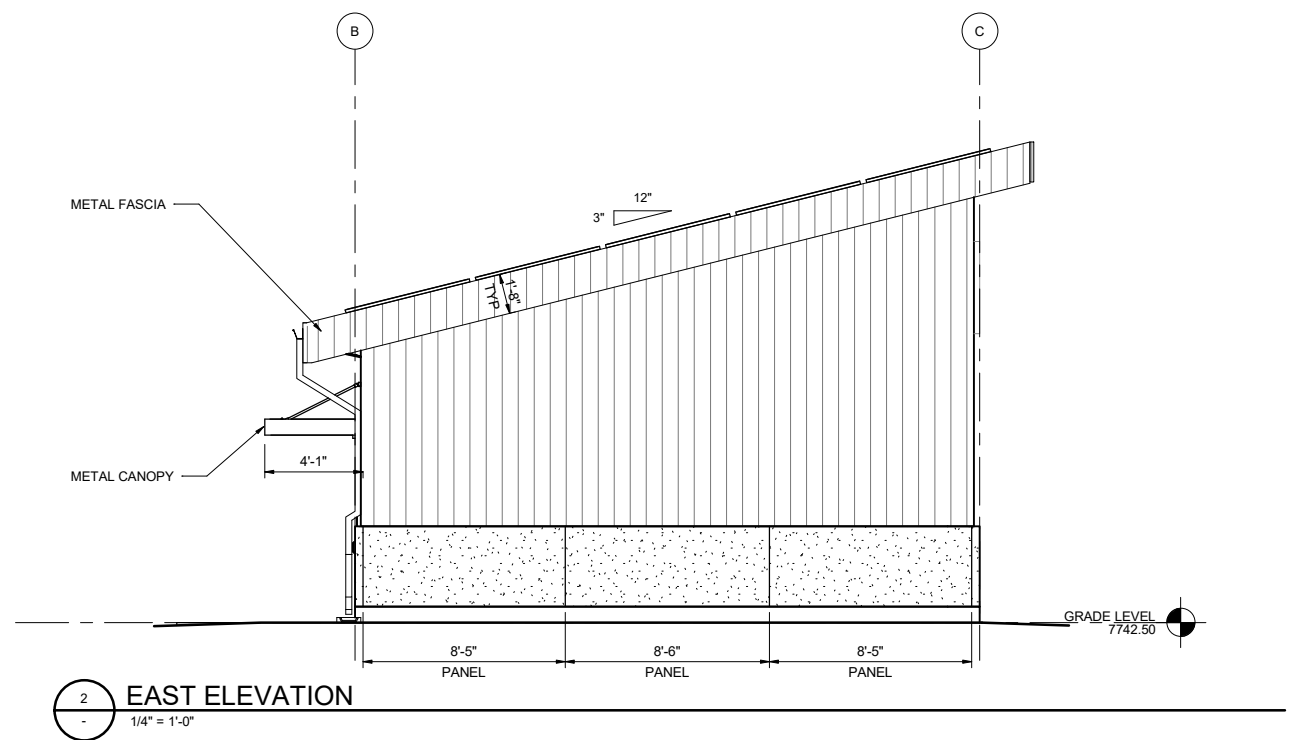
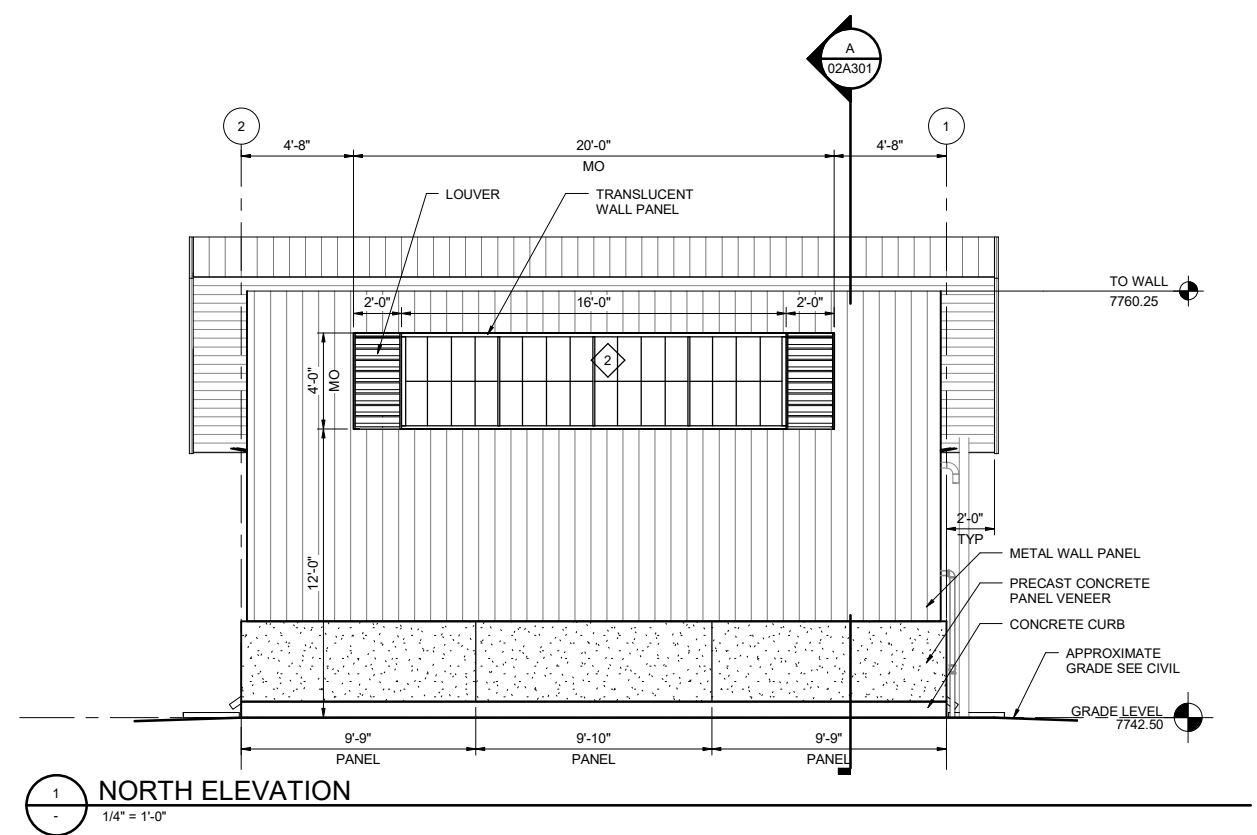
**LIFT STATION  
ROOF PLAN**

FILENAME | 10116326\_02\_Structural.rvt  
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SHEET  
**402A103**

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| ISSUE | DATE       | DESCRIPTION       |
|-------|------------|-------------------|
|       | 10/08/2020 | 90% DESIGN REVIEW |
|       | 08/26/2020 | 60% DESIGN REVIEW |
|       | 02/21/2020 | 30% DESIGN REVIEW |

|                 |              |
|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
| STRUCTURAL      | J. CORONADO  |
| PROCESS         | B. REINER    |
| UTILITIES       | T. SCHARRA   |
| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | R. NELSON    |
| PROJECT NUMBER  | 1011626      |

**PRELIMINARY  
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CONSTRUCTION  
OR  
RECORDING**



**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

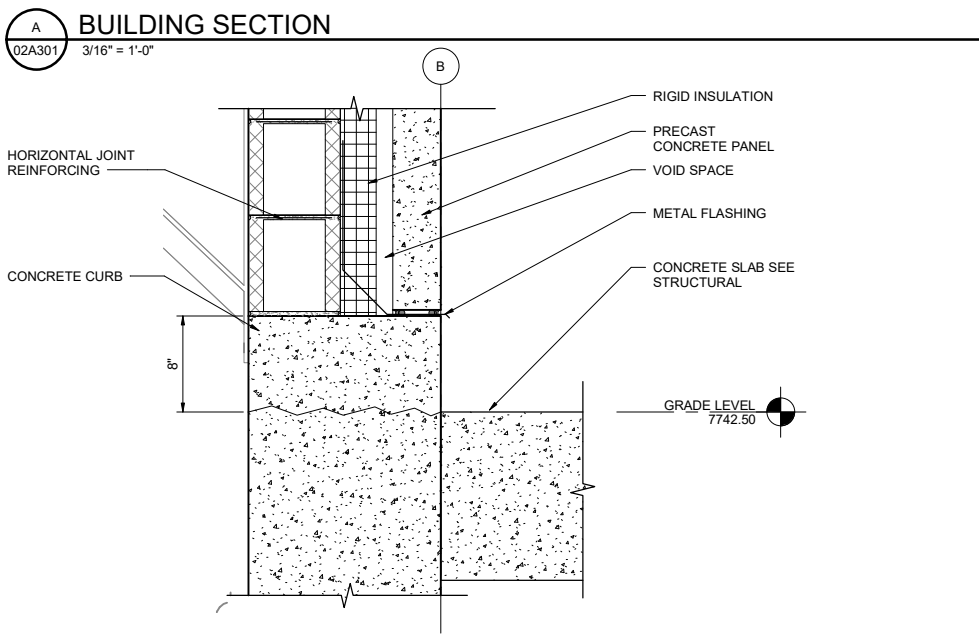
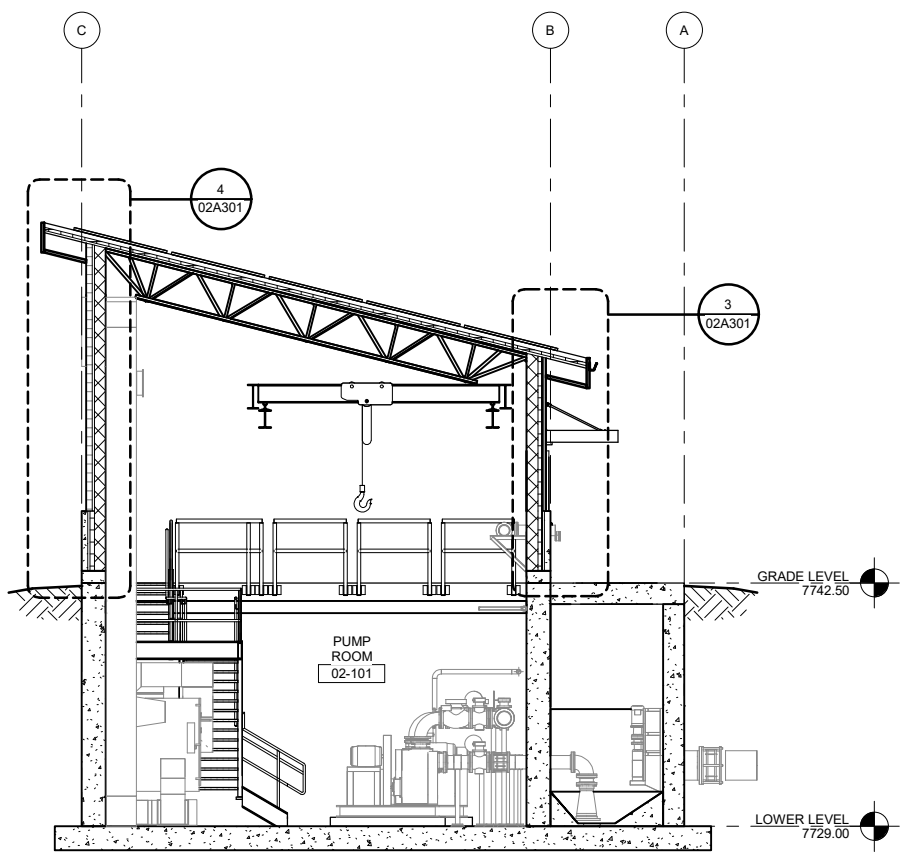
**LIFT STATION  
BUILDING ELEVATIONS**



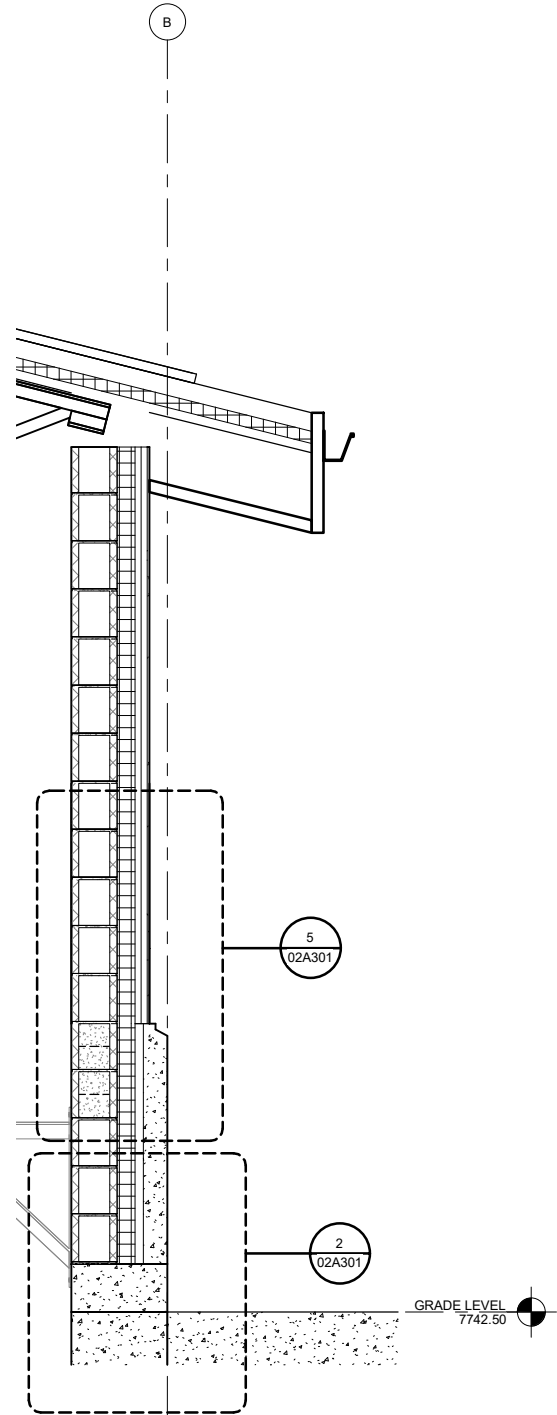
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SHEET  
**402A201**

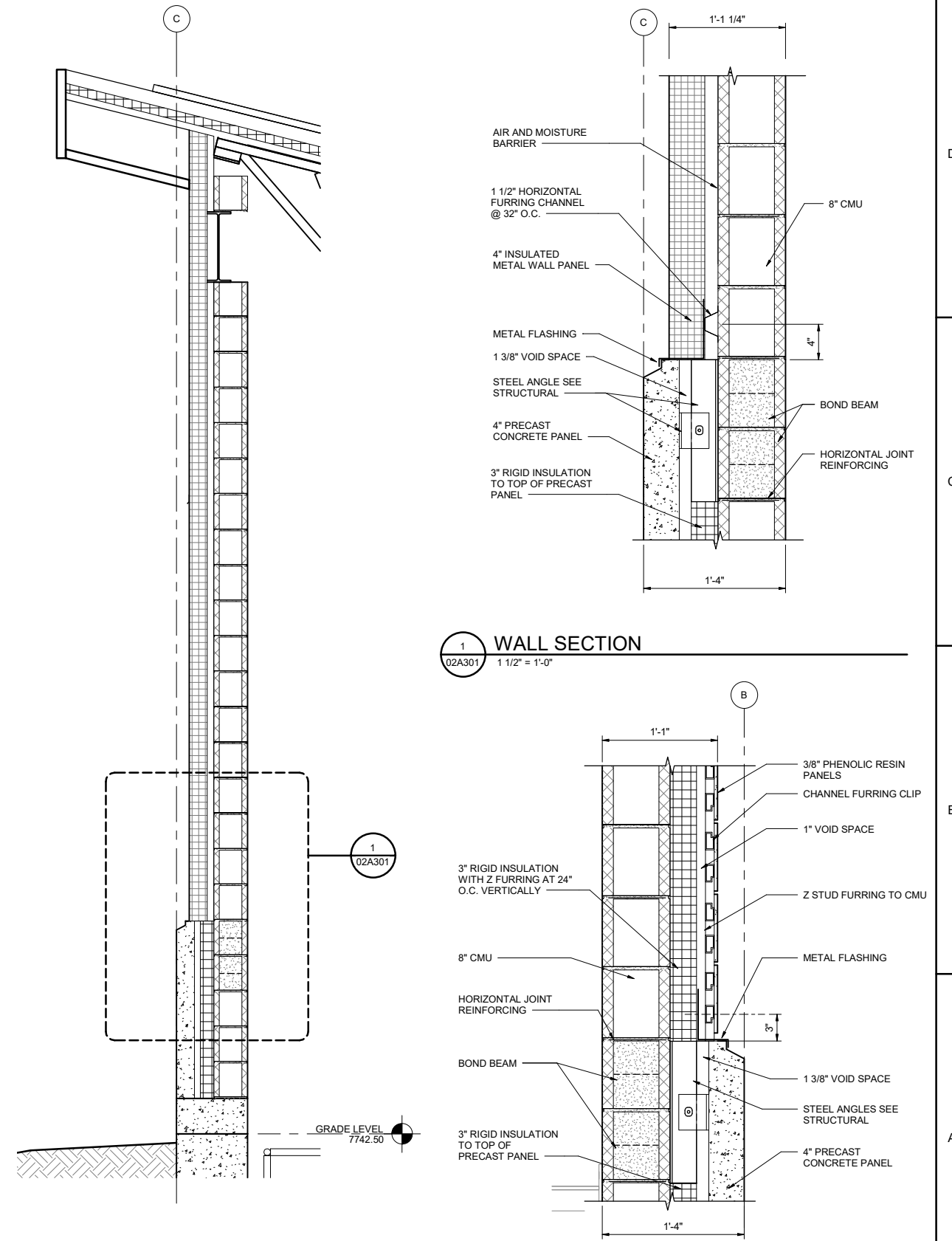
1 2 3 4 5 6 7 8



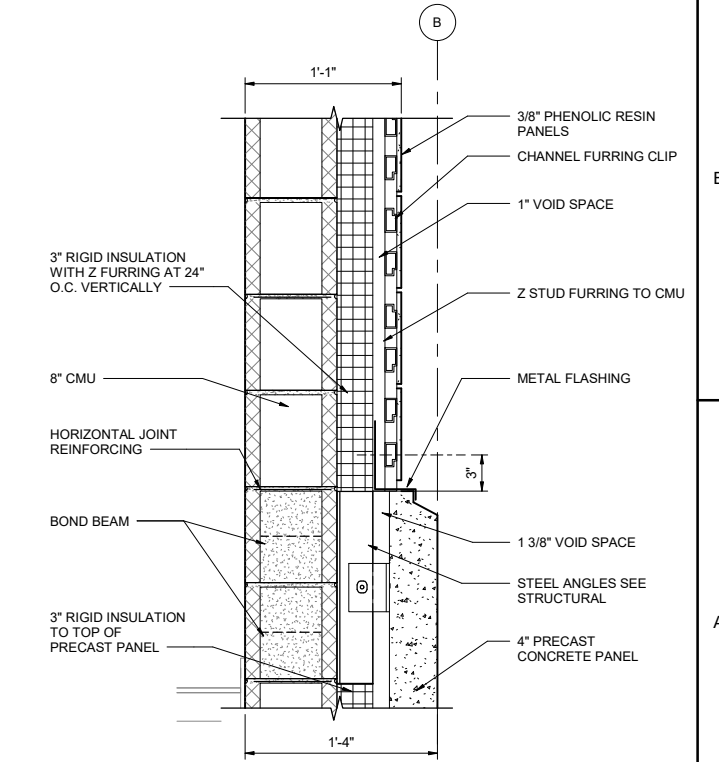
**2 WALL SECTION AT BASE**  
02A301 1 1/2" = 1'-0"



**3 WALL SECTION**  
02A301 3/4" = 1'-0"



**1 WALL SECTION**  
02A301 1 1/2" = 1'-0"



**5 WALL SECTION**  
02A301 1 1/2" = 1'-0"



| ISSUE | DATE       | DESCRIPTION       |
|-------|------------|-------------------|
|       | 10/08/2020 | 90% DESIGN REVIEW |
|       | 08/26/2020 | 60% DESIGN REVIEW |
|       | 02/21/2020 | 30% DESIGN REVIEW |

|                        |              |
|------------------------|--------------|
| <b>PROJECT MANAGER</b> | GINA MACPHEE |
| STRUCTURAL             | J. CORONADO  |
| PROCESS                | B. REINER    |
| UTILITIES              | T. SCHARRA   |
| ARCHITECTURAL          | J. RICKERT   |
| MECHANICAL             | A. THORPE    |
| ELECTRICAL             | B. CASSITY   |
| DRAWN BY               | R. NELSON    |
| <b>PROJECT NUMBER</b>  | 1011626      |

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**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**



FILENAME 1011626\_02\_Structural.rvt  
SCALE As indicated

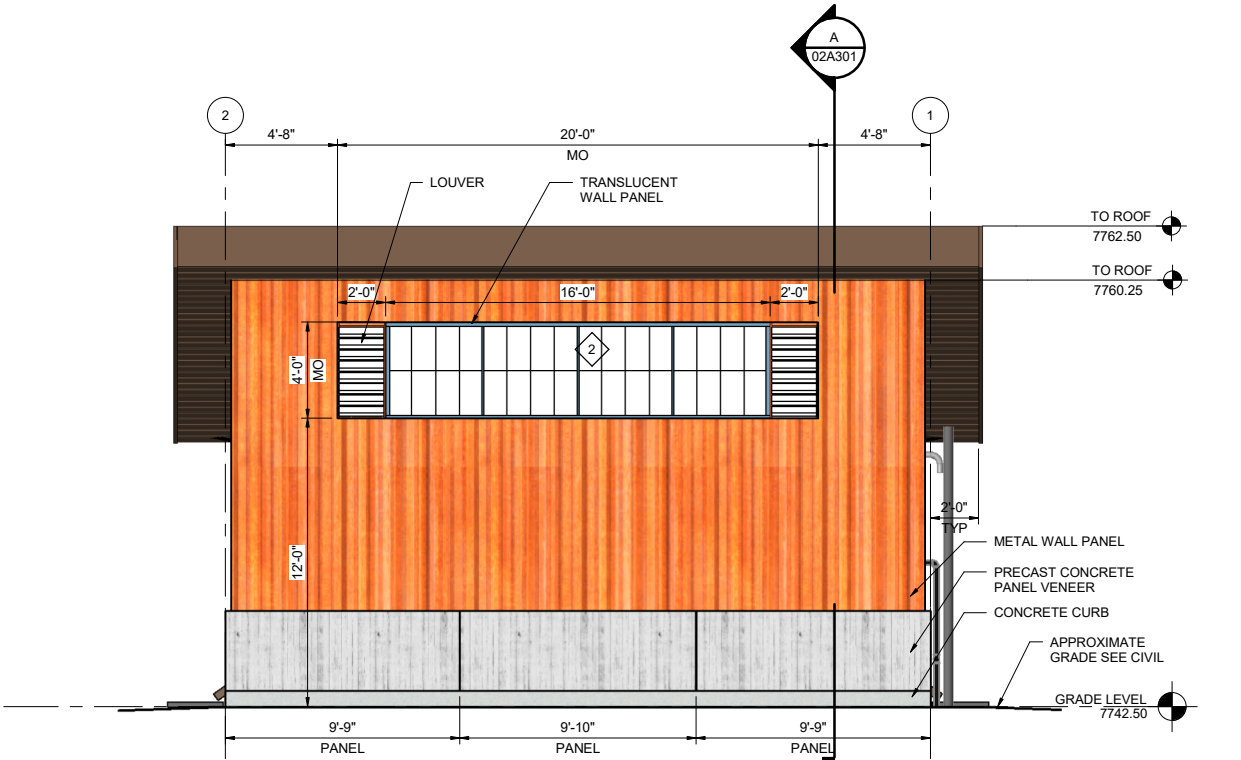
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**502A301**

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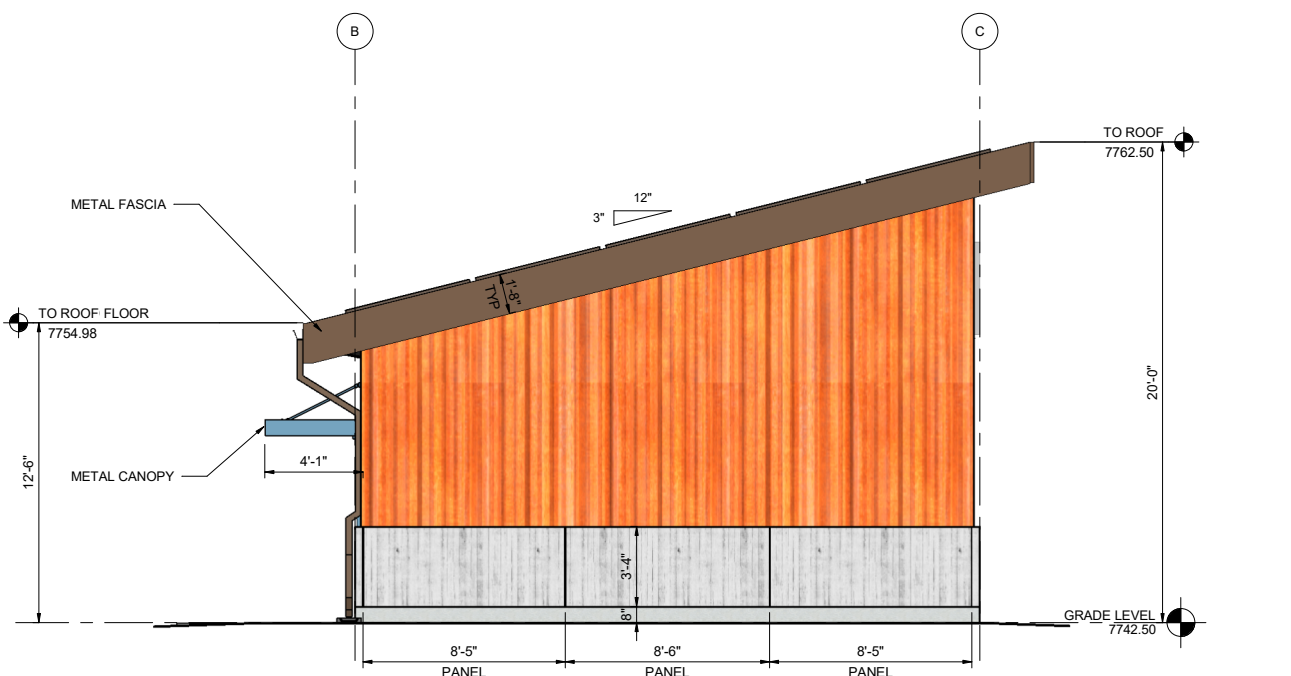


## **Exhibit D**

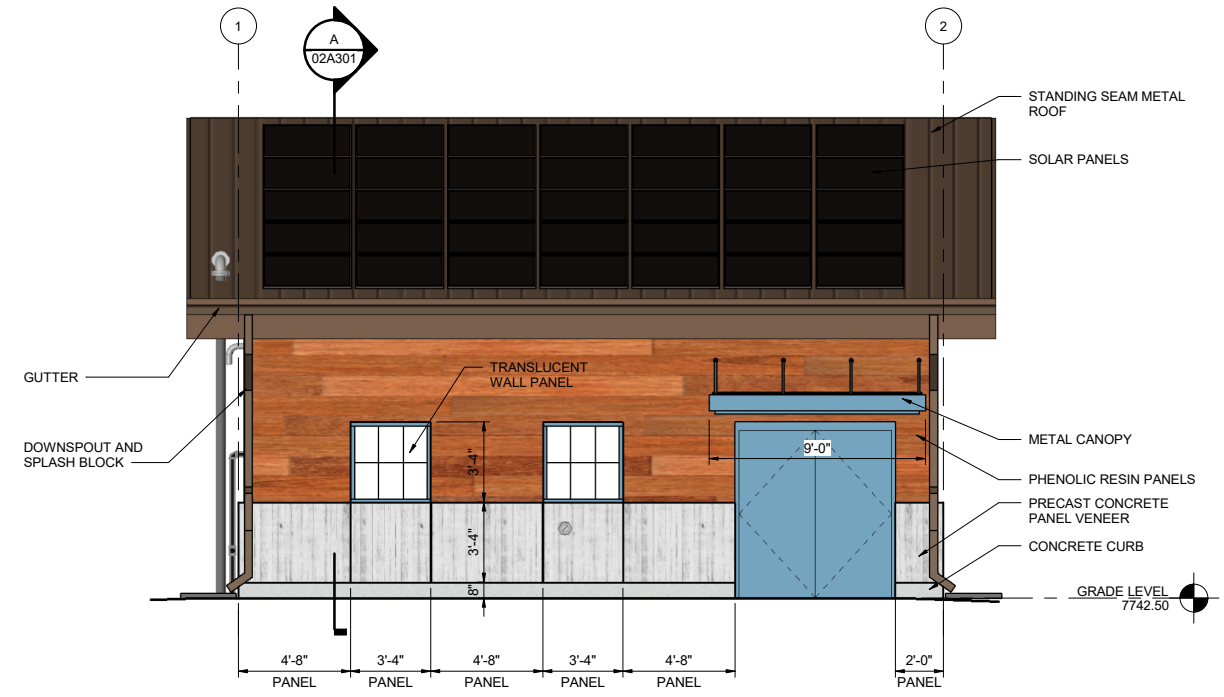
### Architectural Renderings



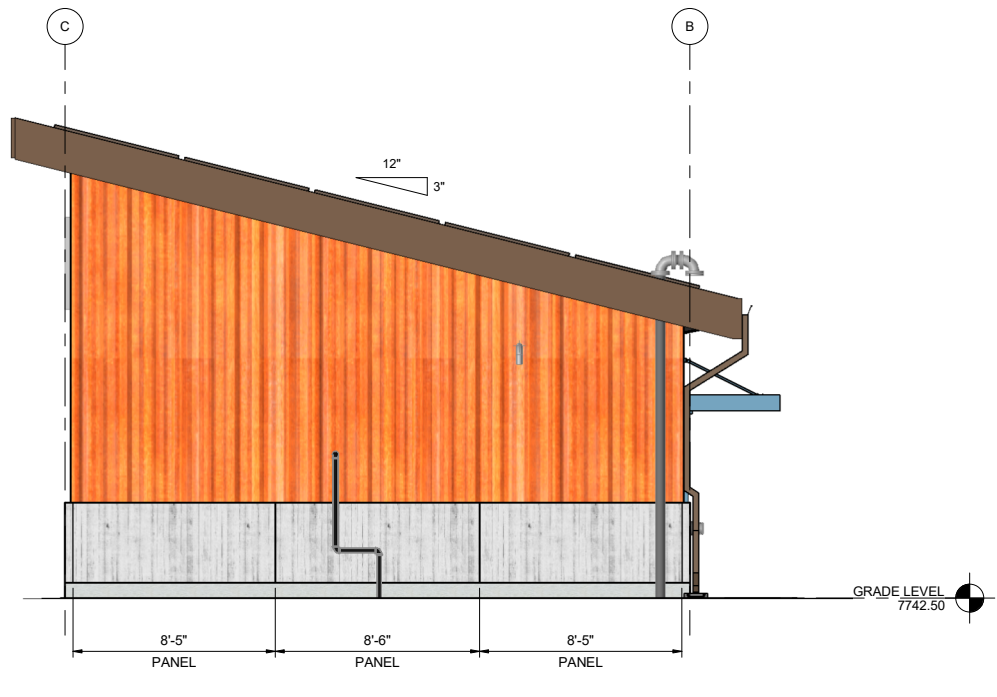
1 NORTH ELEVATION  
02A201 1/4" = 1'-0"



2 EAST ELEVATION  
02A201 1/4" = 1'-0"



3 SOUTH ELEVATION  
02A201 1/4" = 1'-0"



4 WEST ELEVATION  
02A201 1/4" = 1'-0"



| ISSUE | DATE       | DESCRIPTION       |
|-------|------------|-------------------|
|       | 10/08/2020 | 90% DESIGN REVIEW |
|       | 08/28/2020 | 60% DESIGN REVIEW |
|       | 02/21/2020 | 30% DESIGN REVIEW |

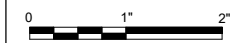
|                 |              |
|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
| STRUCTURAL      | J. CORONADO  |
| PROCESS         | B. REINER    |
| UTILITIES       | T. SCHARRA   |
| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | R. NELSON    |
| PROJECT NUMBER  | 1011626      |

**PRELIMINARY  
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OR  
RECORDING**



**DOWD JUNCTION COLLECTION SYSTEM  
IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

**LIFT STATION  
BUILDING ELEVATIONS**



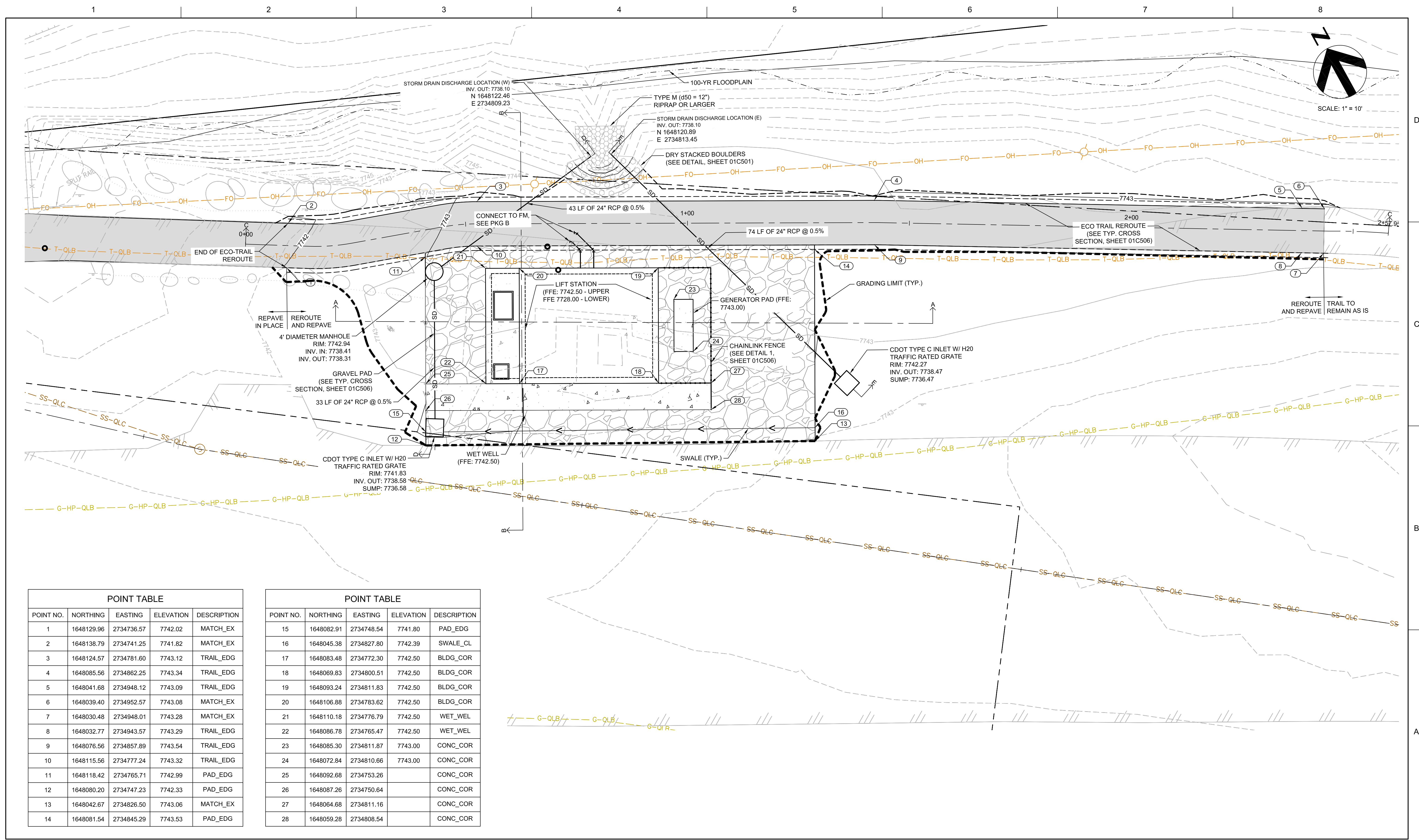
FILENAME | 10116326\_02\_Structural.rvt  
SCALE | 1/4" = 1'-0"

SHEET  
**5302A201**

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| POINT TABLE |            |            |           |             |
|-------------|------------|------------|-----------|-------------|
| POINT NO.   | NORTHING   | EASTING    | ELEVATION | DESCRIPTION |
| 1           | 1648129.96 | 2734736.57 | 7742.02   | MATCH_EX    |
| 2           | 1648138.79 | 2734741.25 | 7741.82   | MATCH_EX    |
| 3           | 1648124.57 | 2734781.60 | 7743.12   | TRAIL_EDG   |
| 4           | 1648085.56 | 2734862.25 | 7743.34   | TRAIL_EDG   |
| 5           | 1648041.68 | 2734948.12 | 7743.09   | TRAIL_EDG   |
| 6           | 1648039.40 | 2734952.57 | 7743.08   | MATCH_EX    |
| 7           | 1648030.48 | 2734948.01 | 7743.28   | MATCH_EX    |
| 8           | 1648032.77 | 2734943.57 | 7743.29   | TRAIL_EDG   |
| 9           | 1648076.56 | 2734857.89 | 7743.54   | TRAIL_EDG   |
| 10          | 1648115.56 | 2734777.24 | 7743.32   | TRAIL_EDG   |
| 11          | 1648118.42 | 2734765.71 | 7742.99   | PAD_EDG     |
| 12          | 1648080.20 | 2734747.23 | 7742.33   | PAD_EDG     |
| 13          | 1648042.67 | 2734826.50 | 7743.06   | MATCH_EX    |
| 14          | 1648081.54 | 2734845.29 | 7743.53   | PAD_EDG     |

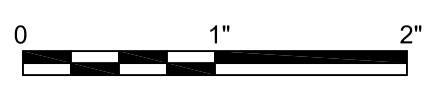
| POINT TABLE |            |            |           |             |
|-------------|------------|------------|-----------|-------------|
| POINT NO.   | NORTHING   | EASTING    | ELEVATION | DESCRIPTION |
| 15          | 1648082.91 | 2734748.54 | 7741.80   | PAD_EDG     |
| 16          | 1648045.38 | 2734827.80 | 7742.39   | SWALE_CL    |
| 17          | 1648083.48 | 2734772.30 | 7742.50   | BLDG_COR    |
| 18          | 1648069.83 | 2734800.51 | 7742.50   | BLDG_COR    |
| 19          | 1648093.24 | 2734811.83 | 7742.50   | BLDG_COR    |
| 20          | 1648106.88 | 2734783.62 | 7742.50   | BLDG_COR    |
| 21          | 1648110.18 | 2734776.79 | 7742.50   | WET_WEL     |
| 22          | 1648086.78 | 2734765.47 | 7742.50   | WET_WEL     |
| 23          | 1648085.30 | 2734811.87 | 7743.00   | CONC_COR    |
| 24          | 1648072.84 | 2734810.66 | 7743.00   | CONC_COR    |
| 25          | 1648092.68 | 2734753.26 | 7743.00   | CONC_COR    |
| 26          | 1648087.26 | 2734750.64 | 7743.00   | CONC_COR    |
| 27          | 1648064.68 | 2734811.16 | 7743.00   | CONC_COR    |
| 28          | 1648059.28 | 2734808.54 | 7743.00   | CONC_COR    |



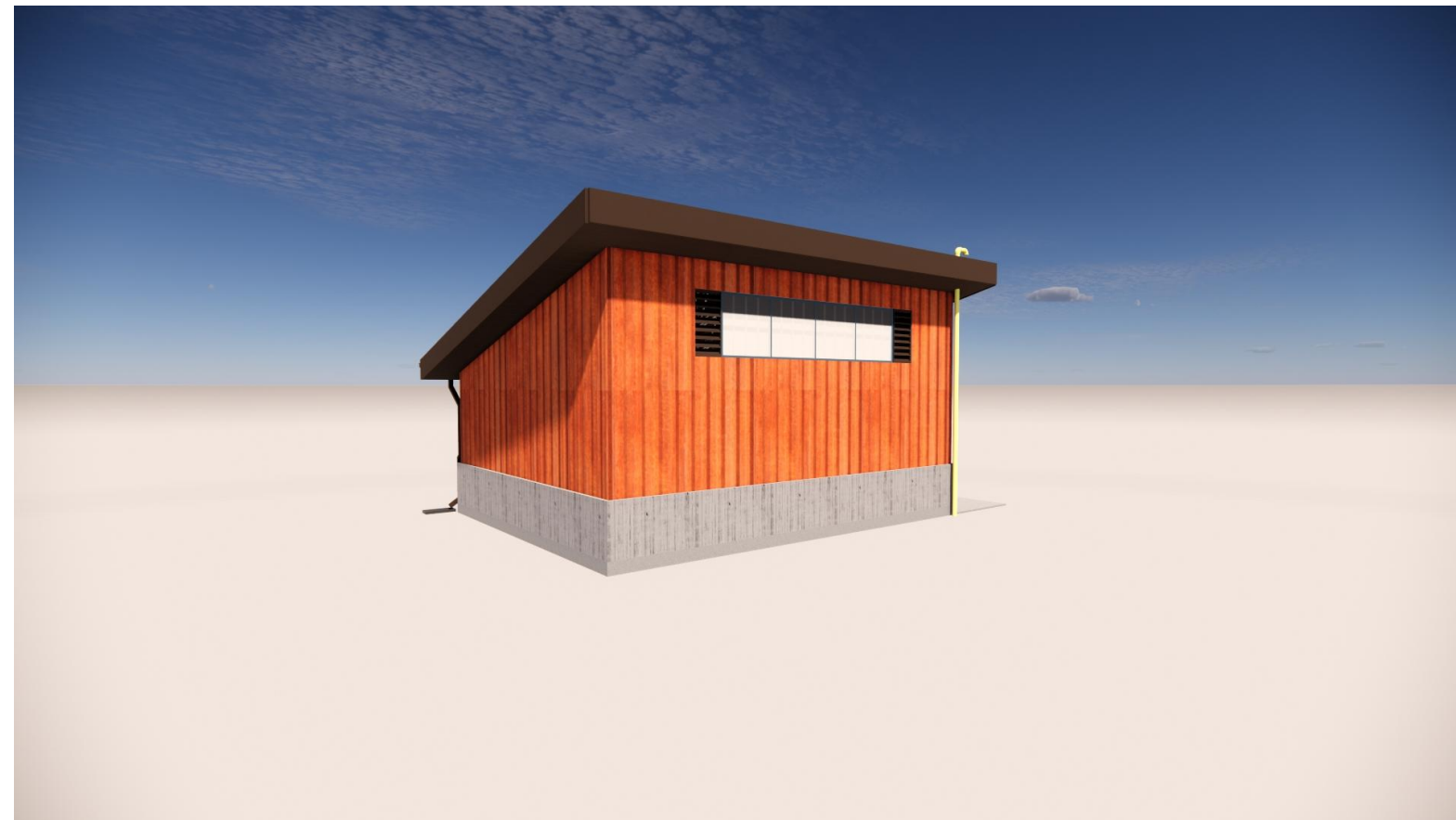
| ISSUE      | DATE              | DESCRIPTION |
|------------|-------------------|-------------|
| 10/08/2020 | 90% DESIGN REVIEW |             |
| 06/26/2020 | 60% DESIGN REVIEW |             |
| 02/21/2020 | 30% DESIGN REVIEW |             |

|                 |              |
|-----------------|--------------|
| PROJECT MANAGER | GINA MACPHEE |
| STRUCTURAL      | J. CORONADO  |
| PROCESS         | B. REINER    |
| UTILITIES       | T. SCHARRA   |
| ARCHITECTURAL   | J. RICKERT   |
| MECHANICAL      | A. THORPE    |
| ELECTRICAL      | B. CASSITY   |
| DRAWN BY        | I. MARTINEZ  |
| PROJECT NUMBER  | 10116326     |

  
**EAGLE RIVER**  
 WATER & SANITATION  
 DISTRICT  
**DOWD JUNCTION COLLECTION SYSTEM**  
**IMPROVEMENTS - LIFT STATION AND CONVEYANCE**

**SITE GRADING PLAN AND SURVEY CONTROL PLAN**  
 FILENAME | 01C101.dwg SHEET | **01C101**  
 SCALE | 1" = 10'





**Minturn Planning Department**  
Minturn Town Center  
302 Pine Street  
Minturn, Colorado 81645



**Minturn Planning Commission**  
Chair – Lynn Teach  
Jeff Armistead  
Elliot Hovey  
Chris Manning  
Tom Priest  
Jena Skinner

**To:** Planning Commission  
**From:** Scot Hunn, Planning Director  
Madison Harris, Planner I  
**Date:** August 5, 2021  
**Re:** 832 Main Street – Jones New Residence Modification to Approved Plans

---

Pamela and Craig Jones, owners of 832 Main Street, received approval from the DRB on March 24, 2021 for a new single family residence located on their property. The Applicant's Representative is Michael Pukas.

The project is currently under construction and during excavation groundwater was encountered. Subsequently, the Applicant has determined that the plans, as approved, require minor modifications to raise the foundation and, therefore, the entire house. As a result, the Applicant is proposing to revise the pitch of the roof. Per the Applicant's Representative's email dated July 8, 2021:

*“The bottom of the basement slab had to be raised approximately 2’-3” to be above the 100 year flood plain, as indicated on the survey. The following changes were made:*

- *The height of the basement was reduced by 1’-0”*
- *The entire building was raised out of the ground approximately 1’-3”*
- *The pitch of the gable roof elements was changed from a 10/12 to an 8/12.*
- *There were no changes to the building footprint, square footage, building lot coverage, or impervious surface coverage.*
- *An underdrain system was added to address any ground water that may pose a threat to the structure.”*

Staff believes that the proposed modifications are minor in nature and will not change the project's conformance with the applicable standards of Chapter 16 – *Zoning*, or Appendix 'B' – *Design Standards and Guidelines* of the Minturn Municipal Code.

Staff has confirmed with the Applicant's Representative that the proposed changes do not affect the proposed finished grading and drainage plan around the structure.

Staff is recommending **approval without conditions**.

GENERAL NOTES

- ALL WORK SHALL BE AS SPECIFIED AND IN ACCORDANCE WITH ALL NATIONAL, STATE AND LOCAL CODES, LAWS, PERMITS AND ORDINANCES, AND SHALL BE PERFORMED TO THE HIGHEST STANDARDS OF CRAFTSMANSHIP BY JOURNEYMEN OF THE APPROPRIATE TRADES.
- THESE DOCUMENTS ARE NOT INTENDED TO INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND SERVICES REQUIRED TO COMPLETE ALL WORK DESCRIBED HEREIN. THE GENERAL CONTRACTOR (G.C.) SHALL VERIFY ALL DIMENSIONS AND SITE CONDITIONS BEFORE STARTING WORK.
- IT IS THE RESPONSIBILITY OF THE G.C. TO BRING TO THE ATTENTION OF THE ARCHITECT ANY CONDITIONS WHICH WILL NOT PERMIT CONSTRUCTION ACCORDING TO THE INTENTIONS OF THESE CONTRACT DOCUMENTS (G.C.S.) AND TO NOTIFY THE ARCHITECT AT ONCE IF ANY DISCREPANCIES APPEAR IN THE G.C.S. OR BETWEEN THE G.C.S. AND EXISTING CONDITIONS. IT IS THE RESPONSIBILITY OF THE ARCHITECT TO PROVIDE DETAILS AND/OR DIRECTIONS REGARDING DESIGN INTENT WHERE IT IS ALTERED BY EXISTING CONDITIONS OR WHERE NEGLECTED IN THE DOCUMENTS.
- SHOULD A CONFLICT OCCUR IN OR BETWEEN DRAWINGS AND SPECIFICATIONS, THE SPECIFICATIONS SHALL TAKE PRECEDENCE, UNLESS A WRITTEN DECISION FROM THE ARCHITECT HAS BEEN OBTAINED WHICH DESCRIBES A CLARIFICATION OR ALTERNATE METHOD AND/OR MATERIALS.
- DIMENSIONS: A) ALL DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS; B) ALL PLAN DIMENSIONS ARE TO FACE OF CONCRETE, FACE OF STUD, AND CENTER LINE OF COLUMN/BEAM; U.O.N.; C) FLOOR TO FLOOR DIMENSIONS ARE FROM TOP OF PLYWOOD SUBSURFACE TO TOP OF PLYWOOD SUBSURFACE; U.O.N.; D) VERIFY IN FIELD (V.I.F.) ALL EXISTING CONDITIONS, DIMENSIONS, LOCATIONS AND SITE CONDITIONS PRIOR TO NEW CONSTRUCTION.
- THE G.C. SHALL SUBMIT SAMPLES OF ANY MATERIALS PROPOSED FOR SUBSTITUTION TO THE ARCHITECT FOR REVIEW AND APPROVAL BEFORE THE WORK IS TO BE PERFORMED. WORK SHALL CONFORM TO THE APPROVED SAMPLES.
- THE G.C. SHALL SUBMIT REQUIRED SHOP DRAWINGS TO THE ARCHITECT FOR REVIEW AND APPROVAL BEFORE THE WORK IS TO BE PERFORMED. WORK SHALL CONFORM TO THE APPROVED SHOP DRAWINGS.
- THE G.C. SHALL BE RESPONSIBLE FOR THE SAFETY AND CARE OF ADJACENT PROPERTIES DURING CONSTRUCTION. FOR COMPLIANCE WITH FEDERAL AND STATE O.S.H.A. REGULATIONS, AND FOR THE PROTECTION OF ALL WORK UNTIL IT IS DELIVERED COMPLETED TO THE OWNER.
- THE G.C. SHALL VERIFY AND COORDINATE ALL OPENINGS THROUGH FLOORS, CEILING, AND WALLS WITH ALL ARCHITECTURAL, STRUCTURAL, MECHANICAL, PLUMBING, AND ELECTRICAL DRAWINGS.
- THE JOB SITE SHALL BE MAINTAINED IN A CLEAN, ORDERLY CONDITION, FREE OF DEBRIS AND LITTER, AND SHALL NOT BE UNREASONABLY ENCLERED. EACH SUB-CONTRACTOR SHALL REMOVE ALL TRASH AND DEBRIS AS OF RESULT OF HIS/HER OPERATION UPON COMPLETION OF HIS/HER WORK.
- THE G.C. SHALL PERFORM ALL PHASES OF CONSTRUCTION SUCH THAT ALL NEW CONSTRUCTION FITS FLIGHT AND SEAMLESS WITH ADJACENT EXISTING CONDITIONS, AND SHALL NOT ENDANGER ANY EXISTING CONDITIONS OR OTHER WORK.
- THE G.C. SHALL PROVIDE ALL NECESSARY BLOCKING, BACKING, AND FRAMING FOR LIGHT FIXTURES, ELECTRICAL UNITS, A.C. EQUIPMENT, RECESSED ITEMS, AND ALL OTHER ITEMS AS REQUIRED.
- ALL CONSTRUCTION, STAGING, CONTRACTOR PARKING AND MATERIALS STORAGE SHALL BE CONFINED TO THE LIMITS OF THE EXISTING DRIVEWAY AND THE IMMEDIATE PERIMETER OF THE EXISTING BUILDING. ALL ACTIVITY PERFORMED AS PART OF THIS PROJECT SHALL BE CONTAINED ON THE PROJECT PROPERTY.
- CHANGES TO THESE CONTRACT DOCUMENTS SHALL BE NOTED IN THE FIELD AND MAINTAINED ON-SITE FOR THE DURATION OF THE PROJECT FOR CONSTRUCTION OBSERVATION.
- AS-BUILT DRAWINGS WILL BE PROVIDED TO THE TOWN/COUNTY, IF REQUIRED.
- UTILITY METER LOCATIONS TO BE APPROVED BY UTILITY COMPANIES.
- ALL INTERIOR NON-BEARING WALLS TO BE 2x4, U.O.N.
- ALL INTERIOR BEARING WALLS TO BE 2x6, U.O.N. SEE STRUCTURAL DWG'S FOR SPECS
- ALL EXTERIOR WALLS TO BE 2x6, U.O.N. SEE STRUCTURAL DWG'S FOR SPECS
- ALL HORIZONTAL FRAMING DIMENSIONS ARE TO THE FACE OF WOOD FRAMING, U.O.N.
- ALL VERTICAL FRAMING DIMENSIONS ARE FROM THE T.O. PLYWOOD, U.O.N.
- ALL HORIZONTAL DIMENSIONS FOR OPENINGS ARE TO THE CENTER LINE OF R.O.
- ALL VERTICAL DIMENSIONS FOR OPENINGS ARE TO THE TOP OF R.O.

SITE NOTES

- THE GENERAL CONTRACTOR SHALL PROVIDE EROSION CONTROL IN CONFORMANCE WITH TOWN/COUNTY GUIDELINES.
- THE CONSTRUCTION LIMIT LINE IS THE CONTRACT LINE. DO NOT DISTURB ANY EXISTING TREES OR VEGETATION DESIGNED TO REMAIN OR LOCATED OUTSIDE OF THE CONSTRUCTION LIMIT LINE WITHOUT APPROVAL OF THE OWNER AND THE TOWN/COUNTY.
- THE GENERAL CONTRACTOR SHALL VERIFY EXISTING SITE INFORMATION, INCLUDING STRUCTURES, UTILITIES, PROPERTY LINES, LIMITS OF ROADWAYS, AND CURBS AND GUTTERS THAT MAY AFFECT THE SCOPE OF WORKING OR TO BEGINNING SITE CONSTRUCTION.
- EXISTING UTILITIES ARE INDICATED FOR INFORMATION ONLY AND NOT INTENDED TO SHOW EXACT LOCATION. THE ARCHITECT IS NOT RESPONSIBLE FOR THE LOCATION OF UNDERGROUND UTILITIES OR STRUCTURES SHOWN AND PROVIDED BY ANY OTHER CONTRACTOR. THE GENERAL CONTRACTOR SHALL LOCATE ALL UTILITIES AND MAINTAIN THE LOCATION DURING ALL PHASES OF THE WORK. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGES TO UTILITIES OR STRUCTURES AND ANY INJURIES THEREFROM. RESTORATION OF ALL UTILITIES DAMAGED BY THE GENERAL CONTRACTOR SHALL BE AT THE GENERAL CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE OWNER.
- CONFORM TO EAGLE RIVER WATER AND SANITATION DISTRICT SPECIFICATIONS FOR SEWER CONSTRUCTION. PROVIDE SEWER CLEANOUT WITH LOCATION TO BE REVIEWED BY TOWN ENGINEER.
- THE GENERAL CONTRACTOR SHALL PROVIDE DRAWINGS SHOWING PROPOSED UTILITY SERVICE CONNECTIONS FOR THE ENGINEER'S REVIEW PRIOR TO CONSTRUCTION.
- ROAD CUTS AND ANY OTHER CONSTRUCTION IN ROAD RIGHT-OF-WAY SHALL CONFORM TO TOWN GUIDELINES.
- ALL COMPACTION SHALL BE IN ACCORDANCE WITH THE SOILS REPORT PREPARED BY THE GEOTECHNICAL ENGINEER.
- PROVIDE WRITTEN NOTIFICATION OF ALL DISCREPANCIES BETWEEN EXISTING AND PROPOSED SITE IMPROVEMENTS.
- CONTRACTOR(S) SHALL TAKE ALL NECESSARY STEPS AS REQUIRED TO PROPERLY PROTECT AND MAINTAIN HIS WORK FOR THE DURATION OF THIS CONTRACT.
- THESE DRAWINGS DO NOT SPECIFY SAFETY MATERIALS, EQUIPMENT, METHODS OR SEQUENCING, TO PROTECT PERSONS AND PROPERTY. IT SHALL BE THE GENERAL CONTRACTOR'S RESPONSIBILITY TO DIRECT AND IMPLEMENT SAFETY OPERATIONS AND PROCEDURES TO PROTECT THE OWNER, OTHER CONTRACTORS, THE PUBLIC AND OTHERS.
- ALL WORK SHALL COMPLY WITH ALL APPLICABLE CODES AND ORDINANCES.
- CONTRACTOR(S) SHALL EMPLOY A LICENSED SURVEYOR TO ESTABLISH ALL WORK LINES.
- CONTRACTOR(S) SHALL STAKE OUT ALL AREAS, INCLUDING WALKS, FAVENENTS, WALLS, POOLS AND FENCES AND SHALL OBTAIN THE APPROVAL OF THE ARCHITECT PRIOR TO PROCEEDING WITH THE WORK.
- CONFLICTS OR DISCREPANCIES WITH GRADES SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT IMMEDIATELY AND PRIOR TO PROCEEDING WITH WORK.
- ALL FINISHED GRADES SHALL PROVIDE FOR NATURAL RUNOFF OF WATER WITHOUT LOW SPOTS OR POCKETS. SET FLOOR LINES ACCURATELY AND PROVIDE A MINIMUM 2% GRADIENT UNLESS OTHERWISE NOTED.
- GRADUALLY ROUND OFF TOPS AND TOES OF ALL PLANTED SLOPES, UNLESS SHOWN OTHERWISE IN GRADING DETAILS.
- GRADE AWAY FROM ALL BUILDINGS AT A MINIMUM SLOPE OF 10% IN UNPAVED AREAS AND MINIMUM SLOPE OF 2% IN PAVED AREAS.
- CLEANOUTS SHALL BE PROVIDED FOR ALL CHANGES IN LINES AND/OR GRADE OR THE SANITARY SEWER SERVICE.
- CONFORM TO TOWN/COUNTY STANDARDS FOR WATER CONSTRUCTION, MAINTAIN 10 FEET MINIMUM SEPARATION BETWEEN WATER AND SEWER UNDERGROUND SERVICE PIPES, MAINTAIN SEVEN FEET MINIMUM COVER AT UNDERGROUND WATER SERVICE AND ASSOCIATED LINES. NEW CURB STOPS AND SERVICE LINES SHALL BE INSTALLED FOR EACH UNIT. CURB STOPS SHALL BE LOCATED ON THE PROPERTY LINE IN A SPOT APPROVED BY THE TOWN PUBLIC WORKS DIRECTOR.
- IF UTILITY CONNECTION POINTS ARE NOT KNOWN AT THE TIME OF BUILDING PERMIT SUBMISSION, LOCATIONS TO BE COORDINATED AND APPROVED BY TOWN/COUNTY AND SERVICE PROVIDERS IN ADVANCE.

ENERGY CODE NOTES

GO TO COMPLY WITH ALL REQUIREMENTS OF THE 2018 INTERNATIONAL BUILDING CODE, THE 2018 INTERNATIONAL RESIDENTIAL CODE, AND THE 2018 INTERNATIONAL ENERGY CONSERVATION CODE AS WELL AS ANY AMENDMENTS ADOPTED BY THE TOWN'S AND/OR COUNTY'S MUNICIPAL CODE REQUIREMENTS, INCLUDING BUT NOT LIMITED TO:

|                              |  |
|------------------------------|--|
| CLIMATE ZONE                 | 1  |
| FENESTRATION U-FACTOR        | 0.30   |
| SKYLIGHT U-FACTOR            | 0.55   |
| CEILING R-VALUE              | 49   |
| WOOD FRAME WALL R-VALUE      | 20-5 OR 19-10 - CAVITY + CONTINUOUS EXTERIOR |
| MASS WALL R-VALUE            | 14/21  |
| FLOOR R-VALUE                | 33   |
| BASEMENT WALL R-VALUE        | 15/14 - CONTINUOUS + CAVITY                  |
| SLAB R-VALUE + DEPTH         | 10, 4"                                       |
| CRANAL SPACE WALL R-VALUE    | 15/14 - CONTINUOUS + CAVITY                  |
| HEATED SLAB ON GRADE R-VALUE | 5, 4"  |

ABBREVIATIONS

|     |                        |             |                       |                  |                |            |                                    |          |                                |                       |                          |            |                           |          |                                   |                      |                  |                      |                        |           |              |           |                     |          |                  |          |                                       |                          |               |             |                                  |               |          |         |                |           |               |           |                        |             |                    |      |          |      |                |      |               |       |                       |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
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| AND | AT                     | ANCHOR BOLT | AC                    | AIR CONDITIONING | ACC            | ACCESSIBLE | ACQ                                | ACOUSTIC | ACT                            | ACOUSTIC CEILING TILE | AD                       | AREA DRAIN | ADJ                       | ADJACENT | AFP                               | ABOVE FINISHED FLOOR | AFG              | ABOVE FINISHED GRADE | AGS                    | AGGREGATE | ALT          | ALTERNATE | ALUM                | ALUMINUM | ANOD             | ANODIZED | APG                                   | ACOUSTICAL PANEL CEILING | APPROX        | APPROXIMATE | ARCH                             | ARCHITECTURAL | ASPH     | ASPHALT | ATTN           | ATTENTION | AUTO          | AUTOMATIC | AV                     | AUDIOVISUAL |                    |      |          |      |                |      |               |       |                       |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| BD  | BOARD                  | BIT         | BITUMINOUS            | BLDG             | BUILDING       | BLK        | BLOCK                              | BLKS     | BLIND                          | BM                    | BEAM                     | BO         | BOTTOM OF                 | BOT      | BOTTOM                            | BRG                  | BEARING          | BRK                  | BRICK                  | BRSK      | BRACKET      | BSMT      | BASEMENT            |          |                  |          |                                       |                          |               |             |                                  |               |          |         |                |           |               |           |                        |             |                    |      |          |      |                |      |               |       |                       |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| C   | CHANNEL                | CAB         | CABINET               | CAT              | CATEGORICAL    | CB         | CATCH BASIN                        | CB       | CEMENT BOARD                   | CBU                   | CEMENTITIOUS BACKER UNIT | CC         | CENTER TO CENTER          | CCTV     | CLOSED CIRCUIT TELEVISION         | CEM                  | CEMENT           | CER                  | CERAMIC                | CG        | CORNER GUARD | CHI       | CHILLER             | CH       | CAST IRON        | CF       | CAST-IN-PLACE                         | CJ                       | CONTROL JOINT | CL          | CENTERLINE                       | CLG           | CEILING  | CLR     | CLEAR          | CNTR      | COUNTER       | CO        | CLEANOUT               | CO          | COLUMN             | CONC | CONCRETE | COND | CONDITION      | CONN | CONNECTION    | CONST | CONSTRUCTION          | CONT | CONTINUOUS | CONTR | CONTRACTOR             | COORD | COORDINATE    | CORR | CORRIDOR  | CPT   | CARPET           | CT   | CERAMIC TILE | CTR  | CENTER   | CTK   | COUNTERSINK            | CK    | COLD WATER       |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| (D) | DEMOLISH OR DEMOLITION | D           | DEEP                  | DEB              | DEPTH          | DEG        | DEGREE                             | DEMO     | DEMOLISH OR DEMOLITION         | DEPT                  | DEPARTMENT               | DF         | DRINKING FOUNTAIN         | DIA      | DIAMETER                          | DIF                  | DIFFUSER         | DM                   | DIMENSION              | DMS       | DIMENSIONS   | DISP      | DISPENSER           | DIV      | DIVISION         | DMFF     | DAMP PROOFING                         | DN                       | DOWN          | MFR         | MANUFACTURER                     | MH            | MAN HOLE | MINI    | MINIMUM        | MISC      | MISCELLANEOUS | MO        | MASONRY OPENING        | MR          | MOISTURE RESISTANT | MTD  | MOUNTED  | MTG  | MOUNTING       | MTL  | METAL         | MILL  | MILLION               |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| (E) | EXISTING               | CMU         | CONCRETE MASONRY UNIT | E                | EAST           | EA         | EACH                               | EB       | EXPANSION BOLT                 | EJ                    | EXPANSION JOINT          | EL         | ELEVATION                 | ELEC     | ELECTRICAL                        | ELEV                 | ELEVATOR         | EMER                 | EMERGENCY              | ENCL      | ENCLOSURE    | ENG       | ENGINEER            | EP       | ELECTRICAL PANEL | EPDM     | ETHYLENE PROPYLENE DIENE M-CLASS      | EG                       | EQUAL         | EQUIP       | EQUIPMENT                        | EXH           | EXHAUST  | EXIST   | EXISTING       | EXP       | EXPANSION     | EXT       | EXTERIOR               |             |                    |      |          |      |                |      |               |       |                       |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| FA  | FIRE ALARM             | FB          | FACE BRICK            | FD               | FLOOR DRAIN    | FD         | FLOOR DRAIN OR FIRE DEPARTMENTMENT | FDG      | FIRE DEPARTMENTMENT CONNECTION | FE                    | FIRE EXTINGUISHER        | FE         | FIRE EXTINGUISHER CABINET | FFIE     | FURNITURE, FIXTURES AND EQUIPMENT | FFB                  | FLUSH FLOOR BOX  | FFEL                 | FINISH FLOOR ELEVATION | FH        | FLAT HEAD    | FHC       | FIRE HOSE CABINET   | FN       | FINISH           | FIXT     | FIXTURE                               | FLASH                    | FLASHING      | FLR         | FLOOR                            |               |          |         |                |           |               |           |                        |             |                    |      |          |      |                |      |               |       |                       |      |            |       |                        |       |               |      |           |       |                  |      |              |      |          |       |                        |       |                  |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| (N) | NEW                    | N           | NORTH                 | NA               | NOT APPLICABLE | NC         | NOISE CRITERIA                     | NC       | NOT IN CONTRACT                | NO                    | NUMBER                   | NOM        | NOMINAL                   | NON      | NON-COMBUSTIBLE                   | NTS                  | NOT TO SCALE     | OA                   | OUTSIDE AIR            | OC        | ON CENTER    | OD        | OUTSIDE DIAMETER    | OD       | OVERFLO/DRAIN    | OFI      | OWNER FURNISHED, CONTRACTOR INSTALLED | OFF                      | OFFICE        | OFI         | OWNER FURNISHED, OWNER INSTALLED | OH            | OVERHEAD | OPG     | OPENING        | OPP       | OPPOSITE      | ORD       | OVERFLOW ROOF DRAIN    | P           | PAINT              | PAV  | PAVING   | PBD  | PARTICLE BOARD | PRES | PREFABRICATED | PF    | POWER DRIVEN FASTENER | PERF | PERFORATED | PERM  | PERIMETER              | PERP  | PERPENDICULAR | PI   | PLATE     | PLAM  | PLASTIC LAMINATE | PLAS | PLASTER      | PLB  | PLUMBING | PLF   | POUNDS PER LINEAR FOOT | PLYWD | PLYWOOD          |    |      |    |               |     |       |     |         |     |                   |   |       |    |            |     |                       |    |            |       |          |    |             |      |         |    |                  |    |                |     |        |     |       |     |         |    |             |      |                 |    |           |      |                            |     |                      |      |         |    |        |    |                 |    |              |     |         |     |                                |     |       |     |         |      |          |        |                         |        |             |      |           |     |             |     |        |   |       |     |                |     |                   |    |           |     |                   |      |           |      |             |      |           |     |           |      |         |     |            |     |        |      |          |     |        |     |             |     |                 |     |              |    |            |    |            |     |         |
| PNL | PANEL                  | PNT         | PAINT OR PAINTED      | POL              | POLISHED       | FR         | FIRE PROTECTION                    | FRFAB    | FIRE FABRICATED                | FRPJ                  | FIRE RESISTANT PROJECT   | PSF        | POUNDS PER SQUARE FOOT    | PT       | POINT                             | PT                   | PRESSURE TREATED | PTD                  | PAINTED                | FTN       | FINISH       | PVC       | POLY-VINYL CHLORIDE | QT       | QUARRY TILE      | QTY      | QUANTITY                              | R                        | RADIUS/RISER  | RA          | GRAB BAR                         | RAD           | RADIUS   | RB      | RESILIENT BASE | RSR       | RUBBER        | RCP       | REFLECTED CEILING PLAN | RD          | ROOF DRAIN         | REG  | RECESSED | RECT | RECTANGLE      | REF  | REFERENCE     | RFR   | REFRIGERATOR          | REG  | REGISTER   | REIN  | REINFORCED REINFORCING | REL   | RELOCATE      | REM  | REMOVABLE | RECOM | RECOMMENDED      | REQ  | REQUIRED     | REQD | REQUIRED | RESIL | RESILIENT              | REV   | REVISION/REVISED | RM | ROOM | RO | ROUGH OPENING | RTD | RATED | RTR | RATINGS | RWL | RAIN WATER LEADER | S | SOUTH | SA | SUPPLY AIR | SAF | SELF ADHERED FLASHING | SG | SOLID GORE | SCHED | SCHEDULE | SD | STORM DRAIN | SECT | SECTION | SF | SQUARE FEET/FOOT | SH | SPRINKLER HEAD | SHR | SHOWER | SHT | SHEET | SIM | SIMILAR | SM | SHEET METAL | SMPT | SURFACE MOUNTED | SP | STANDPIPE | SPEC | SPECIFIED OR SPECIFICATION | SPK | SPRINKLER OR SPEAKER | SPKR | SPEAKER | SQ | SQUARE | SS | STAINLESS STEEL | SK | SERVICE SINK | STA | STATION | STC | SOUND TRANSMISSION COEFFICIENT | STL | STEEL | STR | STORAGE | STRG | STRINGER | STRUCT | STRUCTURE OR STRUCTURAL | SUBCAT | SUBCATEGORY | SUSP | SUSPENDED | SYM | SYMMETRICAL | SYS | SYSTEM | T | TREAD | T&B | TOP AND BOTTOM | T&G | TONGUE AND GROOVE | TB | TOWEL BAR | TEL | TELEPHONE/TELECOM | TELE | TELEPHONE | TEMP | TEMPERATURE | TEMP | TEMPORARY | THK | THICKNESS | THRU | THROUGH | T&B | TACK BOARD | TLT | TOILET | THRP | THROPPED | TOP | TOP OF | TOB | TOP OF BEAM | TOC | TOP OF CONCRETE | TOS | TOP OF STEEL | TS | TUBE STEEL | TV | TELEVISION | TYP | TYPICAL |

ABBREVIATIONS

|       |             |     |            |         |                 |            |                           |                        |           |         |           |         |                      |                       |          |            |           |                    |            |                                 |      |            |                   |       |         |         |  |          |           |        |                 |                               |             |                        |              |                 |                    |        |             |       |            |       |                         |     |          |        |              |     |           |     |         |    |                  |     |       |       |     |       |    |         |    |           |     |          |     |          |    |       |    |                     |     |                   |    |       |     |         |     |         |      |            |     |        |       |          |    |      |     |              |    |          |      |         |      |               |    |                 |    |                    |     |         |     |          |     |       |      |         |     |              |     |          |     |                        |      |          |      |           |     |                 |     |              |    |                |    |            |     |                     |   |          |    |         |    |              |    |      |    |        |    |           |    |                            |     |                     |    |                   |     |          |    |        |    |             |     |                    |     |                  |
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| FLUOR | FLUORESCENT | FND | FOUNDATION | FACE OF | FIRE PROTECTION | ACOUSTICAL | FIBER REINFORCED CONCRETE | FIRE RETARDANT TREATED | FEET/FOOT | FOOTING | FURNITURE | FURRING | FABRIC WALL COVERING | FABRIC UNRAFFED PANEL | GAUZE    | GALVANIZED | GRAB BAR  | GENERAL CONTRACTOR | GENERAL    | GLASS FIBER REINFORCED CONCRETE | GALV | GALVANIZED | GL                | GLASS | GLAZ    | GLAZING | GRAN                                     | GRANULAR | GRD       | GROUND | GRFG            | GLASS FIBER REINFORCED GYPSUM | GSM         | GALVANIZED SHEET METAL | SH VALVE     | GYPM WALL BOARD | GYF                | GYPSUM |             |       |            |       |                         |     |          |        |              |     |           |     |         |    |                  |     |       |       |     |       |    |         |    |           |     |          |     |          |    |       |    |                     |     |                   |    |       |     |         |     |         |      |            |     |        |       |          |    |      |     |              |    |          |      |         |      |               |    |                 |    |                    |     |         |     |          |     |       |      |         |     |              |     |          |     |                        |      |          |      |           |     |                 |     |              |    |                |    |            |     |                     |   |          |    |         |    |              |    |      |    |        |    |           |    |                            |     |                     |    |                   |     |          |    |        |    |             |     |                    |     |                  |
| H     | HIGH/HEIGHT | HB  | HOSE BIBB  | HC      | HANDICAPPED     | HDWD       | HARDWOOD                  | HDVYR                  | HARDWARE  | HST     | HEIGHT    | HM      | HOLLOW METAL         | HNDRL                 | HANDRAIL | HO         | HOLD OPEN | HORIZ              | HORIZONTAL | HR                              | HOUR | HRG        | HOSE REEL CABINET | HTG   | HEATING | HVAC    | HEATING VENTILATION AND AIR CONDITIONING | HX       | HOT WATER | ID     | INSIDE DIAMETER | IN                            | INCH/INCHES | INCDND                 | INCANDESCENT | INCL            | INCLUDED/INCLUDING | INFO   | INFORMATION | INSUL | INSULATION | INSUL | INSULATED OR INSULATION | INT | INTERIOR | INTERM | INTERMEDIATE | INV | INTERVENE | JAN | JANITOR | JC | JANITOR'S CLOSET | JST | JOIST | JOINT | JNT | JOINT | CK | KITCHEN | KO | KNOCK OUT | LAM | LAMINATE | LAV | LAVATORY | LB | LOADS | LW | LONG LEG HORIZONTAL | LLV | LONG LEG VERTICAL | LT | LIGHT | MAS | MASONRY | MAX | MAXIMUM | MECH | MECHANICAL | MED | MEDIUM | MEMBR | MEMBRANE | DN | DOWN | MFR | MANUFACTURER | MH | MAN HOLE | MINI | MINIMUM | MISC | MISCELLANEOUS | MO | MASONRY OPENING | MR | MOISTURE RESISTANT | MTD | MOUNTED | MTG | MOUNTING | MTL | METAL | MILL | MILLION | NTS | NOT TO SCALE | VAR | VARIABLE | VGT | VINYL COMPOSITION TILE | VERT | VERTICAL | VEST | VESTIBULE | VIF | VERIFY IN FIELD | VIF | VISION PANEL | VR | VAPOR RETARDER | VT | VINYL TILE | VVC | VINYL WALL COVERING | N | N/E/WEST | NW | WITHOUT | WC | WATER CLOSET | WD | WOOD | WN | WINDOW | WM | WIRE MESH | WP | WATERPROOF/WATERPROOF FINS | WPM | WATERPROOF MEMBRANE | WS | WEATHER-STRIPPING | WBC | WAINSCOT | WT | WEIGHT | WV | WATER VALVE | WVF | WELDED WIRE FABRIC | WVM | WELDED WIRE MESH |

ABBREVIATIONS

|       |             |     |            |         |                 |            |                           |                        |           |         |           |         |                      |                       |          |            |           |                    |            |                                 |      |            |                   |       |         |         |  |          |           |        |                 |                               |             |                        |              |                 |                    |        |             |       |            |       |                         |     |          |        |              |     |           |     |         |    |                  |     |       |       |     |       |    |         |    |           |     |          |     |          |    |       |    |                     |     |                   |    |       |     |         |     |         |      |            |     |        |       |          |    |      |     |              |    |          |      |         |      |               |    |                 |    |                    |     |         |     |          |     |       |      |         |     |              |     |          |     |                        |      |          |      |           |     |                 |     |              |    |                |    |            |     |                     |   |          |    |         |    |              |    |      |    |        |    |           |    |                            |     |                     |    |                   |     |          |    |        |    |             |     |                    |     |                  |
|-------|-------------|-----|------------|---------|-----------------|------------|---------------------------|------------------------|-----------|---------|-----------|---------|----------------------|-----------------------|----------|------------|-----------|--------------------|------------|---------------------------------|------|------------|-------------------|-------|---------|---------|--|----------|-----------|--------|-----------------|-------------------------------|-------------|------------------------|--------------|-----------------|--------------------|--------|-------------|-------|------------|-------|-------------------------|-----|----------|--------|--------------|-----|-----------|-----|---------|----|------------------|-----|-------|-------|-----|-------|----|---------|----|-----------|-----|----------|-----|----------|----|-------|----|---------------------|-----|-------------------|----|-------|-----|---------|-----|---------|------|------------|-----|--------|-------|----------|----|------|-----|--------------|----|----------|------|---------|------|---------------|----|-----------------|----|--------------------|-----|---------|-----|----------|-----|-------|------|---------|-----|--------------|-----|----------|-----|------------------------|------|----------|------|-----------|-----|-----------------|-----|--------------|----|----------------|----|------------|-----|---------------------|---|----------|----|---------|----|--------------|----|------|----|--------|----|-----------|----|----------------------------|-----|---------------------|----|-------------------|-----|----------|----|--------|----|-------------|-----|--------------------|-----|------------------|
| FLUOR | FLUORESCENT | FND | FOUNDATION | FACE OF | FIRE PROTECTION | ACOUSTICAL | FIBER REINFORCED CONCRETE | FIRE RETARDANT TREATED | FEET/FOOT | FOOTING | FURNITURE | FURRING | FABRIC WALL COVERING | FABRIC UNRAFFED PANEL | GAUZE    | GALVANIZED | GRAB BAR  | GENERAL CONTRACTOR | GENERAL    | GLASS FIBER REINFORCED CONCRETE | GALV | GALVANIZED | GL                | GLASS | GLAZ    | GLAZING | GRAN                                     | GRANULAR | GRD       | GROUND | GRFG            | GLASS FIBER REINFORCED GYPSUM | GSM         | GALVANIZED SHEET METAL | SH VALVE     | GYPM WALL BOARD | GYF                | GYPSUM |             |       |            |       |                         |     |          |        |              |     |           |     |         |    |                  |     |       |       |     |       |    |         |    |           |     |          |     |          |    |       |    |                     |     |                   |    |       |     |         |     |         |      |            |     |        |       |          |    |      |     |              |    |          |      |         |      |               |    |                 |    |                    |     |         |     |          |     |       |      |         |     |              |     |          |     |                        |      |          |      |           |     |                 |     |              |    |                |    |            |     |                     |   |          |    |         |    |              |    |      |    |        |    |           |    |                            |     |                     |    |                   |     |          |    |        |    |             |     |                    |     |                  |
| H     | HIGH/HEIGHT | HB  | HOSE BIBB  | HC      | HANDICAPPED     | HDWD       | HARDWOOD                  | HDVYR                  | HARDWARE  | HST     | HEIGHT    | HM      | HOLLOW METAL         | HNDRL                 | HANDRAIL | HO         | HOLD OPEN | HORIZ              | HORIZONTAL | HR                              | HOUR | HRG        | HOSE REEL CABINET | HTG   | HEATING | HVAC    | HEATING VENTILATION AND AIR CONDITIONING | HX       | HOT WATER | ID     | INSIDE DIAMETER | IN                            | INCH/INCHES | INCDND                 | INCANDESCENT | INCL            | INCLUDED/INCLUDING | INFO   | INFORMATION | INSUL | INSULATION | INSUL | INSULATED OR INSULATION | INT | INTERIOR | INTERM | INTERMEDIATE | INV | INTERVENE | JAN | JANITOR | JC | JANITOR'S CLOSET | JST | JOIST | JOINT | JNT | JOINT | CK | KITCHEN | KO | KNOCK OUT | LAM | LAMINATE | LAV | LAVATORY | LB | LOADS | LW | LONG LEG HORIZONTAL | LLV | LONG LEG VERTICAL | LT | LIGHT | MAS | MASONRY | MAX | MAXIMUM | MECH | MECHANICAL | MED | MEDIUM | MEMBR | MEMBRANE | DN | DOWN | MFR | MANUFACTURER | MH | MAN HOLE | MINI | MINIMUM | MISC | MISCELLANEOUS | MO | MASONRY OPENING | MR | MOISTURE RESISTANT | MTD | MOUNTED | MTG | MOUNTING | MTL | METAL | MILL | MILLION | NTS | NOT TO SCALE | VAR | VARIABLE | VGT | VINYL COMPOSITION TILE | VERT | VERTICAL | VEST | VESTIBULE | VIF | VERIFY IN FIELD | VIF | VISION PANEL | VR | VAPOR RETARDER | VT | VINYL TILE | VVC | VINYL WALL COVERING | N | N/E/WEST | NW | WITHOUT | WC | WATER CLOSET | WD | WOOD | WN | WINDOW | WM | WIRE MESH | WP | WATERPROOF/WATERPROOF FINS | WPM | WATERPROOF MEMBRANE | WS | WEATHER-STRIPPING | WBC | WAINSCOT | WT | WEIGHT | WV | WATER VALVE | WVF | WELDED WIRE FABRIC | WVM | WELDED WIRE MESH |

PROJECT DIRECTORY

**Project Address**  
832 Main Street  
Minturn, CO 81645  
Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

**Owner**  
Pamela & Craig Jones  
265 Willow Glen Court  
Boulder, CO 80302  
Gypsum, CO 81631  
craiga@TEFBoulder.com

**Architect**  
mpp design shop, inc.  
Michael Dukas  
PO Box 288  
Gypsum, CO 81631  
(970) 343-2766  
michael@mppdesignshop.com

**General Contractor**  
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Chris Kintner  
PO Box 352  
Eagle, CO 81631  
(970) 343-2766  
chris@k2m.com

**Structural Engineer**  
Sundquist Design Group  
Joe Sundquist  
PO Box 676  
Conifer, CO 80433  
(303) 941-7651  
joe@sundquistdesign.com

**Land Surveyor**  
Marcin Engineering, LLC  
Antony Vardoulakis  
101 Eagle Road, #5  
PO Box 1062  
Avon, CO 81620  
(970) 748-0274  
tony@marcinengineering.com

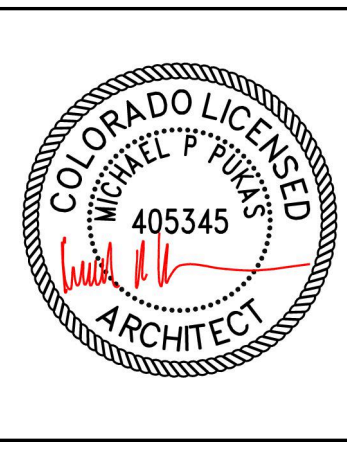
**SHEET INDEX**

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ENERGY CODE COMPLIANCE NOTES

- ENERGY CODE REQUIREMENTS TO BE MET USING THE PRESCRIPTIVE METHOD AS OUTLINED IN THE 2018 INTERNATIONAL BUILDING CODE, THE 2018 INTERNATIONAL RESIDENTIAL CODE AND THE 2018 INTERNATIONAL ENERGY CONSERVATION CODE.
- IRC TABLE N1102.1.2 (R402.1.2):
  - ALL EXTERIOR WINDOWS AND DOORS TO HAVE A U-FACTOR OF <= 0.30
  - UNVENTED CEILING TO RECEIVE 3" CONTINUOUS XPS RIGID FOAM EXTERIOR INSULATION, PLUS 3" R-21 SPFRAY FOAM CAVITY INSULATION @ UNDERSIDE OF ROOF SHEATHING, PLUS R-19 UNFACED BATT CAVITY INSULATION
  - ALL EXTERIOR WALLS TO BE 2x6 @ 16" O.C. WOOD FRAMED WITH R-5 MIN CONTINUOUS EXTERIOR INSULATION, PLUS 1" R-7 SPFRAY FOAM CAVITY INSULATION, PLUS R-13 BATT CAVITY INSULATION
  - ALL CONCRETE WALLS ADJACENT TO CONDITIONED INTERIOR LIVING SPACE TO RECEIVE 2x4 @ 16" O.C. FURRED WALL WITH 1" MIN AIR SPACE, R-19 MIN CAVITY BATT INSULATION
  - FLOORS OVER UNCONDITIONED CRANL SPACES TO RECEIVE R-30 UNFACED BATT CAVITY INSULATION
  - ALL CRANL SPACE WALLS TO RECEIVE R-15 CONTINUOUS BLANKET INSULATION FROM TOP OF FOOTING TO UNDERSIDE OF FLOOR FRAMING
- IRC 402.4.1.1: ALL AIR BARRIER AND THERMAL BARRIERS TO BE INSTALLED PER MANUFACTURER'S INSTRUCTIONS
- IRC 402.4.5: ALL IC-RATED RECESSED LIGHTING FIXTURES TO BE SEALED AT HOUSING/INTERIOR FINISH AND LABELED TO INDICATE <=2.0 CFM LEAKAGE AT 75 Pa.
- IRC 403: AUTOMATIC OR GRAVITY DAMPERS ARE INSTALLED ON ALL OUTDOORS AIR INTAKES AND EXHAUSTS
- IRC 402.4.1.2: BLOWER DOOR TEST @ 50 Pa. <=5 ACH IN CLIMATE ZONE 1
- IRC 303.2: CONDITIONED BASEMENT WALL INSULATION INSTALLED PER MANUFACTURER'S INSTRUCTIONS
- IRC 303.2: UNVENTED CRANL SPACE WALL INSULATION INSTALLED PER MANUFACTURER'S INSTRUCTIONS
- IRC 303.2: WALL INSULATION TO BE INSTALLED PER MANUFACTURER'S INSTRUCTION
- IRC 303.1.1, 303.2: CEILING INSULATION INSTALLED PER MANUFACTURER'S INSTRUCTIONS, BLOWN INSULATION MARKED EVERY 300 FT2
  - UNVENTED CEILING TO RECEIVE 4" R-28 SPFRAY FOAM CAVITY INSULATION PLUS R-25 UNFACED BATT CAVITY INSULATION
  - VENTED CEILING (AT TRUSS ROOF LOCATION ONLY) TO RECEIVE R-49 BATT INSULATION w/ VAPOR RETARDER AT WARM SIDE



Jones Residence  
832 Main Street  
Minturn, CO 81645  
Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 05/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |



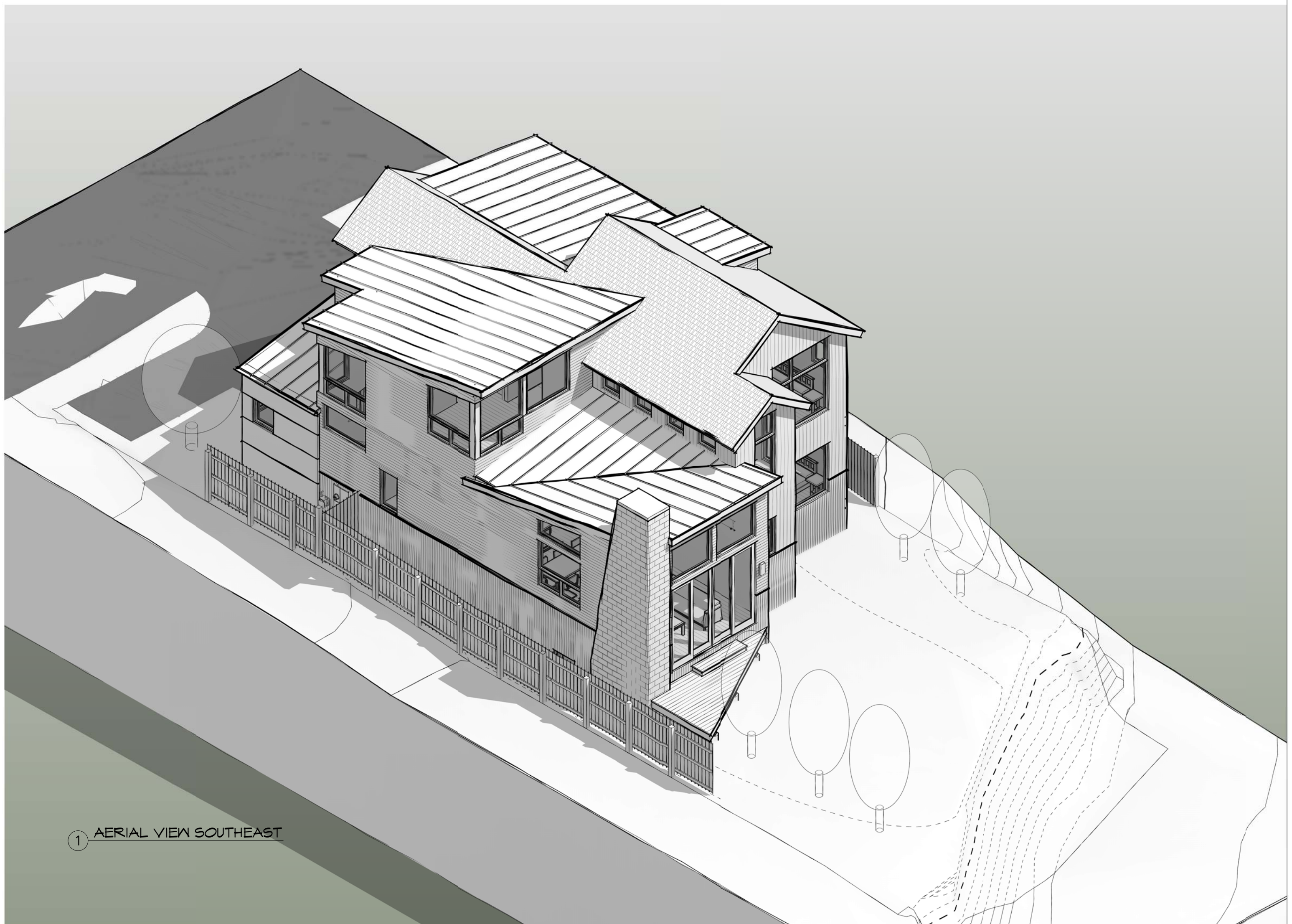
4 AERIAL VIEW NORTHWEST



2 AERIAL VIEW NORTHEAST



3 AERIAL VIEW SOUTHWEST



1 AERIAL VIEW SOUTHEAST

mpp design shop, inc.  
 PO Box 288  
 Gypsum, CO 81637  
 (970) 390-4931  
 michael@mppdesignshop.com

Jones Residence  
 832 Main Street  
 Minturn, CO 81645  
 Parcel Number: 2103-351-02-003  
 South Minturn Addition, Lot 35

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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
|   |                 |          |
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Project Number: 2005  
 Designed by: mpp  
 Drawn by: mpp  
 Checked by: mpp

AERIAL VIEWS



④ PERSPECTIVE NORTHWEST



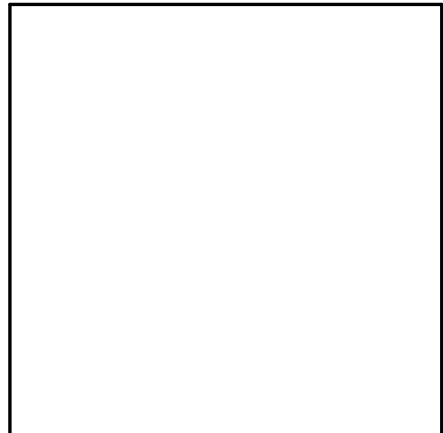
② PERSPECTIVE NORTHEAST



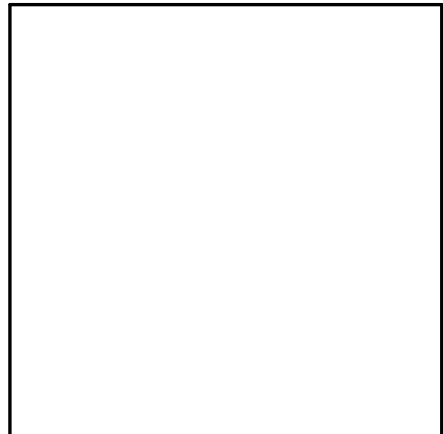
③ PERSPECTIVE SOUTHWEST



① PERSPECTIVE SOUTHEAST



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 michael@mppdesignshop.com



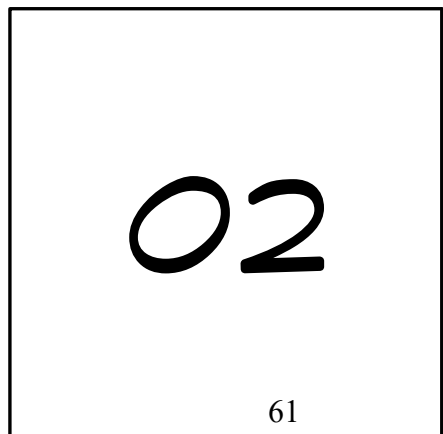
Jones Residence  
 832 Main Street  
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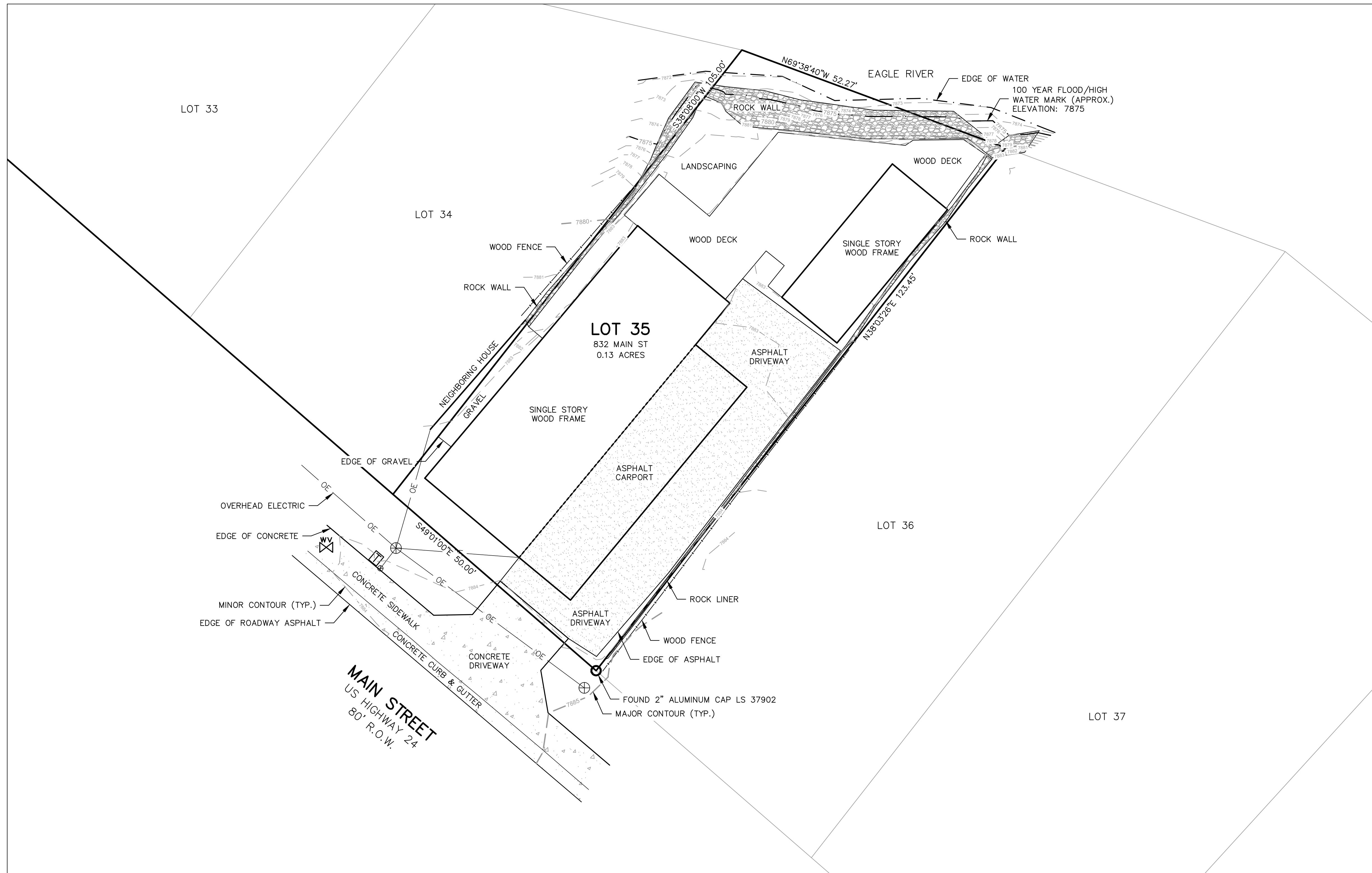
Project Number: 2005  
 Designed by: mpp  
 Drawn by: mpp  
 Checked by: mpp

PERSPECTIVE VIEWS



# EXISTING CONDITIONS SITE MAP LOT 35, SOUTH MINTURN ADDITION

Section 35, Township 5S , Range 81W of the 6th Principal Meridian  
County of Eagle, State of Colorado



### LEGAL DESCRIPTION

Lot 35, South Minturn Addition to the Town of Minturn, recorded on March 1, 1978 at Reception No. 163774 at the Office of the Clerk and Recorder, County of Eagle, State of Colorado.

### NOTES

- 1) Survey Date: September 22, 2020.
- 2) Contour interval: 1-foot.
- 3) Elevation datum: Marcin Engineering control point #58, CDOT Minturn Resurfacing Project. Elevation = 7894.52.
- 4) This Survey does not constitute a boundary survey nor any investigation into record easements or encumbrances associated with this property. No Title Commitment was supplied to the Surveyor.
- 5) Utilities are shown approximately and should be field verified prior to excavation.
- 6) Marcin Engineering LLC does not warrant or certify to the integrity of any Digital Data supplied in conjunction with this map and survey.
- 7) STREET ADDRESS: 832 Main Street, Minturn, CO 81645.
- 8) Parcel Boundary was based on the found 2" aluminum cap at the southern property corner of Lot 35.
- 9) This is not a monumented survey, Land Survey Plat, or Improvement Survey plat. No boundary resolution was performed in making this survey. All lot lines, setback lines, and easement lines shown hereon should be considered approximate and should not be relied upon for the placement of any future improvements.
- 10) No Title Commitment was performed for this survey.
- 11) High water mark: No high water mark was visible. Assume 100-year flood elevation is equal to high water mark. High water flood elevation of 7875 feet was obtained from Eagle County FEMA information collaboration on Eagle County GIS website.

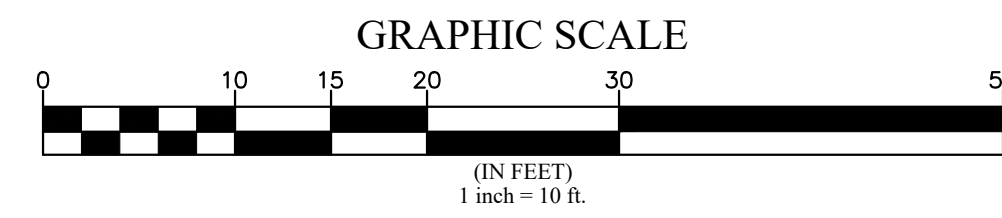
### SURVEYOR'S CERTIFICATION

I, Raymond M. Elko, a Professional Land Surveyor in the State of Colorado, hereby certify that this Existing Conditions Site Map of 832 MAIN STREET was done by me or under my direct supervision and that both are accurate to the best of my knowledge.

Raymond M. Elko, PLS 38444  
Colorado Professional  
Land Surveyor



| LEGEND    |                    |
|-----------|--------------------|
| [Pattern] | CONCRETE           |
| [Pattern] | ASPHALT            |
| [Pattern] | ROCK               |
| [Symbol]  | FOUND ALUMINUM CAP |
| [Symbol]  | UTILITY POLE       |
| [Symbol]  | WATER VALVE        |
| [Symbol]  | TELEPHONE PEDESTAL |



| TOPOGRAPHIC MAP<br>LOT 35, SOUTH MINTURN ADDITION<br>Town of Minturn, County of Eagle,<br>State of Colorado |                   |
|---|-------------------|
| DRAWN BY: TLV   | DATE: 9/23/2020   |
| CHECKED BY: RME   | DRAWING NO.: TOPO |
| JOB NO: 20074   | SHEET: 1 OF 1     |

## MARCIN ENGINEERING LLC

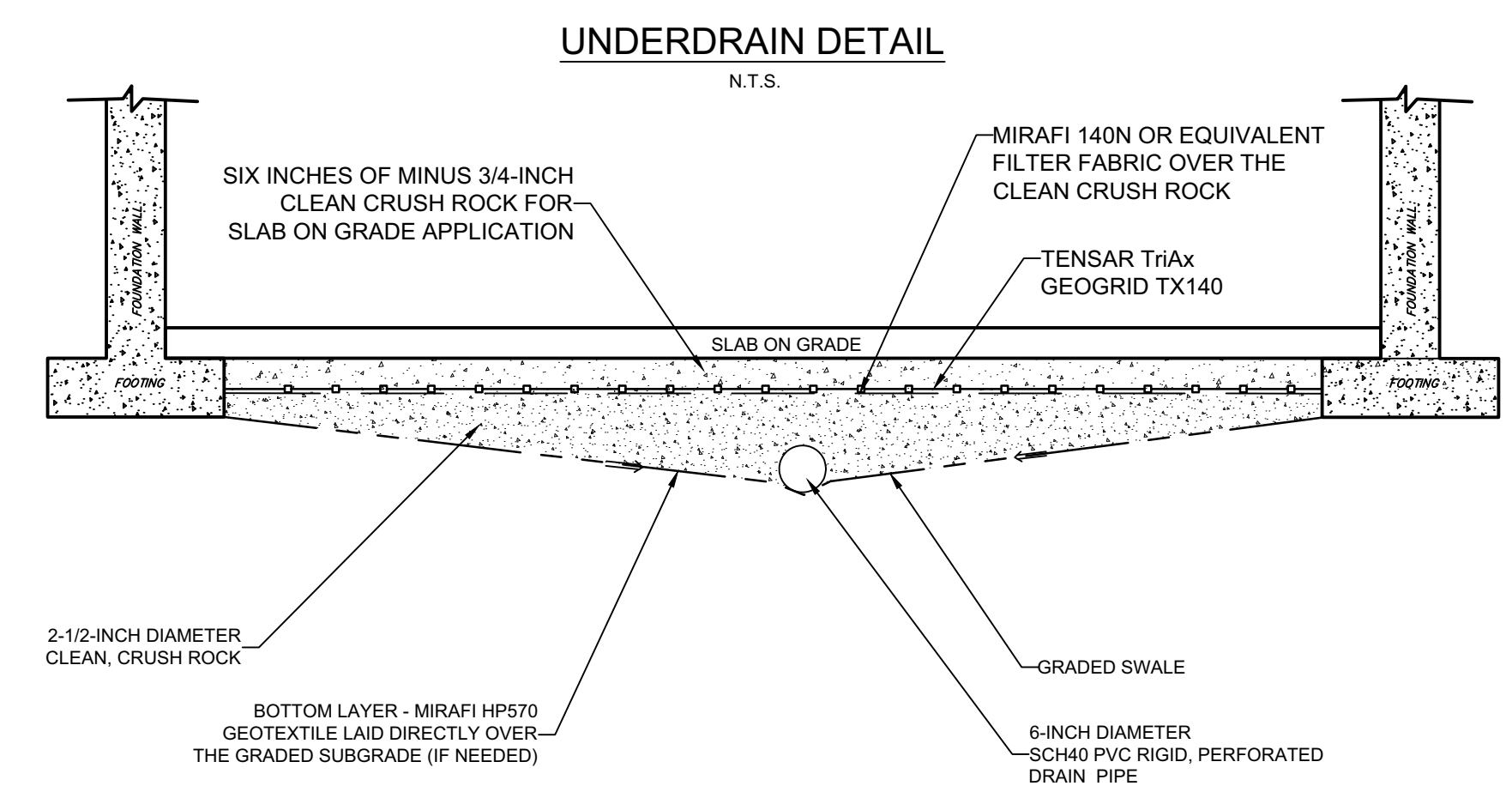
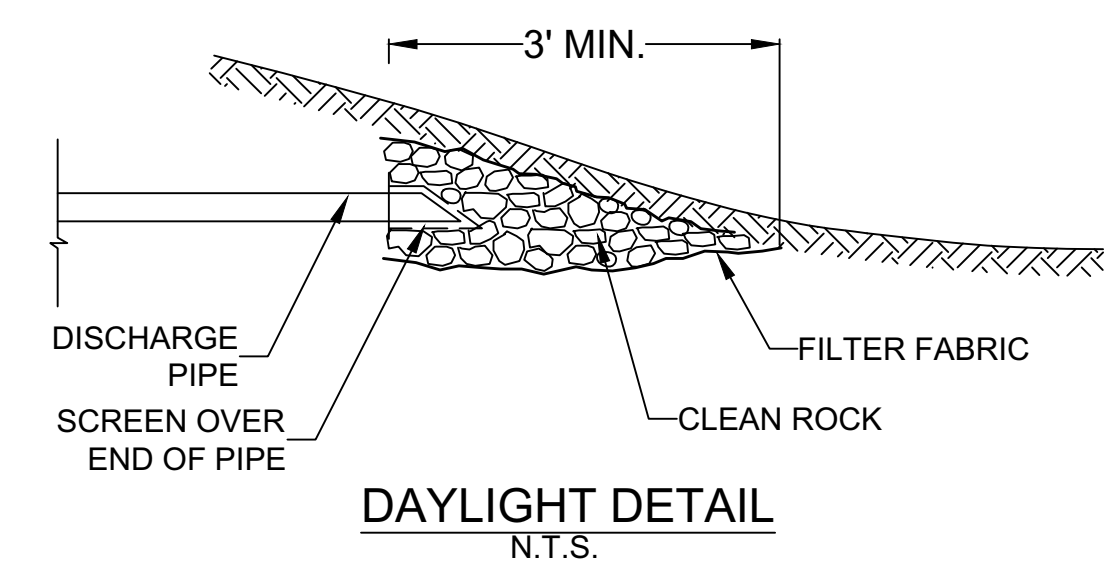
101 EAGLE RD, #5  
P.O. BOX 1062  
AVON, CO 81620  
(970) 748-0274



NOTICE: According to Colorado law you MUST commence any legal action based upon any defect in this survey within three years after you first discovered such defect. In no event, may any action based upon any defect in this survey be commenced more than ten years from the date of certification shown hereon.

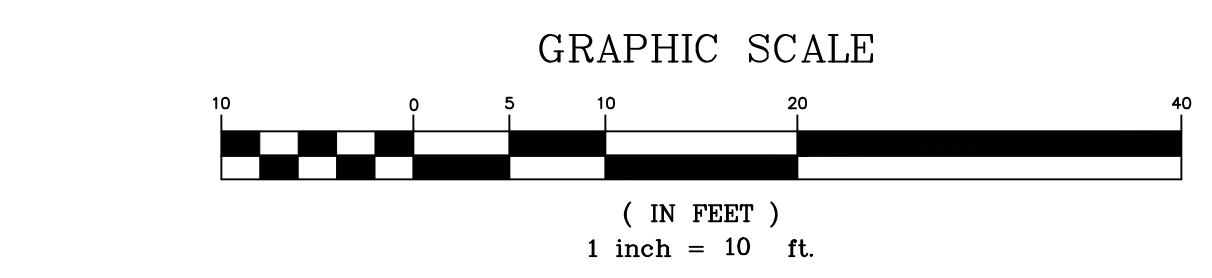
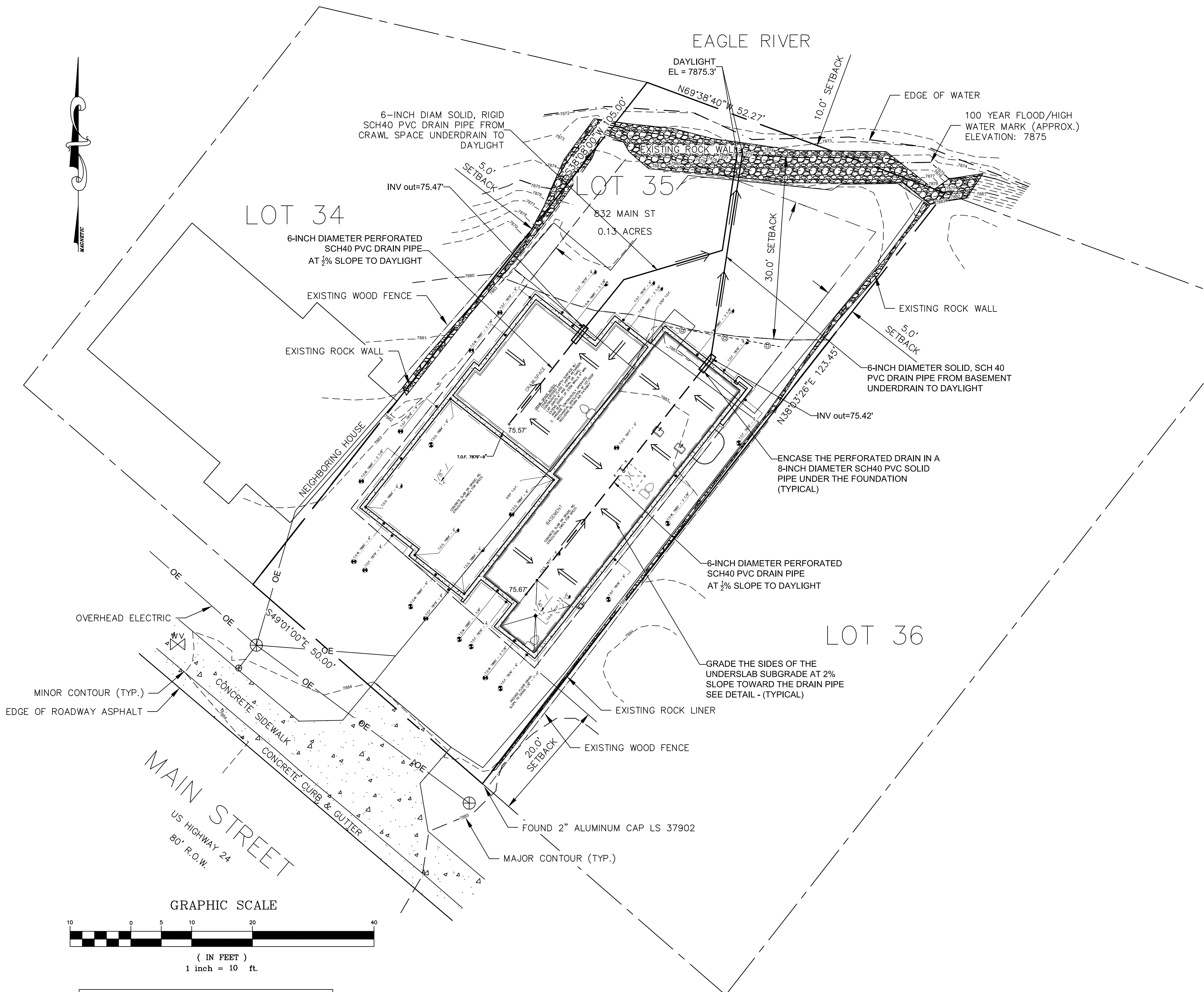
**UNDERDRAIN NOTE:**

- ALL EXTERIOR (PERIMETER) DRAINS WILL START A MINIMUM OF ONE FOOT AWAY (2 FEET IF POSSIBLE) AND ONE FOOT BELOW THE BOTTOM OF FOOTING ELEVATION
- ALL INTERIOR DRAINS WILL START A MINIMUM OF 12 INCHES BELOW THE BOTTOM OF SLAB ELEVATION AND TWO FEET AWAY FROM THE FOOTINGS.



- NOTE:**
- GRADE THE SUBGRADE, INCLUDING THE SWALE FOR THE DRAIN PIPE.
  - CONSTRUCT THE MIRAFI HP570 GEOTEXTILE OVER THE GRADED SUBGRADE (IF NEEDED for very soft or yielding subgrade soils)
  - CONSTRUCT THE UNDERDRAIN PIPE
  - BACKFILL WITH A MINIMUM OF 12-INCHES OF MINUS 2-1/2-INCH CLEAN, CRUSH ROCK.
  - COVER THE CLEAN CRUSH ROCK LAYER WITH MIRAFI 140N FILTER FABRIC OR EQUIVALENT
  - CONSTRUCT A LAYER OF TENSAR Triax GEOGRID OVER THE FILTER FABRIC
  - BACKFILL WITH 6-INCHES OF MINUS 3/4-INCH CLEAN CRUSH ROCK FOR SLAB ON GRADE APPLICATION
  - DO NOT VIBRATE THE FILL, THIS WILL BRING OUT THE MOISTURE.
  - ANY SUBSTITUTIONS MUST BE VERIFIED WITH THE ENGINEER.
- THE GEOGRID AND GEOTEXTILE PLACEMENT WILL BE ACCORDING TO THE MANUFACTURERS RECOMMENDATIONS.

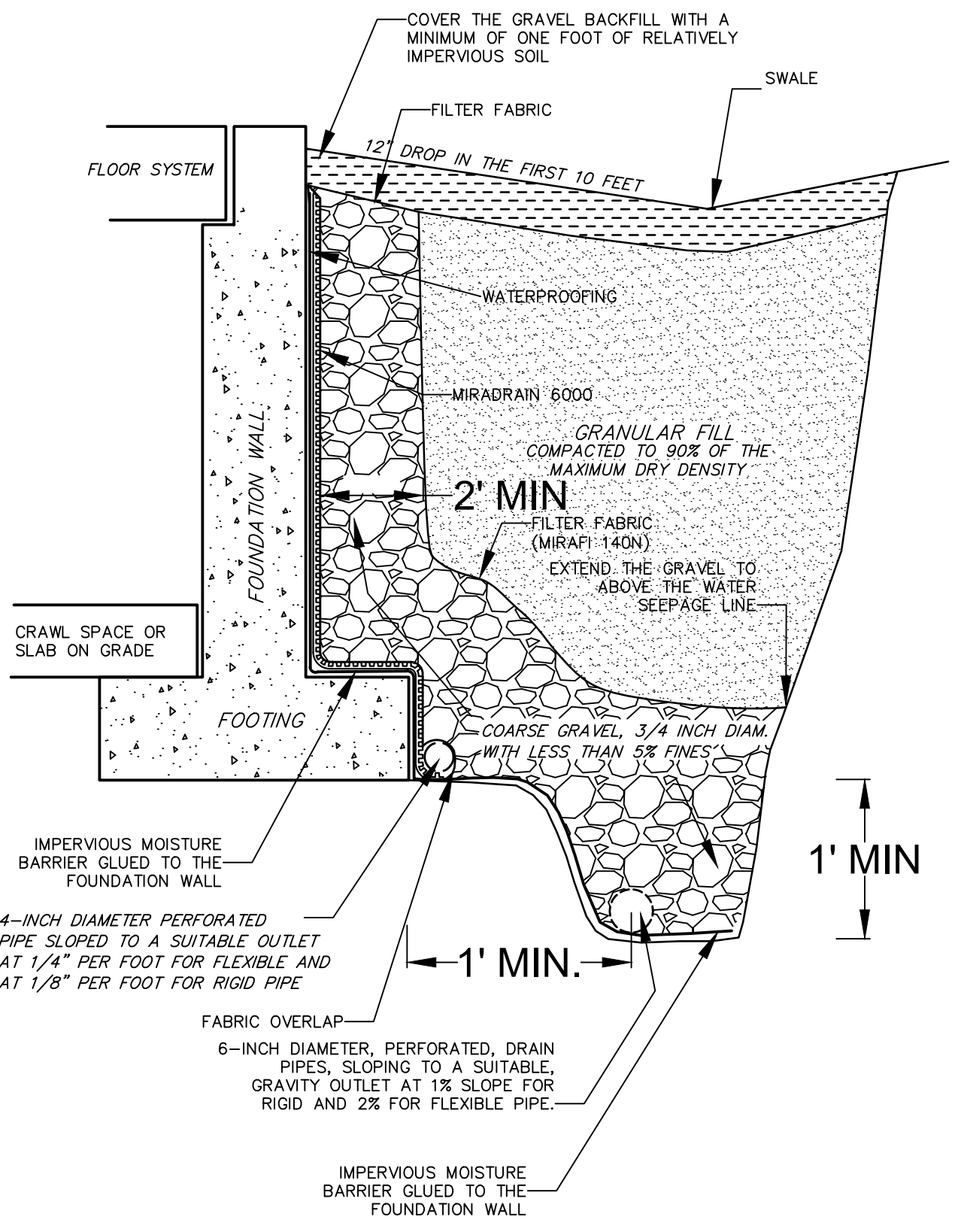
- UNDERDRAIN NOTES:**
- TEMPORARY GROUND WATER DEWATERING MIGHT BE NECESSARY DURING THE CONSTRUCTION OF THE UNDERDRAIN SYSTEM.
  - FOR THE TEMPORARY GROUND WATER DEWATERING, EXCAVATE HOLES OR TRENCHES AS NECESSARY TO CREATE A LOW SPOT. LINE THE HOLES OR TRENCHES WITH FILTER FABRIC AND PLACE CLEAN CRUSH ROCK. PLACE A SUMP PUMP IN THE LOW POINT AREA.
  - ONCE THE EXCAVATION IS RELATIVELY DRY, CONSTRUCT SWALES FOR THE PLACEMENT OF THE COLLECTOR DRAINS IN EACH OF THE SEPARATE FOUNDATION AREAS. LINE THE ENTIRE AREA WITH FILTER FABRIC. CONSTRUCT THE 6-INCH DIAMETER, PERFORATED COLLECTOR DRAINS.
  - BACKFILL THE ENTIRE AREA WITH A MINIMUM OF 12 INCHES OF MINUS 2-1/2-INCH CLEAN CRUSH ROCK. SOME AREAS ON THE DOWNSLOPE OF THE DRAINS WILL REQUIRE MORE GRAVEL.
  - COVER THE GRAVEL WITH FILTER FABRIC.
  - WHERE THE COLLECTOR DRAINS CROSS UNDER FOOTINGS, CHANGE TO SOLID, SCH40 RIGID PIPE.



**NOTE:**

- FOUNDATION AND SITE PLAN BY MICHAEL PUKAS, AIA, MPP DESIGN SHOP, INC.

**PERIMETER DRAIN WITH AN UNDERDRAIN**



| NO. | DATE | DESCRIPTION | BY |
|-----|------|-------------|----|
|     |      |             |    |
|     |      |             |    |
|     |      |             |    |

**UNDERDRAIN DESIGN**

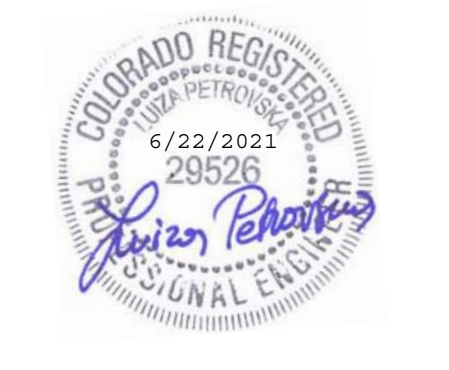
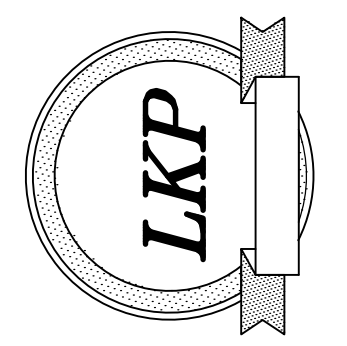
LOT 35, SOUTH MINTURN ADDITION  
832 MAIN STREET  
MINTURN, EAGLE COUNTY, CO

**JONES RESIDENCE**

**CIVIL/GEOTECHNICAL**

**Engineering, Inc.**

P.O. Box 724 Eagle, CO 81631  
Tel (970) 390-0307 www.LKPEngineering.com



|                   |                       |
|-------------------|-----------------------|
| DRAWN BY:         | L.P.                  |
| CHECKED BY:       | L.P.                  |
| PROJECT NO.:      | 21-3494               |
| SEE REVISIONS BOX |                       |
| DATE:             | JUNE 22, 2021         |
| DRAWING NO.:      | 21-3494Underdrain.DWG |
| SHEET             | C-1                   |

**CONSTRUCTION MANAGEMENT NOTES**

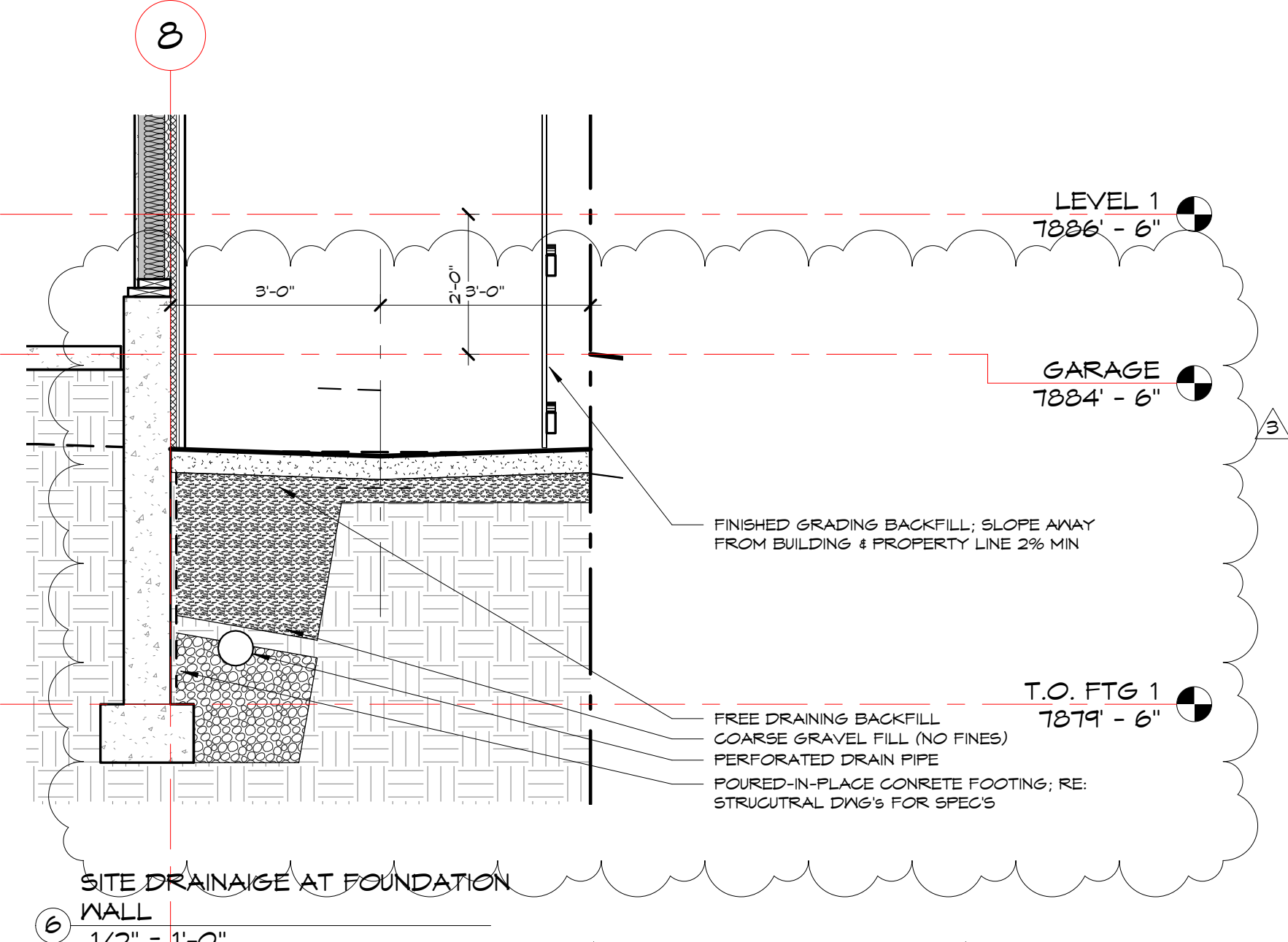
- ALL CONSTRUCTION MATERIALS & EQUIPMENT TO BE STORED ON SITE IN PARKING AND GRADED AREAS OUTSIDE OF 30' HHM SETBACK
- ALL SOIL REMOVED DURING EXCAVATION, THAT WILL NOT BE REQUIRED FOR BACK-FILLING AND ROUGH GRADING, TO BE REMOVED FROM SITE AS REG'D. NO EXCESS SOILS TO BE STORED OUTSIDE OF LIMITS OF DISTURBANCE FENCING.
- FINISH GRADING SOILS TO BE PROCURED OFF-SITE AS REG'D.
- ALL EXCAVATION, SOILS STORAGE, AND ROUGH & FINISHED GRADING TO BE MAINTAINED WITHIN PROPERTY BOUNDARIES AND LIMITS OF DISTURBANCE.
- ALL CONTRACTOR PARKING TO BE ON-SITE IN PARKING AREA, AS MUCH AS POSSIBLE, OR IN-TOWN ON-STREET PARKING ACCORDING TO MUNICIPAL PARKING REGULATIONS

**SITE UTILITY GENERAL NOTES**

1. WATER LINE (DESIGN BUILD BY CONTRACTOR ASSIGNED BY GC):
  - A. EXISTING WATER TO BE EVALUATED AT DEMO TO DETERMINE SIZE AND CONDITION. REPLACE WITH 3/4" LINE AS REG'D.
  - B. 4"-6" MIN BELOW GRADE W/ TRACING WIRE AND MARKING TAPE
  - C. INSTALLED PER ERVSD RULES AND REGULATIONS APPENDIX B
  - D. GC TO FIELD LOCATE
2. SEWER LINE (DESIGN BUILD BY CONTRACTOR ASSIGNED BY GC):
  - A. EXISTING SEWER LINE TO BE REMOVED
  - B. NEW SEWER LINE TO BE 4" MIN.
  - C. 4'-6" MIN BELOW GRADE W/ TRACING WIRE AND MARKING TAPE
  - D. INSTALLED PER ERVSD RULES AND REGULATIONS APPENDIX B
  - E. GC TO FIELD LOCATE
3. GAS LINE (DESIGN BUILD BY CONTRACTOR ASSIGNED BY GC):
  - A. EXISTING GAS LINE TO BE EVALUATED AT DEMO TO DETERMINE CONDITION. REPLACE AS REG'D.
  - B. CONNECT TO PUBLIC SERVICE AT STREET
  - C. GC TO FIELD LOCATE
4. ELECTRIC LINE (DESIGN BUILD BY CONTRACTOR ASSIGNED BY GC):
  - A. EXISTING OVERHEAD LINES TO BE REMOVED
  - B. NEW ELECTRIC LINE TO BE BURIED
  - C. CONNECT TO EXISTING PUBLIC SERVICE AT EXISTING UTILITY POLE
  - D. GC TO FIELD LOCATE

**Planting Schedule**

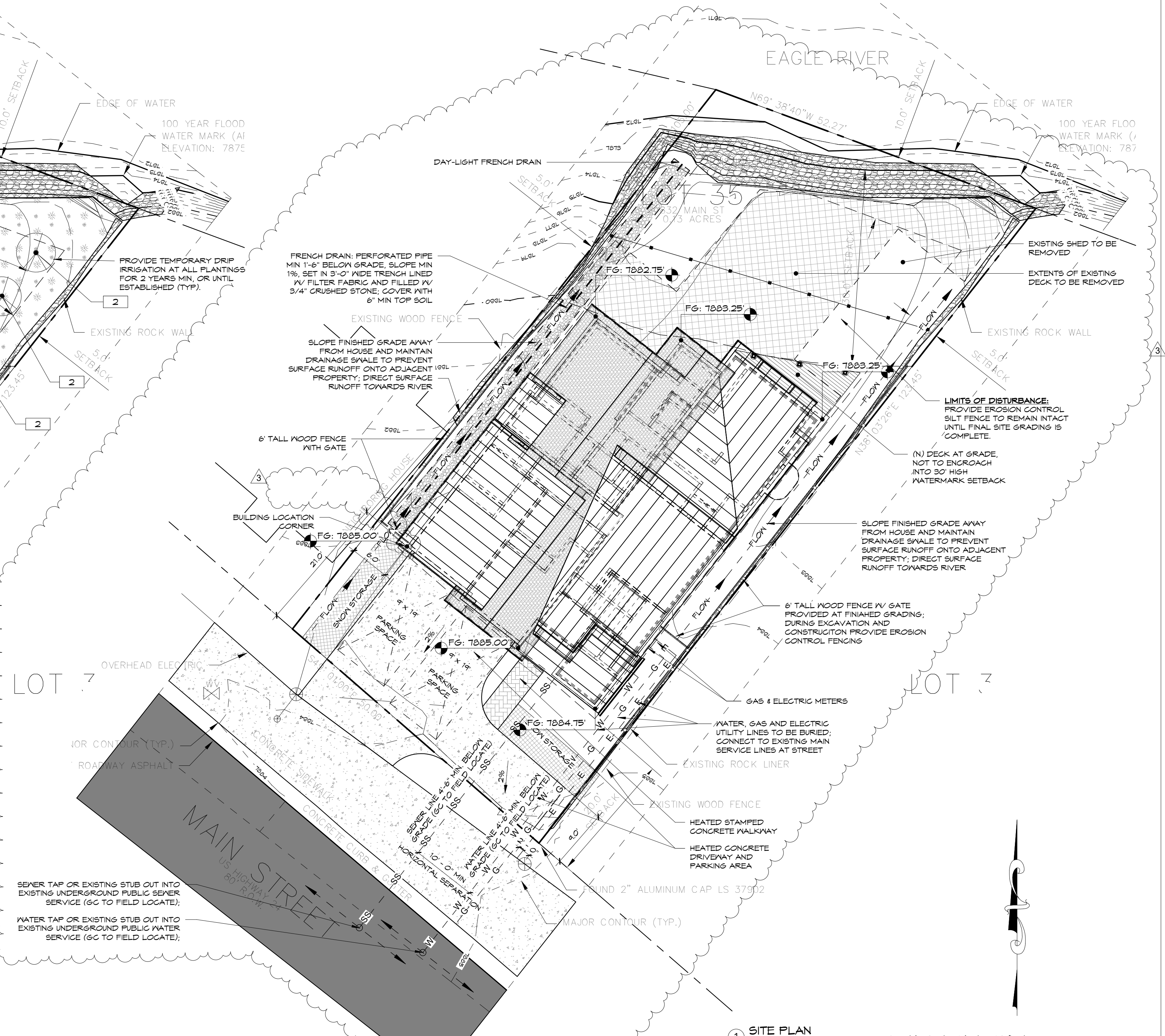
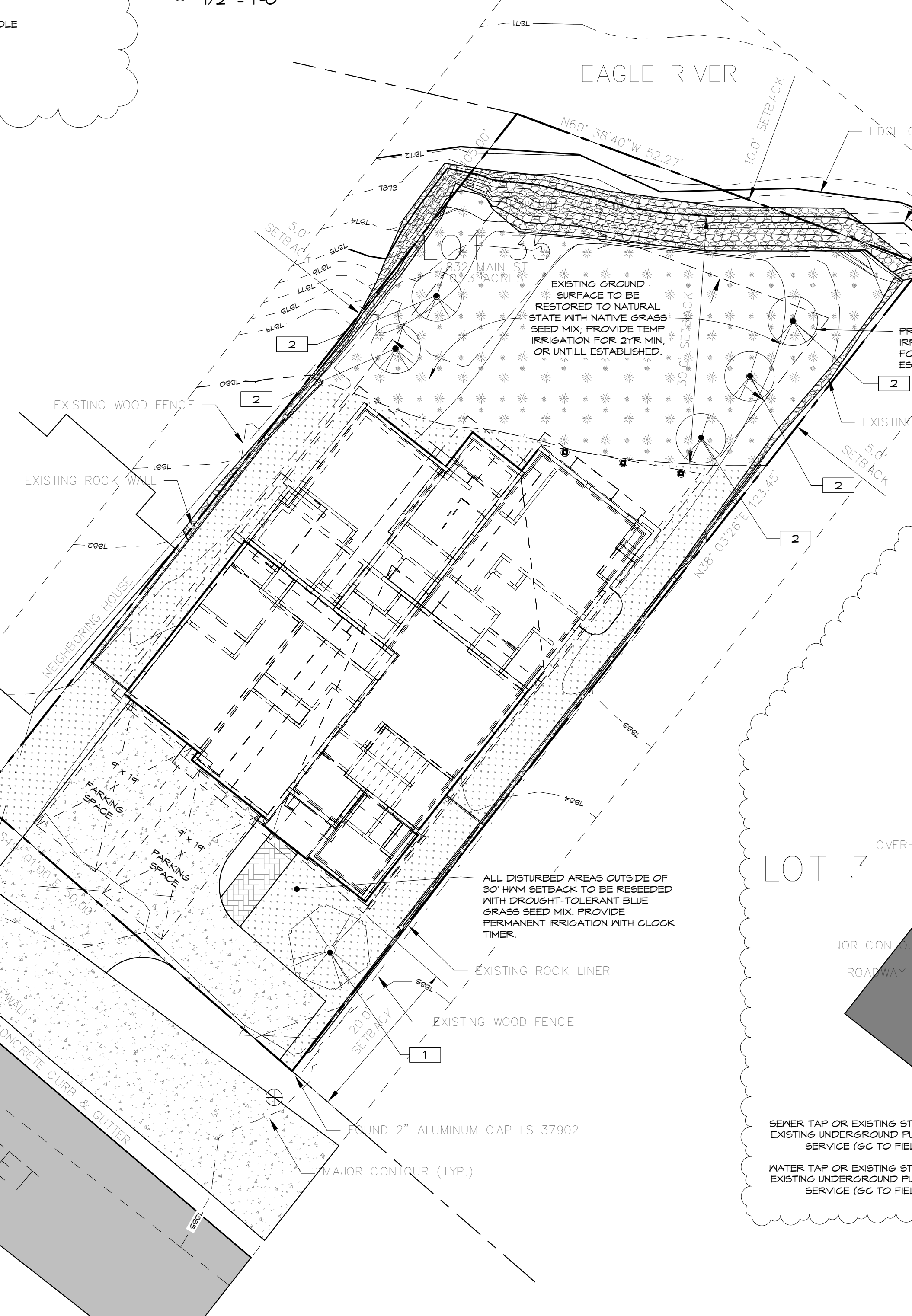
| Type Mark    | Count | Type                              |
|--------------|-------|-----------------------------------|
| 1            | 1     | Chokecherry/Prunus virginiana     |
| 2            | 5     | Quaking aspen/Populus tremuloides |
| Grand total: |       | 6                                 |



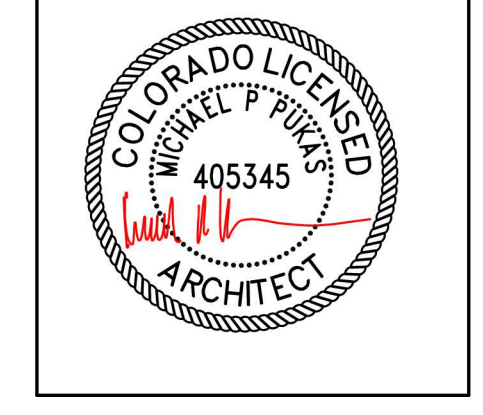
ERVSD DETAIL B-09  
1/4" = 1'-0"

ERVSD DETAIL B-08  
1/4" = 1'-0"

ERVSD DETAIL B-05  
1/4" = 1'-0"



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 michael@mppdesignshop.com



Jones Residence  
 1832 Main Street  
 Minturn, CO 81645  
 Parcel Number: 2103-351-02-003  
 South Minturn Addition, Lot 35

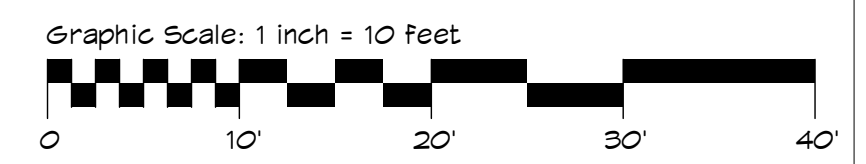
2021 Final Design Drawings  
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| # | Description      | Date     |
|---|------------------|----------|
| 1 | DRB Submission   | 03/18/21 |
| 2 | Building Permit  | 05/07/21 |
| 3 | Blgd Permit Revs | 05/19/21 |
| 4 | Revision 1       | 06/14/21 |

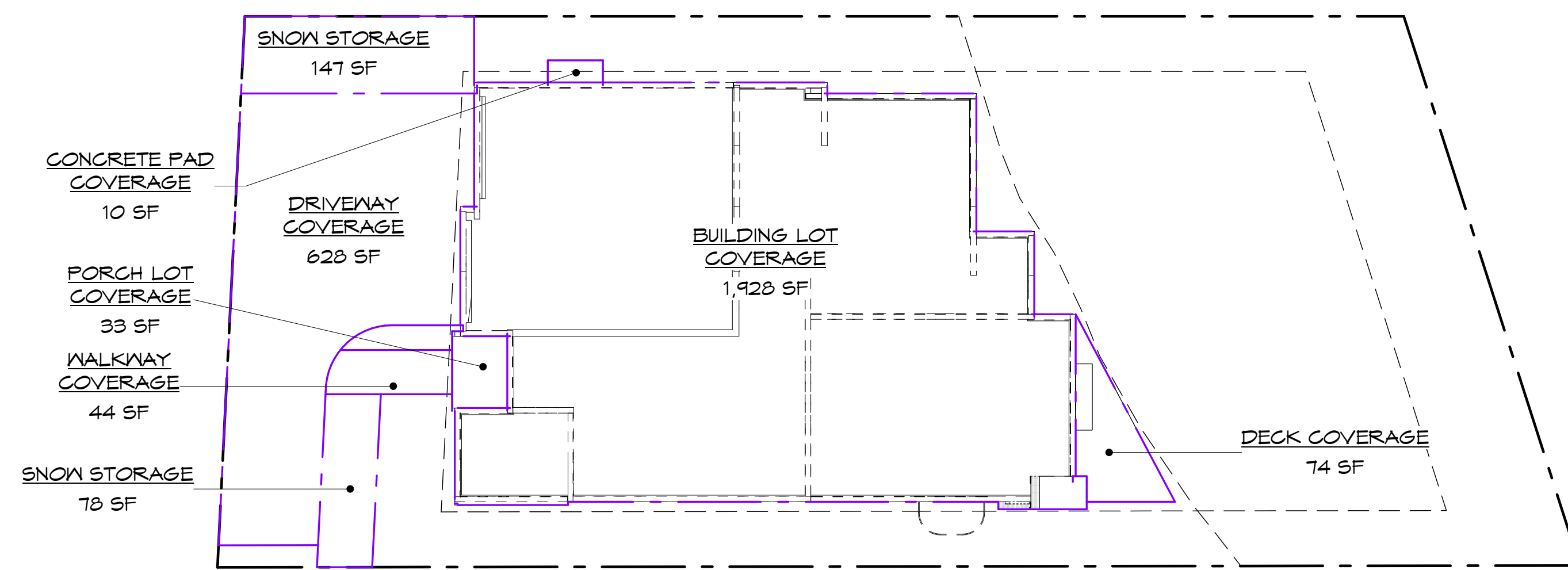
Project Number: 2005  
 Designed by: mpp  
 Drawn by: mpp  
 Checked by: mpp

**SITE & LANDSCAPE PLANS**

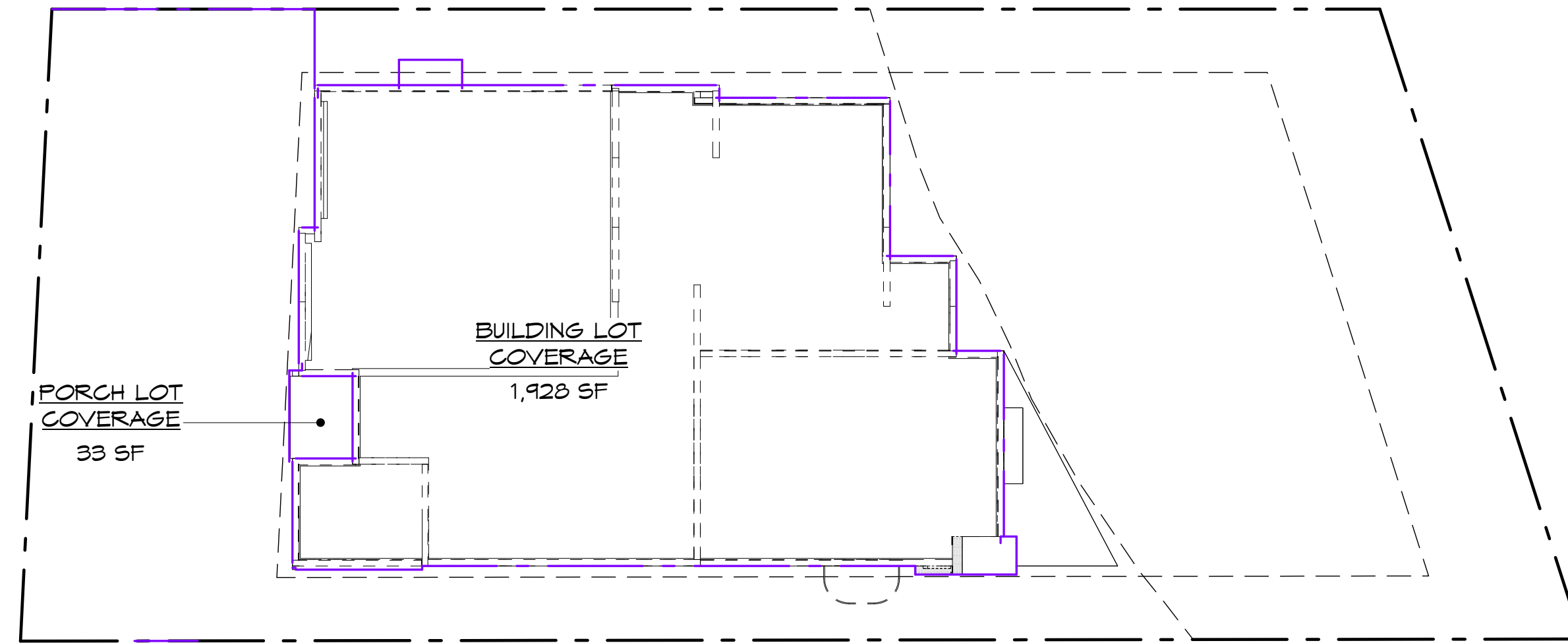
**AS101**







6 IMPERVIOUS SURFACE COVERAGE  
1" = 10'-0"



7 BUILDING LOT COVERAGE  
1" = 10'-0"



5 EFFECTIVE LOT AREA  
1" = 10'-0"



4 LOT AREA  
1" = 10'-0"

| LOT AREA & ALLOWABLE COVERAGE |                             |                                   |
|-------------------------------|-----------------------------|-----------------------------------|
| EFFECTIVE LOT AREA            | BUILDING LOT COVERAGE - 40% | IMPERVIOUS SURFACE COVERAGE - 50% |
| 5,508 SF                      | 2,203 SF                    | 2,754 SF                          |

| LOT AREA           |          |
|--------------------|----------|
| Name               | Area     |
| LOT AREA           | 5,695 SF |
| EFFECTIVE LOT AREA | 5,508 SF |

| BUILDING LOT COVERAGE |          |
|-----------------------|----------|
| PORCH LOT COVERAGE    | 33 SF    |
| BUILDING LOT COVERAGE | 1,928 SF |
| TOTAL                 | 1,960 SF |

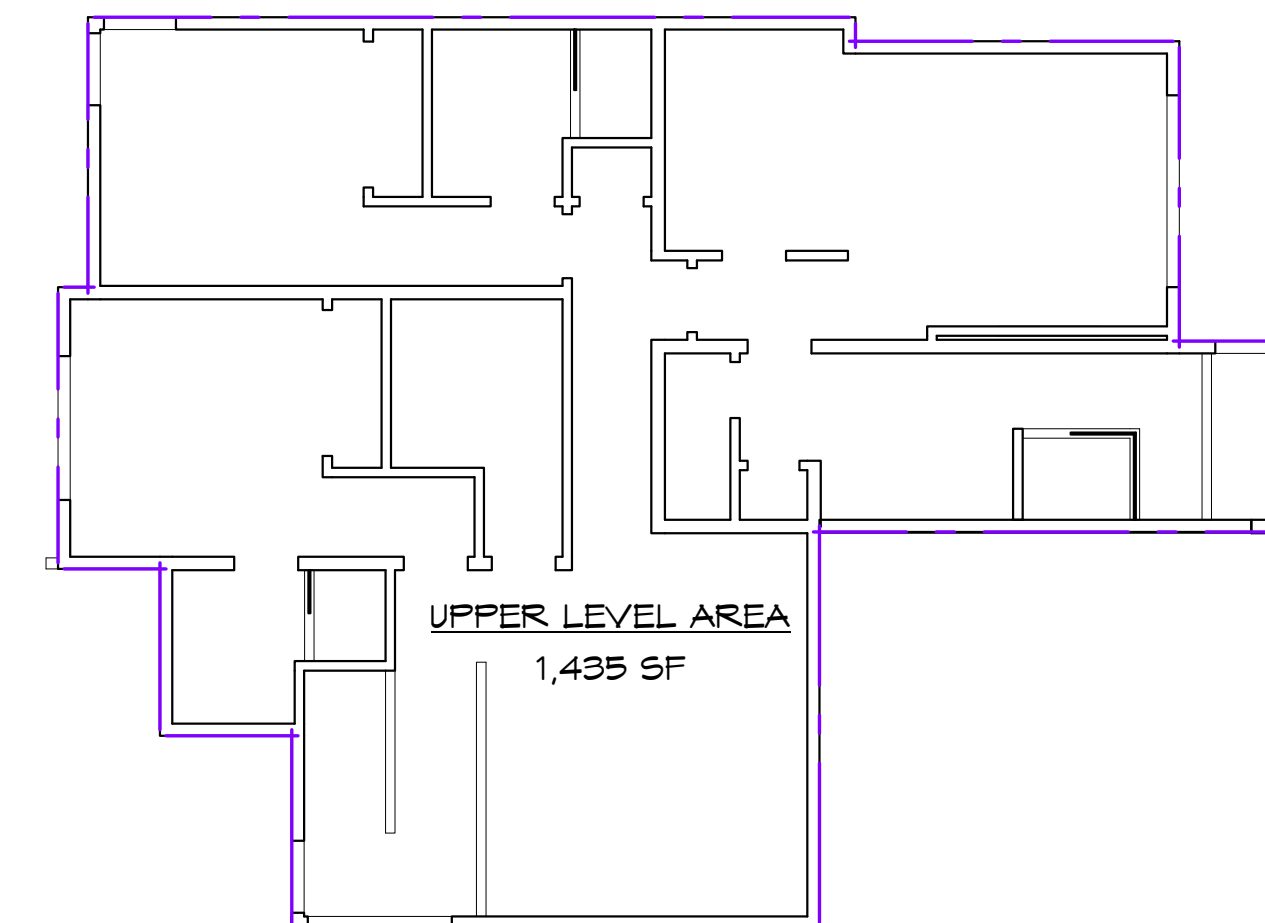
| IMPERVIOUS SURFACE COVERAGE |          |
|-----------------------------|----------|
| PORCH LOT COVERAGE          | 33 SF    |
| BUILDING LOT COVERAGE       | 1,928 SF |
| DECK COVERAGE               | 74 SF    |
| DRIVEWAY COVERAGE           | 628 SF   |
| WALKWAY COVERAGE            | 44 SF    |
| CONCRETE PAD COVERAGE       | 10 SF    |
| TOTAL                       | 2,717 SF |

| SNOW STORAGE AREA       |        |
|-------------------------|--------|
| SNOW STORAGE            | 147 SF |
| SNOW STORAGE            | 78 SF  |
| TOTAL SNOW STORAGE AREA | 225 SF |

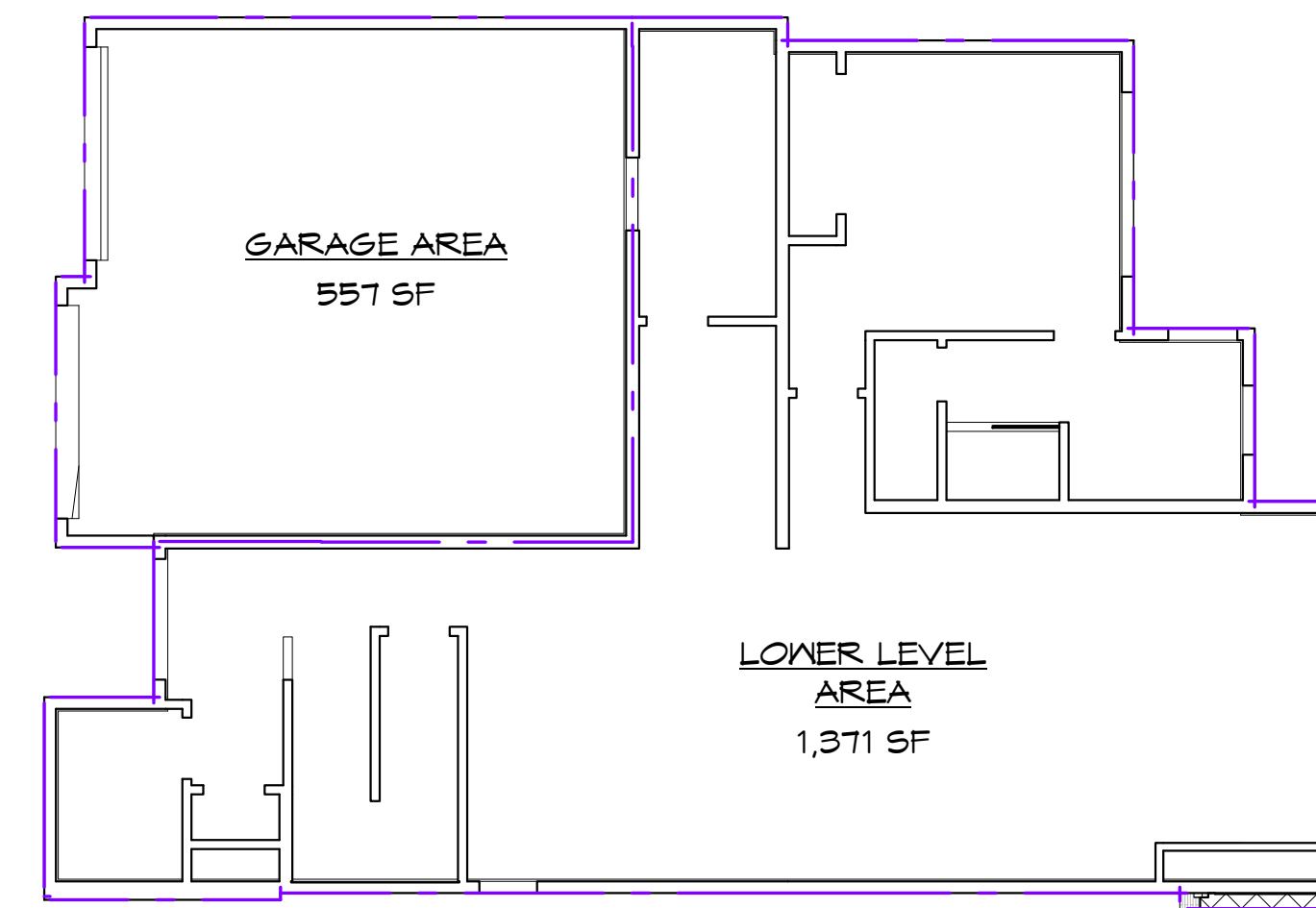
| LIVEABLE FLOOR AREA |          |
|---------------------|----------|
| BASEMENT LEVEL AREA | 763 SF   |
| LOWER LEVEL AREA    | 1,371 SF |
| UPPER LEVEL AREA    | 1,435 SF |
| TOTAL               | 3,569 SF |

| GARAGE AREA |        |
|-------------|--------|
| GARAGE AREA | 557 SF |
| TOTAL       | 557 SF |

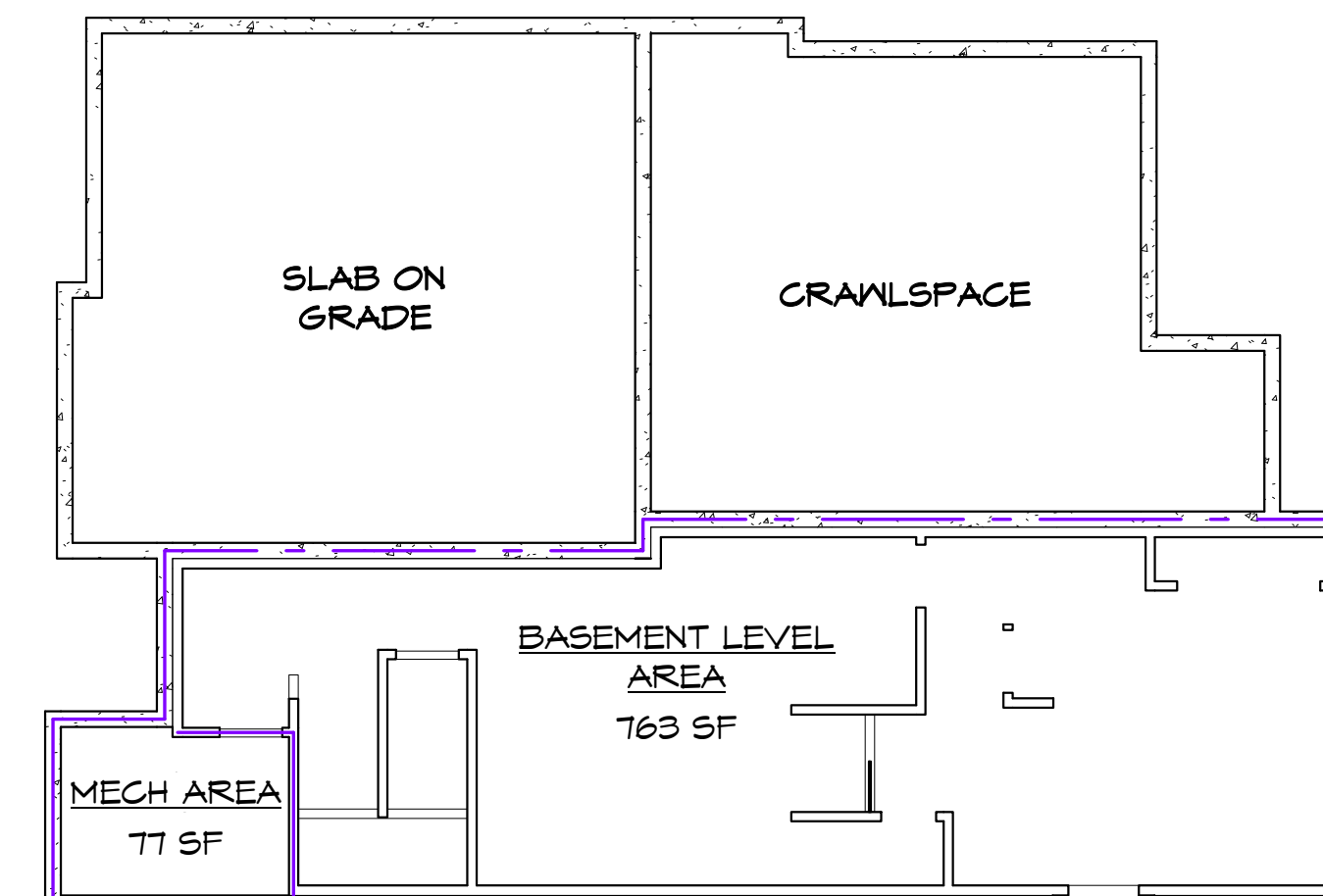
| TOTAL BUILDING AREA |          |
|---------------------|----------|
| BASEMENT LEVEL AREA | 763 SF   |
| MECH AREA           | 77 SF    |
| GARAGE AREA         | 557 SF   |
| LOWER LEVEL AREA    | 1,371 SF |
| UPPER LEVEL AREA    | 1,435 SF |
| TOTAL BUILDING AREA | 4,203 SF |



3 UPPER LEVEL AREA PLAN  
1/8" = 1'-0"



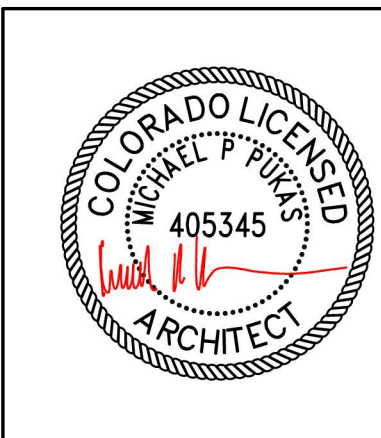
2 LOWER LEVEL AREA PLAN  
1/8" = 1'-0"



1 BASEMENT LEVEL AREA PLAN  
1/8" = 1'-0"

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Jones Residence

832 Main Street  
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Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

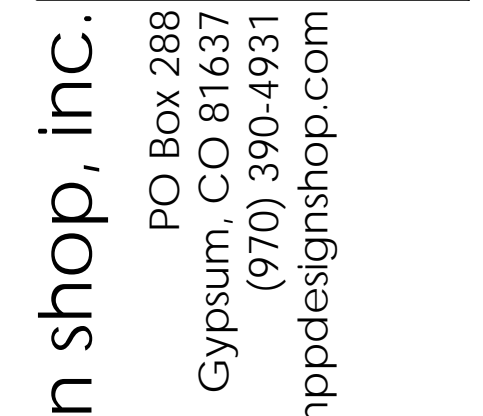
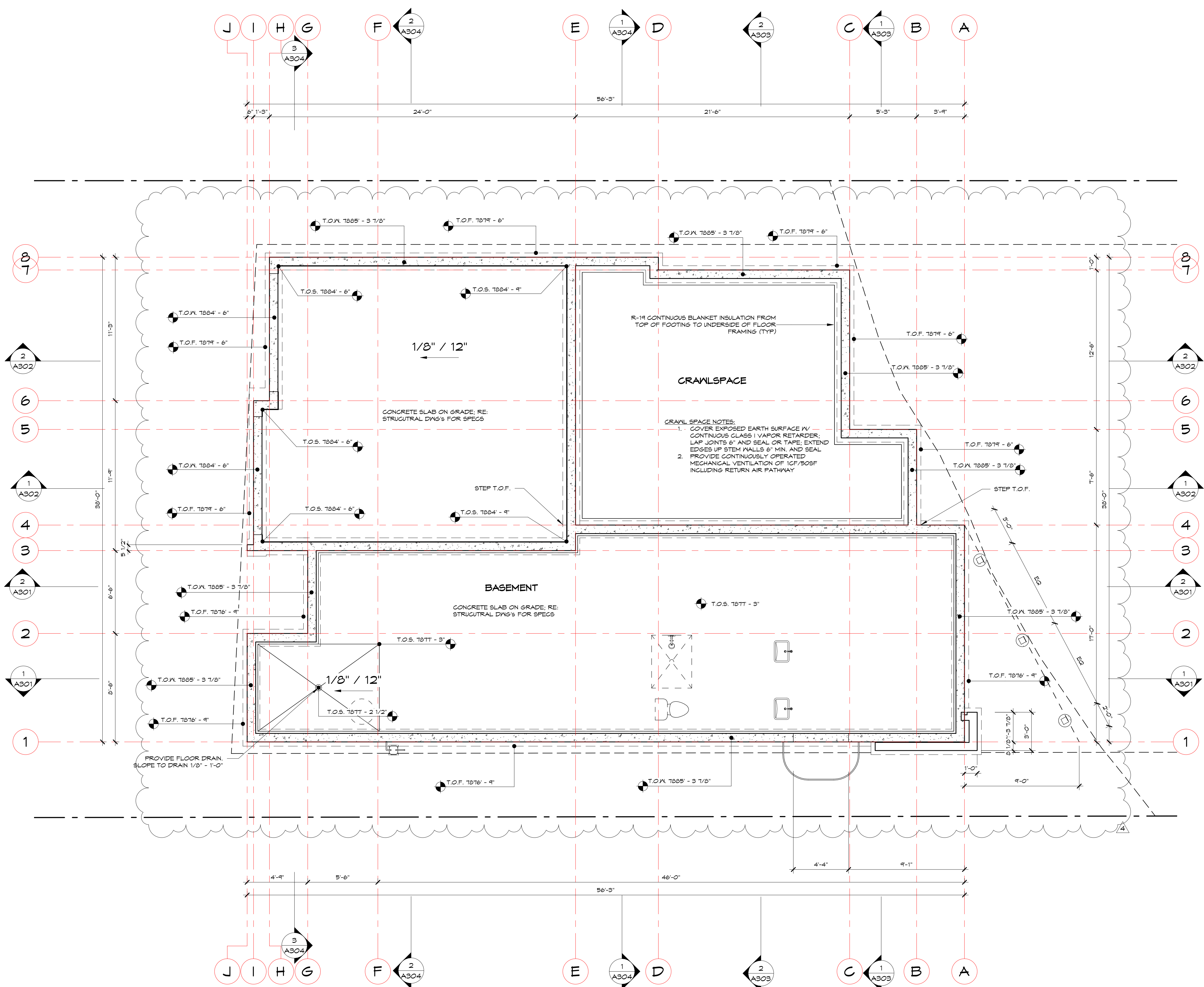
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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/16/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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AREAS, SCHEDULES

A100

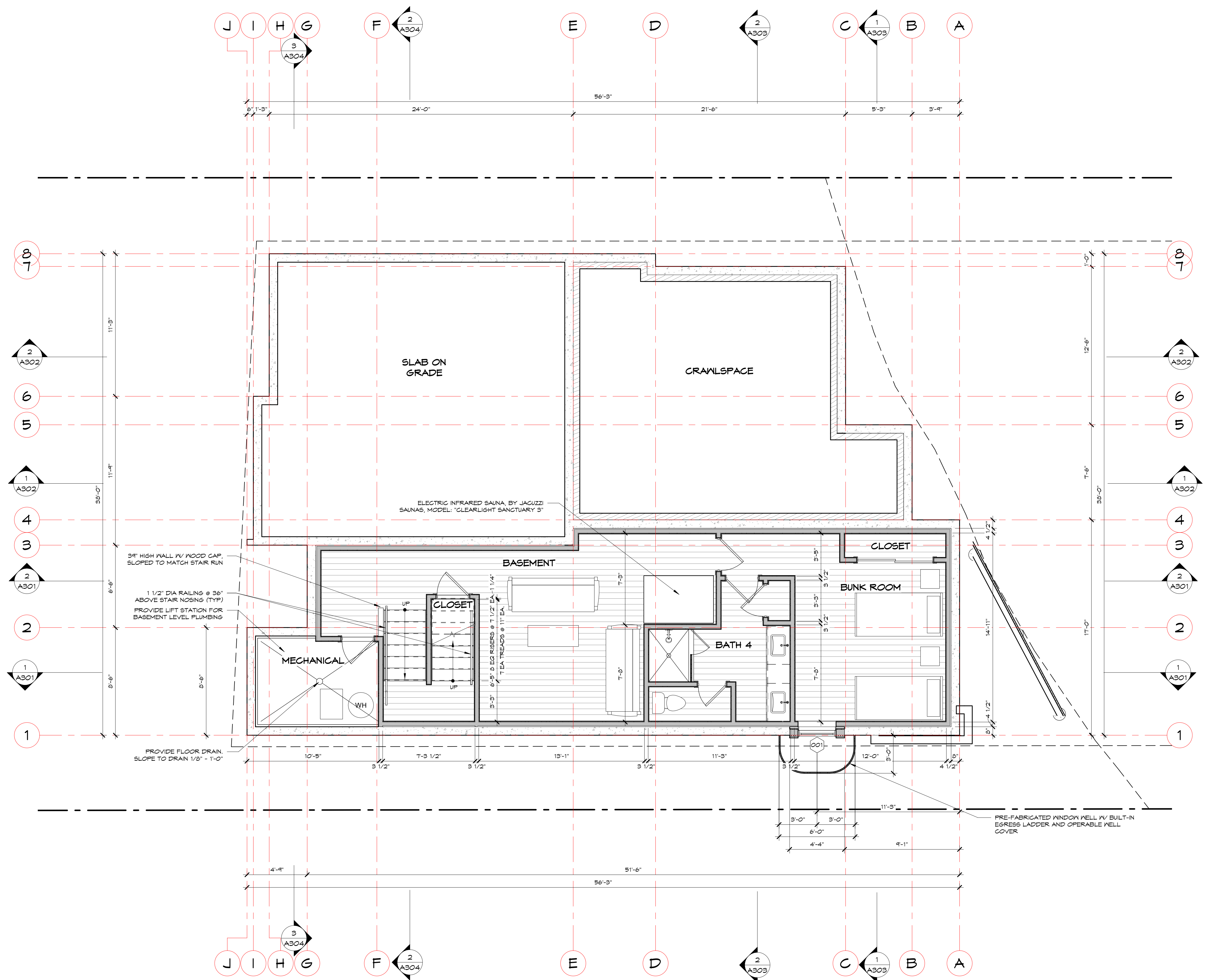


**Jones Residence**  
 1832 Main Street  
 Minturn, CO 81645  
 Parcel Number: 2103-351-02-003  
 South Minturn Addition, Lot 35

| # | Description     | Date     |
|---|-----------------|----------|
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| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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**FOUNDATION PLAN**



① BASEMENT FLOOR PLAN  
1/4" = 1'-0"

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South Minturn Addition, Lot 35

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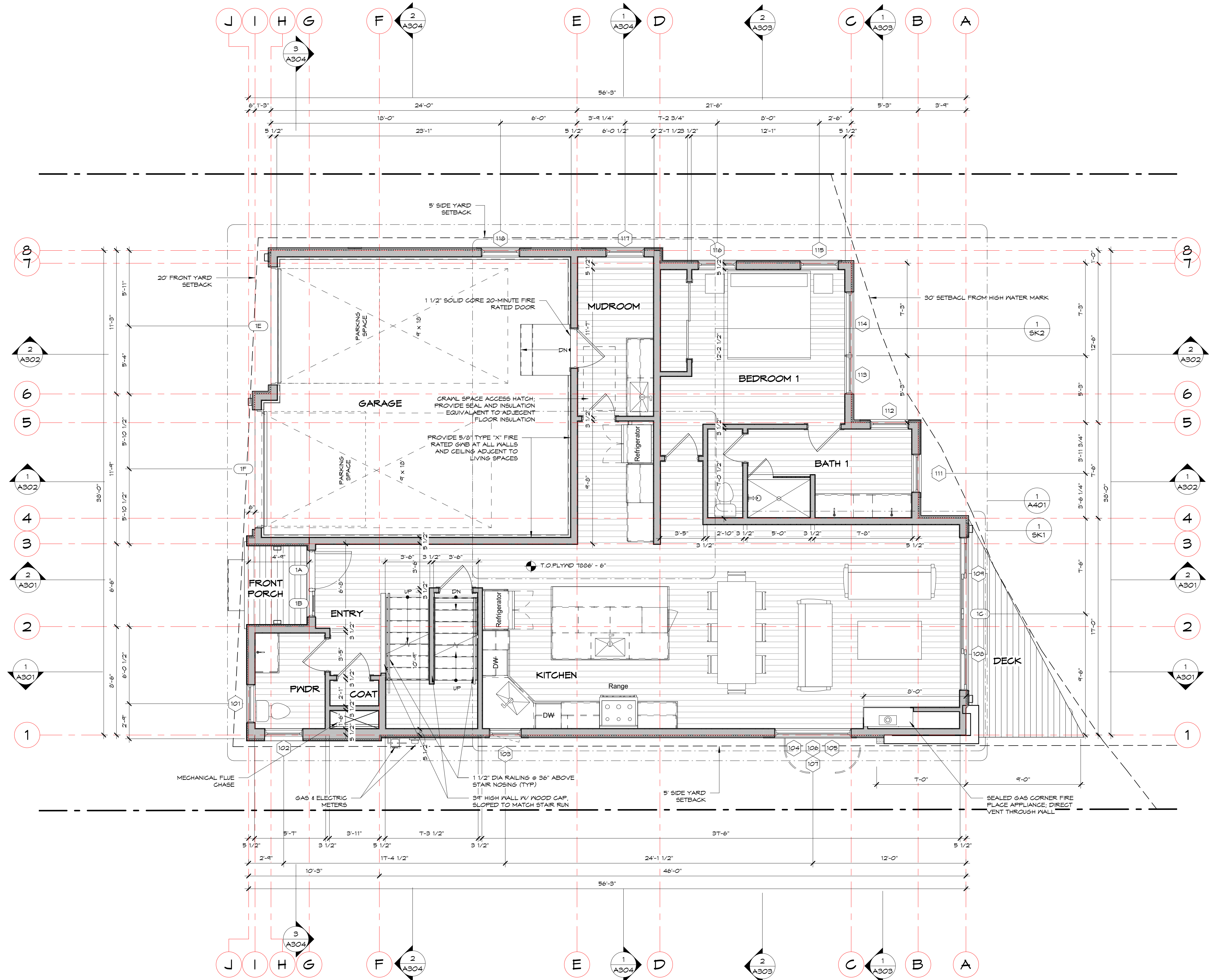
| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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Designed by: mpp  
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BASEMENT FLOOR PLAN

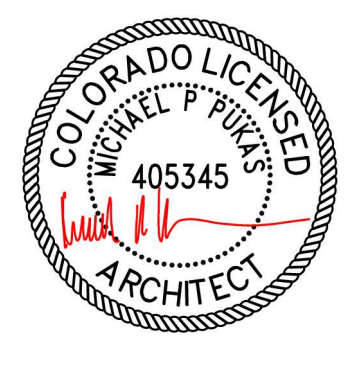
A102

67



1 LOWER LEVEL FLOOR PLAN  
1/4" = 1'-0"

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Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

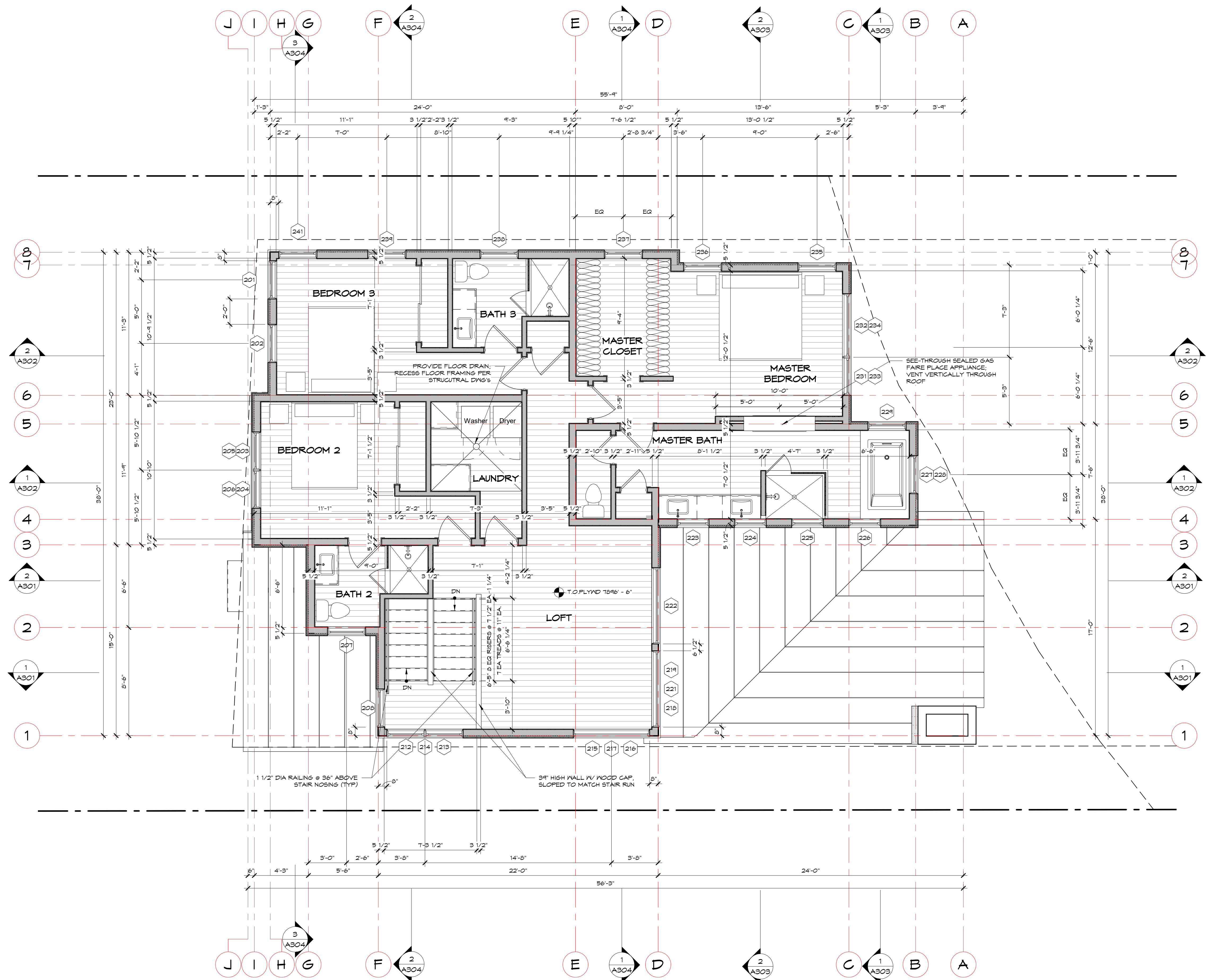
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LOWER LEVEL FLOOR PLAN

A103



1 UPPER LEVEL FLOOR PLAN  
1/4" = 1'-0"

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South Minturn Addition, Lot 35

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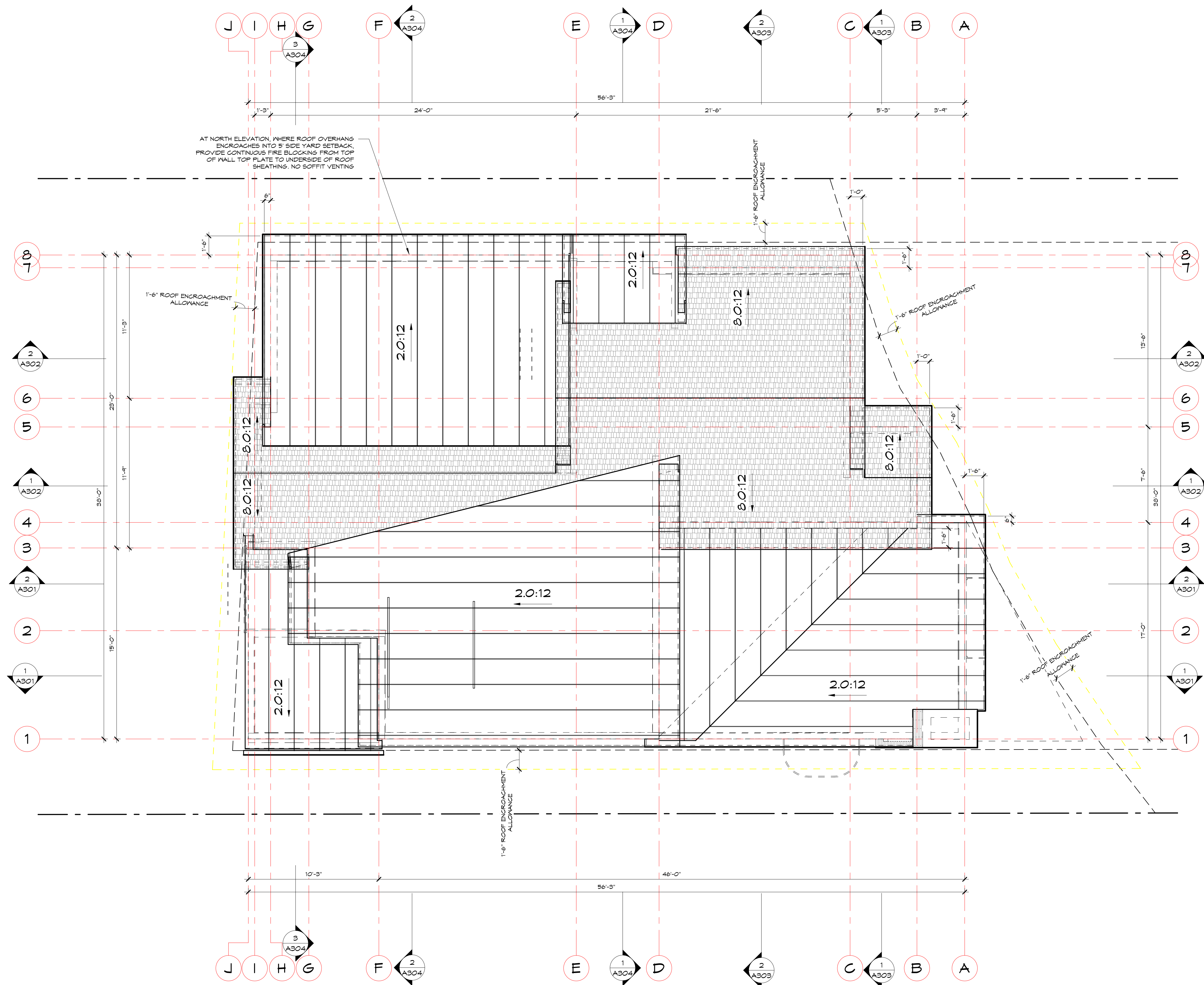
| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/16/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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Drawn by: mpp  
Checked by: mpp

UPPER LEVEL FLOOR PLAN

A104

69



J I H G F E D C B A

2 A304

1 A304

2 A303

1 A303

5'-3"

3'-9"

21'-6"

56'-3"

24'-0"

1'-3"

8 7

2 A302

6 5

1 A302

4 3

2 A301

2 2

1 A301

1

19'-6"

2 A302

6 5

1 A302

4 3

2 A301

2 2

1 A301

1

11'-3"

23'-0"

11'-4"

39'-0"

18'-0"

19'-0"

7'-6"

38'-0"

11'-0"

J I H G F E D C B A

3 A304

2 A304

1 A304

2 A303

1 A303

10'-3"

56'-3"

46'-0"

1 ROOF PLAN  
1/4" = 1'-0"

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Project Number: 2005  
 Designed by: mpp  
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ROOF PLAN

A105

70

### EXTERIOR DOOR SCHEDULE

| Mark  | Type | Description           | Width        | Height       | U-FACTOR<br>R | Comments |
|-------|------|-----------------------|--------------|--------------|---------------|----------|
| 1A    | 1    | FRONT DOOR            | 3' - 1 7/16" | 7' - 11 1/2" | <0.3          |          |
| 1A: 1 |      |                       |              |              |               |          |
| 1B    | 2    | SIDE LITE             | 2' - 1 7/16" | 7' - 11 1/2" | <0.3          |          |
| 1B: 1 |      |                       |              |              |               |          |
| 1C    | 3    | 4-PANEL SLIDER        | 12' - 0"     | 10' - 0"     | <0.3          |          |
| 1C: 1 |      |                       |              |              |               |          |
| 1E    | 4    | OVER HEAD GARAGE DOOR | 9' - 0"      | 8' - 0"      |               |          |
| 1E: 1 |      |                       |              |              |               |          |
| 1F    | 4    | OVER HEAD GARAGE DOOR | 9' - 0"      | 8' - 0"      |               |          |
| 1F: 1 |      |                       |              |              |               |          |

TOTAL EXTERIOR DOOR COUNT: 5

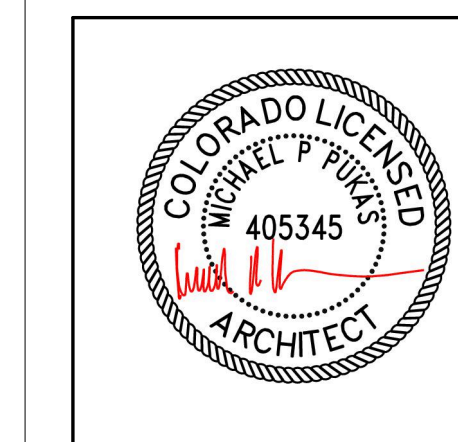
### WINDOW SCHEDULE

| Mark | Type | Description | Width       | Height       | U-FACTOR<br>R | Comments |
|------|------|-------------|-------------|--------------|---------------|----------|
| 205  |      | 4848 FIXED  | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 206  |      | 4848 FIXED  | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 001  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 113  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 201  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 203  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 204  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 208  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          |          |
| 227  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          |          |
| 229  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          |          |
| 231  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 241  | 1    | CASEMENT    | 3' - 0"     | 6' - 0"      | <0.3          | EGRESS   |
| 10   |      |             |             |              |               |          |
| 111  | 2    | CASEMENT    | 3' - 0"     | 4' - 6"      | <0.3          |          |
| 112  | 2    | CASEMENT    | 3' - 0"     | 4' - 6"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 103  | 3    | CSMT        | 2' - 6"     | 4' - 6"      | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 101  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 102  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 115  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 116  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 117  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 118  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 202  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 207  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 235  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 236  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 237  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 238  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 239  | 5    | AWNG        | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 13   |      |             |             |              |               |          |
| 104  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 105  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 212  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 213  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 215  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 216  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 218  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 219  | 6    | ANNING      | 3' - 0"     | 2' - 0"      | <0.3          |          |
| 8    |      |             |             |              |               |          |
| 106  | 7    | FIXED       | 6' - 0"     | 3' - 6"      | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 228  | 8    | FIXED       | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 233  | 8    | FIXED       | 3' - 0"     | 3' - 0"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 223  | 9    | FIXED       | 2' - 6"     | 2' - 6"      | <0.3          |          |
| 224  | 9    | FIXED       | 2' - 6"     | 2' - 6"      | <0.3          |          |
| 225  | 9    | FIXED       | 2' - 6"     | 2' - 6"      | <0.3          |          |
| 226  | 9    | FIXED       | 2' - 6"     | 2' - 6"      | <0.3          |          |
| 4    |      |             |             |              |               |          |
| 114  | 11   | FIXED       | 5' - 0"     | 6' - 0"      | <0.3          |          |
| 232  | 11   | FIXED       | 5' - 0"     | 6' - 0"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 234  | 12   | FIXED       | 5' - 0"     | 3' - 0"      | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 221  | 13   | FIXED       | 6' - 0"     | 5' - 0"      | <0.3          |          |
| 222  | 13   | FIXED       | 6' - 0"     | 5' - 0"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 211  | 14   | FIXED       | 6' - 0"     | 3' - 0"      | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 108  | 15   | FIXED       | 5' - 8 1/2" | 4' - 3"      | <0.3          |          |
| 109  | 15   | FIXED       | 5' - 8 1/2" | 4' - 3"      | <0.3          |          |
| 2    |      |             |             |              |               |          |
| 107  | 16   | FIXED TRAP  | 6' - 0"     | 2' - 10"     | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 214  | 17   | FIXED TRAP  | 6' - 0"     | 5' - 1 1/4"  | <0.3          |          |
| 1    |      |             |             |              |               |          |
| 217  | 18   | FIXED TRAP  | 6' - 0"     | 4' - 10 3/4" | <0.3          |          |
| 1    |      |             |             |              |               |          |

TOTAL WINDOW COUNT: 54

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Jones Residence

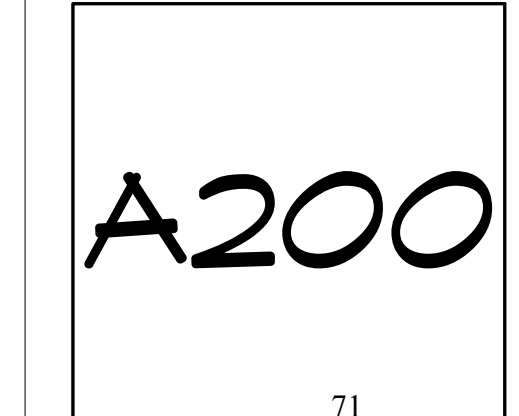
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Minturn, CO 81645  
Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

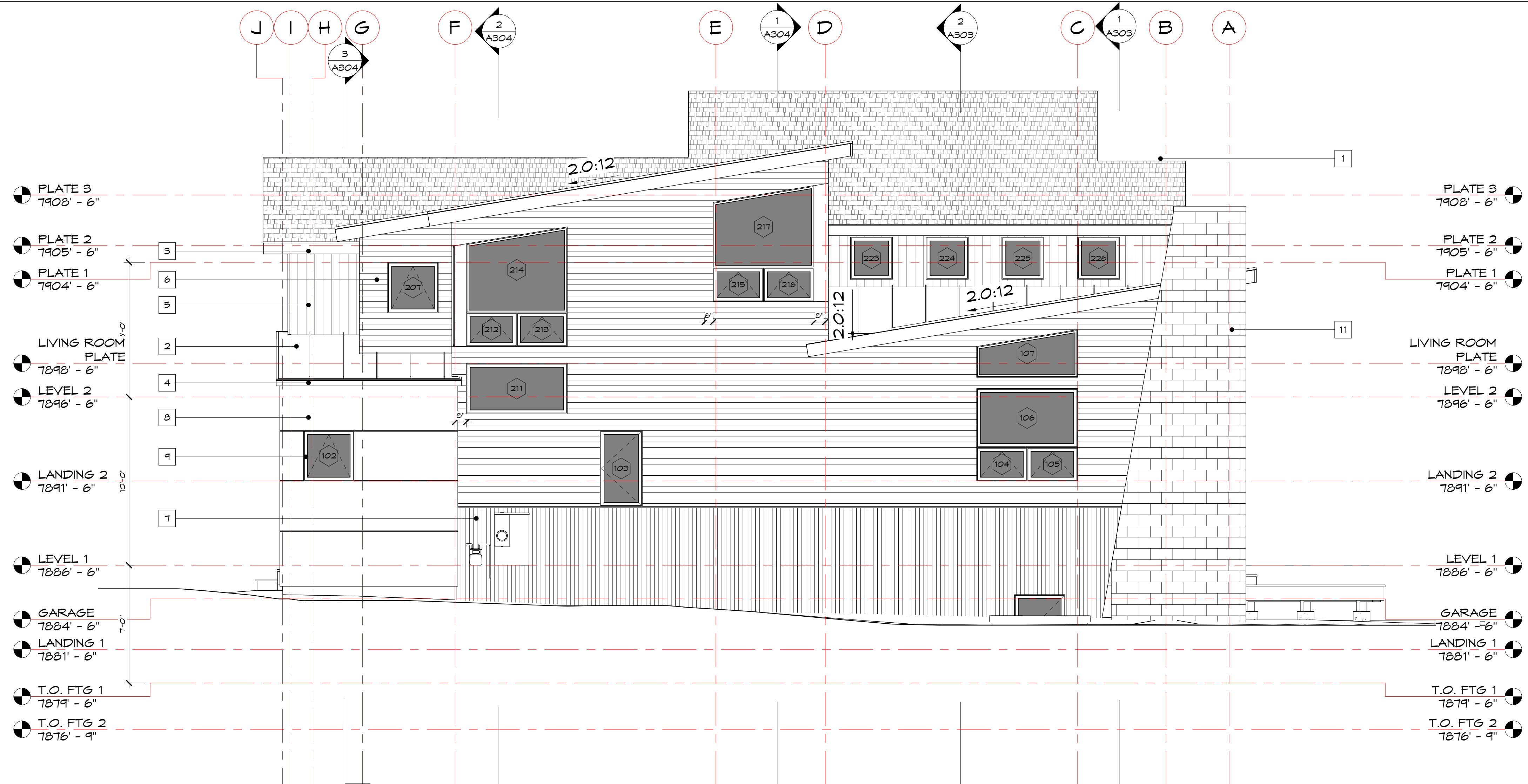
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| # | Description | Date     |
|---|-------------|----------|
| 4 | Revision 1  | 08/14/21 |
|   |             |          |
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|   |             |          |

Project Number: 2005  
Designed by: mpp  
Drawn by: mpp  
Checked by: mpp

SCHEDULES & NOTES

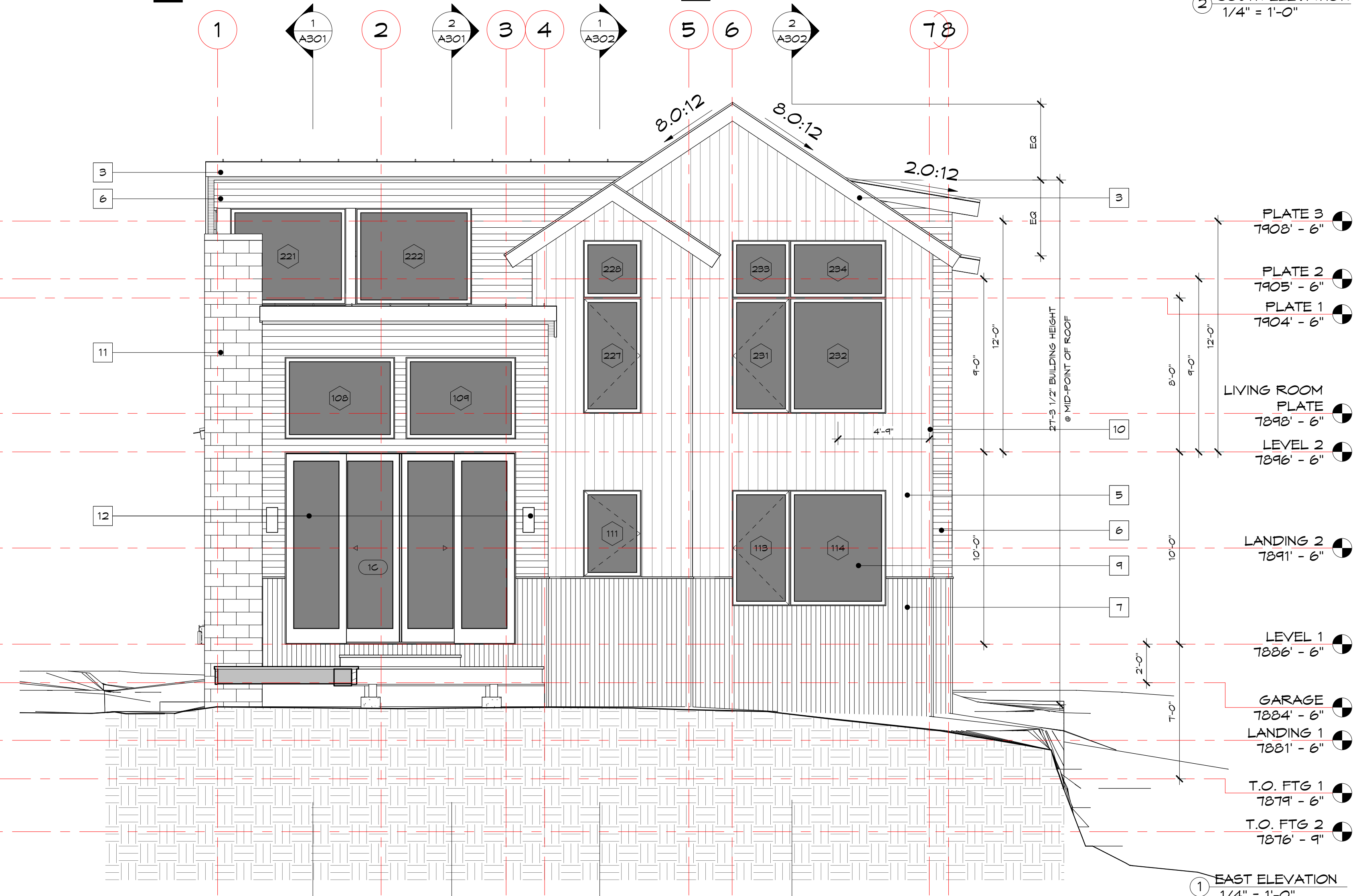




2 SOUTH ELEVATION  
1/4" = 1'-0"

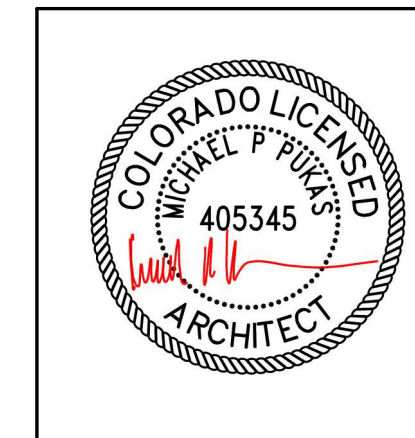
EXTERIOR MATERIAL LEGEND

| Key | Material                         | Manufacturer | Model | Color          | Finish             | Notes   |
|-----|----------------------------------|--------------|-------|----------------|--------------------|---|
| 1   | ASPHALT SHINGLES                 | GAF          |       | WEATHERED WOOD |                    |   |
| 2   | STANDING SEAM METAL ROOF         |              |       | DARK GRAY      | PRE-FINISHED PAINT | MECHANICALLY SEAMED                                   |
| 3   | 2X10 FASCIA, WOOD                |              |       | CHARCOAL GRAY  | OPAQUE STAIN       |   |
| 4   | METAL FASCIA                     |              |       | CHARCOAL GRAY  | PRE-FINISHED PAINT |   |
| 5   | VERTICAL BARN WOOD SIDING        |              |       | NATURAL        | WEATHERED          |   |
| 6   | HORIZONTAL SIDING                |              |       | MIXED BROWN    |                    |   |
| 7   | CORRUGATED METAL SIDING          |              |       |                | PRE-FINISHED PAINT |   |
| 8   | METAL PANEL SIDING               |              |       | PLATINUM       | PRE-FINISHED PAINT |   |
| 9   | WOOD FRAME ALUMINUM GLAD WINDOWS |              |       | BLACK          | PRE-FINISHED PAINT |   |
| 10  | METAL RAILING                    |              |       | BLACK          | POWDER COATED      |   |
| 11  | MASONRY BLOCK                    |              |       | BUFF           |                    |   |
| 12  | EXTERIOR LIGHTING                |              |       | BLACK          | PAINT              | EXTERIOR FULL CUT OFF, DARK SKY COMPLIANT WALL SCONCE |



1 EAST ELEVATION  
1/4" = 1'-0"

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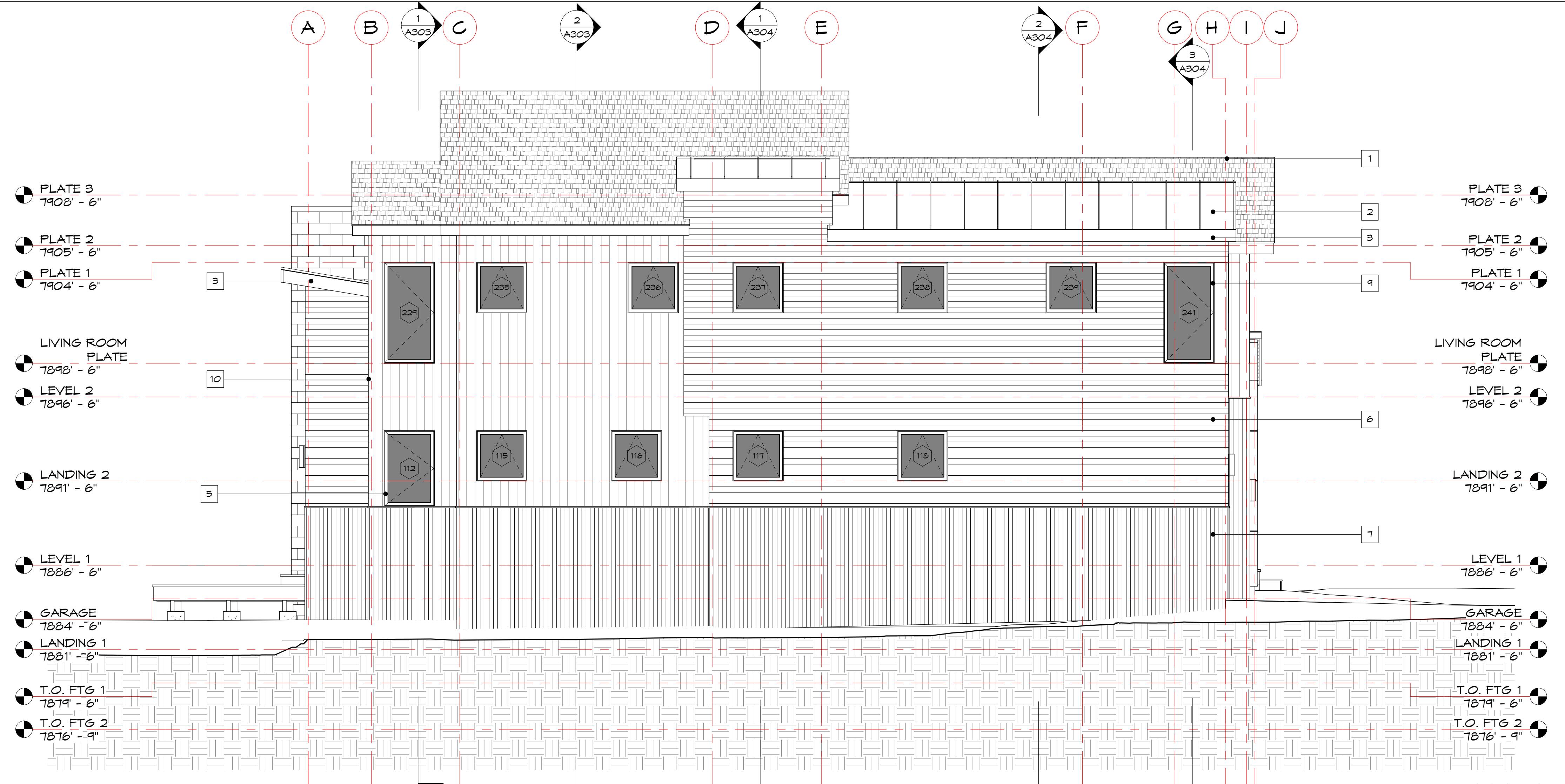
| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 05/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |

Project Number: 2005  
Designed by: mpp  
Drawn by: mpp  
Checked by: mpp

EAST & SOUTH ELEVATIONS

A201





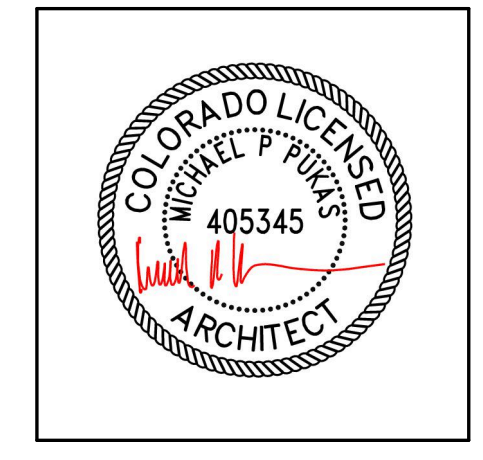
2 NORTH ELEVATION  
1/4" = 1'-0"

| EXTERIOR MATERIAL LEGEND |                                  |              |       |                    |                    |   |
|--------------------------|----------------------------------|--------------|-------|--------------------|--------------------|---|
| Key                      | Material                         | Manufacturer | Model | Color              | Finish             | Notes   |
| 1                        | APHALT SHINGLES                  | GAF          |       | WEATHERED WOOD     |                    |   |
| 2                        | STANDING SEAM METAL ROOF         |              |       | DARK GRAY          | PRE-FINISHED PAINT | MECHANICALLY SEAMED                                   |
| 3                        | 2X10 FASCIA, WOOD                |              |       | CHARCOAL GRAY      | OPAQUE STAIN       |   |
| 4                        | METAL FASCIA                     |              |       | CHARCOAL GRAY      | PRE-FINISHED PAINT |   |
| 5                        | VERTICAL BARN WOOD SIDING        |              |       | NATURAL WHEATHERED |                    |   |
| 6                        | HORIZONTAL SIDING                |              |       | MIXED BROWN        |                    |   |
| 7                        | CORRUGATED METAL SIDING          |              |       |                    | PRE-FINISHED PAINT |   |
| 8                        | METAL PANEL SIDING               |              |       | PLATINUM           | PRE-FINISHED PAINT |   |
| 9                        | WOOD FRAME ALUMINUM GLAD WINDOWS |              |       | BLACK              | PRE-FINISHED PAINT |   |
| 10                       | METAL RAILING                    |              |       | BLACK              | POWDER COATED      |   |
| 11                       | MASONRY BLOCK                    |              |       | BUFF               |                    |   |
| 12                       | EXTERIOR LIGHTING                |              |       | BLACK              | PAINT              | EXTERIOR FULL CUT OFF, DARK SKY COMPLIANT WALL SCONCE |



1 WEST ELEVATION  
1/4" = 1'-0"

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South Minturn Addition, Lot 35

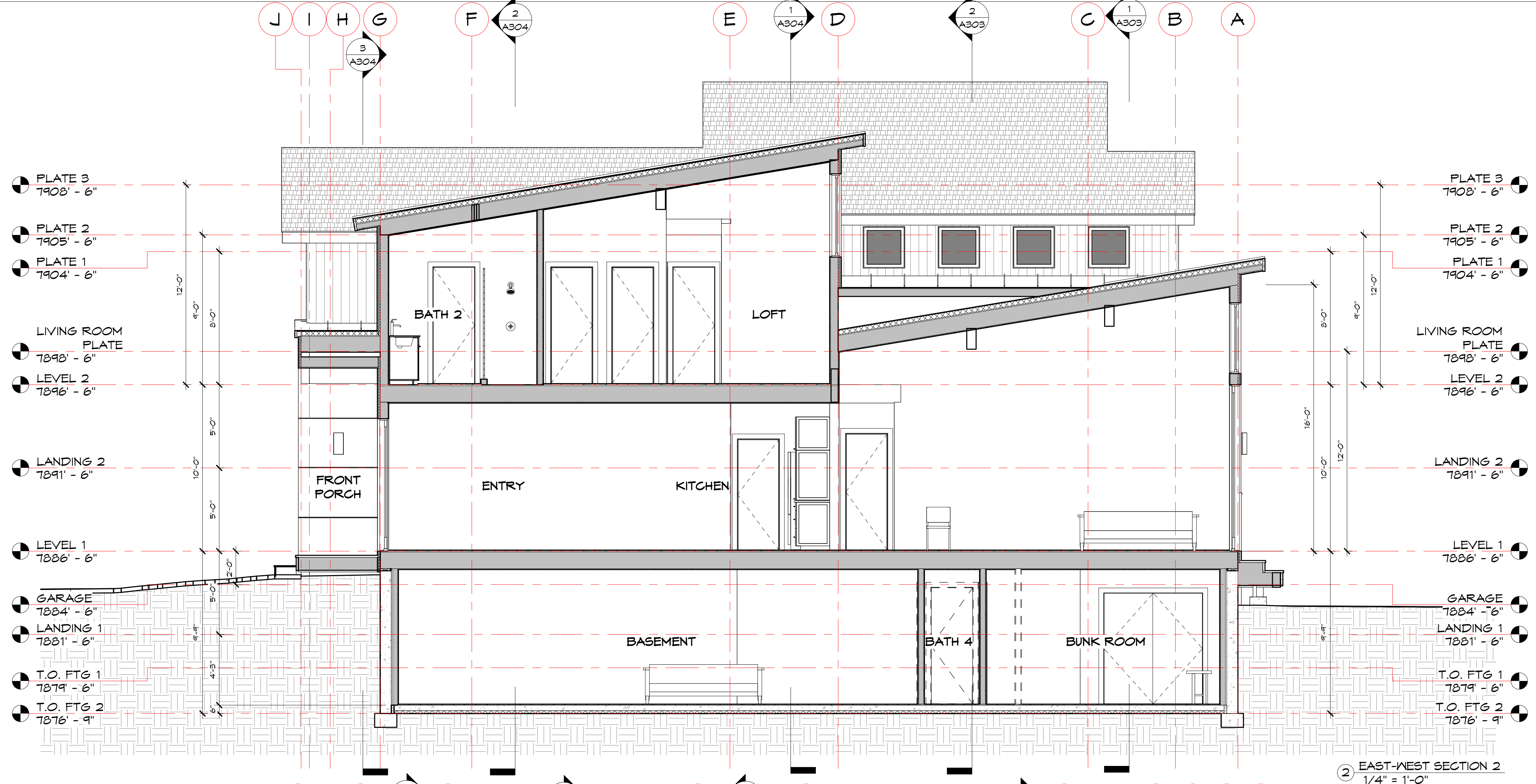
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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |

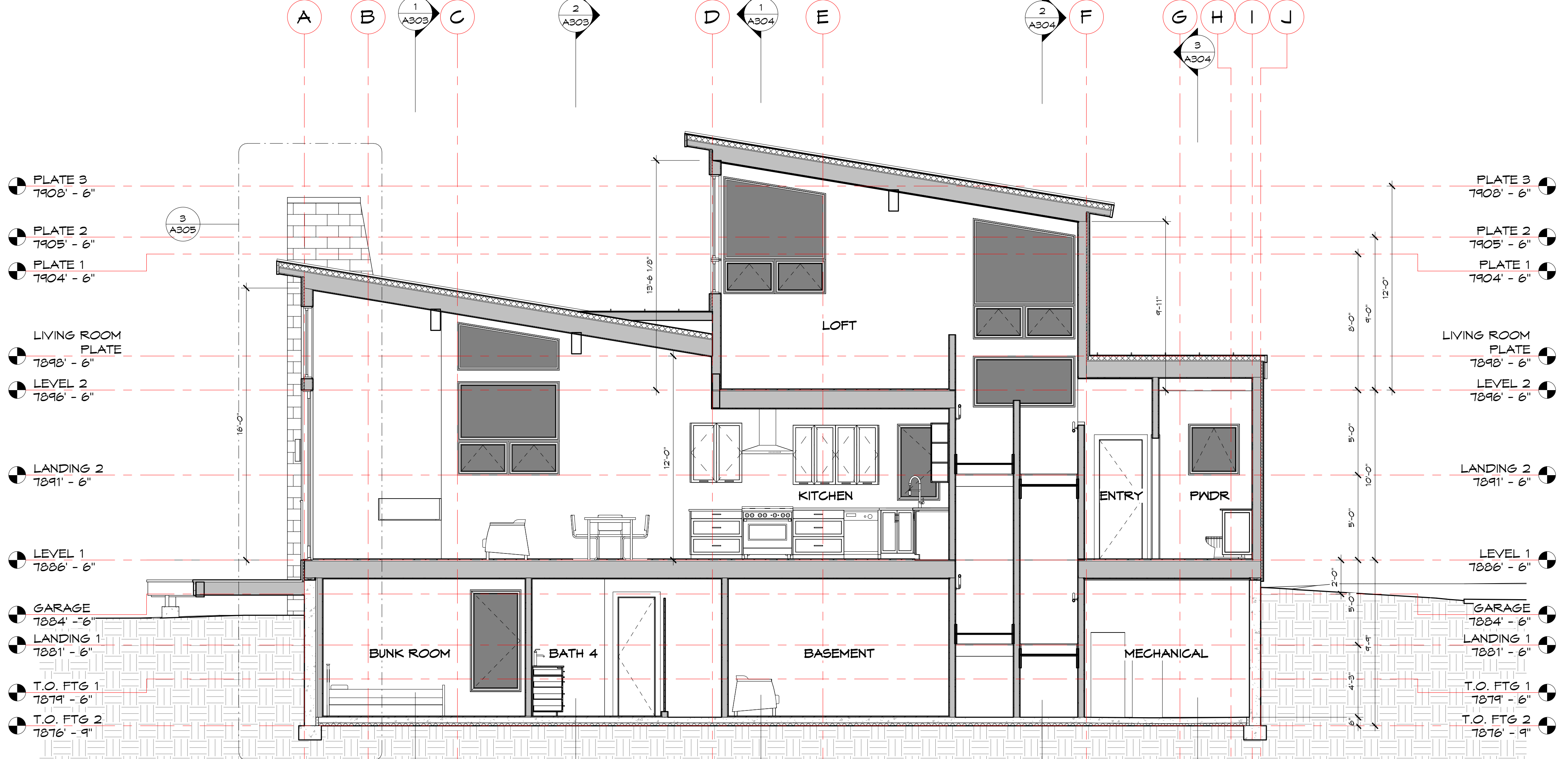
Project Number: 2005  
Designed by: mpp  
Drawn by: mpp  
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WEST & NORTH ELEVATIONS

A202

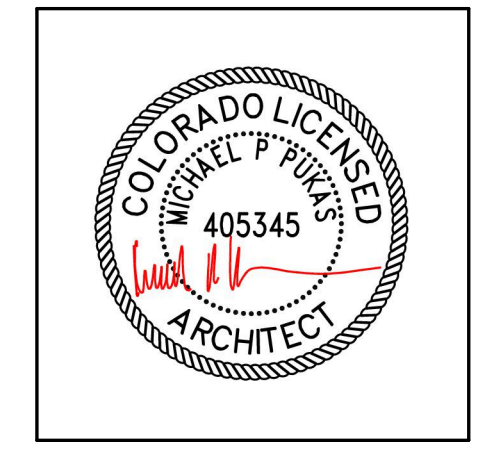


2 EAST-WEST SECTION 2  
1/4" = 1'-0"



1 EAST-WEST SECTION 1  
1/4" = 1'-0"

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Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

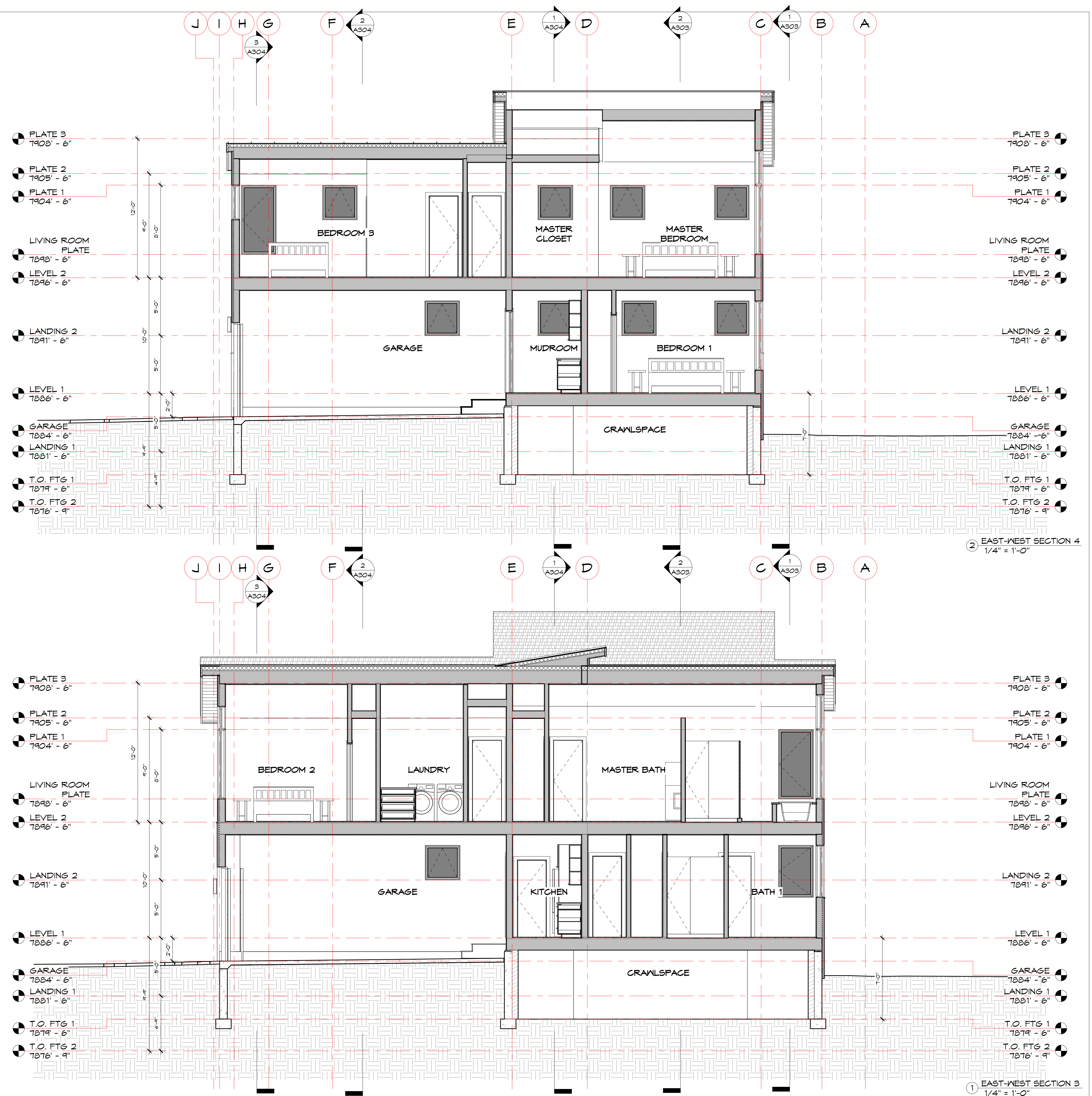
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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |

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| Project Number: | 2005 |
| Designed by:    | mpp  |
| Drawn by:       | mpp  |
| Checked by:     | mpp  |

BUILDING SECTIONS

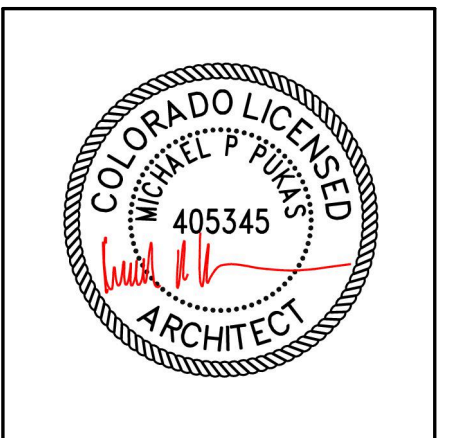
A301  
74



② EAST-WEST SECTION 4  
1/4" = 1'-0"

① EAST-WEST SECTION 3  
1/4" = 1'-0"

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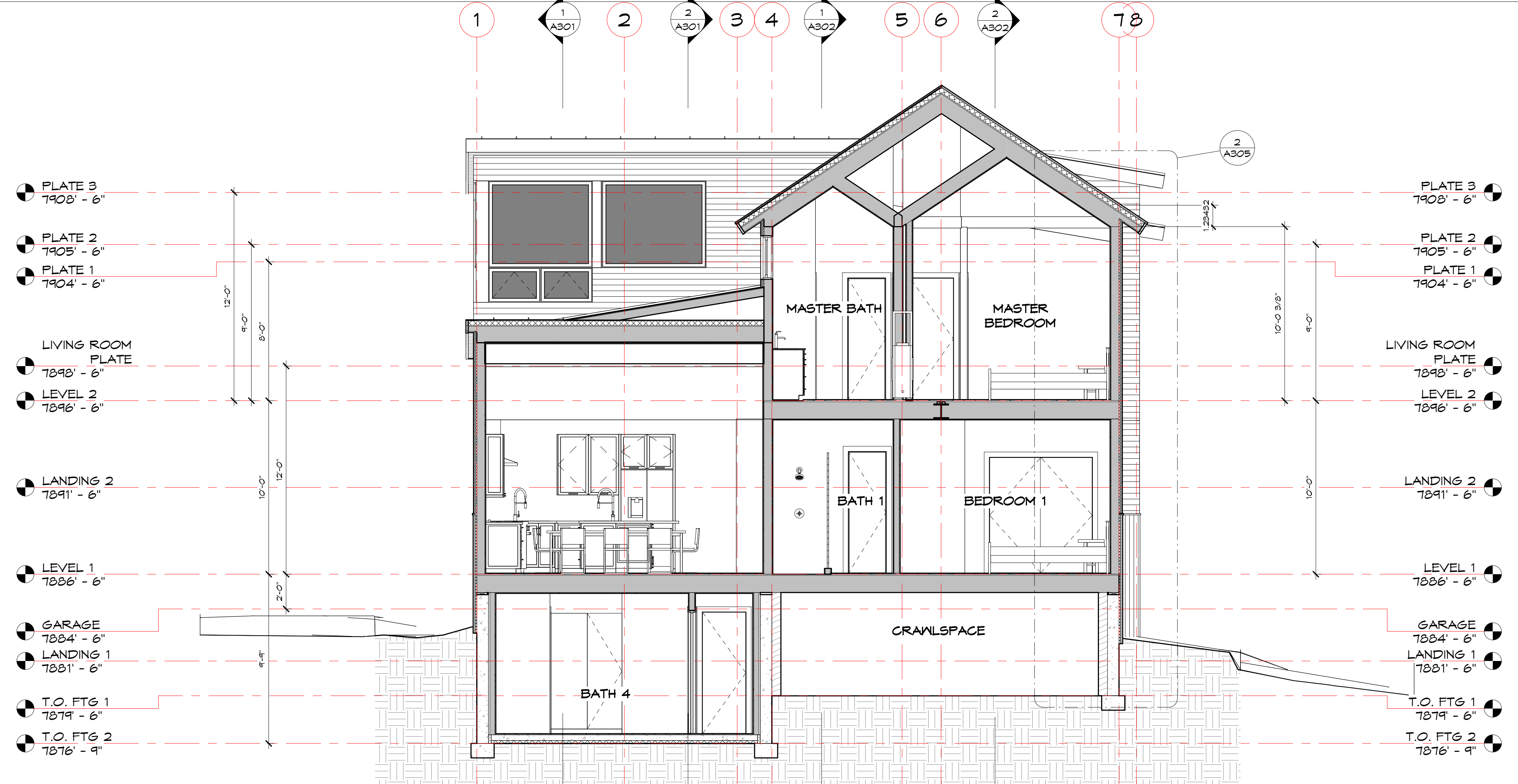
| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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Project Number: 2005  
 Designed by: mpp  
 Drawn by: mpp  
 Checked by: mpp

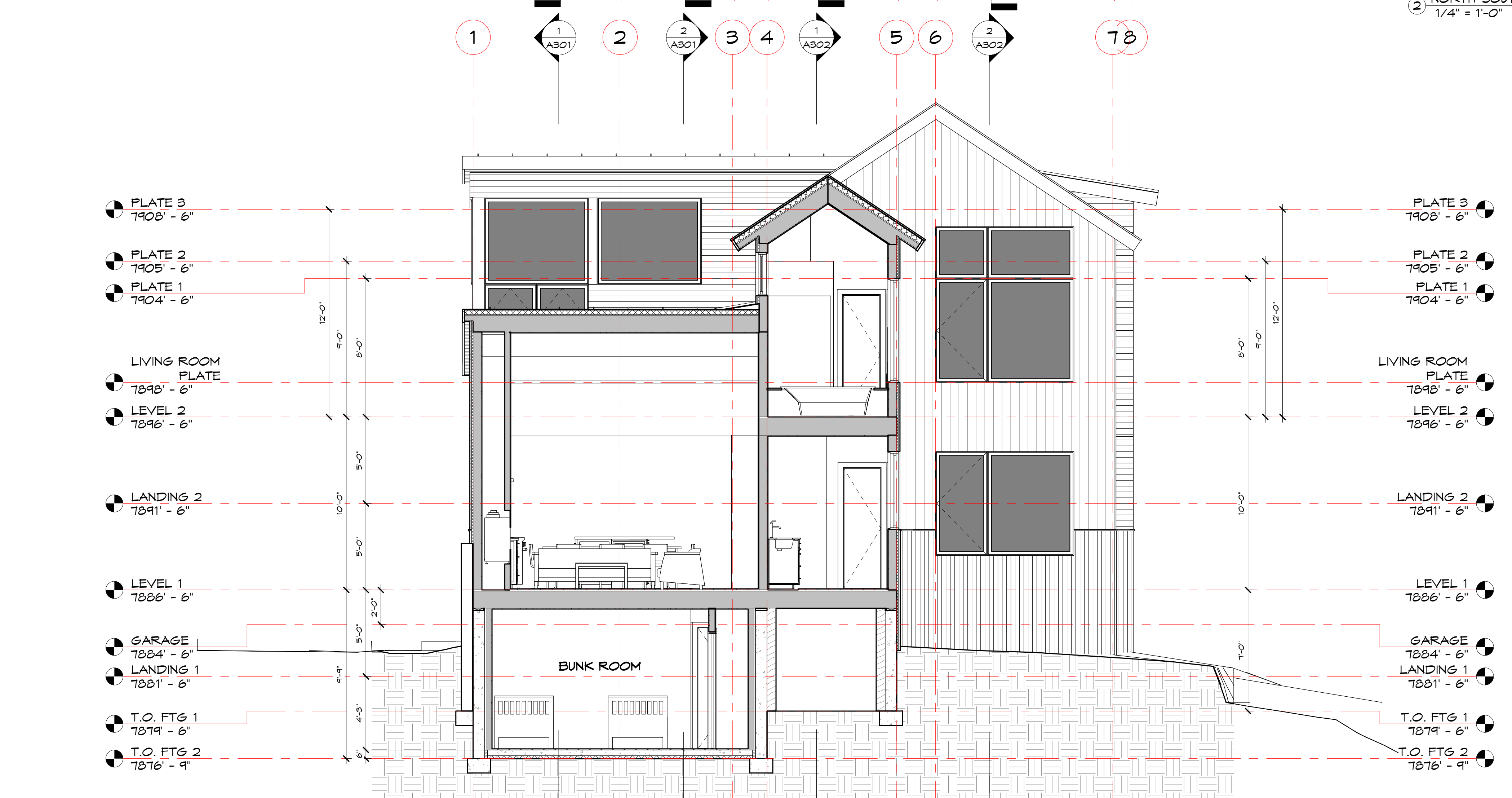
BUILDING SECTIONS

A302

75

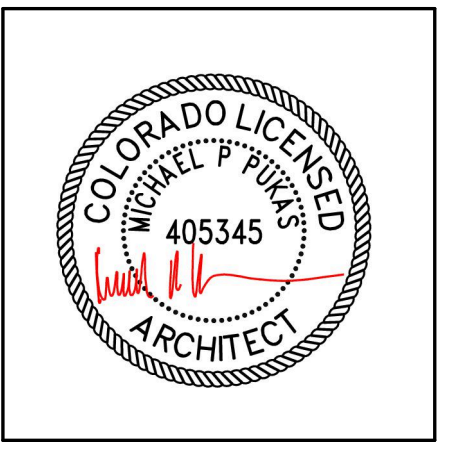


② NORTH-SOUTH SECTION 2  
1/4" = 1'-0"



① NORTH-SOUTH SECTION 1  
1/4" = 1'-0"

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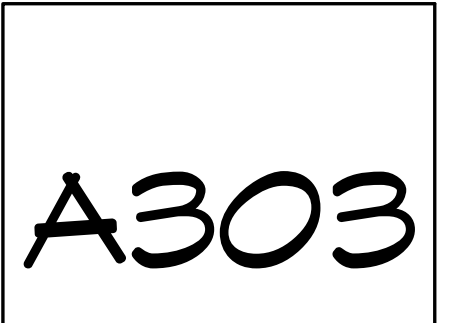
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Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

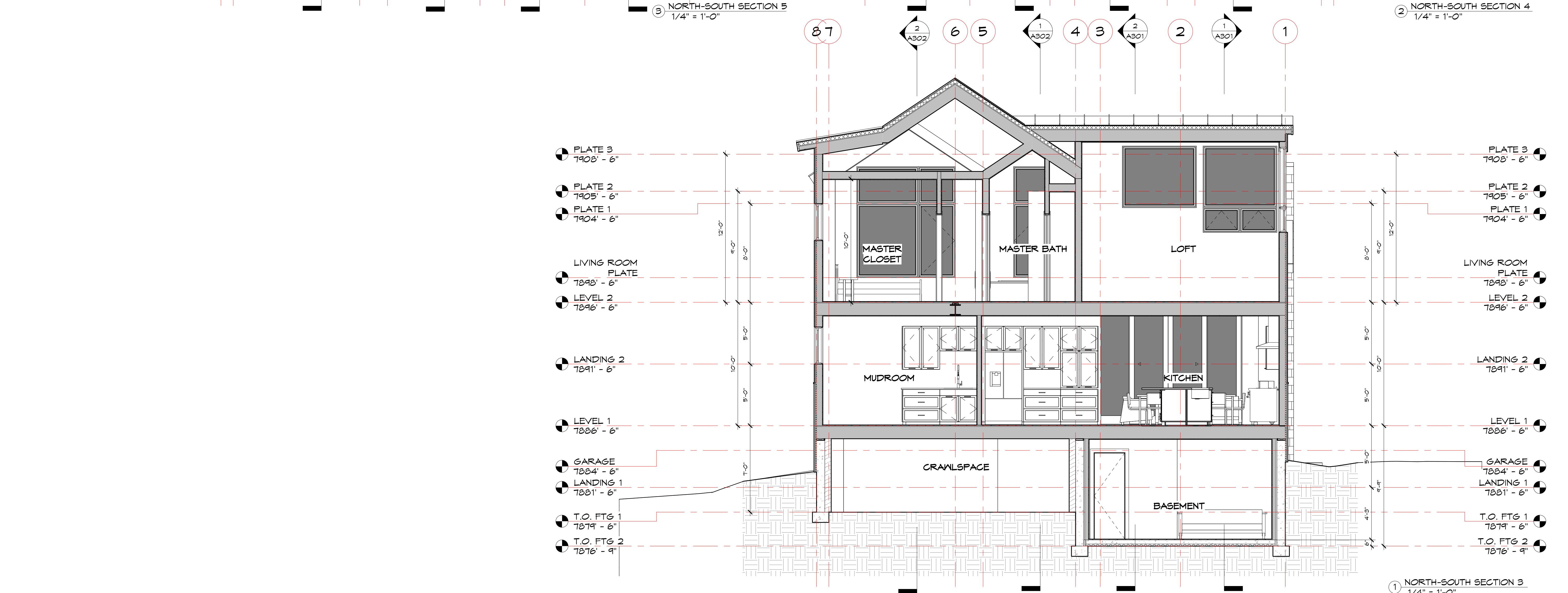
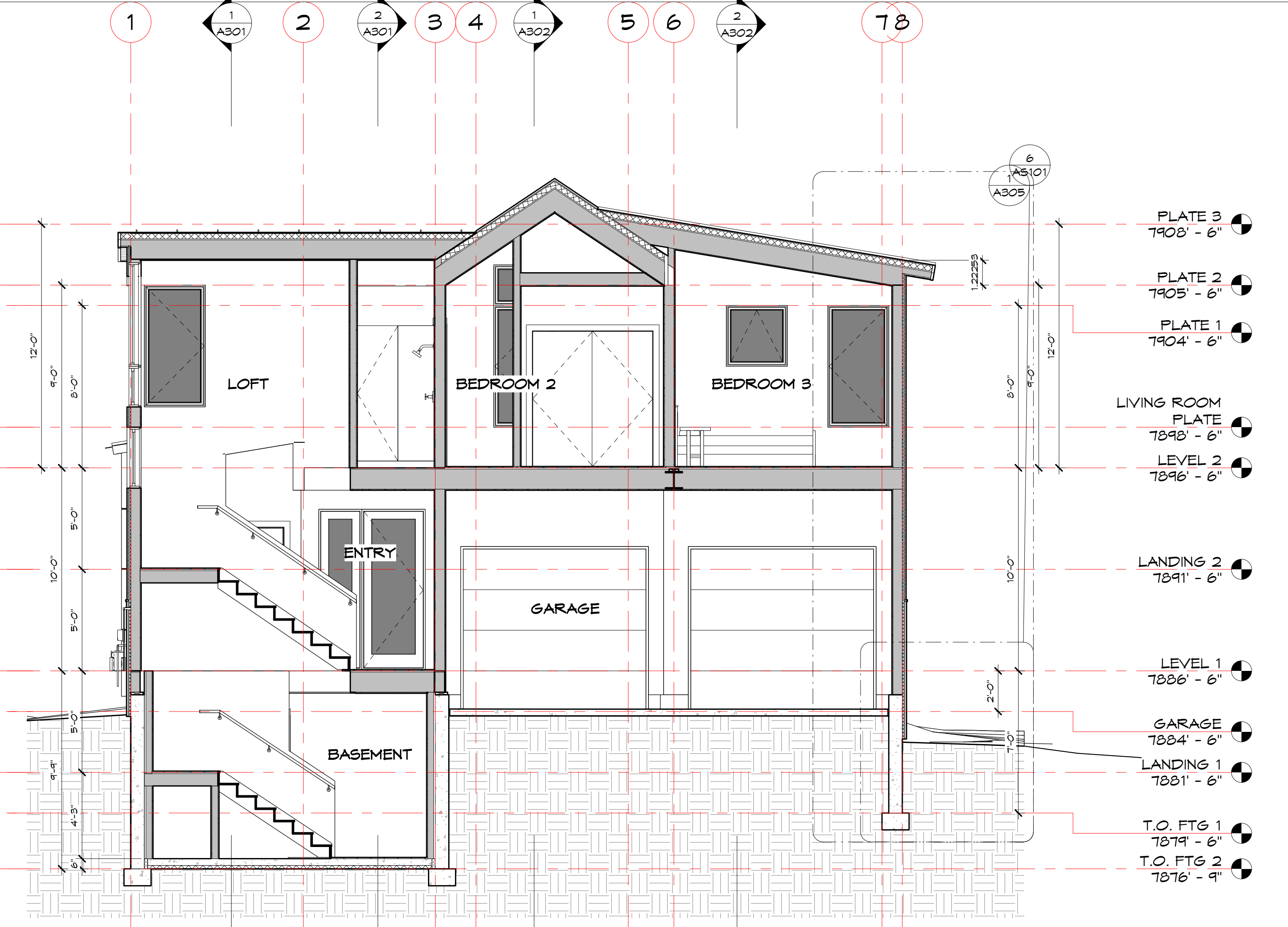
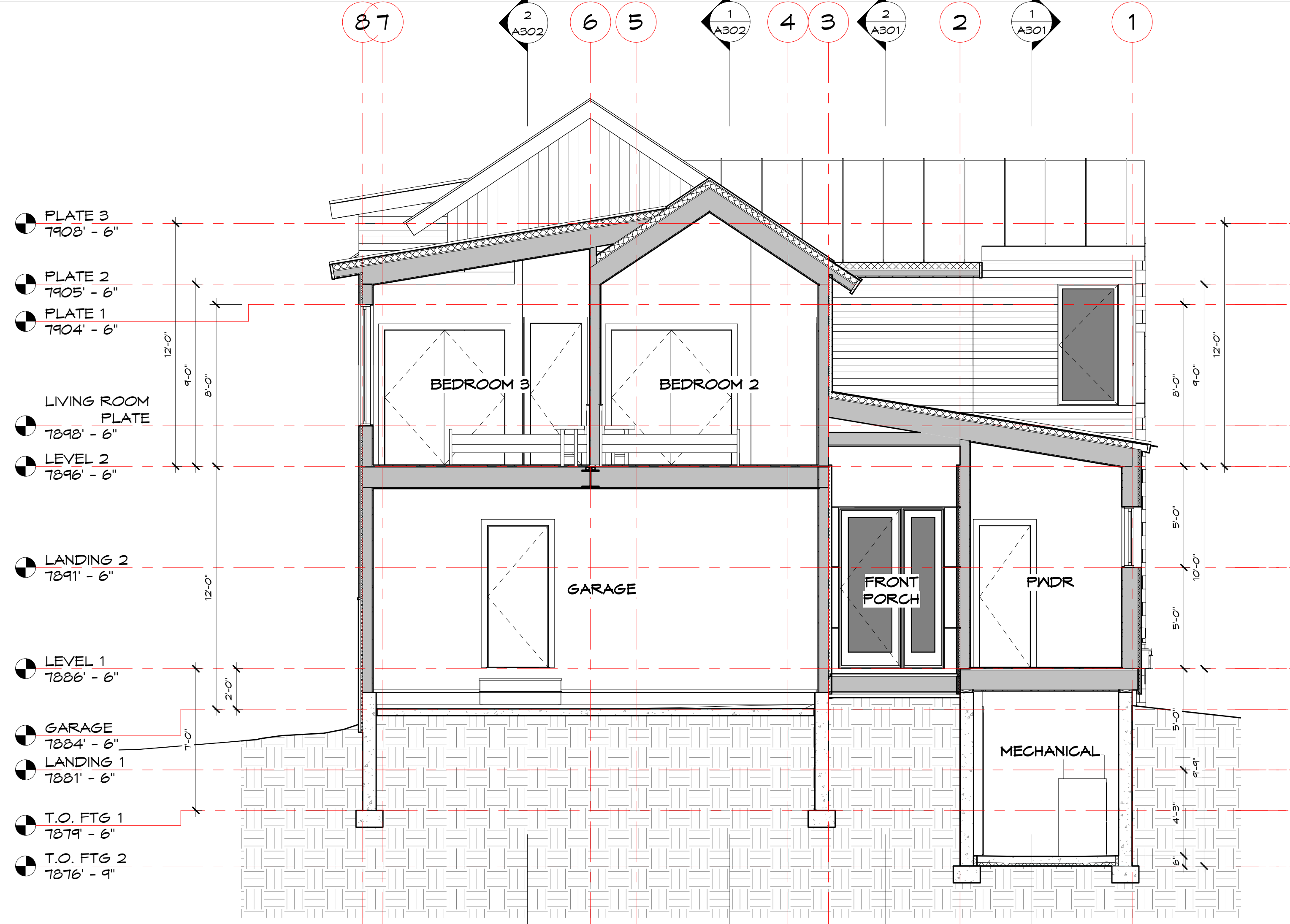
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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
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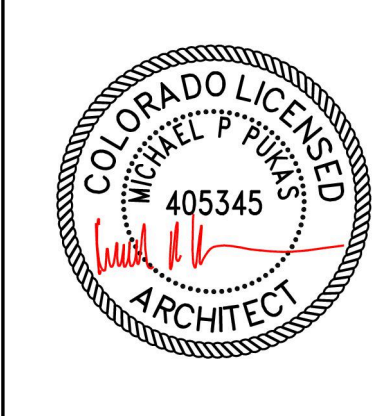
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**BUILDING SECTIONS**





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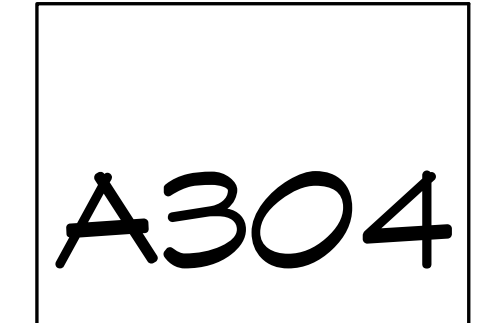
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Parcel Number: 2103-351-02-003  
South Minturn Addition, Lot 35

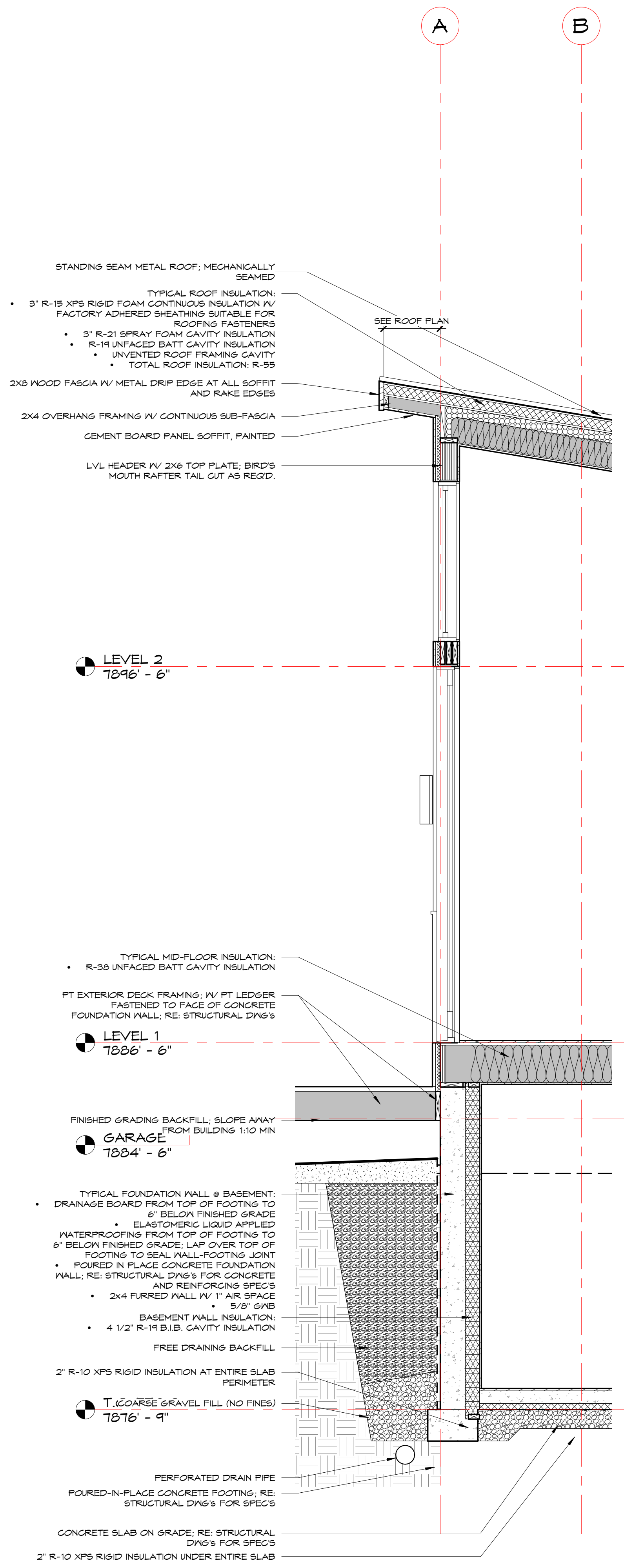
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| # | Description     | Date     |
|---|-----------------|----------|
| 1 | DRB Submission  | 03/18/21 |
| 2 | Building Permit | 05/07/21 |
| 4 | Revision 1      | 06/14/21 |
|   |                 |          |
|   |                 |          |
|   |                 |          |

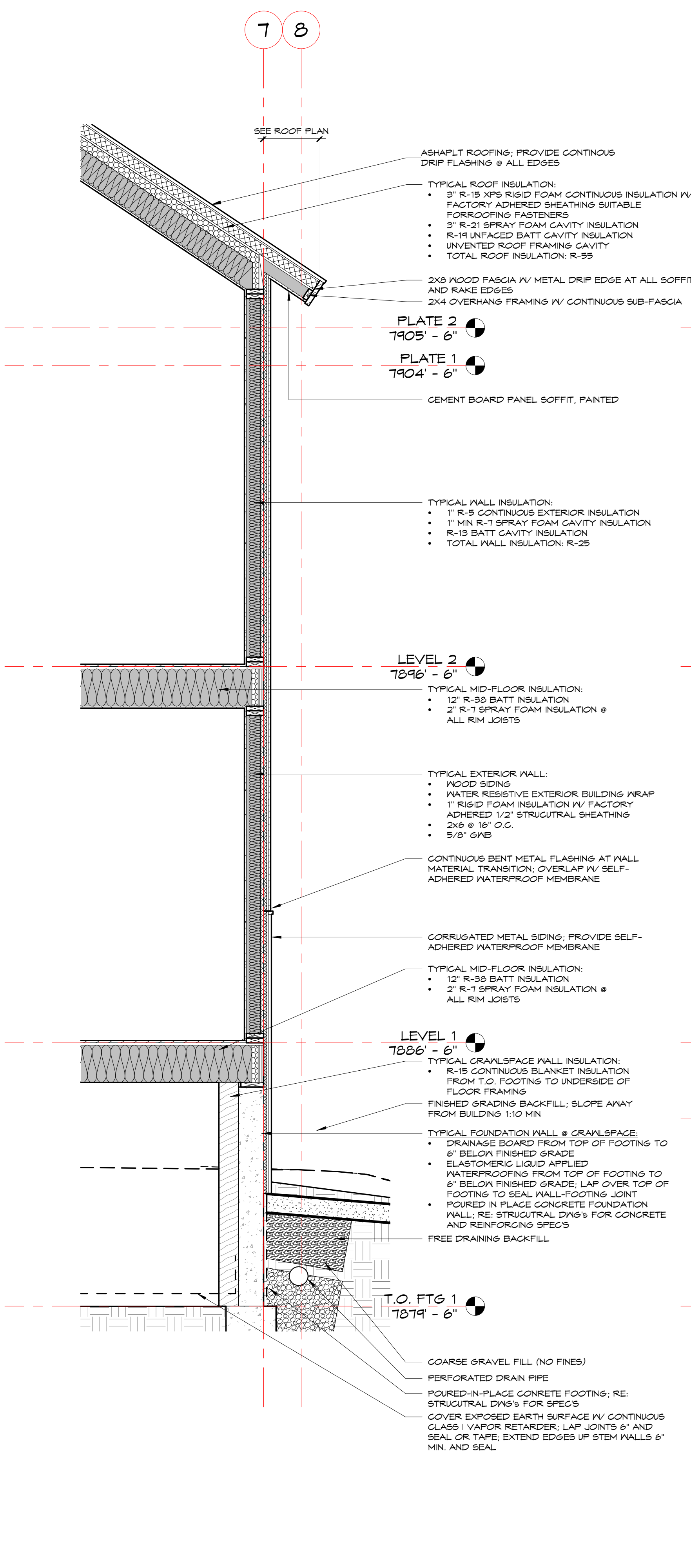
Project Number: 2005  
Designed by: mpp  
Drawn by: mpp  
Checked by: mpp

**BUILDING SECTIONS**

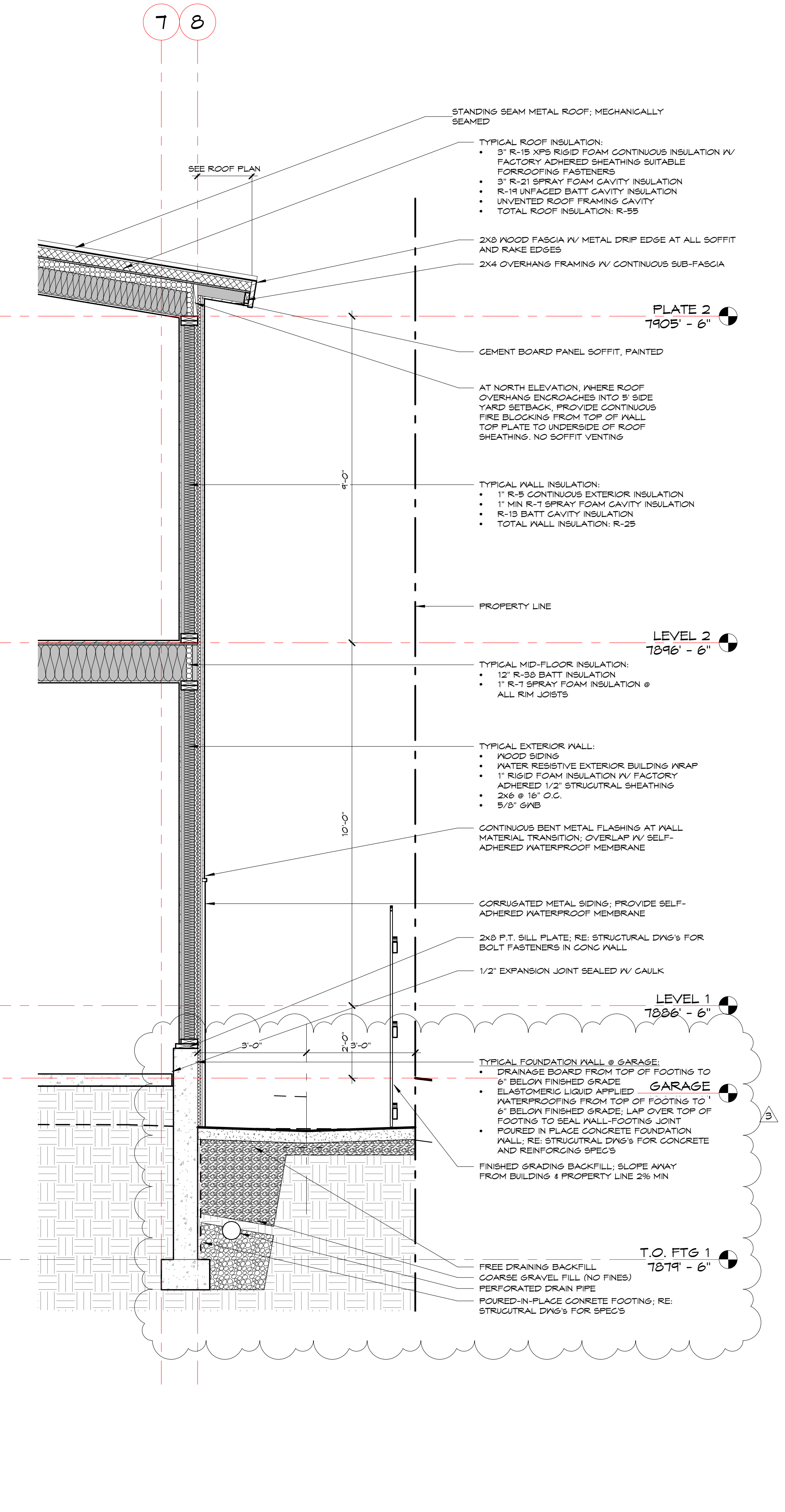




3 WALL SECTION 3  
1/2" = 1'-0"

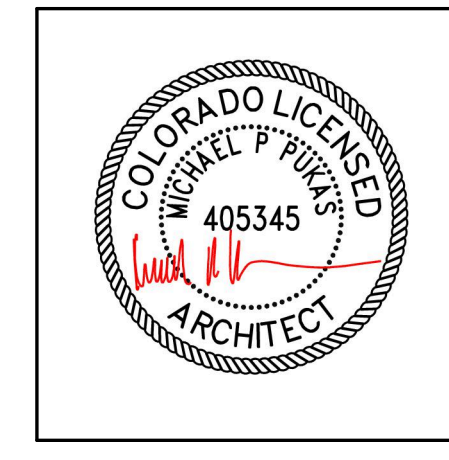


2 WALL SECTION 2  
1/2" = 1'-0"



1 WALL SECTION 1  
1/2" = 1'-0"

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| # | Description      | Date     |
|---|------------------|----------|
| 1 | Building Permit  | 05/01/21 |
| 2 | Blgd Permit Revs | 05/19/21 |
| 3 |                  |          |
| 4 | Revision 1       | 06/14/21 |
|   |                  |          |
|   |                  |          |

Project Number: 2005  
Designed by: mpp  
Drawn by: mpp  
Checked by: mpp

WALL SECTIONS

A305

## DRAWING SYMBOLS AND ABBREVIATIONS

|   |  |                                    |   |                  |                                |                 |                                |
|---|--|------------------------------------|---|------------------|--------------------------------|-----------------|--------------------------------|
| <p>DETAIL NUMBER<br/>X<br/>X<br/>X</p>    | <p>DETAIL, SECTION, OR ELEVATION REFERENCE</p> | <p>XX'-XX''<br/>T.O. SLAB</p>      | <p>TOP OF SLAB ELEVATION</p>              | <p>A. BOLT</p>   | <p>ANCHOR BOLT</p>             | <p>L</p>        | <p>ANGLE (AISC)</p>            |
| <p>DETAIL LOCATION<br/>(SHEET NUMBER)</p> | <p>DETAIL, SECTION, OR ELEVATION REFERENCE</p> | <p>XX'-XX''<br/>T.O. SHEATHING</p> | <p>TOP OF PLYWOOD SHEATHING ELEVATION</p> | <p>ARCH.</p>     | <p>ARCHITECTURAL</p>           | <p>LBS.</p>     | <p>POUNDS</p>                  |
| <p>DETAIL NUMBER</p>                      | <p>DETAIL, SECTION, OR ELEVATION REFERENCE</p> | <p>XX'-XX''<br/>T.O. DECKING</p>   | <p>TOP OF DECKING ELEVATION</p>           | <p>B.O.</p>      | <p>BOTTOM OF</p>               | <p>LL</p>       | <p>DOUBLE ANGLE (AISC)</p>     |
| <p>DETAIL LOCATION<br/>(SHEET NUMBER)</p> | <p>DETAIL, SECTION, OR ELEVATION REFERENCE</p> | <p>(A)</p>                         | <p>PAD FOOTING MARK</p>                   | <p>BCI</p>       | <p>I-JOIST (BOISE CASCADE)</p> | <p>LVL</p>      | <p>LAMINATED VENEER LUMBER</p> |
| <p>DETAIL NUMBER</p>                      | <p>PLAN DETAIL REFERENCE</p>                   | <p>(A)</p>                         | <p>REVISION NUMBER MARK</p>               | <p>BRG.</p>      | <p>BEARING</p>                 | <p>MAX.</p>     | <p>MAXIMUM</p>                 |
| <p>DETAIL LOCATION<br/>(SHEET NUMBER)</p> | <p>TOP OF WALL ELEVATION</p>                   | <p>STEP<br/>T.O.W.</p>             | <p>LOCATION OF STEP IN TOP OF WALL</p>    | <p>C</p>         | <p>CHANNEL (AISC)</p>          | <p>MC</p>       | <p>MISC. CHANNEL (AISC)</p>    |
| <p>T.O.W.<br/>XX'-XX''</p>                | <p>TOP OF FOOTING ELEVATION</p>                | <p>STEP<br/>T.O.F.</p>             | <p>LOCATION OF STEP IN TOP OF FOOTING</p> | <p>C.J.</p>      | <p>CONTROL JOINT</p>           | <p>MIN.</p>     | <p>MINIMUM</p>                 |
| <p>T.O.F.<br/>XX'-XX''</p>                | <p>TOP OF CONCRETE ELEVATION</p>               |                                    |   | <p>CMU</p>       | <p>CONCRETE MASONRY UNIT</p>   | <p>N.S.</p>     | <p>NEAR SIDE</p>               |
| <p>T.O.C.<br/>XX'-XX''</p>                | <p>TOP OF BRICK / STONE LEDGE ELEVATION</p>    |                                    |   | <p>CONC.</p>     | <p>CONCRETE</p>                | <p>o.c.</p>     | <p>ON-CENTER SPACING</p>       |
| <p>T.O.L.<br/>XX'-XX''</p>                | <p>TOP OF STEEL ELEVATION</p>                  |                                    |   | <p>CONT.</p>     | <p>CONTINUOUS</p>              | <p>plf</p>      | <p>POUNDS PER LINEAL FOOT</p>  |
| <p>(XX'-XX'')</p>                         | <p>TOP OF PAD FOOTING ELEVATION</p>            |                                    |   | <p>CTR.</p>      | <p>CENTER</p>                  | <p>psf</p>      | <p>POUNDS PER SQUARE FOOT</p>  |
| <p>(XX'-XX'')</p>                         |  |                                    |   | <p>DIA.</p>      | <p>DIAMETER</p>                | <p>psi</p>      | <p>POUNDS PER SQUARE INCH</p>  |
|   |  |                                    |   | <p>DL</p>        | <p>DEAD LOAD</p>               | <p>PRE-ENG.</p> | <p>PRE-ENGINEERED MEMBER</p>   |
|   |  |                                    |   | <p>EXP.</p>      | <p>EXPANSION</p>               | <p>PROJ.</p>    | <p>PROJECTED</p>               |
|   |  |                                    |   | <p>EXT.</p>      | <p>EXTERIOR</p>                | <p>RE:</p>      | <p>REFERENCE; REFER TO</p>     |
|   |  |                                    |   | <p>F.O.</p>      | <p>FACE OF</p>                 | <p>REINF.</p>   | <p>REINFORCE (MENT)</p>        |
|   |  |                                    |   | <p>F.S.</p>      | <p>FAR SIDE</p>                | <p>REV.</p>     | <p>REVISION</p>                |
|   |  |                                    |   | <p>FDN.</p>      | <p>FOUNDATION</p>              | <p>SL</p>       | <p>SNOW LOAD</p>               |
|   |  |                                    |   | <p>FT.</p>       | <p>FEET</p>                    | <p>STD.</p>     | <p>STANDARD</p>                |
|   |  |                                    |   | <p>GL</p>        | <p>GLU-LAMINATED MEMBER</p>    | <p>T&amp;G</p>  | <p>TONGUE AND GROOVE</p>       |
|   |  |                                    |   | <p>H.A.S.</p>    | <p>HEADED ANCHOR STUD</p>      | <p>T.O.</p>     | <p>TOP OF</p>                  |
|   |  |                                    |   | <p>HORIZ. H.</p> | <p>HORIZONTAL</p>              | <p>TS</p>       | <p>TUBE STEEL (AISC)</p>       |
|   |  |                                    |   | <p>HSS</p>       | <p>HOLLOW STEEL SECTION</p>    | <p>TYP.</p>     | <p>TYPICAL</p>                 |
|   |  |                                    |   | <p>IN.</p>       | <p>INCHES</p>                  | <p>VERT. V.</p> | <p>VERTICAL</p>                |
|   |  |                                    |   | <p>INT.</p>      | <p>INTERIOR</p>                | <p>W</p>        | <p>WIDE FLANGE (AISC)</p>      |
|   |  |                                    |   | <p>JT.</p>       | <p>JOINT</p>                   | <p>W.W.F.</p>   | <p>WELDED WIRE FABRIC</p>      |
|   |  |                                    |   | <p>KIP</p>       | <p>1,000 POUNDS</p>            | <p>WL</p>       | <p>WIND LOAD</p>               |
|   |  |                                    |   | <p>klf</p>       | <p>KIPS PER LINEAL FOOT</p>    | <p>WT</p>       | <p>TEE SECTION (AISC)</p>      |
|   |  |                                    |   | <p>ksf</p>       | <p>KIPS PER SQUARE FOOT</p>    |                 |                                |
|   |  |                                    |   | <p>ksi</p>       | <p>KIPS PER SQUARE INCH</p>    |                 |                                |

## GENERAL STRUCTURAL NOTES

**STRUCTURAL DESIGN CRITERIA:**

- These General Structural Notes apply to these Structural Drawings and supplement the project Specifications. Refer to the Project Manual for additional requirements.
- This project is located in Minturn, Colorado and has been designed in accordance with the 2015 Edition of the International Residential Code (including applicable amendments and supplements).
- Design Loads:**

|                        |  |                             |
|------------------------|--|-----------------------------|
| A. Roof Live Load:     | Uniform Snow   | 100 psf                     |
| B. Floor Live Loads:   | Residential  | 40 psf                      |
|                        | Light Storage  | 125 psf                     |
|                        | Stairs & Corridors<br>Balconies & Decks                                  | 100 psf<br>100 psf          |
| C. Wind Analysis:      | Basic Wind Speed<br>Exposure   | 90 mph (3-second gust)<br>C |
| D. Seismic Analysis:   | Seismic Design Category 'B'  |                             |
| E. Frost / Fdn. Depth: | 48" minimum below exterior ground surface to bottom of footing elevation |                             |
- Structural Design References:
  - American Concrete Institute (ACI): ACI 301, ACI 318, ACI 530
  - American Institute of Steel Construction (AISC)
  - American Institute of Timber Construction (AITC)
  - American Iron and Steel Institute (AISI)
  - American Plywood Association (APA)
  - American Society of Civil Engineers (ASCE): ASCE 7
  - American Society for Testing and Materials (ASTM)
  - American Welding Society (AWS)
  - National Design Specification for Wood Construction (NDS)
  - International Residential Code (IRC)

**STRUCTURAL CONCRETE:**

- Concrete has been designed and shall be constructed in accordance with the American Concrete Institute (ACI). Refer to the "Field Observations" paragraph of these General Structural Notes for observation requirements. All concrete shall be of stone aggregate, unless noted otherwise. Refer to the Project Manual and Specifications for additional requirements (durability, color, finish, etc.). **The use of any concrete additive containing chlorides is prohibited.**
- Minimum 28-day compressive strength shall be as follows:

|                       |           |
|-----------------------|-----------|
| A. Foundation Walls   | 3,000 psi |
| B. Footings           | 3,000 psi |
| C. Slabs on Grade     | 4,000 psi |
| D. Topping Slabs      | 4,000 psi |
| E. All other concrete | 3,000 psi |
- Reinforcing is to be new billet steel ASTM A615 Grade 60 (field bent or welded bars shall be ASTM A706, Grade 60). No welding of reinforcement is permitted unless detailed. No splices of reinforcement are permitted except as detailed or authorized by Structural Engineer. Provide corner and bars to match all horizontal reinforcing. **Provide minimum (2)#5 bars around all sides of all openings in concrete and extend 2'-0" minimum past edges of openings.** Where permitted, use contact lap splices (40 bar diameters minimum lap). Welded wire fabric (W.W.F.) shall be in accordance with ASTM A185. Provide a minimum (1) full mesh lap at splice locations.
- Placing of Reinforcement: Form ties are to be used for bar supports only when the clear dimensions shown on the details can be maintained. Provide wire chairs, bolsters, additional reinforcement, and accessories necessary to support reinforcement at position shown in the Structural Drawings. Support of reinforcement on wood, brick, or other unacceptable material will not be permitted.
- The following minimum concrete cover over reinforcing shall be provided unless noted otherwise:

|   |              |
|---|--------------|
| A. Concrete cast against and permanently exposed to earth     | 3 inches     |
| B. Concrete exposed to earth or weather (#5 bars and smaller) | 1-1/2 inches |
| C. Piers, Beams and Columns                                   | 1-1/2 inches |

**STRUCTURAL STEEL:**

- Structural steel, including embedded angles, plates or other sections has been designed and shall be detailed and erected in accordance with the American Institute of Steel Construction (AISC), ASD Specifications and Code of Standard Practice. Steel detailing and erection shall accommodate provisions of AISC and OSHA standards (including OSHA Steel Erection Standard Part 1926, Subpart "K").
- Minimum yield strengths (Fy) are 35 ksi for pipes (ASTM A53, Grade B), 46 ksi for tubes (ASTM A509, Grade B), 50 ksi for wide flange members (ASTM A572, Grade 50), and 36 ksi for all others (ASTM A36).
- Connections: Use standard framed beam connections meeting requirements of the AISC Manual of Steel Construction (ASD). Use 3/4" diameter minimum A325N bolts or welded equivalent. Minimum welds are per AISC and not less than 3/16" continuous fillet using E70XX electrodes (unless noted otherwise). Welding of rebar anchors to angles or plates shall be completed to develop a minimum 150% of the yield strength of the reinforcing bar. Headed stud anchors shall conform to AWS D1.1 and shall be automatically end welded in the shop (unless noted otherwise). See specifications for testing requirements.

**STRUCTURAL TIMBER:**

- Structural Timber has been designed and shall be constructed in conformance with provisions of the NDS Specification for Stress Grade Lumber and its Fastenings. The Contractor shall not cut, notch, or otherwise modify timber members without the written consent of the Structural Engineer. Provide cross-briding at 8'-0" o.c. maximum, and provide solid blocking between joists at all bearing supports.
- Framing Lumber (minimum requirements) shall be as follows:

|                              |   |
|------------------------------|---|
| A. Wall Studs                | Hem-Fir (HF) Construction Grade           |
| B. Floor Joists (2x members) | Hem-Fir (HF) #2 Grade or Better           |
| C. Floor Beams               | Douglas-Fir (DF) #1 Grade Beam & Stringer |
| D. Roof Rafters (2x members) | Hem-Fir (HF) #2 Grade or Better           |
| E. Roof Beams                | Douglas-Fir (DF) #1 Grade Beam & Stringer |
| F. Posts / Columns           | Douglas-Fir (DF) #1 Grade Post & Timber   |
| G. T&G Wood Decking          | Douglas-Fir (DF) Commercial Grade         |
- Wall, roof, and floor sheathing shall meet the minimum requirements of the APA and AITC. Provide thicknesses and fasten to framing members as noted in the Structural Drawings. Panels are to be oriented to span along their strong axis, and all panel joints are to be staggered.
- Pre-fabricated structural connectors shall be as manufactured by Simpson Strong-Tie Company or approved equivalent.

**STRUCTURAL TIMBER (CONT.):**

- I-joists (BCI) shall be as manufactured by Boise Cascade Corporation or approved equivalent. Provide member size and series as noted in the Structural Drawings. The Supplier shall furnish shop drawings showing all joist members, bridging, blocking, and miscellaneous accessories for review by the Structural Engineer prior to installation. Refer to the Manufacturer Installation Guide for further construction requirements.
- Laminated Veneer Lumber (LVL) shall be as manufactured by Boise Cascade Corporation or approved equivalent. Provide member size as noted in the Structural Drawings (Fb = 2,800 psi minimum). Refer to the Manufacturer Installation Guide for further construction requirements.
- Glue-Laminated (GL) members shall be Visually Graded Western Species, conforming to combination 24F-V4 (multiple span beams shall be 24F-V8) strength and stiffness requirements, unless otherwise noted. Ship members to the jobsite stored in manufacturer's protective wrapping. Refer to Architectural drawings for appearance grade requirements.
- Pre-Engineered wood roof trusses shall be designed by a registered Professional Engineer licensed in the State of Colorado. Calculated live load deflections of all trusses shall not exceed 1/240 of the span length. Shop drawings and calculations bearing the seal and signature of the design engineer shall be submitted for the review of the Structural Engineer. These submittals shall indicate the design loads, locations of all trusses, connection plate sizes and capacities, and the size and grade of lumber to be used. Shop drawing review by the Structural Engineer must be completed prior to truss fabrication. The truss manufacturer shall indicate and provide blocking at bearing locations and lateral bridging as required for truss stability.

**FOUNDATIONS:**

- The structure shall be founded upon spread footings placed upon APPROVED undisturbed native soils with a maximum allowable bearing pressure of 1,500 psf.
- Retaining walls have been designed in accordance with the following design values:

|                            |         |
|----------------------------|---------|
| A. Coefficient of Friction | 0.50    |
| B. Lateral Earth Pressure  | 60 pcf  |
| C. Passive Pressure        | 350 pcf |
- Refer to Geotechnical Report #21-3494, dated 06-06-2021, prepared by LKP Engineering, Inc.; Eagle, Colorado for other pertinent soils information. The Contractor shall be fully familiar with all aspects of the report before beginning construction and shall refer to the report for specifications and details for placement of structural fill, vapor barriers, gravel layers, perimeter drains, and any other soils related items.
- Foundation walls are designed to be supported top and bottom by floor construction. Walls are not to be backfilled until such floors are in place or adequate shoring is provided.
- Provide 1-1/2" void below all non-bearing partitions constructed upon concrete slabs-on-grade.

**The Soils Engineer shall review all foundation excavations prior to the placement of formwork or reinforcement. All structural fill shall be observed and tested by the Soils Engineer during the grading and fill placement process.**

**The Contractor shall be fully familiar with all aspects of these reports before beginning construction and shall refer to these reports for specifications and details for placement of structural fill, gravel layers, perimeter drains, and any other soils related items.**

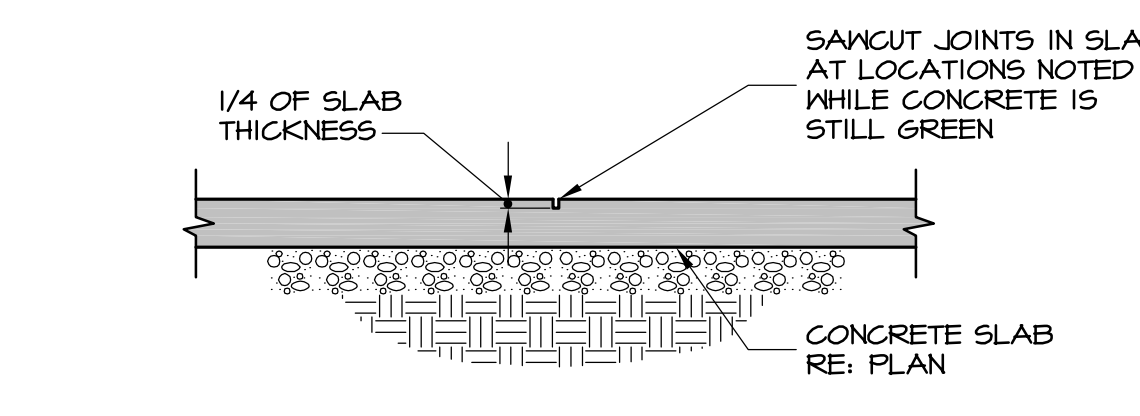
**FIELD OBSERVATIONS:**

- The Contractor shall inform the Structural Engineer at least 72 hours prior to casting any concrete so as to allow the Structural Engineer the opportunity to review the placement of reinforcement and embedments.

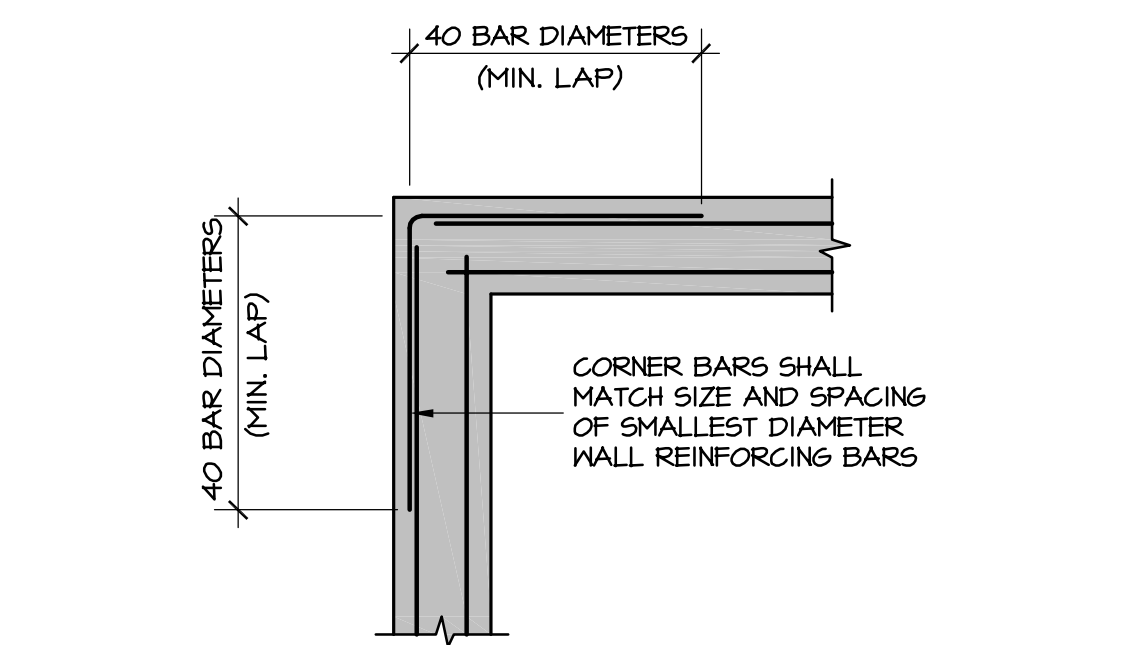
**Contact Sundquist Design Group, Inc. at (303) 838-2222.**

**SPECIAL INSPECTIONS:**

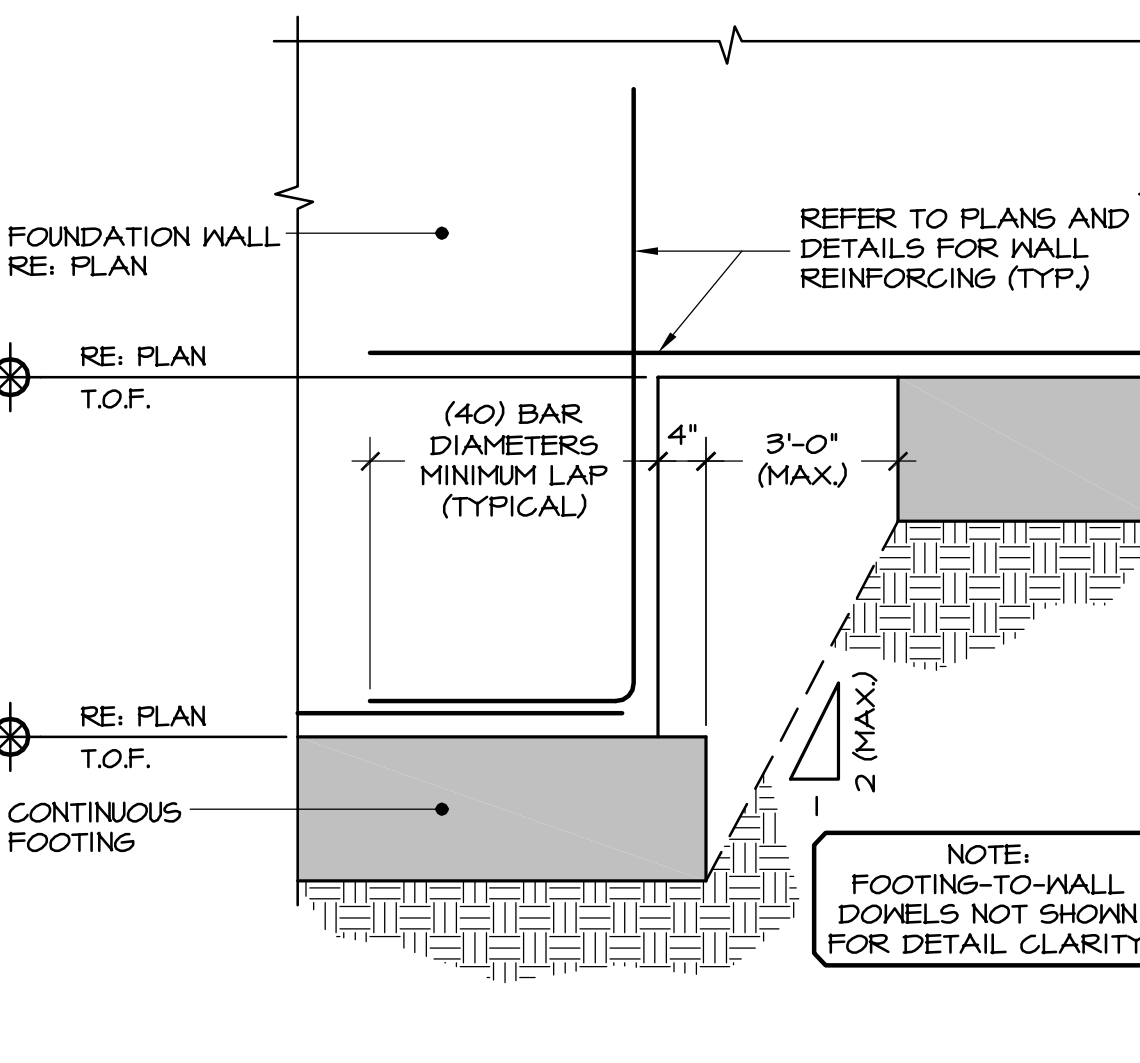
- It is the responsibility of the Contractor to coordinate all structural inspections as required by governing building codes and the Construction Documents. A qualified independent testing company is to provide Special Inspections for portions of the Work, as indicated in the Construction Documents and as required by local jurisdictions.
- The Special Inspector shall be responsible for completing, maintaining, and resubmitting all special inspection logs and forms required by Local Building Officials.



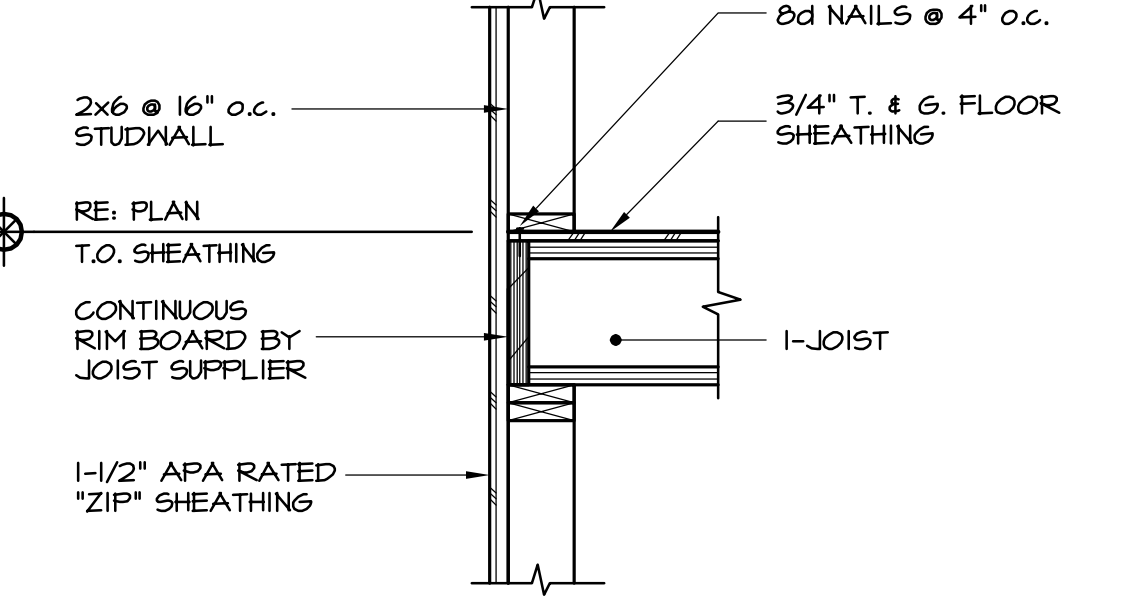
1 Typical Control Joint (C.J.)  
Sl.I 3/4"=1'-0"



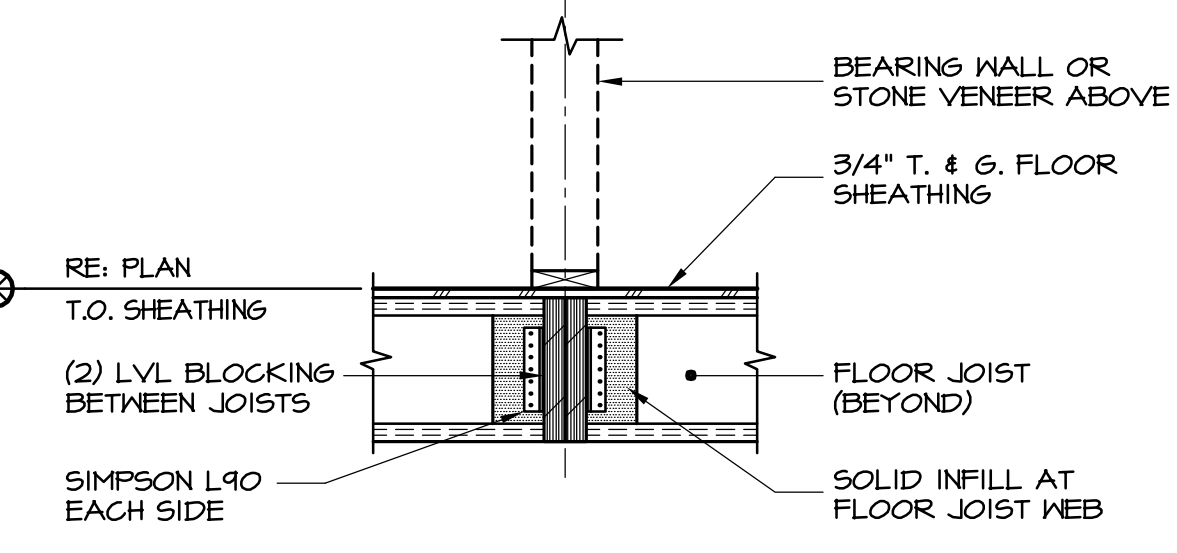
3 Typical Corner Reinforcing  
Sl.I 3/4"=1'-0"



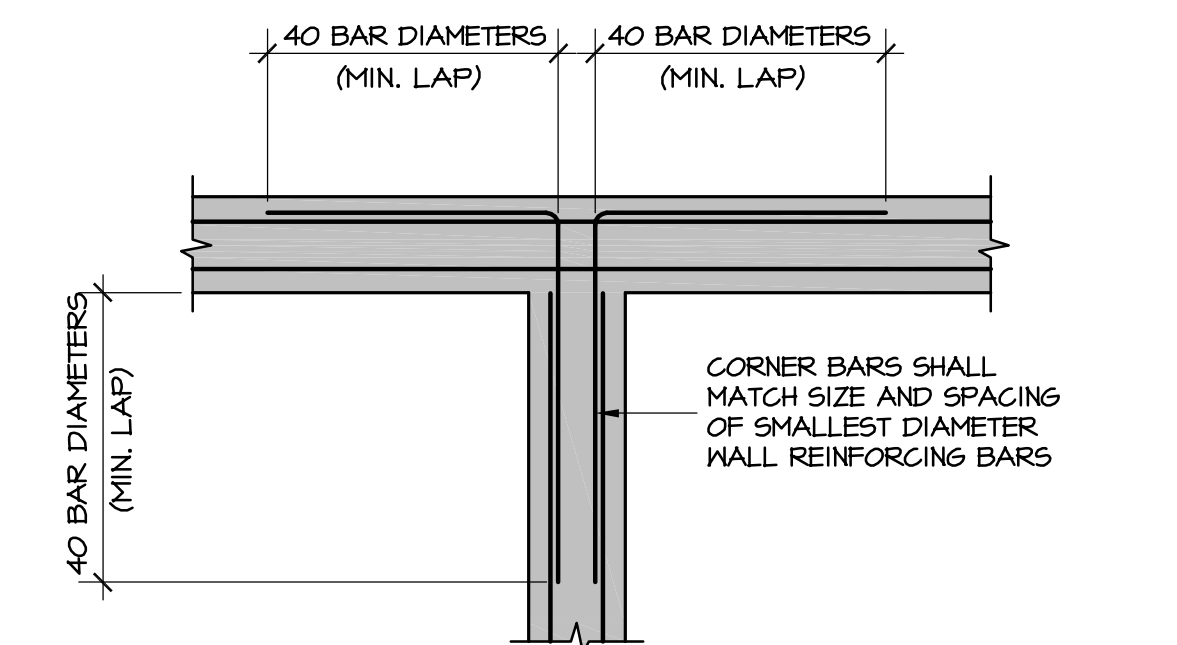
5 Typical Footing Step Detail  
Sl.I 3/4"=1'-0"



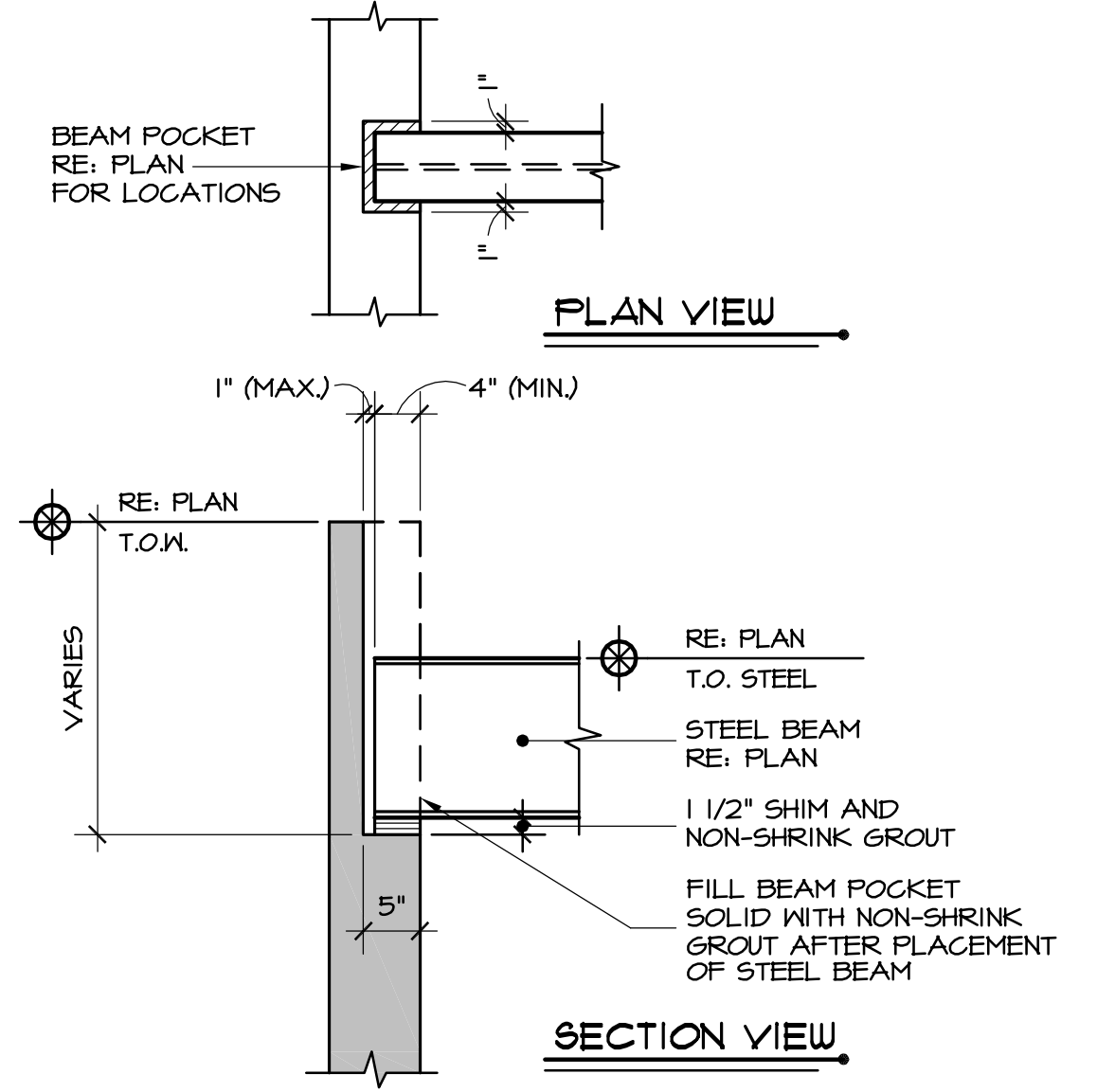
7 Typical Floor Framing Detail  
Sl.I 3/4"=1'-0"



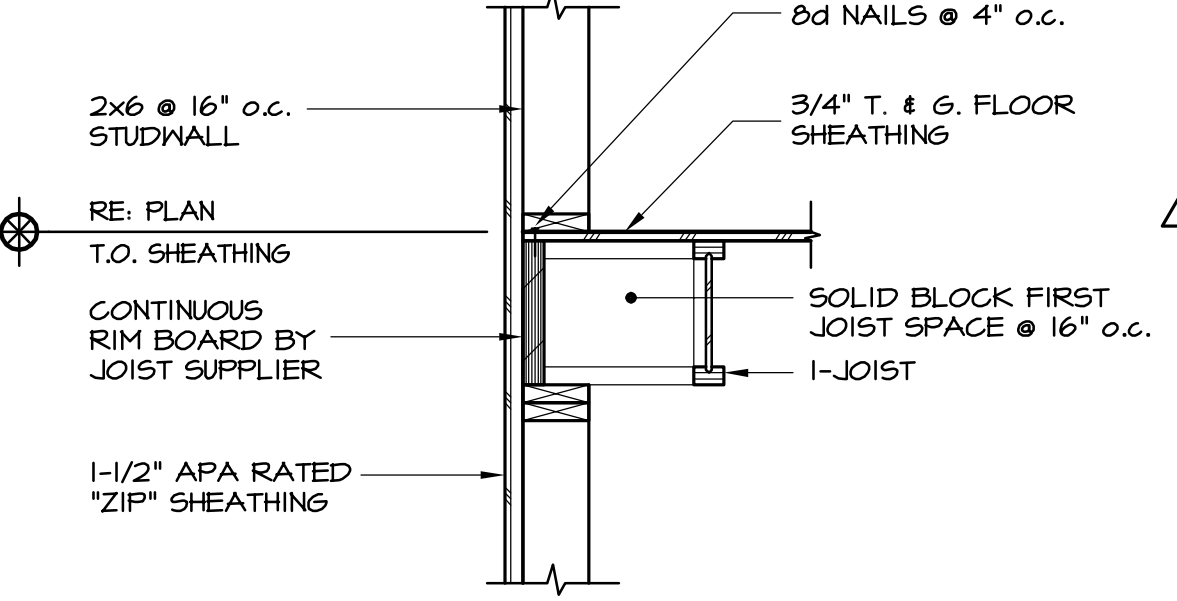
2 Typical Floor Blocking Detail  
Sl.I 3/4"=1'-0"



4 Typical Intersection Reinforcing  
Sl.I 3/4"=1'-0"



6 Typical Beam Pocket Detail  
Sl.I 3/4"=1'-0"



8 Typical Floor Framing Detail  
Sl.I 3/4"=1'-0"



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832 Main Street  
Minturn, Colorado

|                 |                                      |
|-----------------|--------------------------------------|
| Date            | • 06/14/2021                         |
| SDG Project No. | • 21-015                             |
| Drawn By        | • SDG                                |
| Checked By      | • JDS                                |
| Date            | • Issue                              |
| 04/30/21        | • Review Set                         |
| 05/06/21        | • Review Set                         |
| 05/07/21        | • Construction                       |
| 06/14/21        | • Addendum #1                        |
|                 | •                                    |
|                 | •                                    |
| Title           | • General Notes and Standard Details |

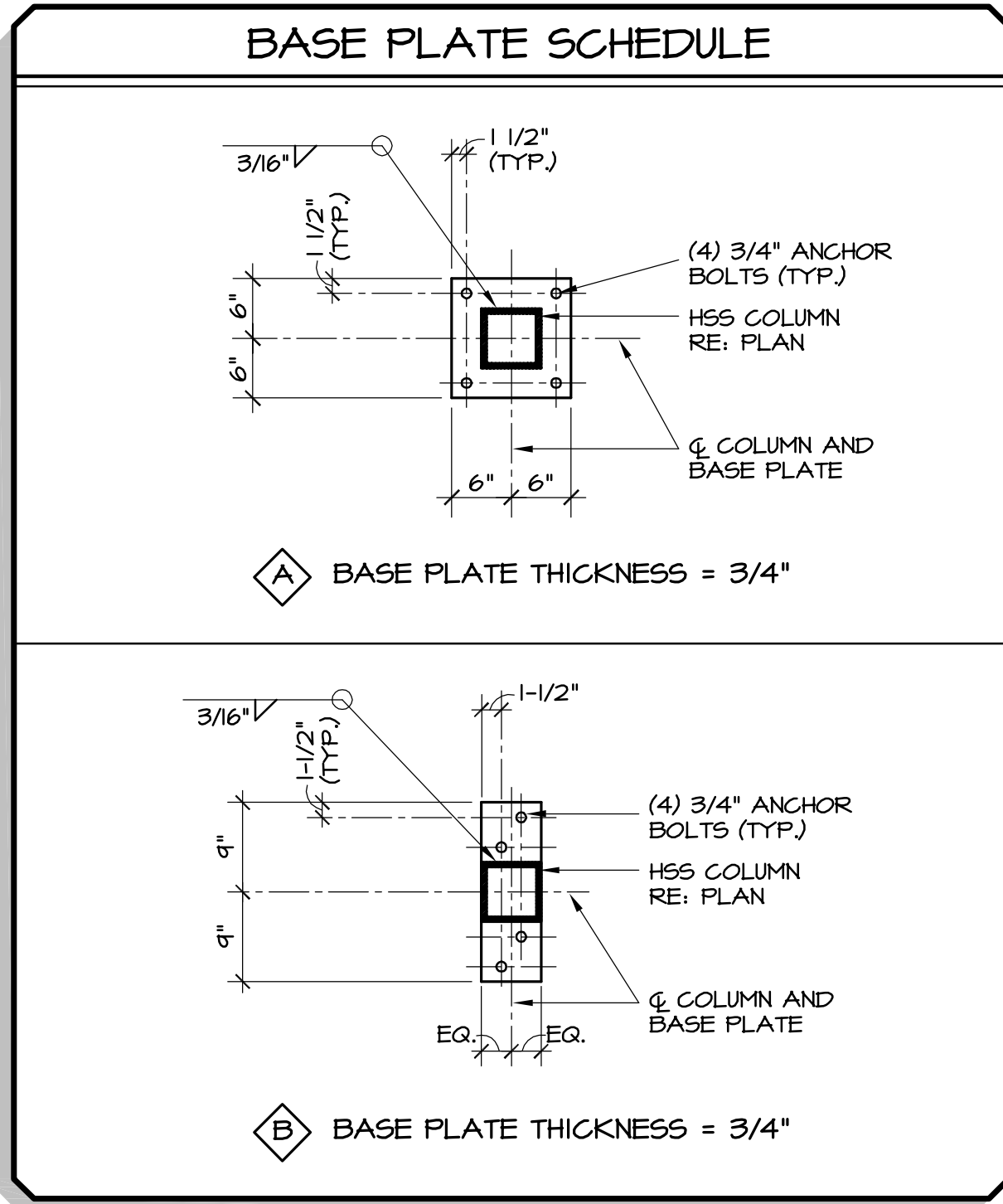
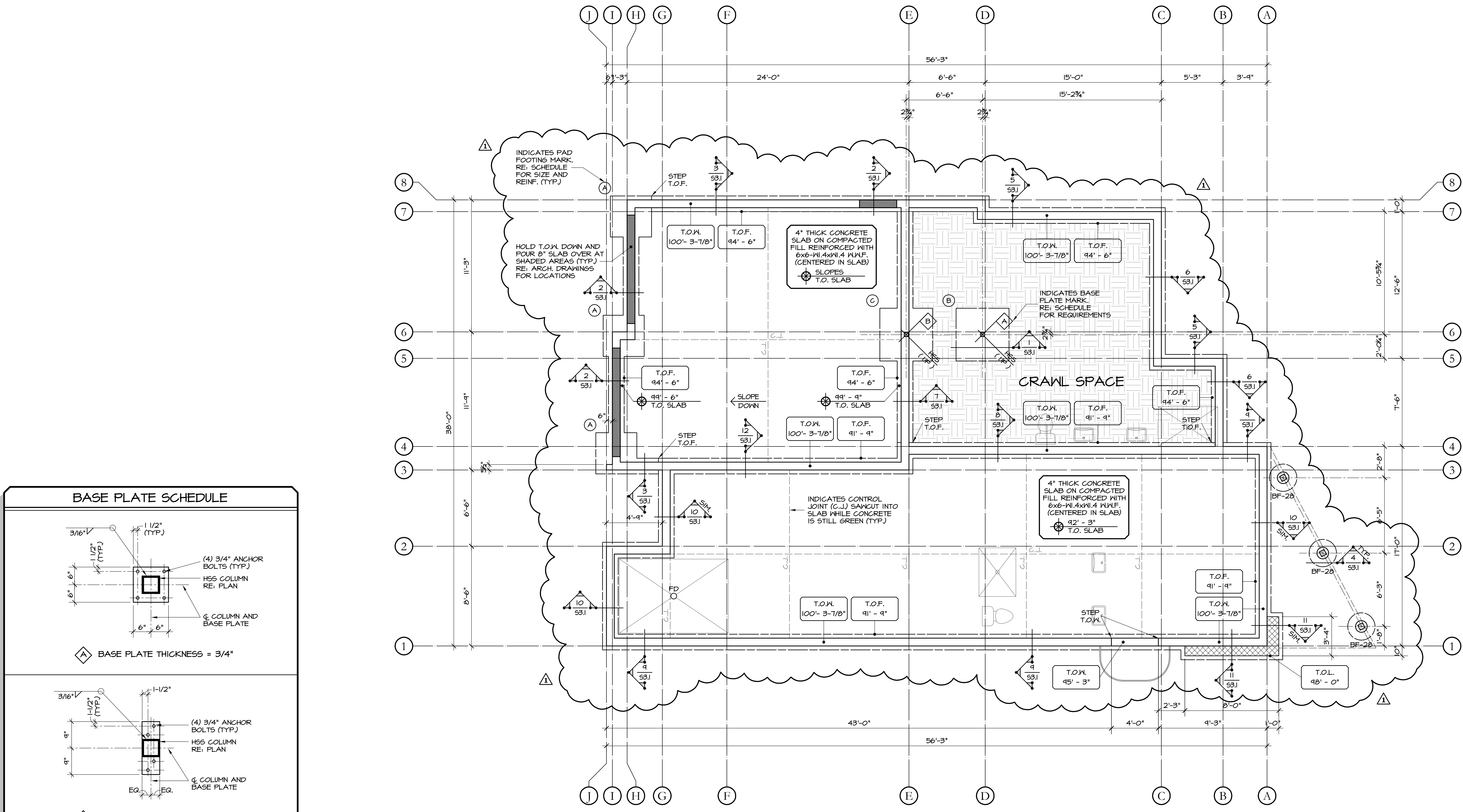


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**PAD FOOTING SCHEDULE**

| MARK | FOOTING DIMENSIONS<br>Plan Size | Depth  | REINFORCING           |
|------|---------------------------------|--------|-----------------------|
| A    | 3'-6" x 3'-6"                   | 0'-10" | (4) # 5 Bars Each Way |
| B    | 4'-6" x 4'-6"                   | 1'-0"  | (5) # 5 Bars Each Way |
| C    | 4'-6" x 4'-6"                   | 0'-10" | (5) # 5 Bars Each Way |
| D    |                                 |        |                       |

NOTES: 1. PAD FOOTINGS ARE TO BE CENTERED BELOW COLUMN CENTERLINES (I.N.O.).  
2. PROVIDE 3" CLEAR DISTANCE FROM BOTTOM OF FOOTING TO REINFORCING.  
3. REFER TO PLAN FOR TOP OF FOOTING ELEVATIONS.

**1 Foundation Plan**  
SCALE: 1/4"=1'-0"  
NORTH

- NOTES:
- REFER TO SHEET S1.1 FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS.
  - REFER TO SOILS REPORT FOR ALL EXCAVATION, FILL, COMPACTION, AND OTHER SUBGRADE CONSIDERATIONS. COORDINATE ALL DRAINAGE, WATERPROOFING, VAPOR BARRIERS AND OTHER PREPARATIONS WITH ARCHITECTURAL DRAWINGS AND SOILS REPORT.
  - REFER TO ARCHITECTURAL DRAWINGS FOR MISC. FOUNDATION PENETRATIONS.
  - REFER TO ARCHITECTURAL DRAWINGS FOR PLUMBING FIXTURE LOCATIONS.
  - S.C. IS TO VERIFY THAT A MINIMUM FROST DEPTH OF 48" HAS BEEN ACHIEVED FOR THE FOUNDATION WALLS AND EXTERIOR PIERS.
  - S.C. TO COORDINATE UFER GROUND LOCATION (CEE).

|                 |              |
|-----------------|--------------|
| Date            | • 06/14/2021 |
| SDG Project No. | • 21-015     |
| Drawn By        | • SDG        |
| Checked By      | • JDS        |

|          |                |
|----------|----------------|
| Date     | • Issue        |
| 04/30/21 | • Review Set   |
| 05/06/21 | • Review Set   |
| 05/20/21 | • Construction |
| 06/14/21 | • Addendum #1  |

Title • Foundation Plan





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SDG Project No. • 21-015  
Drawn By • SDG  
Checked By • JDS

Date • Issue  
04/30/21 • Review Set  
05/06/21 • Review Set  
05/20/21 • Construction  
06/14/21 • Addendum #1

Title • Lower Level Floor  
Framing Plan

**TYPICAL WALL HEADER NOTES**

ALL DOOR, WINDOW AND OTHER BEARING WALL PENETRATIONS ARE TO BE HEADERED WITH INSULATED (2) 1-3/4" x 9-1/2" LVL'S (MINIMUM) OR (3) 2x10'S WITH A SINGLE 2x6 TRIMMER AND A SINGLE 2x6 KING STUD AT EACH END (UNLESS NOTED OTHERWISE).

**I-JOIST HANGER SCHEDULE**

| SINGLE I-JOIST        |                  |                  |
|-----------------------|------------------|------------------|
| JOIST HEIGHT          | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8" BCI 6000 / 60 | ITT3511.88       | IUS2.37/11.88    |
| 11-7/8" BCI 90        | ITT411.88        | IUS3.56/11.88    |

**LVL BEAMS AND HEADER HANGER SCHEDULE**

| 1-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | MIT11.88         | HJ11             |

| 2-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNU3.56/11.88    | HHU5.410         |

| 3-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNU5.50/11.88    | HHU5.50/10       |

| 4-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNU7.25/11.88    | HHU7.25/10       |

**TYPICAL SHEATHING NAILING NOTES**

| APPLICATION    | SHEATHING                             | PANEL EDGE NAILING | INTERMEDIATE (FIELD) NAILING | ATTACHMENTS  |
|----------------|---------------------------------------|--------------------|------------------------------|--|
| FLOORS         | 3/4" T. & G. APA RATED STURD-I-FLOOR  | 6" o.c.            | 10" o.c.                     | 8d RING SHANK NAILS (ALT. 10d COMMON) GLUED WITH CONST. ADHESIVE |
| WALLS (NOTE 4) | 1/2" APA RATED C-C, C-D               | 6" o.c.            | 12" o.c.                     | 8d COMMON NAILS  |
| ROOFS          | 5/8" APA RATED EXPOSURE 1 RATED 40/20 | 6" o.c.            | 12" o.c.                     | 10d COMMON NAILS   |

NOTES:  
1. USE REQUIREMENTS SHOWN ABOVE UNLESS NOTED OTHERWISE.  
2. REFER TO THE 2015 INTERNATIONAL RESIDENTIAL CODE FOR ADDITIONAL SHEATHING REQUIREMENTS.  
3. EDGE NAILING PERPENDICULAR TO FRAMING MEMBERS REQUIRED AT WALL PANELS ONLY.  
4. REFER TO ARCHITECTURAL SPECIFICATIONS FOR EXTERIOR WALL SHEATHING (1-1/2" ZIP SHEATHING) REQUIREMENTS.

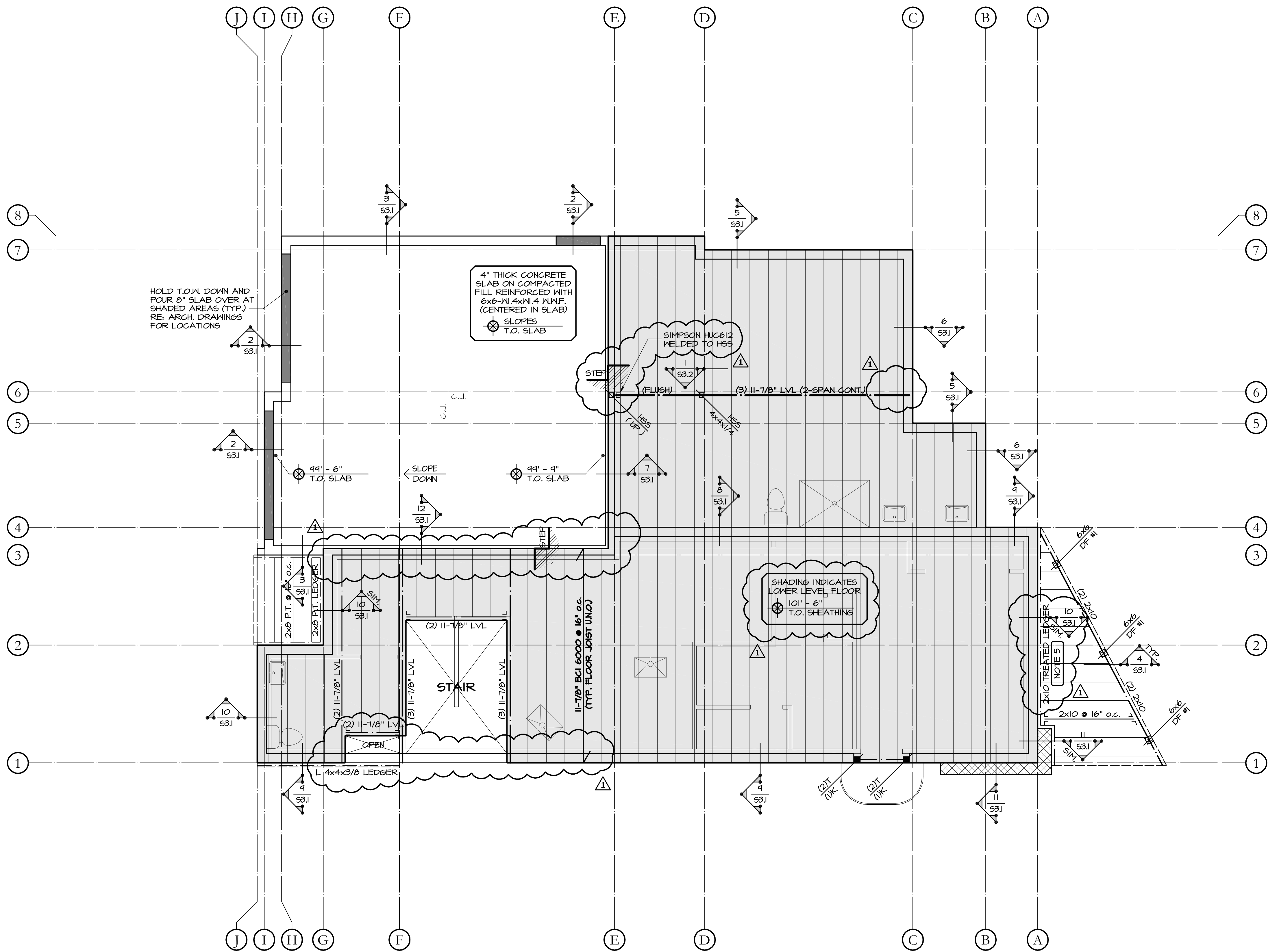
**TYPICAL FRAMING PLAN NOTES**

**PLAN NOTATIONS:**  
GL: GLU-LAMINATED BEAM  
BCI: I-JOIST  
LVL: LAMINATED VENEER BEAM

**BEAM BEARING CONDITION:**  
INDICATES TOTAL NUMBER OF GANG STUDS TO BE LOCATED AT BEAM BEARING LOCATIONS. ALL GANG STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (U.N.O.)

**HEADER BEARING CONDITION:**  
INDICATES NUMBER OF MULTIPLE TRIMMER AND KING STUDS TO BE LOCATED AT HEADER BEARING LOCATIONS. MULTIPLE TRIMMER AND KING STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (U.N.O.)  
T = TRIMMER STUDS  
K = KING STUDS

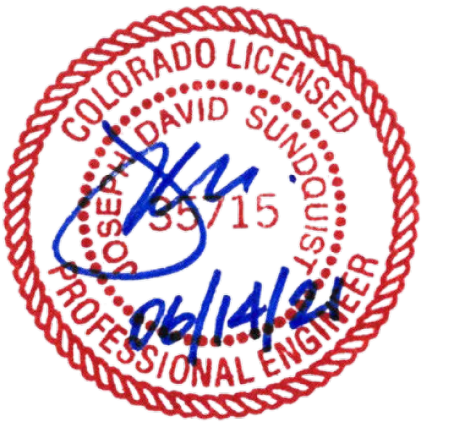
**MISCELLANEOUS CONDITIONS:**  
INDICATES COLUMN / STUDS FROM A LEVEL ABOVE POSTING DOWN TO THE CURRENT FRAMING LEVEL.  
INDICATES COLUMN / STUDS POSTING DOWN TO FRAMING MEMBER NOTED ON THE CURRENT FRAMING LEVEL.



**1 Lower Level Floor Framing Plan** SCALE: 1/4"=1'-0"

NOTES:  
1. REFER TO SHEET S11 FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS.  
2. REFER TO ARCHITECTURAL DRAWINGS FOR MISC. FLOOR PENETRATIONS.  
3. ALL FLOOR BEAMS ARE FLUSH-FRAMED (UNLESS NOTED OTHERWISE).  
4. REFER TO ARCHITECTURAL DRAWINGS FOR PLUMBING FIXTURE LOCATIONS.  
5. ATTACH LEDGER TO WALL/RIM WITH (2) ROWS OF 6"-LONG LEDGER-LOCK SCREWS SPACED @ 16" o.c. (MAX).

NORTH



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**JONES RESIDENCE**

832 Main Street  
Minturn, Colorado

**TYPICAL WALL HEADER NOTES**

ALL DOOR, WINDOW AND OTHER BEARING WALL PENETRATIONS ARE TO BE HEADERED WITH INSULATED (2) 1-3/4" x 9-1/2" LVL'S (MINIMUM) OR (3) 2x10'S WITH A SINGLE 2x6 TRIMMER AND A SINGLE 2x6 KING STUD AT EACH END (UNLESS NOTED OTHERWISE).

**I-JOIST HANGER SCHEDULE**

| SINGLE I-JOIST        |                  |                  |
|-----------------------|------------------|------------------|
| JOIST HEIGHT          | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8" ECI 6000 / 60 | ITT3511.08       | IUS2.37/11.08    |
| 11-7/8" ECI 90        | ITT411.08        | IUS3.56/11.08    |

**LVL BEAMS AND HEADER HANGER SCHEDULE**

| 1-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | MIT11.08         | HU11             |

| 2-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS56/11.08     | HHUS410          |

| 3-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS50/11.08     | HHUS550/10       |

| 4-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS7.25/11.08   | HHUS7.25/10      |

**TYPICAL SHEATHING NAILING NOTES**

| APPLICATION    | SHEATHING                             | PANEL EDGE NAILING | INTERMEDIATE (FIELD) NAILING | ATTACHMENTS  |
|----------------|---------------------------------------|--------------------|------------------------------|--|
| FLOORS         | 3/4" T. & G. APA RATED STURD-I-FLOOR  | 6" o.c.            | 10" o.c.                     | 8d RING SHANK NAILS (ALT. 10d COMMON) GLUED WITH CONST. ADHESIVE |
| WALLS (NOTE 4) | 1/2" APA RATED C-C, C-D               | 6" o.c.            | 12" o.c.                     | 8d COMMON NAILS  |
| ROOFS          | 5/8" APA RATED EXPOSURE 1 RATED 40/20 | 6" o.c.            | 12" o.c.                     | 10d COMMON NAILS   |

NOTES:  
1. USE REQUIREMENTS SHOWN ABOVE UNLESS NOTED OTHERWISE.  
2. REFER TO THE 2015 INTERNATIONAL RESIDENTIAL CODE FOR ADDITIONAL SHEATHING REQUIREMENTS.  
3. EDGE NAILING PERPENDICULAR TO FRAMING MEMBERS REQUIRED AT WALL PANELS ONLY.  
4. REFER TO ARCHITECTURAL SPECIFICATIONS FOR EXTERIOR WALL SHEATHING (1-1/2" ZIP SHEATHING) REQUIREMENTS.

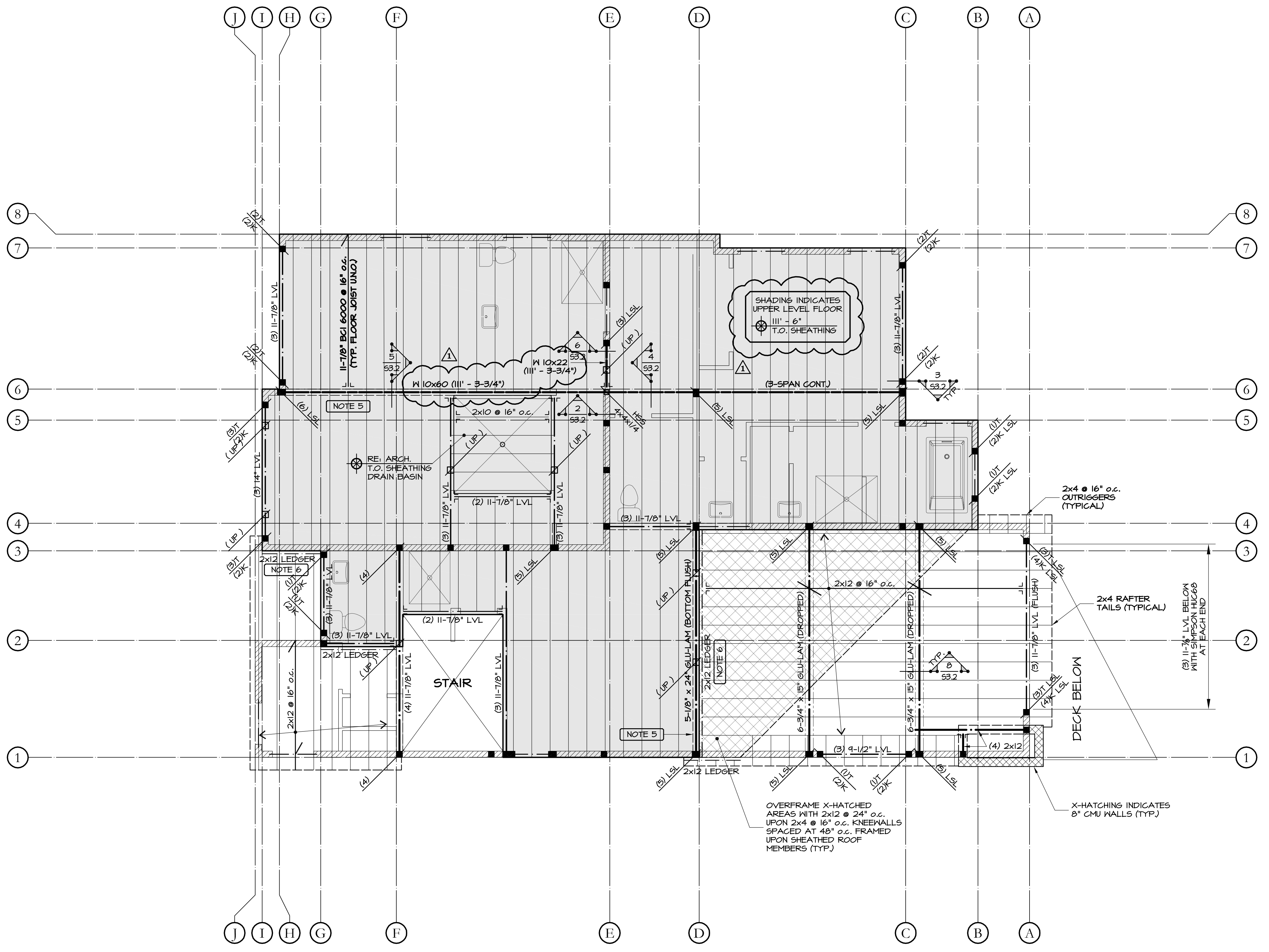
**TYPICAL FRAMING PLAN NOTES**

**PLAN NOTATIONS:**  
GL: GLU-LAMINATED BEAM  
BCI: I-JOIST  
LVL: LAMINATED VENEER BEAM

**BEAM BEARING CONDITION:**  
INDICATES TOTAL NUMBER OF GANG STUDS TO BE LOCATED AT BEAM BEARING LOCATIONS. ALL GANG STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (U.N.O.)

**HEADER BEARING CONDITION:**  
INDICATES NUMBER OF MULTIPLE TRIMMER AND KING STUDS TO BE LOCATED AT HEADER BEARING LOCATIONS. MULTIPLE TRIMMER AND KING STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (U.N.O.)  
T = TRIMMER STUDS  
K = KING STUDS

**MISCELLANEOUS CONDITIONS:**  
[UP] INDICATES COLUMN / STUDS FROM A LEVEL ABOVE, POSTING DOWN TO THE CURRENT FRAMING LEVEL.  
[DN] INDICATES COLUMN / STUDS POSTING DOWN TO FRAMING MEMBER NOTED ON THE CURRENT FRAMING LEVEL.



**1 Upper Level Floor & Low Roof Framing Plan**  
SCALE: 1/4"=1'-0"

- NOTES:
- REFER TO SHEET S11 FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS.
  - REFER TO ARCHITECTURAL DRAWINGS FOR MISC. FLOOR PENETRATIONS.
  - ALL LVL FLOOR AND ROOF BEAMS ARE FLUSH-FRAMED (UNLESS NOTED OTHERWISE).
  - REFER TO ARCHITECTURAL DRAWINGS FOR PLUMBING FIXTURE LOCATIONS.
  - INDICATES 2x6 @ 16" o.c. BEARING WALLS ABOVE.
  - ATTACH LEDGER TO WALL/RIM WITH (2) ROWS OF 6"-LONG LEDGER-LOCK SCREWS SPACED @ 16" o.c. (MAX).

|                 |              |
|-----------------|--------------|
| Date            | • 06/14/2021 |
| SDG Project No. | • 21-015     |
| Drawn By        | • SDG        |
| Checked By      | • JDS        |

|          |                |
|----------|----------------|
| Date     | • Issue        |
| 04/30/21 | • Review Set   |
| 05/06/21 | • Review Set   |
| 05/20/21 | • Construction |
| 06/14/21 | • Addendum #1  |

Title • Upper Level Floor and Low Roof Framing Plan



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Minturn, Colorado

**TYPICAL WALL HEADER NOTES**

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**I-JOIST HANGER SCHEDULE**

| SINGLE I-JOIST        |                  |                  |
|-----------------------|------------------|------------------|
| JOIST HEIGHT          | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8" BCI 6000 / 60 | ITT3511.08       | IUS2.37/11.08    |
| 11-7/8" BCI 90        | ITT411.08        | IUS3.56/11.08    |

**LVL BEAMS AND HEADER HANGER SCHEDULE**

| 1-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | MIT11.08         | HU11             |

| 2-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS5.56/11.08   | HHUS410          |

| 3-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS5.50/11.08   | HHUS5.50/10      |

| 4-PLY : 1-3/4" VERSA-LAM LVL |                  |                  |
|------------------------------|------------------|------------------|
| MEMBER                       | TOP FLANGE MODEL | FACE MOUNT MODEL |
| 11-7/8"                      | HNUS7.25/11.08   | HHUS7.25/10      |

**TYPICAL SHEATHING NAILING NOTES**

| APPLICATION    | SHEATHING                             | PANEL EDGE NAILING | INTERMEDIATE (FIELD) NAILING | ATTACHMENTS  |
|----------------|---------------------------------------|--------------------|------------------------------|--|
| FLOORS         | 3/4" T. & G. APA RATED STURD-I-FLOOR  | 6" o.c.            | 10" o.c.                     | 8d RING SHANK NAILS (ALT. 10d COMMON) GLUED WITH CONST. ADHESIVE |
| WALLS (NOTE 4) | 1/2" APA RATED C-C, C-D               | 6" o.c.            | 12" o.c.                     | 8d COMMON NAILS  |
| ROOFS          | 5/8" APA RATED EXPOSURE 1 RATED 40/20 | 6" o.c.            | 12" o.c.                     | 10d COMMON NAILS   |

NOTES:  
1. USE REQUIREMENTS SHOWN ABOVE UNLESS NOTED OTHERWISE.  
2. REFER TO THE 2015 INTERNATIONAL RESIDENTIAL CODE FOR ADDITIONAL SHEATHING REQUIREMENTS.  
3. EDGE NAILING PERPENDICULAR TO FRAMING MEMBERS REQUIRED AT WALL PANELS ONLY.  
4. REFER TO ARCHITECTURAL SPECIFICATIONS FOR EXTERIOR WALL SHEATHING (1-1/2" ZIP SHEATHING) REQUIREMENTS.

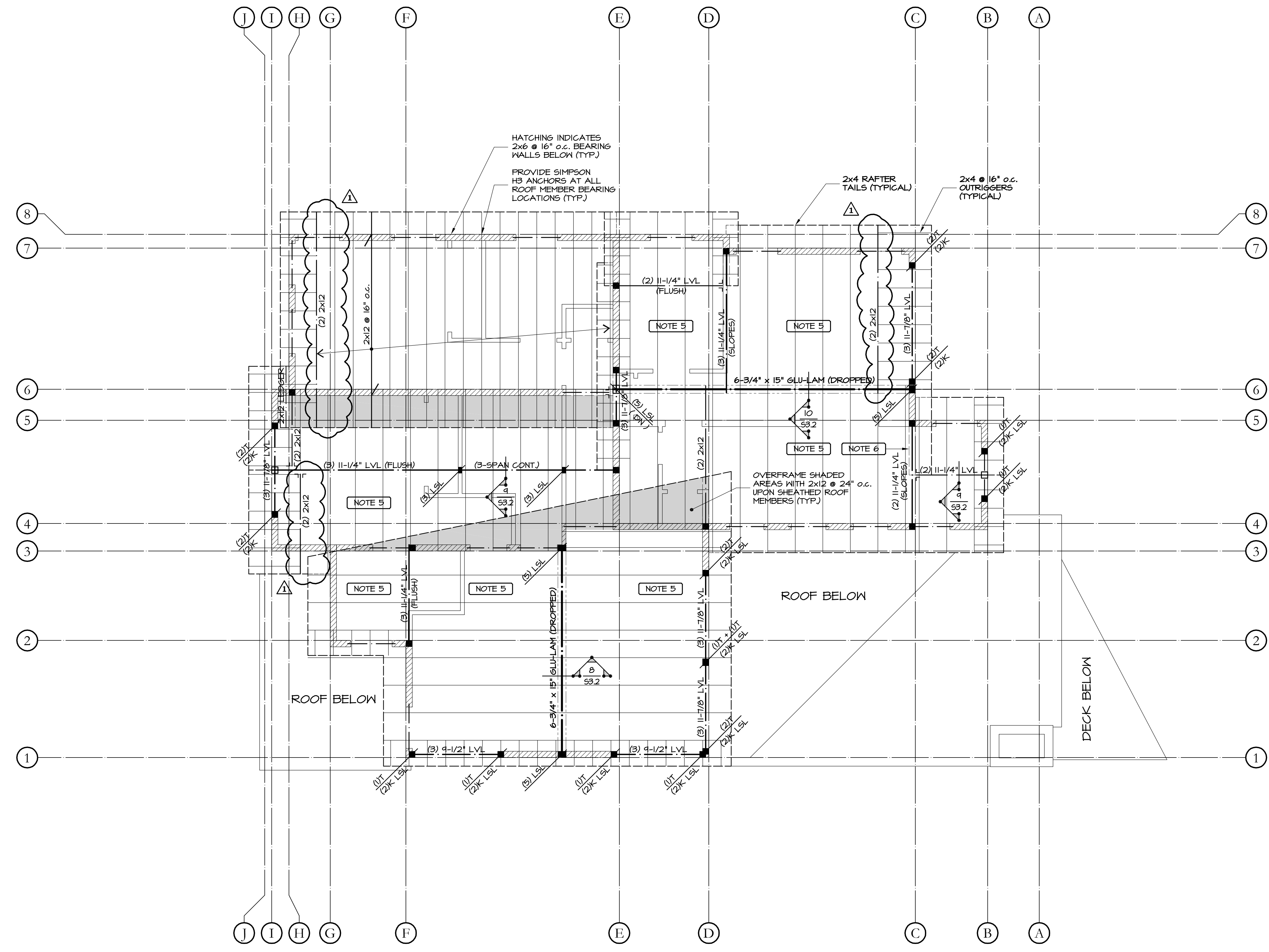
**TYPICAL FRAMING PLAN NOTES**

**PLAN NOTATIONS:**  
GL: GLU-LAMINATED BEAM  
BCI: I-JOIST  
LVL: LAMINATED VENEER BEAM

**BEAM BEARING CONDITION:**  
INDICATES TOTAL NUMBER OF GANG STUDS TO BE LOCATED AT BEAM BEARING LOCATIONS. ALL GANG STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (I.N.O.)

**HEADER BEARING CONDITION:**  
INDICATES NUMBER OF MULTIPLE TRIMMER AND KING STUDS TO BE LOCATED AT HEADER BEARING LOCATIONS. MULTIPLE TRIMMER AND KING STUDS ARE TO TRACK DOWN TO FOUNDATION WALLS (I.N.O.)  
T = TRIMMER STUDS  
K = KING STUDS

**MISCELLANEOUS CONDITIONS:**  
INDICATES COLUMN / STUDS FROM A LEVEL ABOVE, POSTING DOWN TO THE CURRENT FRAMING LEVEL.  
INDICATES COLUMN / STUDS POSTING DOWN TO FRAMING MEMBER NOTED ON THE CURRENT FRAMING LEVEL.



**1 High Roof Framing Plan**  
SCALE: 1/4"=1'-0"  
NORTH

NOTES:  
1. REFER TO SHEET S11 FOR GENERAL STRUCTURAL NOTES AND TYPICAL DETAILS.  
2. REFER TO ARCHITECTURAL DRAWINGS FOR MISC. ROOF PENETRATIONS.  
3. ALL LVL ROOF BEAMS ARE FLUSH-FRAMED (UNLESS NOTED OTHERWISE).  
4. ALL HEAVY TIMBER ROOF BEAMS ARE TO BE DROPPED (UNLESS NOTED OTHERWISE).  
5. ALL ROOF RAFTERS ARE 2x12 @ 24" o.c. (I.N.O.).  
6. INDICATES 2x6 @ 16" o.c. KNEEWALL DOWN TO LOWER ROOF PLANE.

|                 |              |
|-----------------|--------------|
| Date            | • 06/14/2021 |
| SDG Project No. | • 21-015     |
| Drawn By        | • SDG        |
| Checked By      | • JDS        |

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|----------|----------------|
| Date     | • Issue        |
| 04/30/21 | • Review Set   |
| 05/06/21 | • Review Set   |
| 05/07/21 | • Construction |
| 06/14/21 | • Addendum #1  |

Title • High Roof Framing Plan



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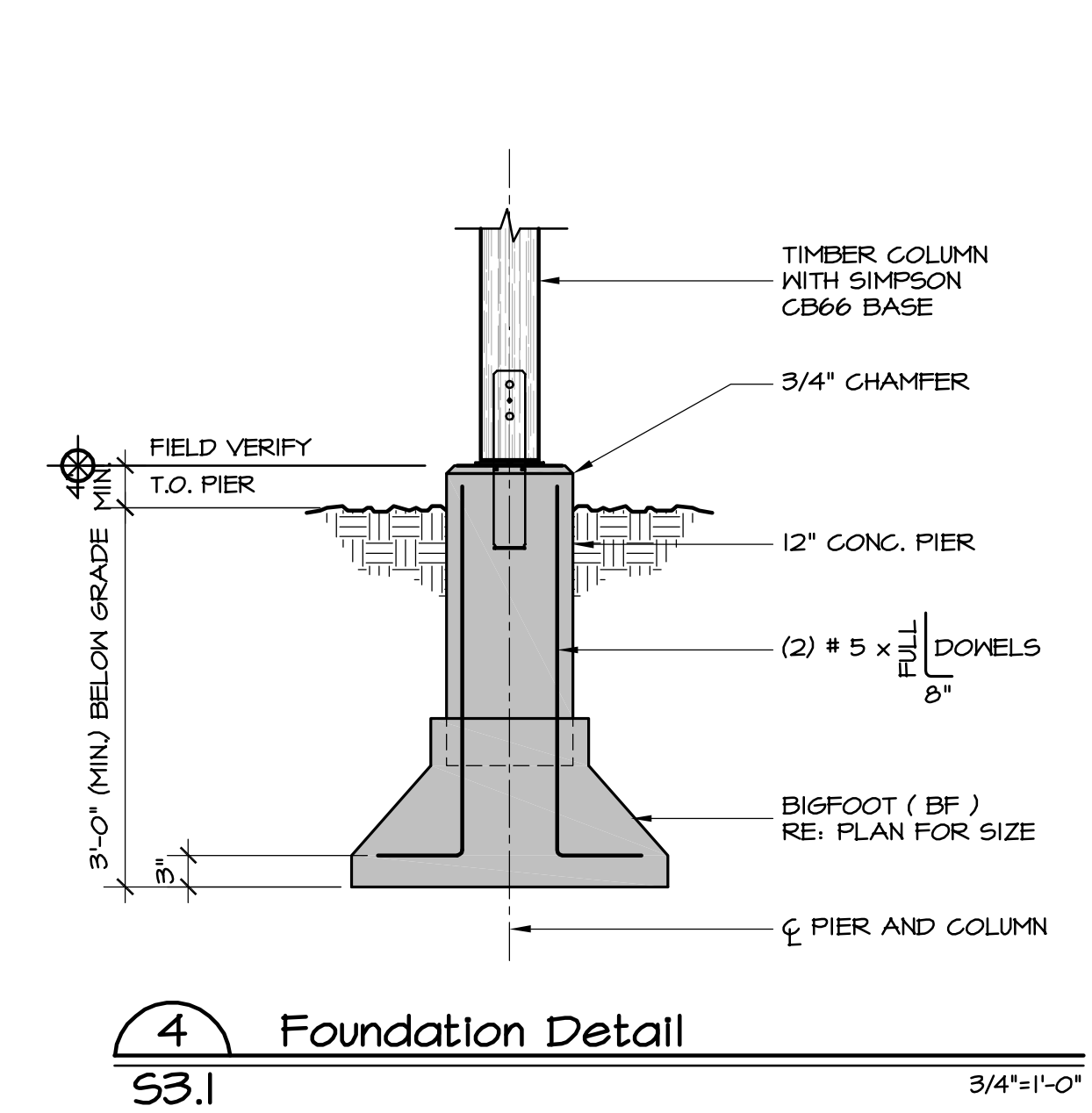
832 Main Street  
Minturn, Colorado

|                 |              |
|-----------------|--------------|
| Date            | • 06/14/2021 |
| SDG Project No. | • 21-015     |
| Drawn By        | • SDG        |
| Checked By      | • JDS        |

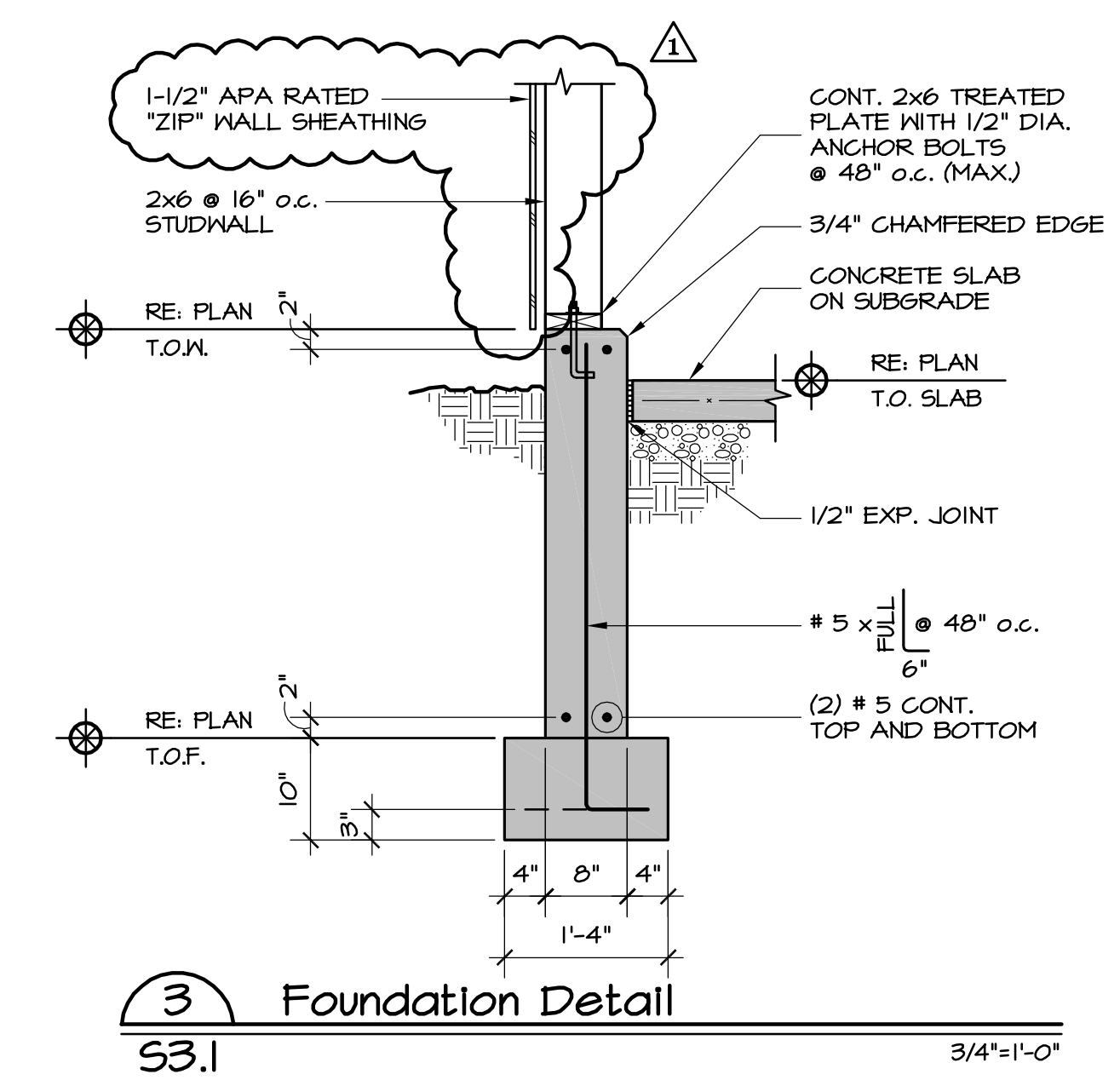
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|----------|----------------|
| Date     | • Issue        |
| 05/06/21 | • Review Set   |
| 05/07/21 | • Construction |
| 06/14/21 | • Addendum #1  |

Title • Foundation Details

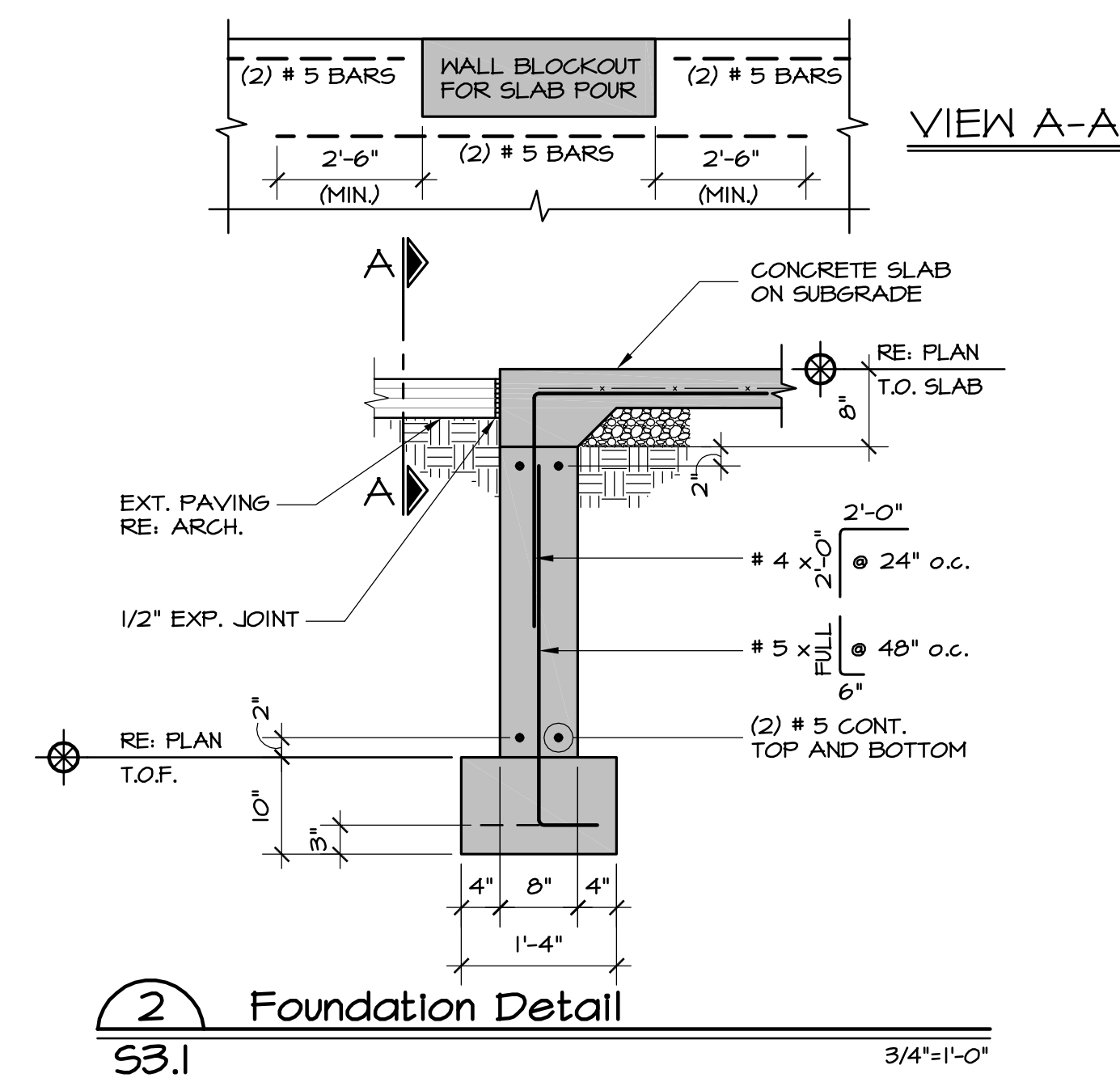
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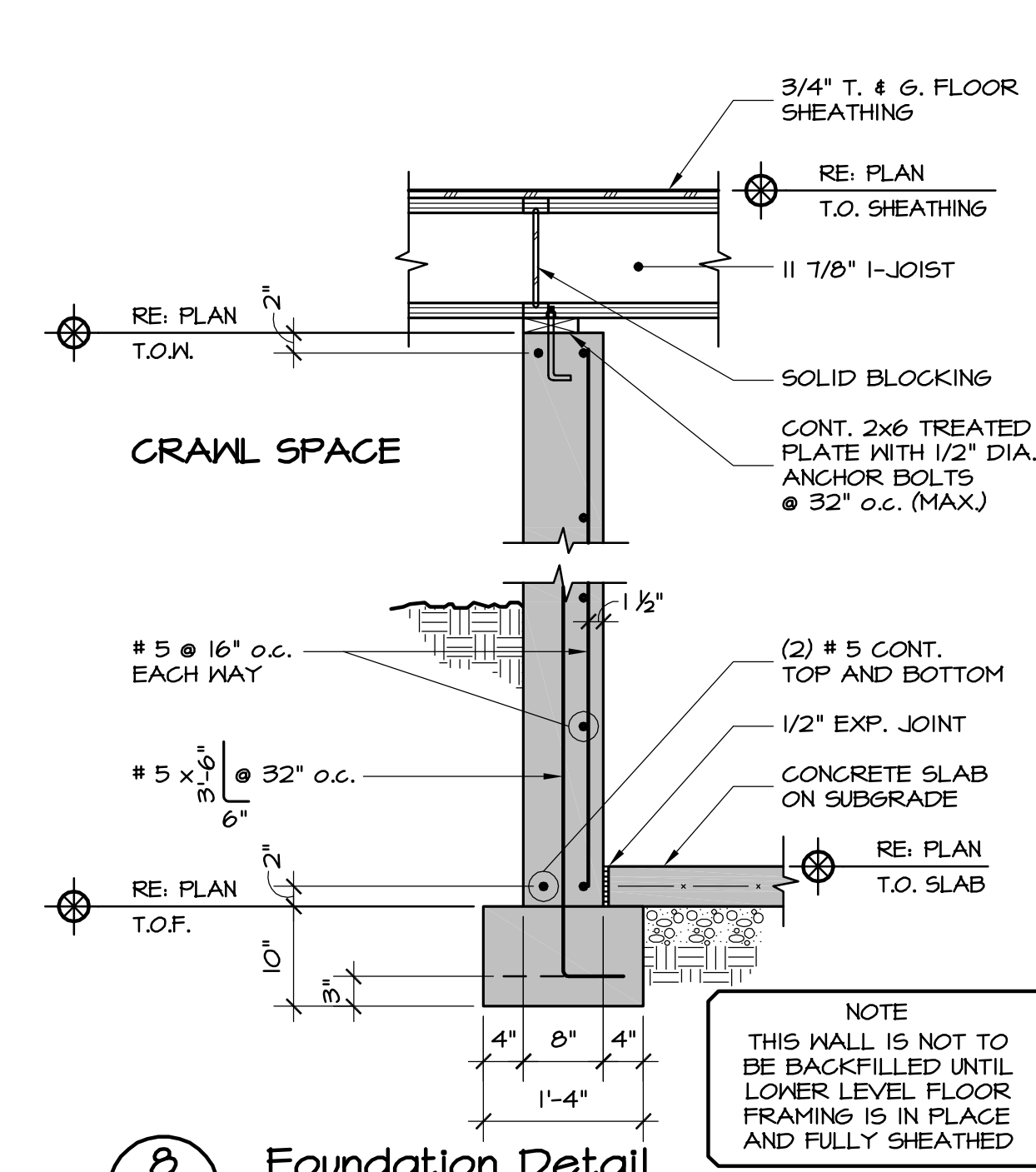
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S3.1 3/4"=1'-0"



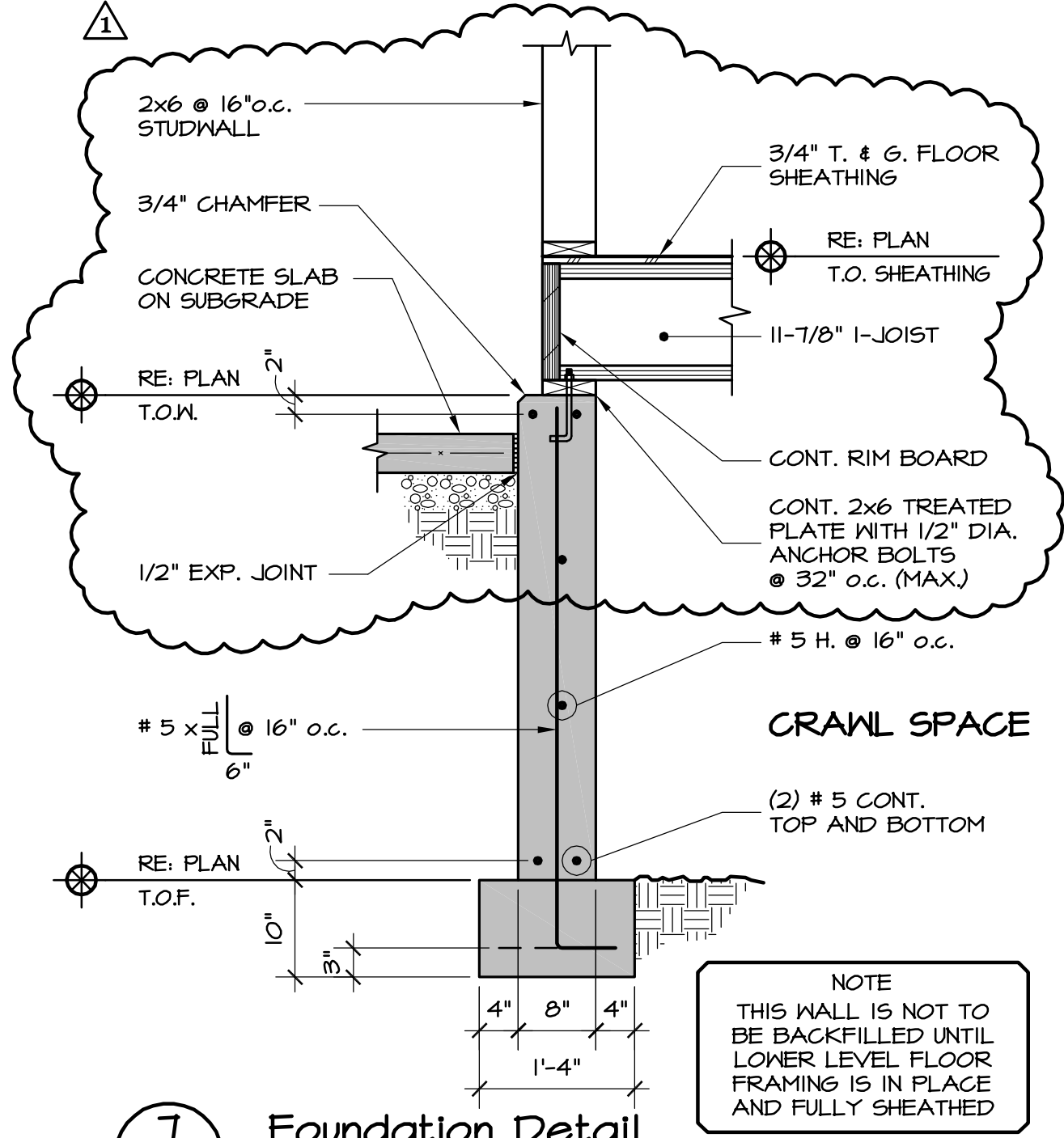
**3 Foundation Detail**  
S3.1 3/4"=1'-0"



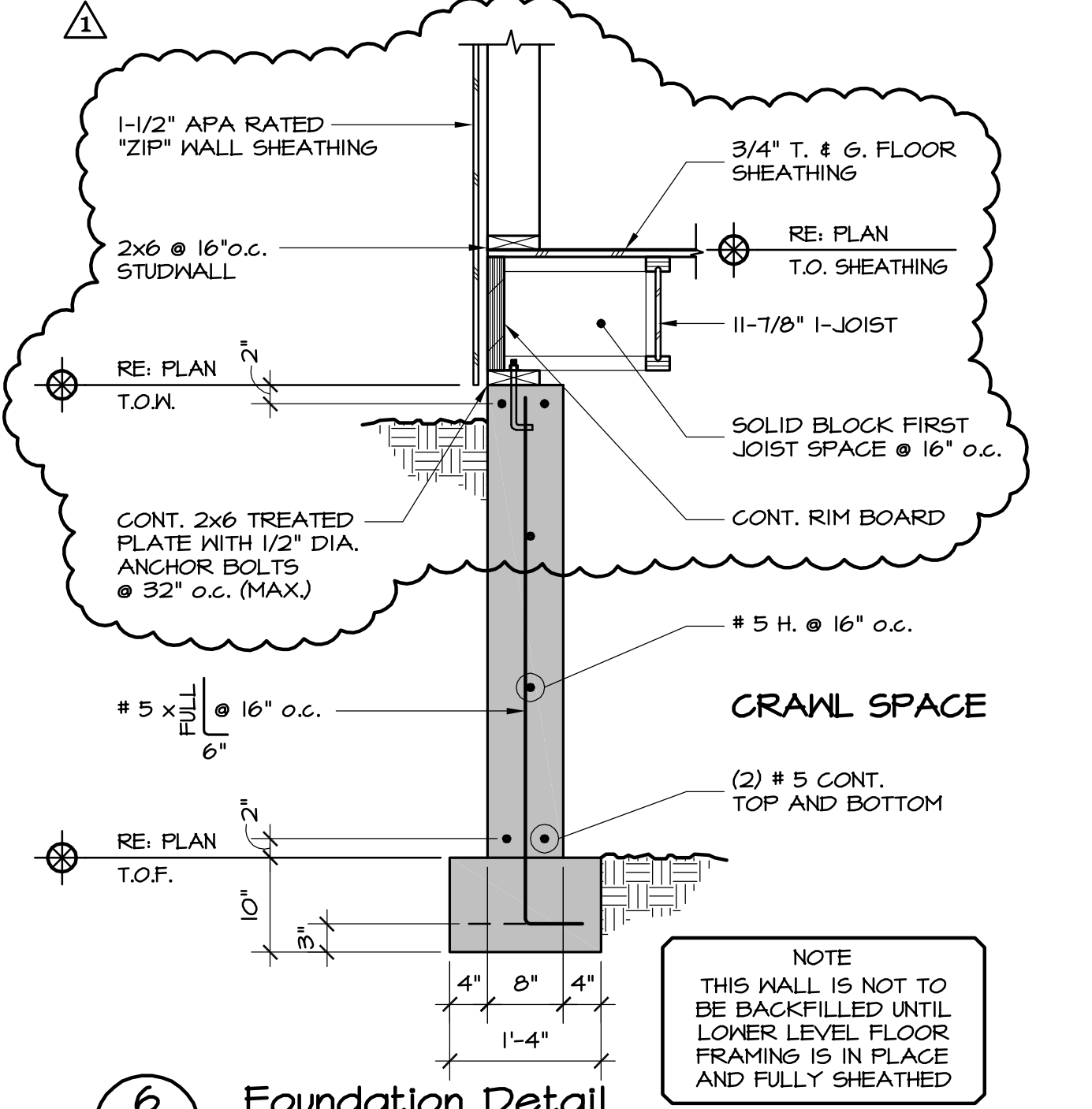
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S3.1 3/4"=1'-0"



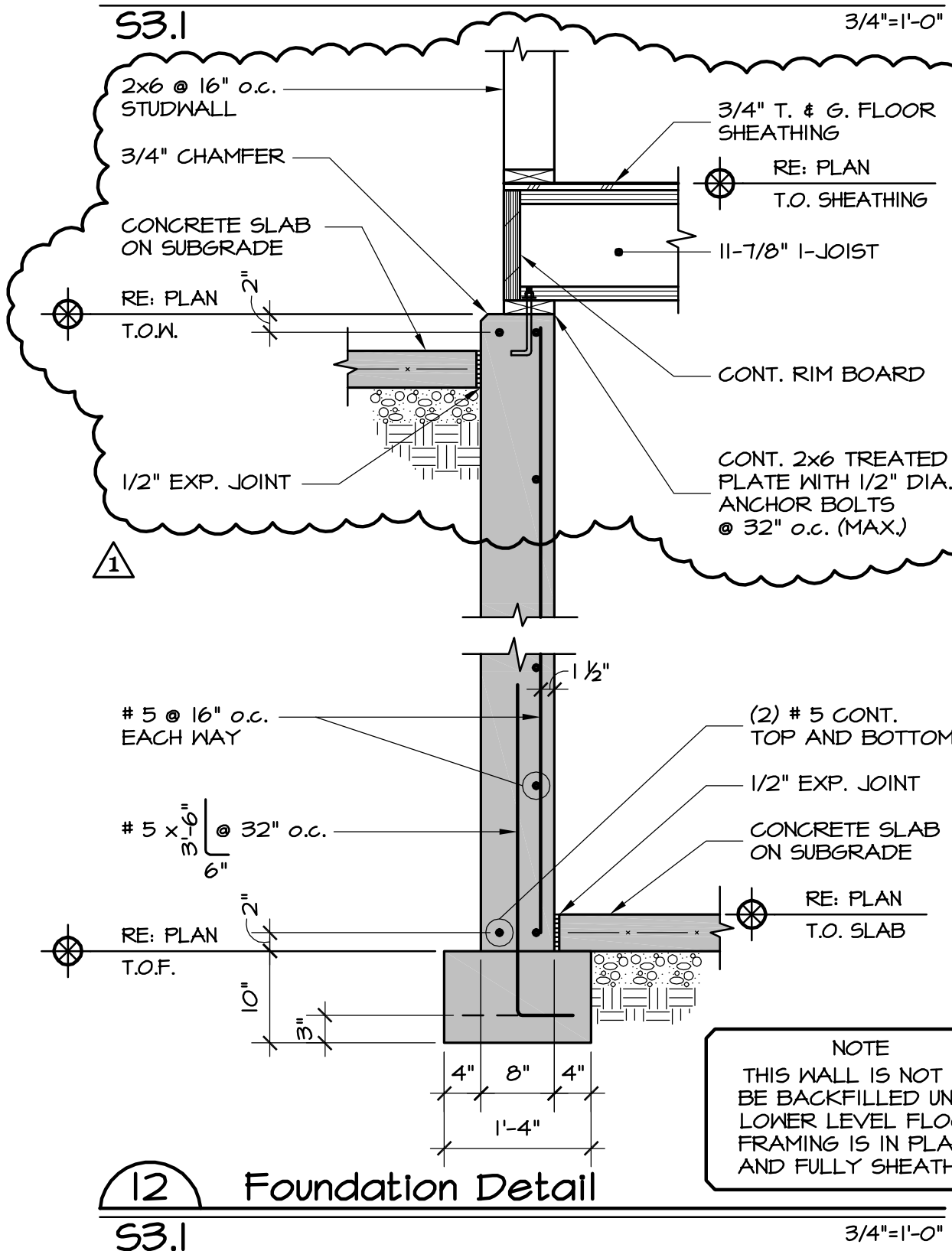
**8 Foundation Detail**  
S3.1 3/4"=1'-0"



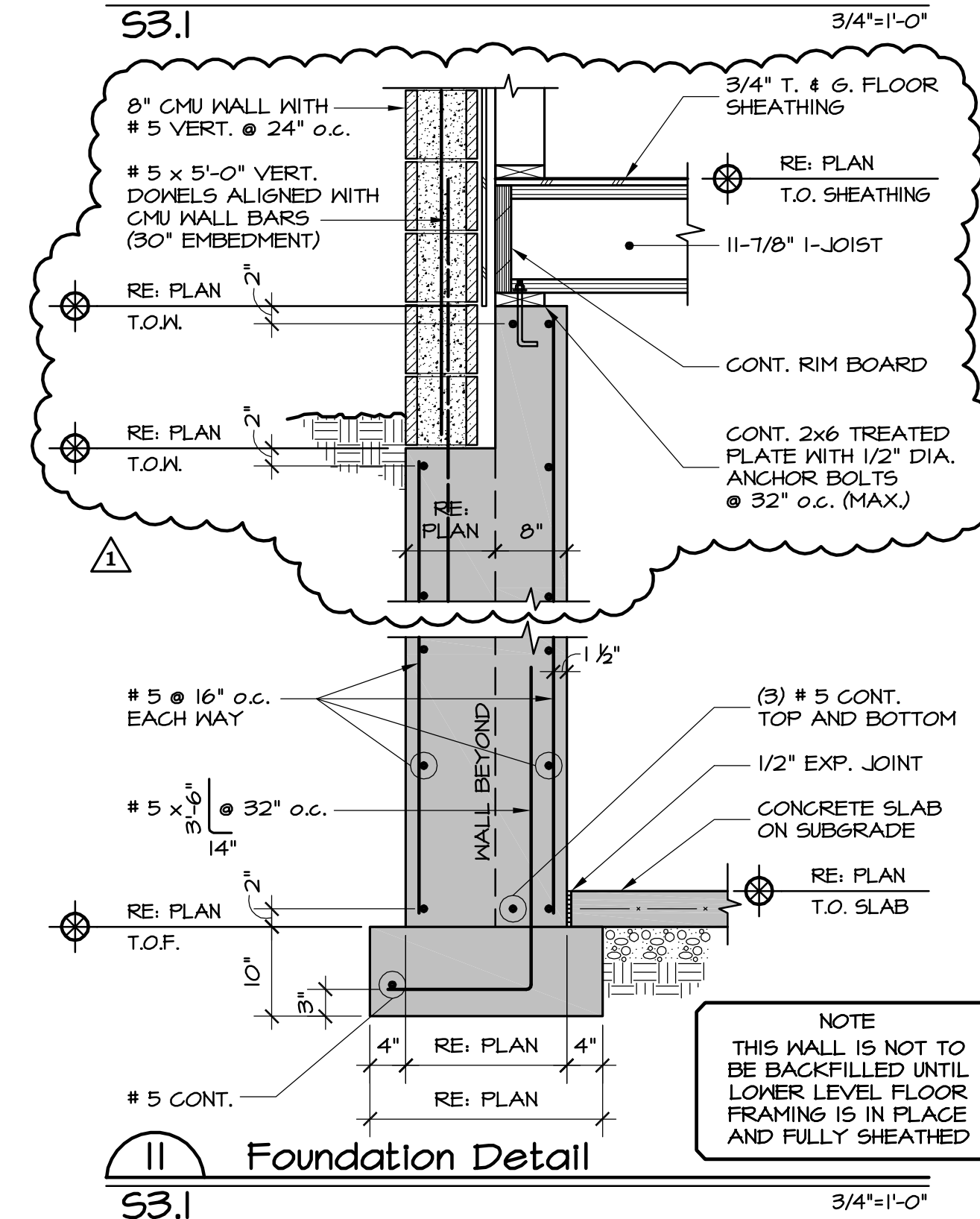
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S3.1 3/4"=1'-0"



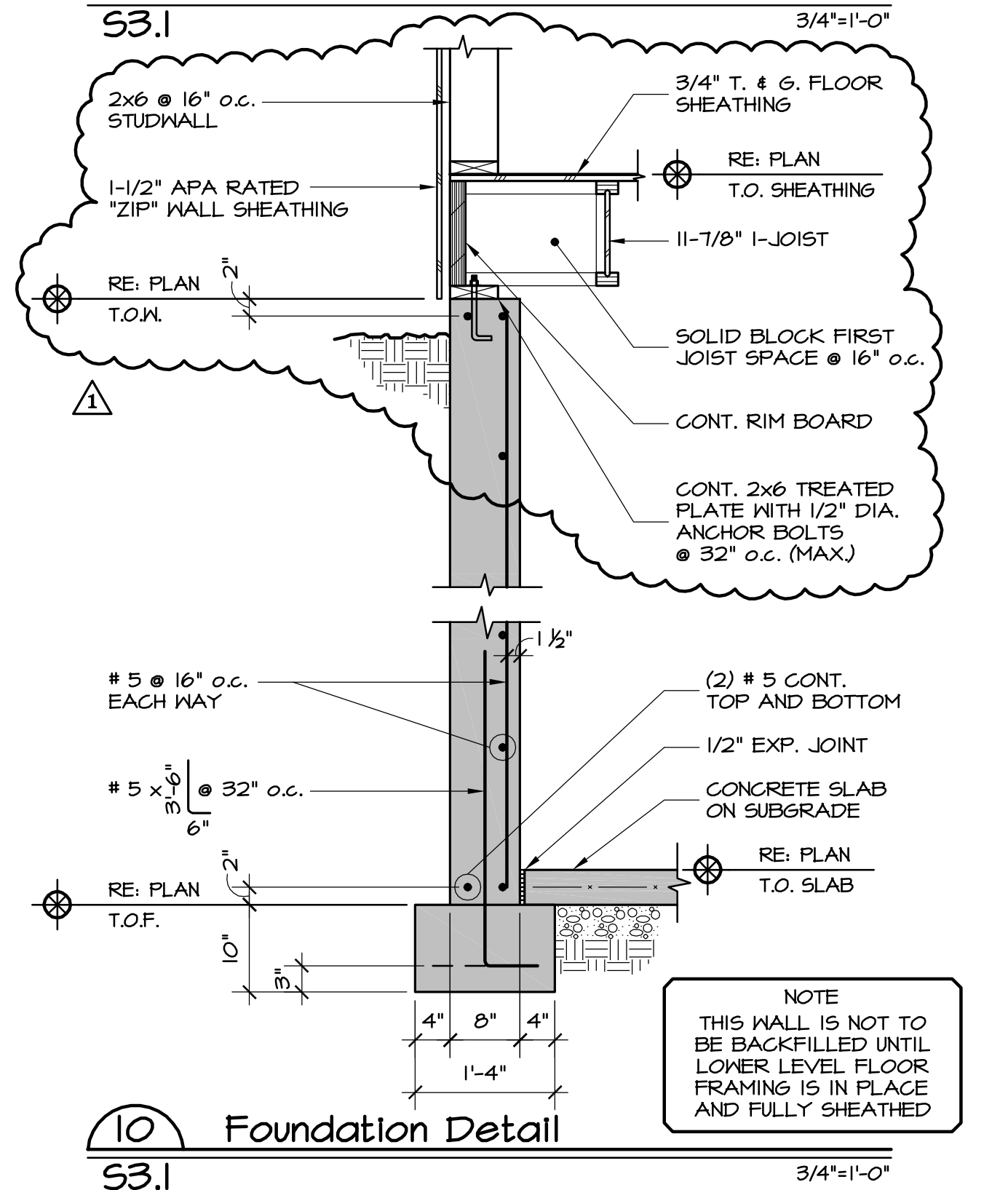
**6 Foundation Detail**  
S3.1 3/4"=1'-0"



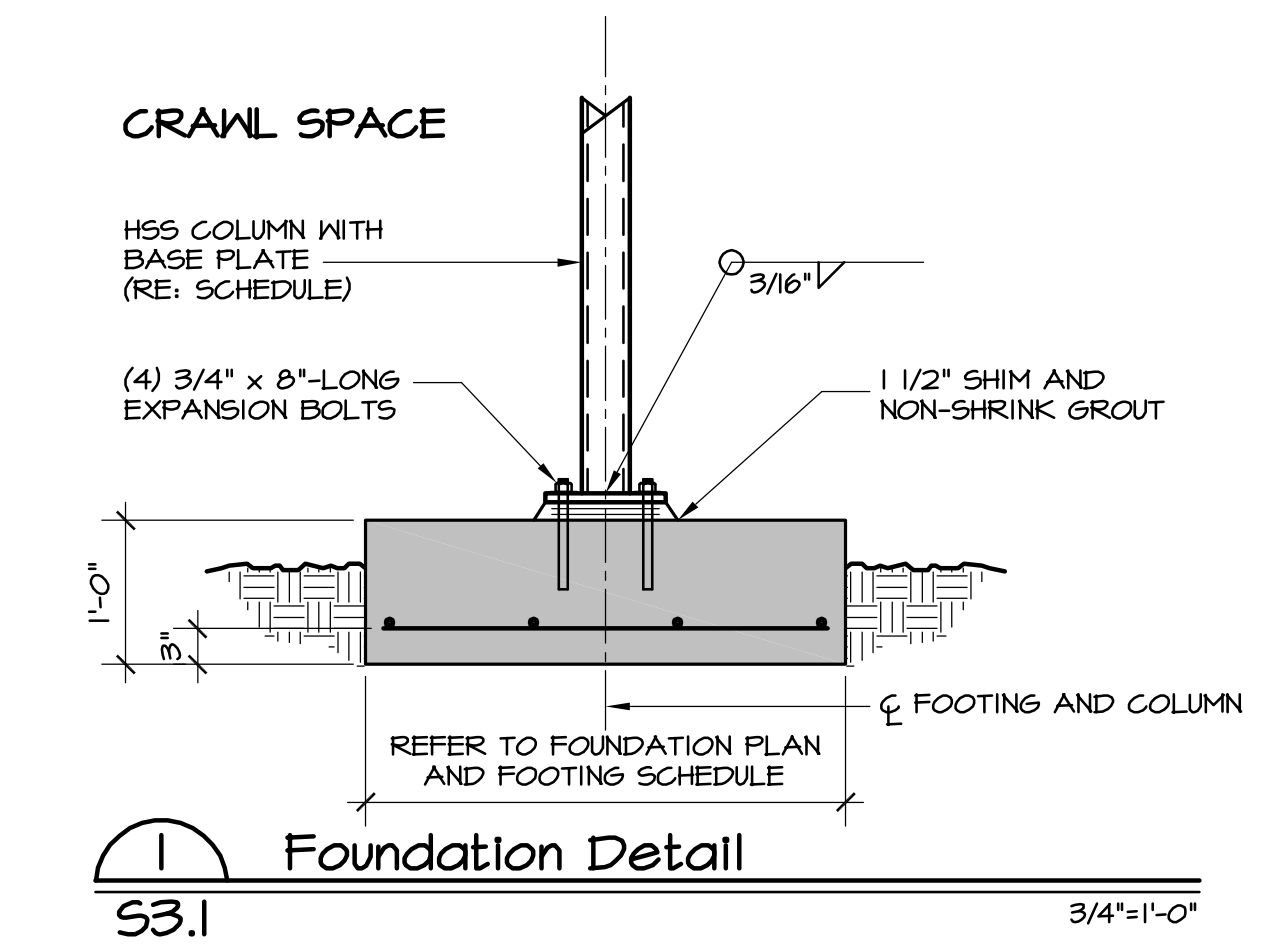
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S3.1 3/4"=1'-0"



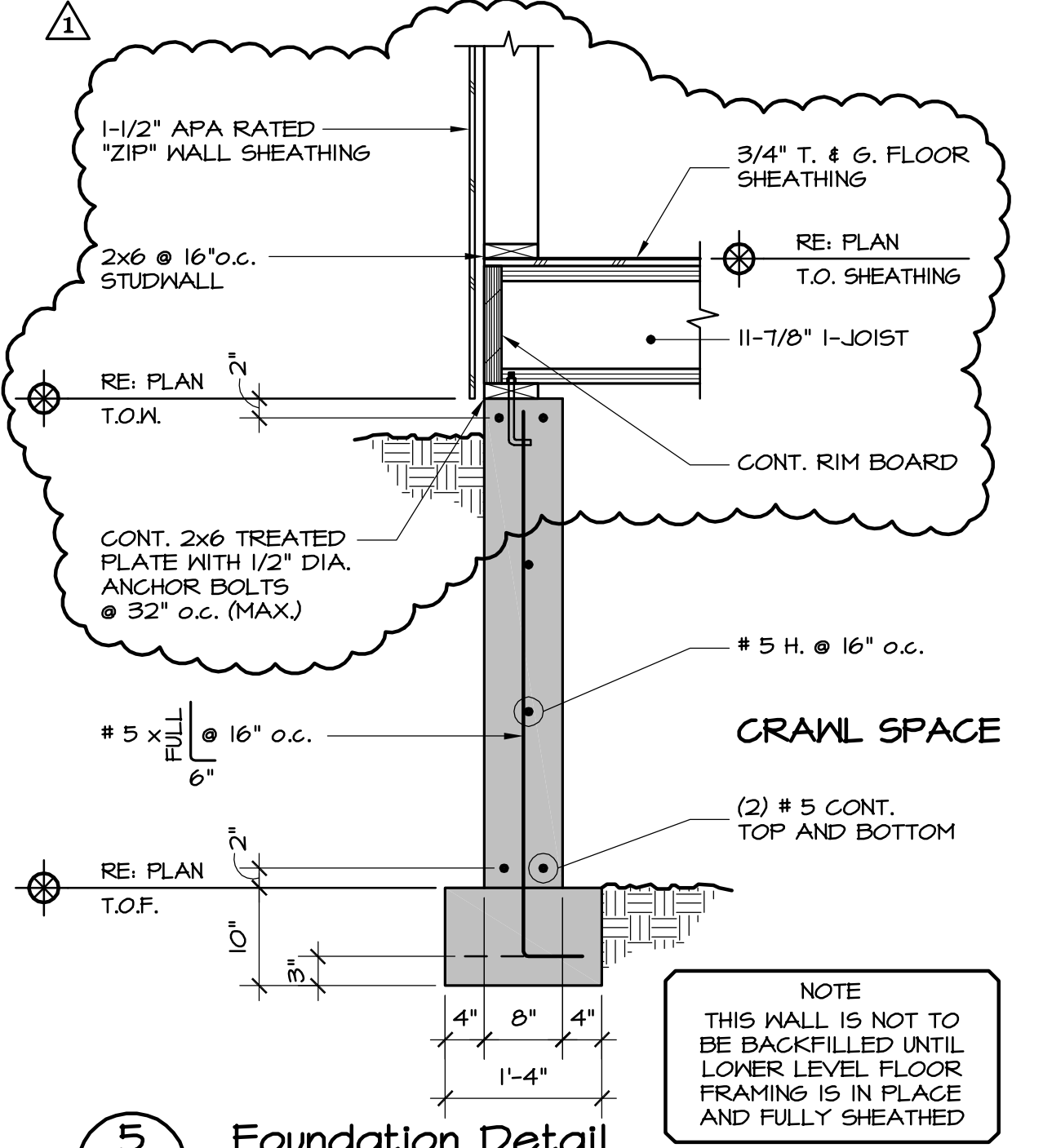
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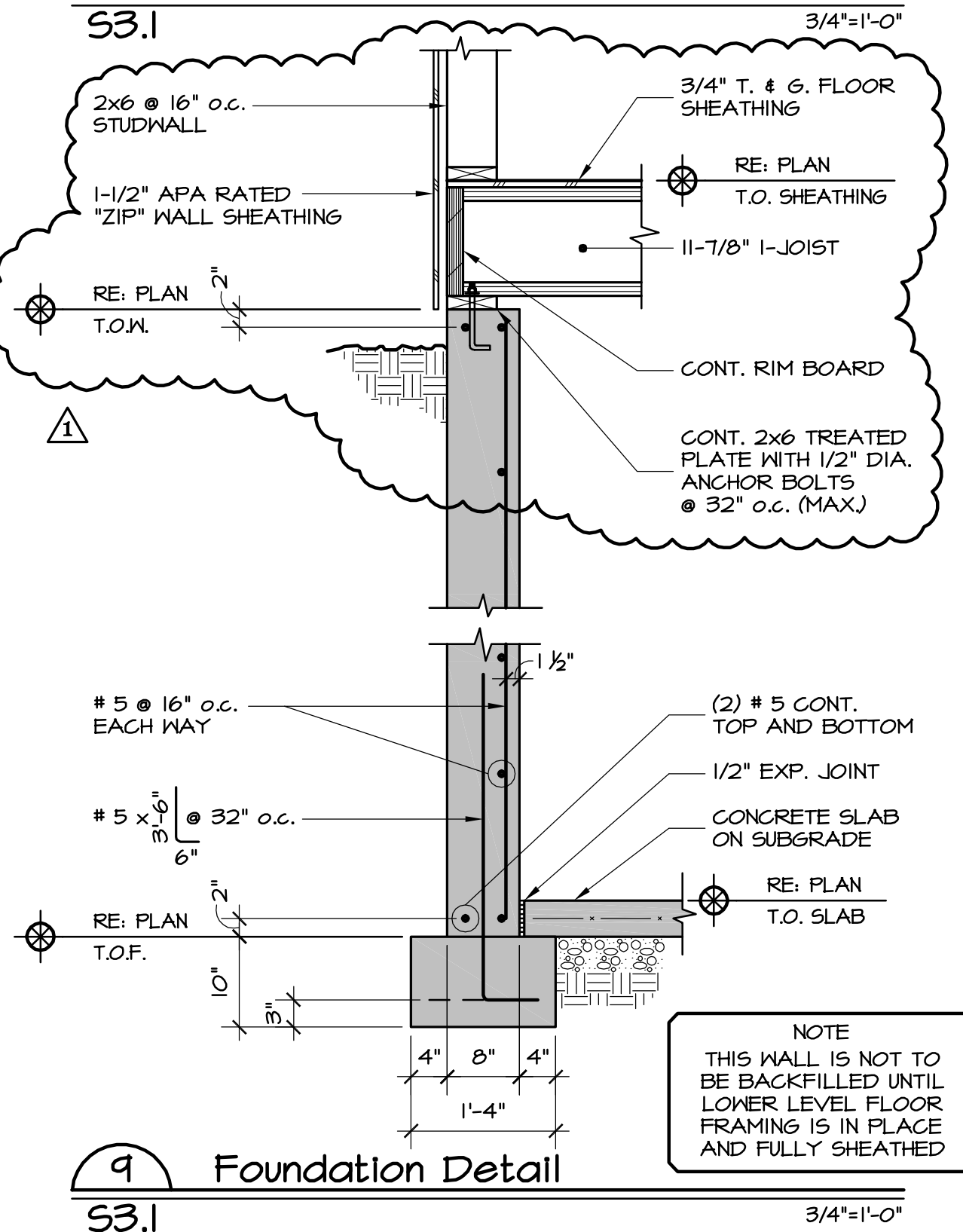
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S3.1 3/4"=1'-0"



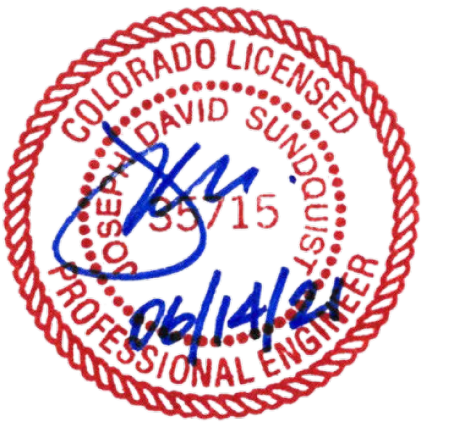
**1 Foundation Detail**  
S3.1 3/4"=1'-0"



**5 Foundation Detail**  
S3.1 3/4"=1'-0"

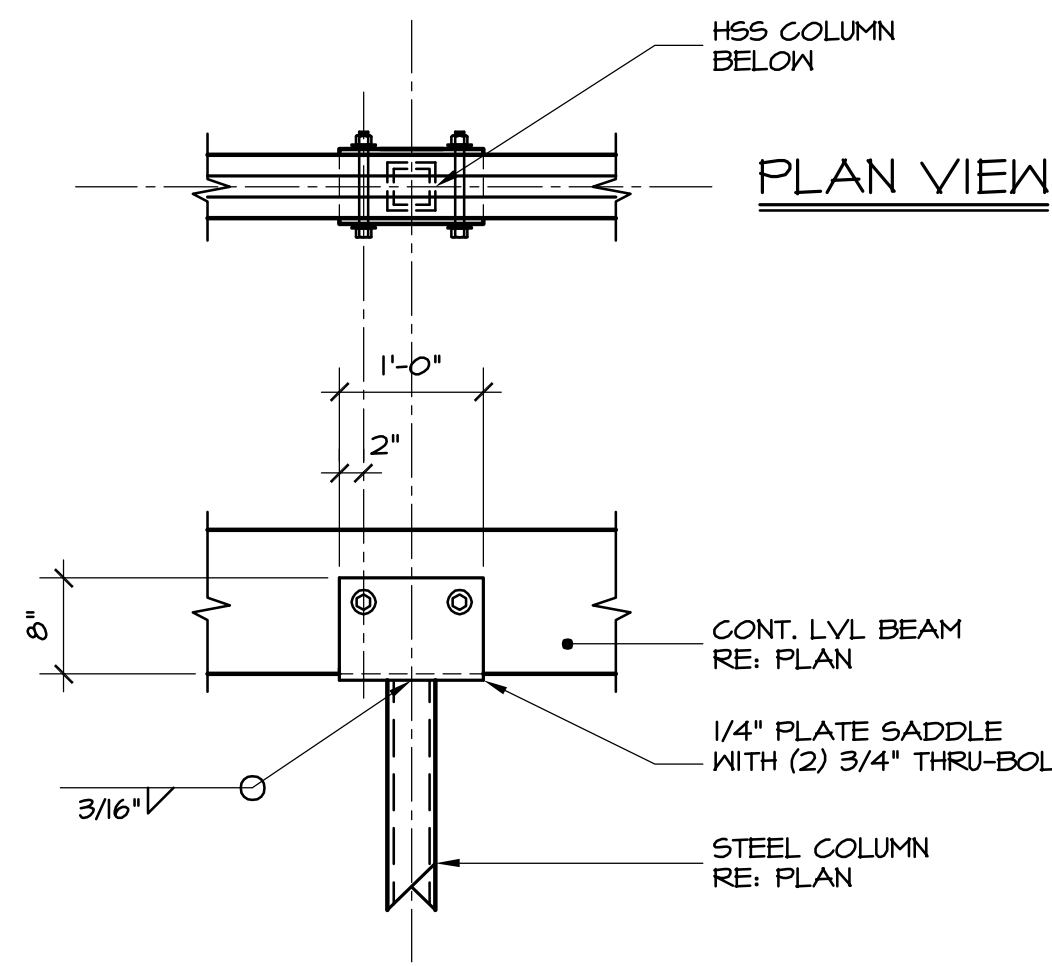


**9 Foundation Detail**  
S3.1 3/4"=1'-0"

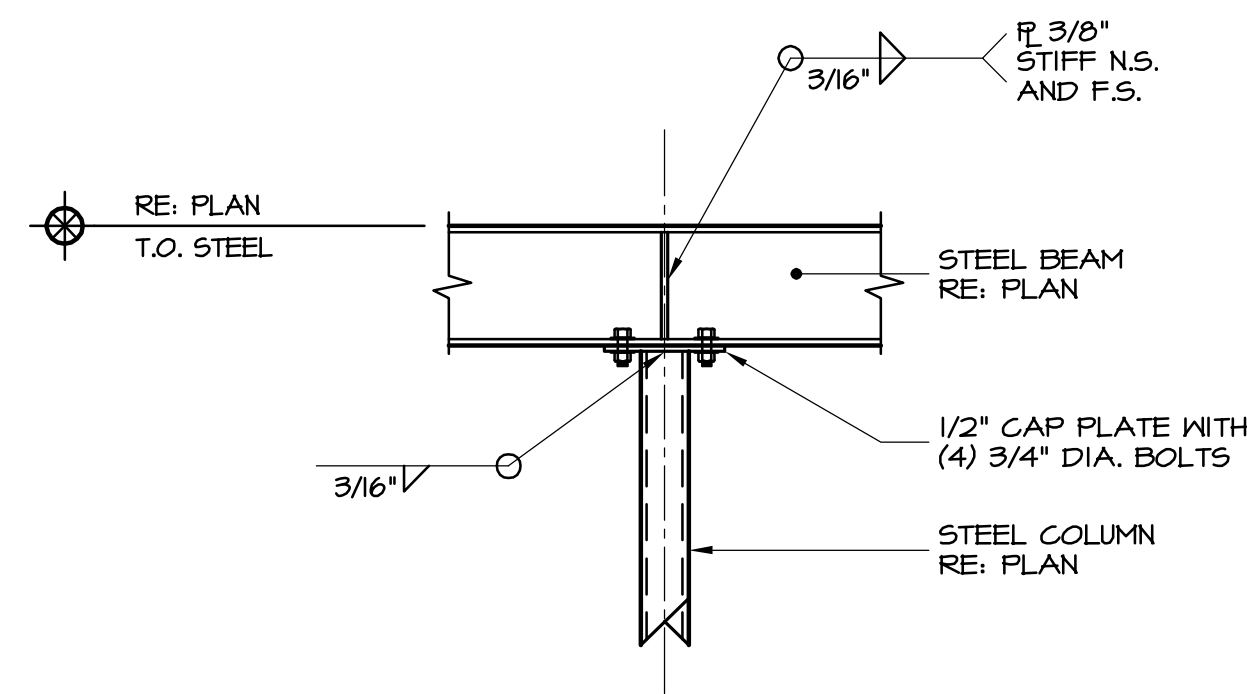


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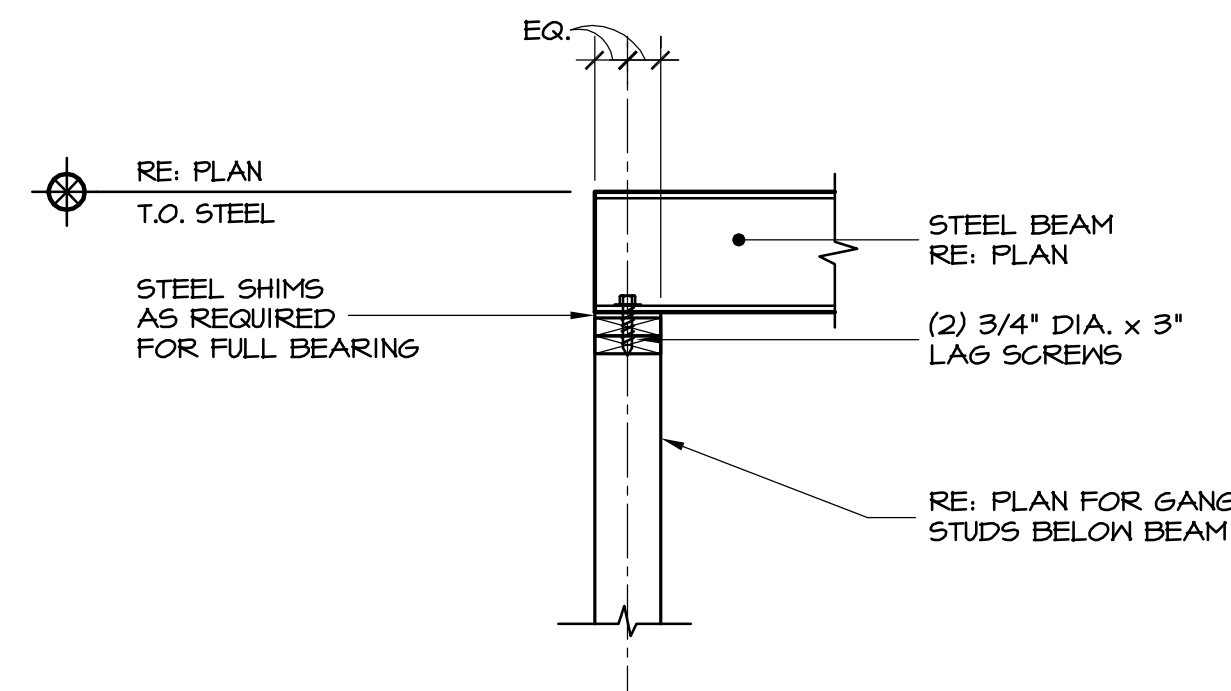
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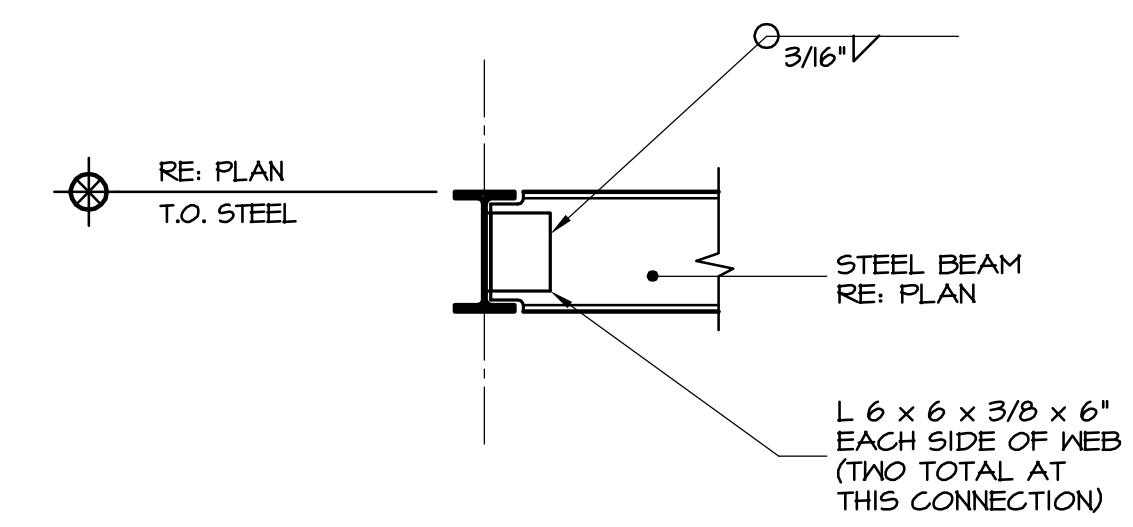
1 Framing Detail  
S3.2 3/4"=1'-0"



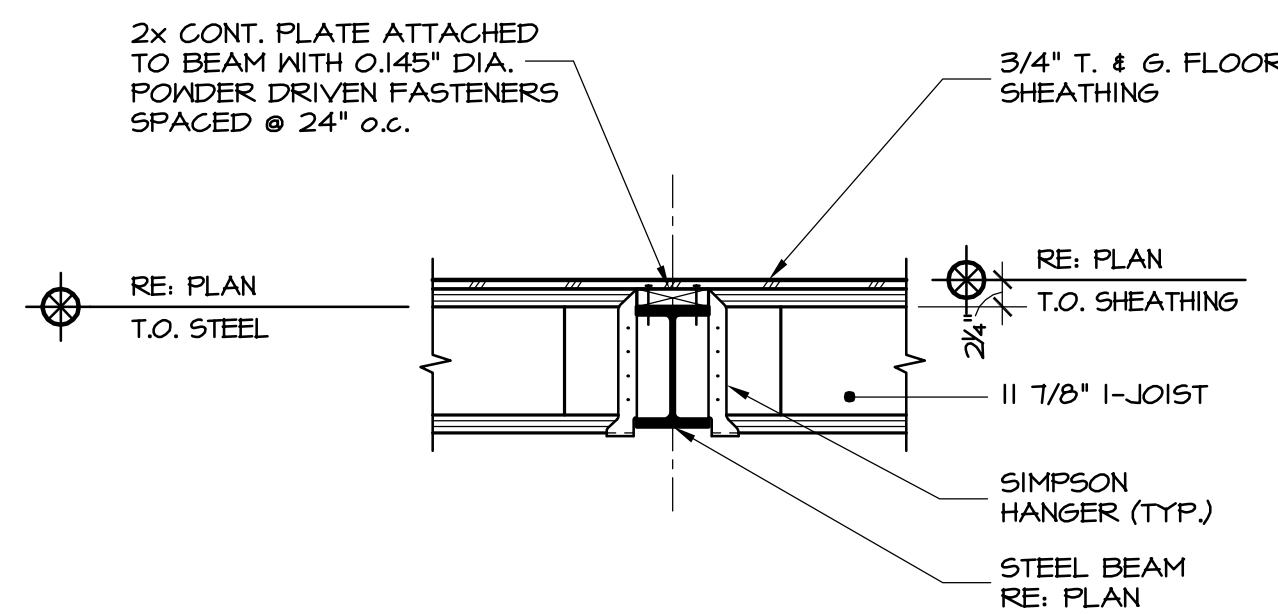
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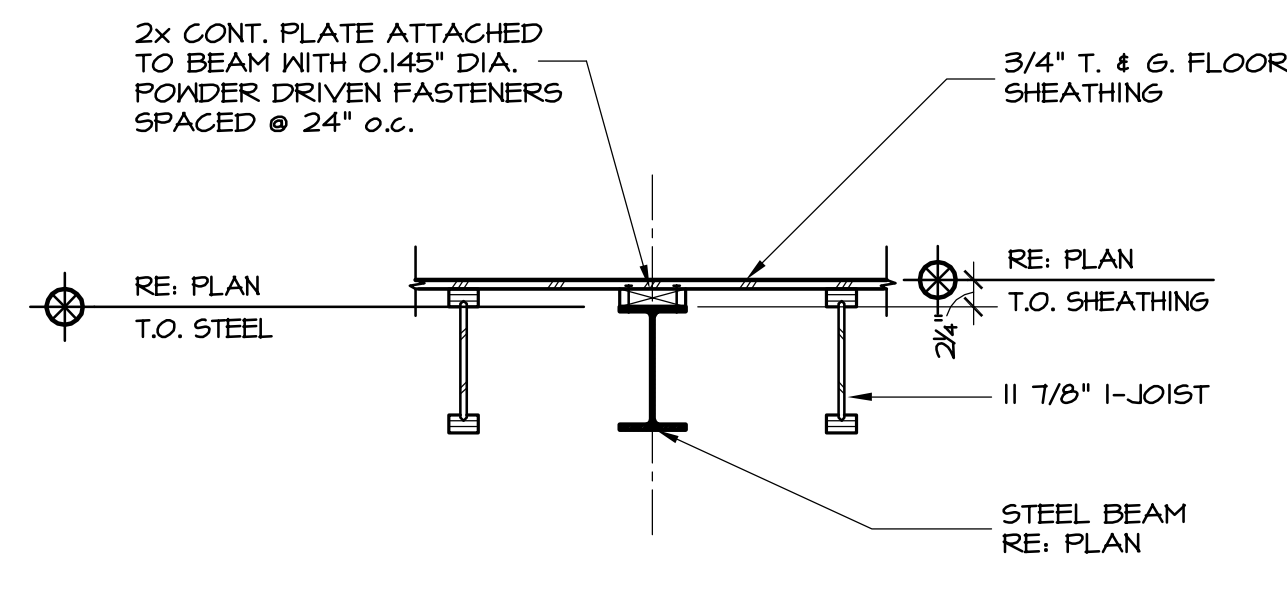
3 Framing Detail  
S3.2 3/4"=1'-0"



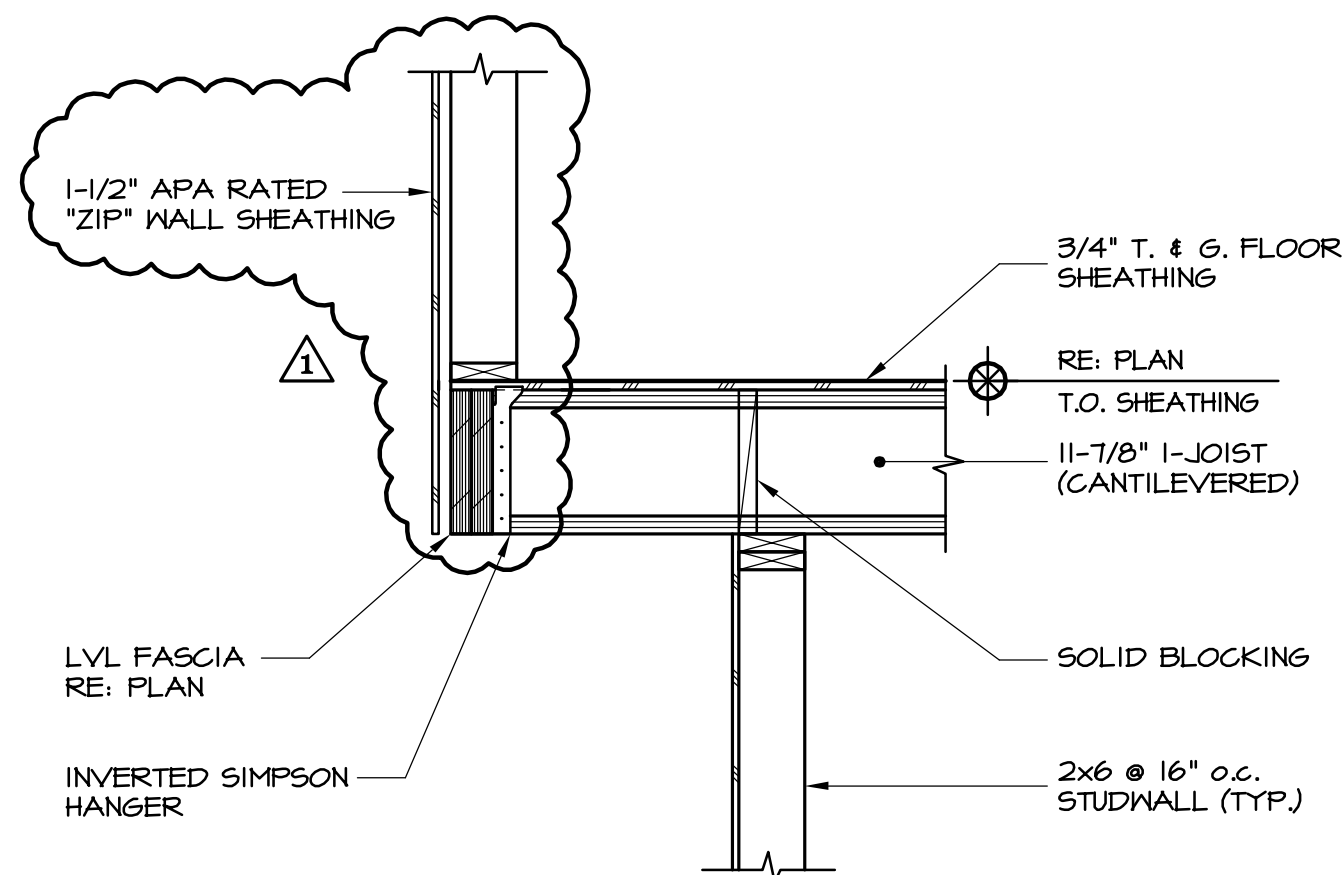
4 Framing Detail  
S3.2 3/4"=1'-0"



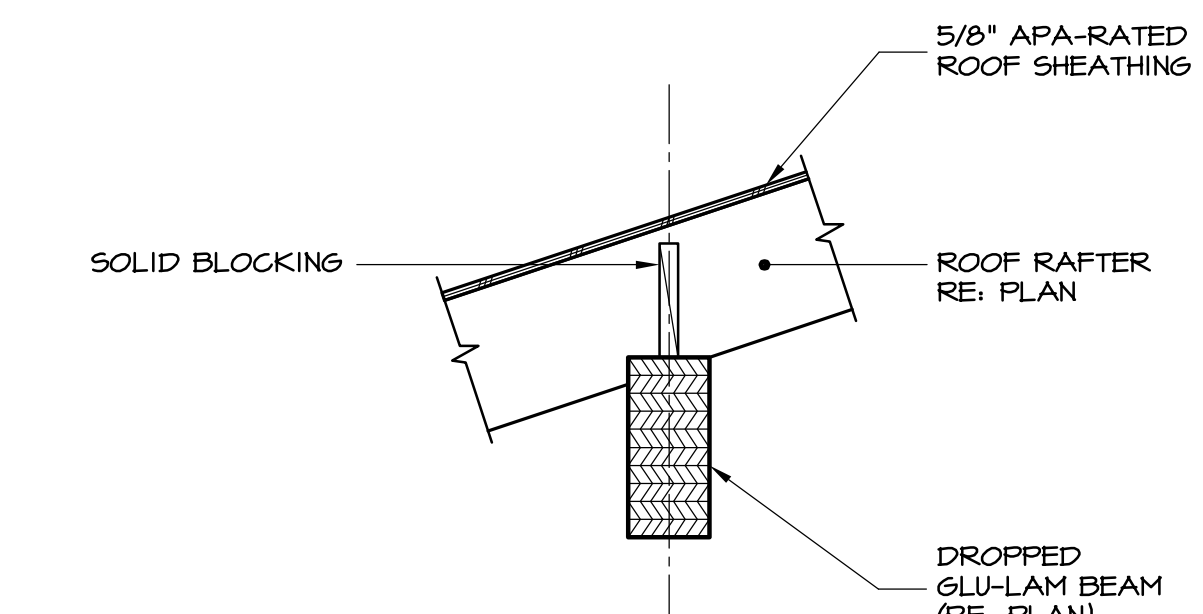
5 Framing Detail  
S3.2 3/4"=1'-0"



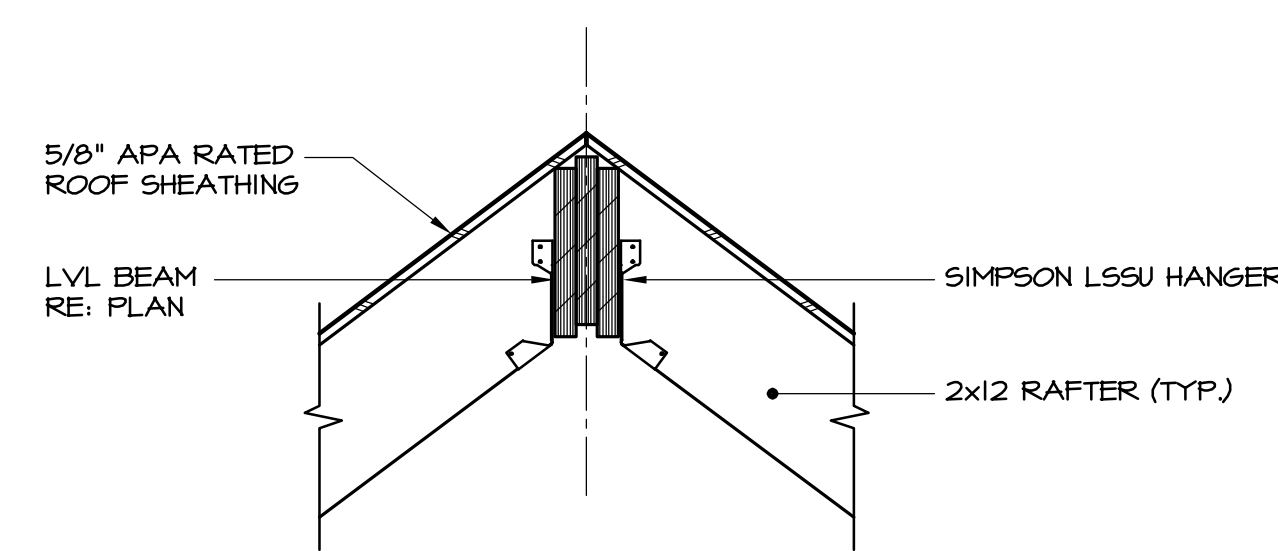
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S3.2 3/4"=1'-0"



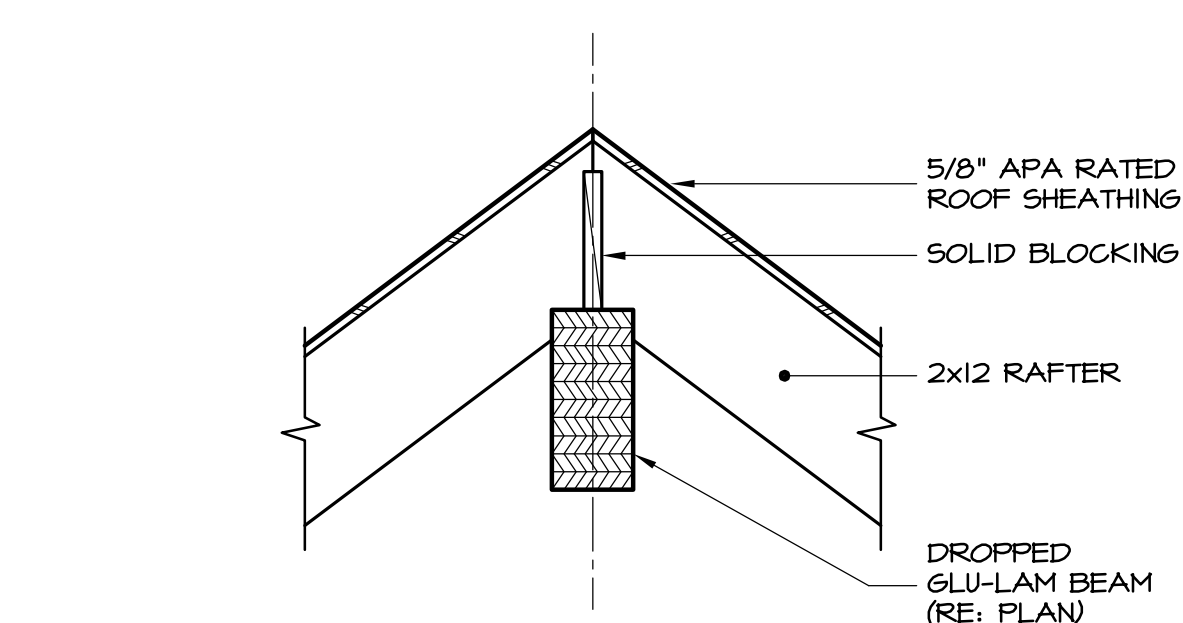
7 Framing Detail  
S3.2 3/4"=1'-0"



8 Framing Detail  
S3.2 3/4"=1'-0"



9 Framing Detail  
S3.2 3/4"=1'-0"



10 Framing Detail  
S3.2 3/4"=1'-0"

JONES RESIDENCE

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Minturn, Colorado

|                 |              |
|-----------------|--------------|
| Date            | • 06/14/2021 |
| SDG Project No. | • 21-015     |
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| Checked By      | • JDS        |

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| Date     | • Issue        |
| 05/06/21 | • Review Set   |
| 05/07/21 | • Construction |
| 06/14/21 | • Addendum #1  |
| •        |                |
| •        |                |
| •        |                |

Title • Framing Details

**Minturn Planning Department**  
Minturn Town Center  
302 Pine Street  
Minturn, Colorado 81645



**Minturn Planning Commission**  
Chair – Lynn Teach  
Jeff Armistead  
Elliot Hovey  
Chris Manning  
Tom Priest  
Jena Skinner

**To:** Planning Commission  
**From:** Scot Hunn, Planning Director  
Madison Harris, Planner I  
**Date:** August 5, 2021  
**Re:** 151 Main Street – Mikvah Conceptual

---

The Applicant, Chabad Vail, recently acquired property located at 151 Main Street in Minturn. The Applicant's representative, Kyle Webb, KH Webb Architects, has been working with Town of Minturn Planning Department staff since December 2020 with regard to Chabad Vail's intentions to explore redevelopment opportunities on the subject property for retail as well as religious uses.

The subject property is the site of the "Uptown Store," and while not listed as a historic structure on any local or state registries, is a recognizable structure within the Minturn 100 Block Commercial area.

The subject property includes lands located between Hwy. 24 and Williams Street, as well as a small parcel located west of Williams Street. Applicant has developed multiple scenarios for redevelopment of both parcels/areas. The main focus of the Applicant's development plans is a "Mikvah" or bath house - to be used by members of Chabad Vail for religious purposes. Mikvahs require specific elements and design considerations. Per a Letter of Intent from the Applicant's Representative:

*"In order to meet both the ritual and spiritual needs of the Jewish community, building strictly kosher water (rain water) collection pools (boros) that will flow into immersion pools that are designed and constructed under observation of strict Orthodox authorities. In addition to the spiritual experience of immersion pools for men and women, spaces specific for preparing/changing for men and women as well as associated service and mechanical spaces are required."*

KH Webb Architects has evaluated designs on the parcel lying west of Williams Street right-of-way and next to the Hotel Minturn, only to conclude that without significant variances from setback requirements, the program and dimensions of the Mikvah will not work on that portion of the property. Additionally, that portion of the property is the location of parking managed under a parking management plan first approved by the Town in the late 1990's.

With these constraints in mind, KH Webb Architects met with Town staff recently to discuss alternatives including raising the existing "Uptown Store" building, rebuilding a new retail space fronting Main Street, and developing the Mikvah at the rear portion of the site between

Main Street and Williams Street. Two scenarios for redevelopment of the subject property will be presented at the August 25th meeting of the Design Review Board.

KH Webb Architects is requesting a conceptual design review and discussion for the primary purpose of gaining feedback from the DRB as to the appropriateness of the conceptual design, preferences considering the two alternatives, and consideration of other site constraints.

The concepts presented at this time are not detailed in nature but do offer a starting point for discussions and the identification of potential issues for the Applicant's consideration.

August 4, 2021

Town of Minturn Planning Commission  
C/O Madison Harris  
Planning & Zoning Department  
301 Boulder St. # 309  
Minturn, CO 81645

RE: 151 Main Street Limited Use Application Update

Dear Town of Minturn Planning Commission,

A unique request for you, one that we have been working on with Staff and our clients for several months!

Our client is looking at creating a tiny, less than 1,000sf space, on rear part of 151 Main Street in the Old Town Zone District. This would be tied to the Jewish community in Eagle County (the non-profit, Chabad Vail) and help serve some of their spiritual needs. The existing commercial space fronting on Main Street would be re-built in a similar manner and on the rear portion of the lot would be a "Mikvah" which is a ritual bath space for men and women. The best definition I can find is a "club" in zoning, but here is the Oxford Dictionary definition of a Mikvah.

*A mikvah is an immersion pool that dates to the establishment of Judaism, thousands of years ago in the Garden of Eden.*

In order to meet both the ritual and spiritual needs of the Jewish community, building strictly kosher water (rain water) collection pools (boros) that will flow into immersion pools that are designed and constructed under observation of strict Orthodox authorities. In addition to the spiritual experience of immersion pools for men and women, spaces specific for preparing/changing for men and women as well as associated service and mechanical spaces are required.

The Mikvah will be used constantly throughout the year, but will certainly ebb and flow as our seasonal economy does. Traditionally levels of use vary with the religious spirituality of the individual, but women will primarily use the pools after sunset and men will use them first thing in the morning, but there are no restrictions on timing. Often it is a family affair to visit the Mikvah and we project 50-75 visits per month on average with most elevated number of visits in July and August. Aside from bringing more people to visit Minturn, we intend to keep the commercial space on Main Street as it functioning now, but as numbers grow over time as expected, we could foresee turning the Commercial space into a Kosher Deli, in keeping with Mikvah's theme.



Mikvah Letter,  
Page 2

### **Options Discussion:**

Originally, we proposed after initial staff discussions to locate the Mikvah across Williams Street on the rear portion of the lot in a 0' Setback configuration. This was conceived so we could preserve the existing structure on Main Street. After thorough vetting of this option, the costs as well as complexities of zoning and construction became clear, moreover we were losing significant parking spaces. It became clear that the required setback variances were not viable and we needed to pursue options that would front Main Street and reconsider the existing structure. That said, we are open and receptive to what this re-development means to the Town of Minturn and want your feedback as to what will be fronting Main Street.

The challenge is that this lot is “residential” in scale but in a location you may want to be more urban in nature. Our hope is that you can help us figure out what is more desirable for the Main Street context and the Old Town Zone District.

### **Option One: Urban Solution**

In this option, we would put the Mikvah component on the rear of the lot, and reconceive the Main Street front of the lot to continue the context of the Thai Kitchen/Minturn Country Club row with tight street fronting facades on Main Street. This option would not leave greenspace on the street, but would continue the existing look of “entry” into Town from the North.

### **Option Two: Contextual Solution**

Again, we would put the Mikvah on the rear of the lot, but keep the existing residential scale and look of the existing structure. In this configuration, we are basically proposing to recreate the look of the existing commercial home, but adjust it closer to the street as setbacks dictate and permit.

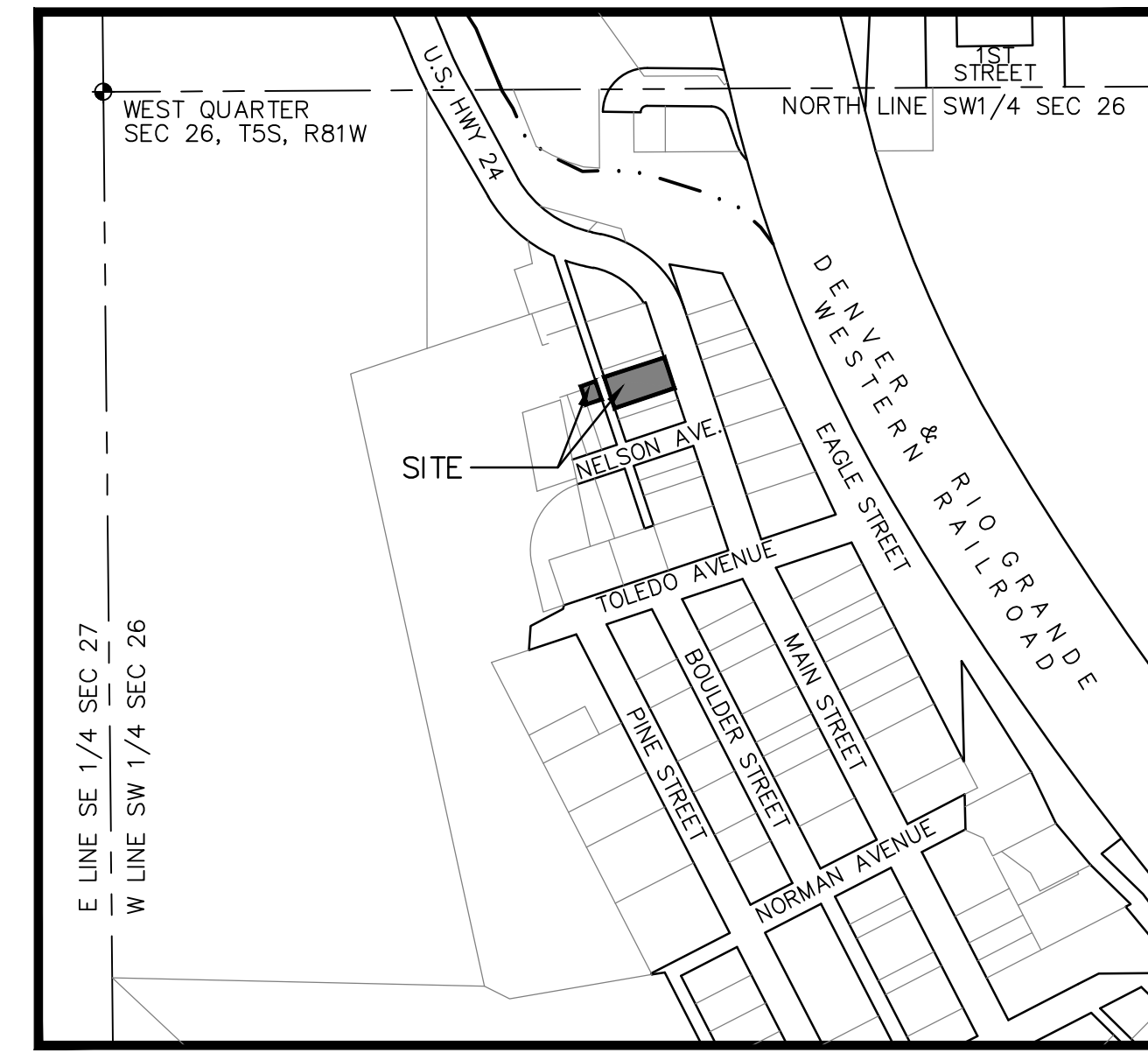
We look forward to further discussions and dialogue on this proposal and want to be clear that we are open and flexible to what is best for the community. If you do have questions, please feel free to contact me or we can discuss further at your Planning Commission meeting on August 11<sup>th</sup>!

Sincerely,

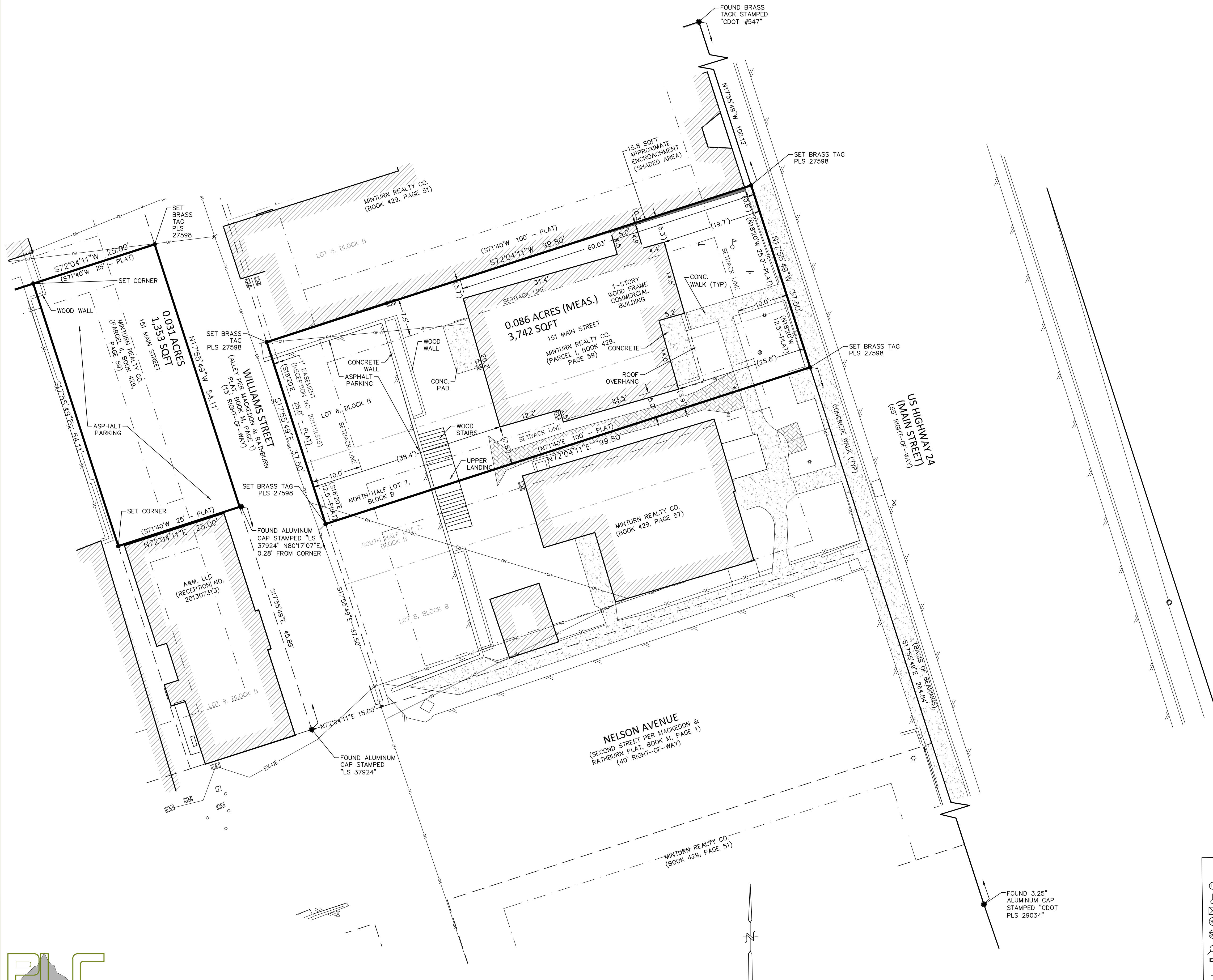
*KHW*

Kyle H. Webb, AIA  
KH Webb Architects PC

ALTA/NSPS LAND TITLE SURVEY  
 LOT 6, THE NORTH HALF OF LOT 7,  
 AND PART OF LOT 9, BLOCK B,  
 MACKEDON & RATHBURN SUBDIVISION,  
 SECTION 26, TOWNSHIP 5 SOUTH, RANGE 81 WEST  
 TOWN OF MINTURN, COUNTY OF EAGLE, STATE OF COLORADO



VICINITY MAP  
 SCALE: 1"=500'



LEGAL DESCRIPTION:

PARCEL L: Order No. ABN50044289-2  
 LOT EIGHT (8) AND THE NORTH HALF (1/2) OF LOT SEVEN (7), BLOCK "B" MACKEDON AND RATHBURN'S SUBDIVISION OF BOOGO'S ADDITION TO TOWN OF MINTURN, ACCORDING TO THE RECORDED PLAT THEREOF RECORDED JANUARY 5, 1892 UNDER RECEPTION NO. 10580, COUNTY OF EAGLE, STATE OF COLORADO.

PARCEL M: Order No. ABN50044289-2  
 LOT NINE (9) IN BLOCK "B" MACKEDON AND RATHBURN SUBDIVISION OF BOOGO'S ADDITION TO MINTURN, ACCORDING TO THE RECORDED PLAT THEREOF, COUNTY OF EAGLE, STATE OF COLORADO EXCEPTING THEREFROM THE SOUTHEASTERLY 45.89 FEET, DESCRIBED AS FOLLOWS: BEGINNING AT CORNER NO. 1 (SW) OF LOT 9, BLOCK "B" OF SAID SUBDIVISION; THENCE NORTH 71 DEGREES 40 MINUTES EAST, 22.65 FEET TO A WITNESS CORNER FOR CORNER NO. 2; THENCE NORTH 71 DEGREES 40 MINUTES EAST, 2.35 FEET TO THE TRUE POSITION FOR CORNER NO. 2 (SE); THENCE NORTH 18 DEGREES 20 MINUTES WEST 45.89 FEET TO THE TRUE POSITION FOR CORNER NO. 3 (NE); THENCE SOUTH 71 DEGREES 40 MINUTES WEST, 3.57 FEET TO A WITNESS CORNER FOR CORNER NO. 3; THENCE SOUTH 71 DEGREES 40 MINUTES WEST 21.43 FEET TO CORNER NO. 4 (NW); THENCE SOUTH 18 DEGREES 20 MINUTES EAST 45.89 FEET TO CORNER NO. 1 (SW) THE PLACE OF BEGINNING.

GENERAL NOTES:

- ALTA SURVEY COMPLETED ON AUGUST 23, 2016.
- "SET CORNER" DENOTES A SET 1.5" ALUMINUM CAP STAMPED "LS 27598" ON A #5 REBAR
- BUILDING TIES ARE PERPENDICULAR OR RADIAL TO PROPERTY LINES (X,X').
- BASIS OF BEARINGS: THE COLORADO STATE PLANE COORDINATE SYSTEM, COLORADO CENTRAL ZONE 0502, NORTH AMERICAN DATUM 1983 (NAD83). THE FOUND BRASS TACK STAMPED "CDOT #547" & THE FOUND 3.25" ALUMINUM CAP STAMPED "CDOT PLS 29034" ON THE WEST LINE OF US HWY 24 WERE HELD FOR ROTATION HAVING A BEARING OF S17°55'49"E. ADDITIONALLY THE ALUMINUM CAP STAMPED "LS 37924" FOUND FOR THE SOUTHEAST CORNER OF LOT 9, BLOCK B, MACKEDON & RATHBURN, AND THE ALUMINUM CAP STAMPED "LS 9337" FOR THE SOUTHWEST CORNER OF LOT 4, BLOCK C, MACKEDON & RATHBURN WERE FOUND TO BE IN AGREEMENT WITH THIS ROTATIONAL SOLUTION. DISTANCES SHOWN HEREON ARE SURFACE VALUES DERIVED USING A SCALE FACTOR OF 1.0004037201.
- SUBJECT PROPERTY IS ZONED "COMMERCIAL" PER THE TOWN OF MINTURN STANDARDS. NEW BUILDINGS SHALL DRAW ON INTERPRETATIONS OF EXISTING STOREFRONT STRUCTURES. THE SCALE OF THE NEW BUILDINGS SHALL BE CONSISTENT WITH ADJACENT BUILDINGS. HEIGHT SHALL BE CONSISTENT AS VIEWED FROM THE STREET; ADDITIONAL HEIGHT MAY BE APPROVED IF IT STEPS BACK FROM THE FACADE TO REDUCE THE PERCEIVED SCALE OF THE NEW DEVELOPMENT.
- THE SUBJECT PROPERTY IS LOCATED WITHIN FLOOD ZONE DESIGNATION X BY THE SECRETARY OF HOUSING AND URBAN DEVELOPMENT, ON FLOOD INSURANCE RATE MAP NO. 08037C0658D, WITH A DATE OF IDENTIFICATION OF DECEMBER 4, 2007, IN EAGLE COUNTY, STATE OF COLORADO, WHICH IS THE CURRENT FLOOD INSURANCE RATE MAP FOR THE COMMUNITY IN WHICH SAID PROPERTY IS SITUATED.
- PEAK LAND CONSULTANTS, INC. DID NOT PERFORM A TITLE SEARCH OF THE SUBJECT PROPERTY TO ESTABLISH OWNERSHIP OR RIGHTS-OF-WAY OF RECORDS. RECORD DOCUMENTS UTILIZED ON THIS ALTA/NSPS SURVEY WERE PROVIDED BY LAND TITLE GUARANTEE COMPANY, ORDER NO. ABN50044289-2, HAVING AN EFFECTIVE DATE OF JUNE 15, 2016, AT 5:00 P.M.
- NOTICE: ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT SHALL ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.

TITLE EXCEPTIONS:

- THE EFFECT OF ORDINANCE NO. 129, RECORDED DECEMBER 20, 1974, IN BOOK 238 AT PAGE 5.
- ANY TAX, LIEN, FEE, OR ASSESSMENT BY REASON OF INCLUSION OF SUBJECT PROPERTY IN THE EAGLE RIVER FIRE PROTECTION DISTRICT, AS EVIDENCED BY INSTRUMENT RECORDED MAY 30, 2002, UNDER RECEPTION NO. 799500.
- ANY TAX, LIEN, FEE, OR ASSESSMENT BY REASON OF INCLUSION OF SUBJECT PROPERTY IN THE EAGLE RIVER WATER AND SANITATION DISTRICT, AS EVIDENCED BY INSTRUMENT RECORDED DECEMBER 31, 2009, UNDER RECEPTION NO. 200927997, SPECIAL DISTRICT PUBLIC DISCLOSURE DOCUMENT RECORDED DECEMBER 3, 2013 UNDER RECEPTION NO. 201323922 AND RECORDED DECEMBER 18, 2014 UNDER RECEPTION NO. 201421767 IN REGARDS THEREOF.
- EASEMENT GRANTED TO PUBLIC SERVICE COMPANY OF COLORADO, FOR UTILITY LINES AND INCIDENTAL PURPOSES, BY INSTRUMENT RECORDED JUNE 29, 2011 UNDER RECEPTION NO. 201112315 (SHOWN HEREON).

LEGEND

|   |                      |   |                     |
|---|----------------------|---|---------------------|
| ⊕ | ELECTRIC OUTLET      | ⊕ | ANCHOR, 6" CONCRETE |
| ⊕ | FLAG POLE            | ⊕ | BOLLARD             |
| ⊕ | SPIGOT               | ⊕ | LIGHT POLE          |
| ⊕ | STORM MANHOLE        | ⊕ | SIGN                |
| ⊕ | SEWER MANHOLE        | ⊕ | WATER VALVE         |
| ⊕ | UTILITY POLE         | ⊕ | GAS METER           |
| ⊕ | ELECTRIC METER       | ⊕ | SEWER CLEANOUT      |
| — | OVERHEAD ELECTRIC    |   |                     |
| — | ASPHALT PAVING       |   |                     |
| — | EX-UE                |   |                     |
| — | UNDERGROUND ELECTRIC |   |                     |
| — | FENCE                |   |                     |

ALTA/NSPS LAND TITLE SURVEY:  
 TO MINTURN REALTY, INC.: ONT MINTURN LLC, A COLORADO LIMITED LIABILITY COMPANY; MR MINTURN LLC, A COLORADO LIMITED LIABILITY COMPANY & LAND TITLE GUARANTEE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 6(b), 7(a), 8, 9, 11, 13, & 19 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON AUGUST 23, 2016.



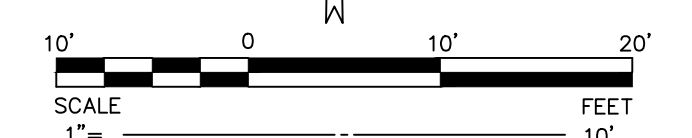
BRENT BIGGS  
 P.L.S. NO. 27598

|                |                |               |
|----------------|----------------|---------------|
| DRAWN: KPJ     | REVIEWED: BB   | SHEET 8 OF 12 |
| DATE: 11/22/17 | PLC JOB#: 2062 |               |



PEAK LAND CONSULTANTS, INC.  
 PEAK LAND SURVEYING, INC.  
 970-478-8644 FAX 970-478-8616  
 1000 LION'S RIDGE LOOP VAIL, CO 81657

DEPOSITED THIS 20 DAY OF 2017 AT \_\_\_\_\_ IN BOOK \_\_\_\_\_ OF THE EAGLE COUNTY SURVEYOR'S LAND SURVEY PLAT/RIGHTS-OF-WAY SURVEYS AT PAGE \_\_\_\_\_ RECEPTION NO. \_\_\_\_\_



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MINTURN

THE UPTOWN STORE  
GIFTS & ACCESSORIES

Thai  
KITCHEN

HOME DECOR

OPEN

Jeep



THE  
UPTOWN  
STORE  
GIFTS & ACCESSORIES

Minbur Realty  
PARKING  
RESERVED FOR  
CUSTOMERS & VENDORS  
NO OTHER VEHICLES  
WILL BE TOLDED OFF  
PROPERTY



MINTURN

# ZONING ANALYSIS

ZONING ANALYSIS:  
 PARCEL I & II  
 BOOK 429, PAGE 59  
 151 MAIN ST  
 MINTURN, COLORADO

**LOT SIZE:** PARCEL I: .086 ACRES (3,742 SF)  
 PARCEL II: .031 ACRES (1,353 SF)

**ZONING:** CLUB

**SETBACKS:** N/A  
 N/A  
 N/A

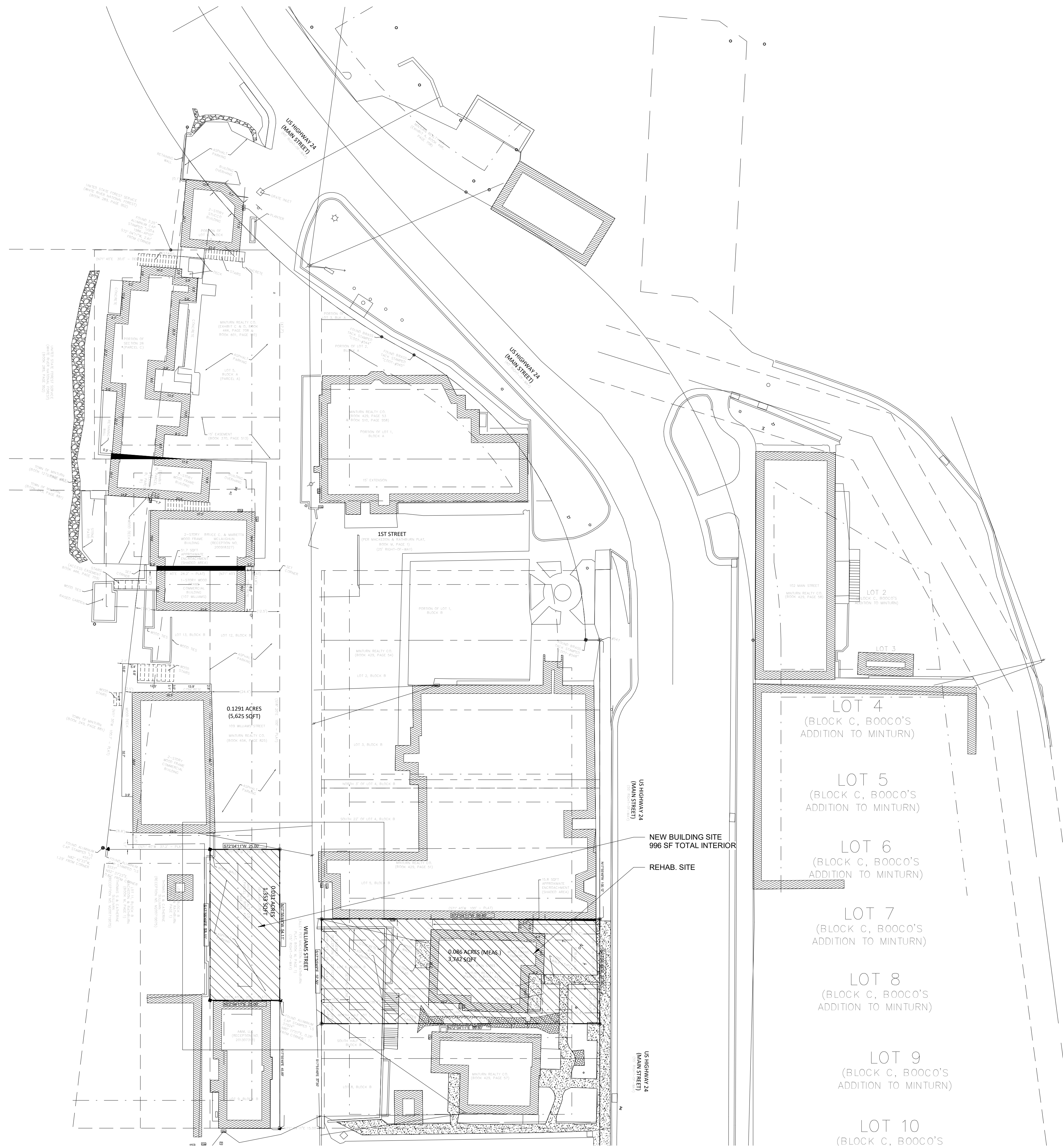
**HEIGHT:** N/A

**LANDSCAPING:**

**SITE COVERAGE:** PARCEL I EXISTING: 1,071 SF  
 PARCEL II PROPOSED: 733 SF

**PARKING:** PARCEL I:  
 1 PER 300 SF OF RETAIL SPACE  
 XXX SF = 3 PARKING SPOTS  
 PARCEL II:  
 1 PER 350 SF OF MEETING SPACE  
 650 SF = 2 PARKING SPOTS

② ZONING  
 12" = 1'-0"



① SITE PLAN  
 1" = 20'-0"

k . h . w e b b  
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 710 WEST LOUISHEAD CIRCLE  
 WAIL SPA CONDOMINIUMS UNIT A  
 WAIL, COLORADO 81657

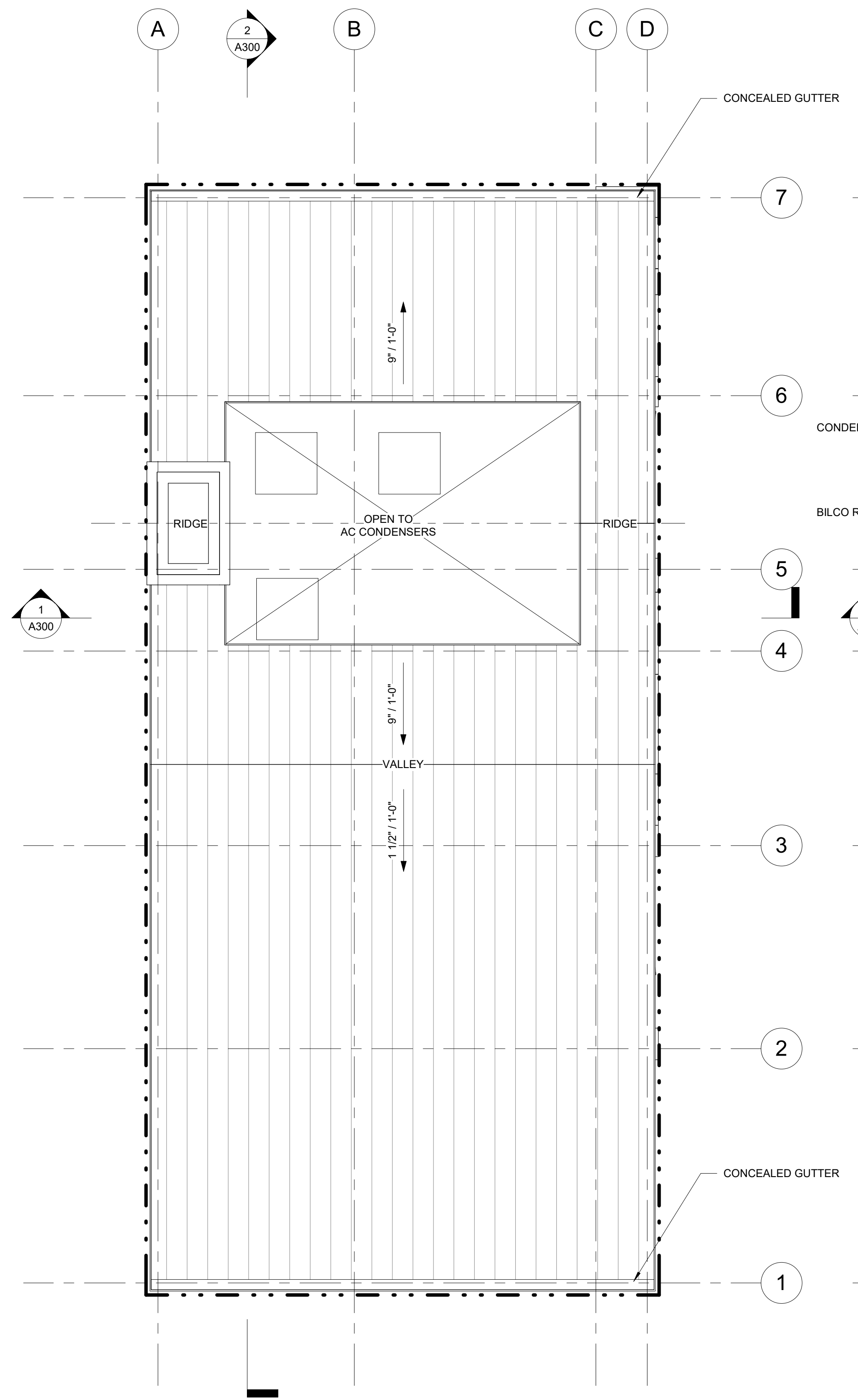
MINTURN MIKVEH  
 151 MAIN ST  
 MINTURN, COLORADO

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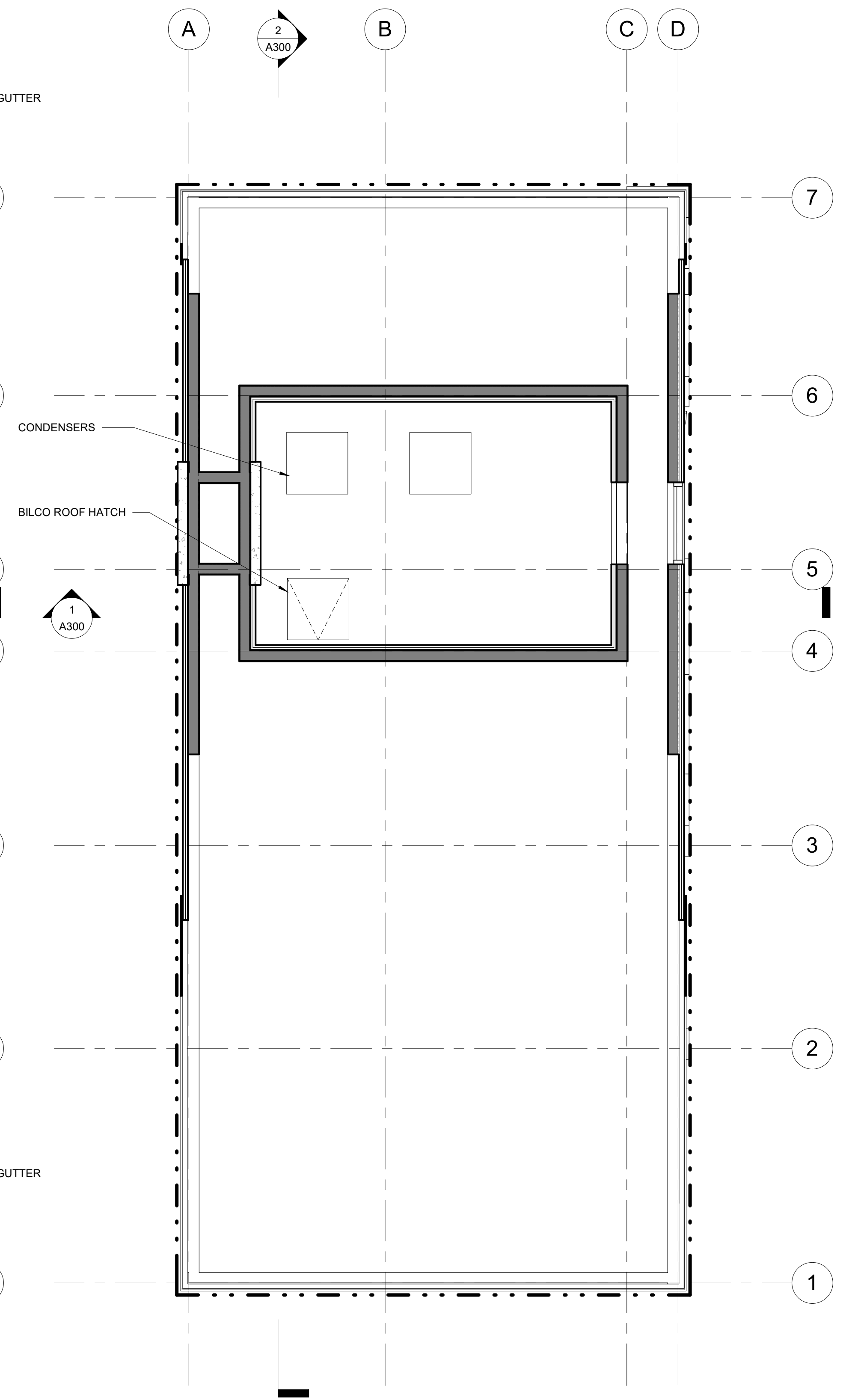
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SITE PLAN & ZONING

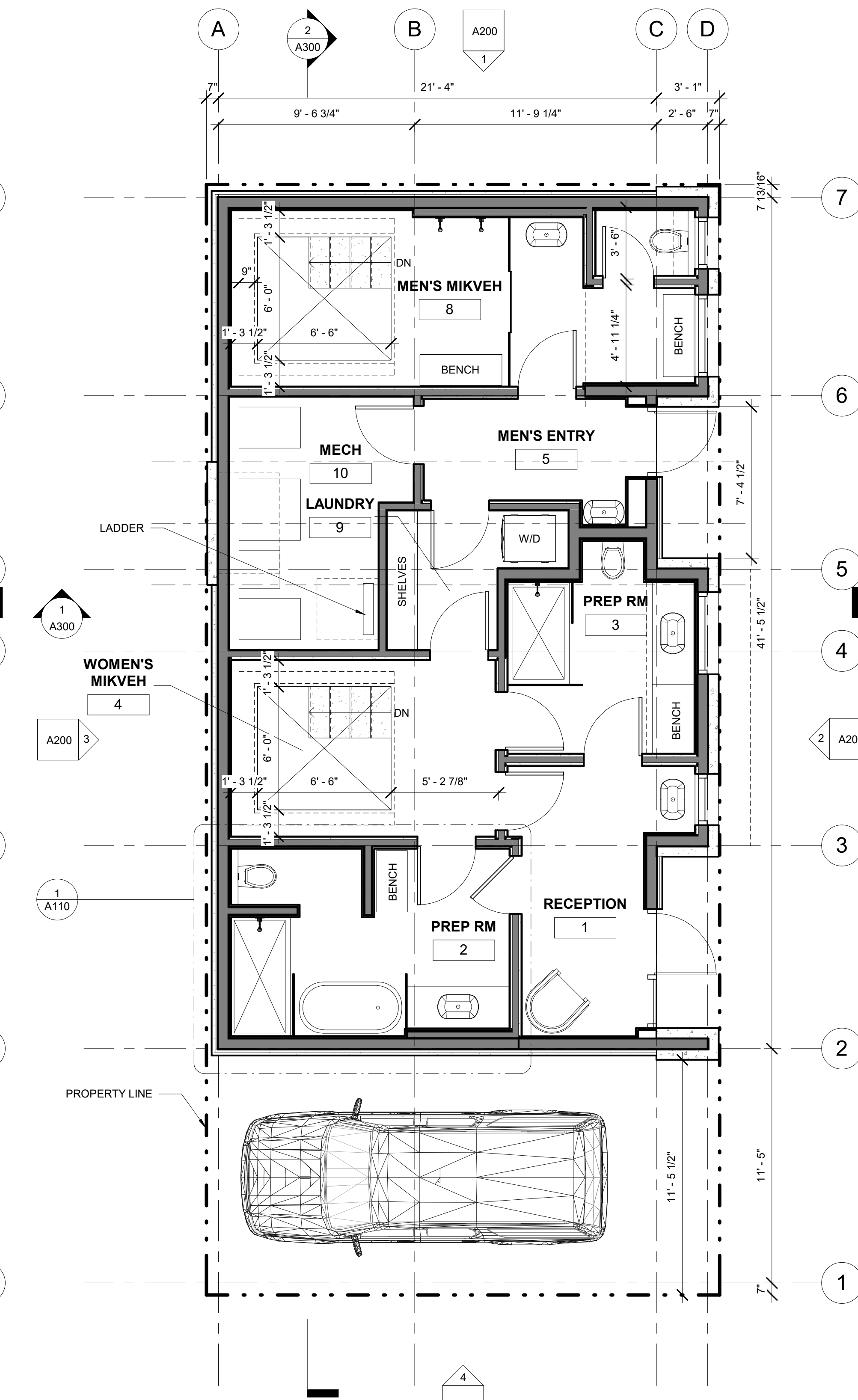
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3 ROOF PLAN  
1/4" = 1'-0"



2 ROOF MECH PLAN  
1/4" = 1'-0"



1 STREET LEVEL PLAN  
1/4" = 1'-0"

710 WEST LOUISHEAD CIRCLE  
WALL SPA CONDOMINIUMS UNIT A  
WALL, COLORADO 81657

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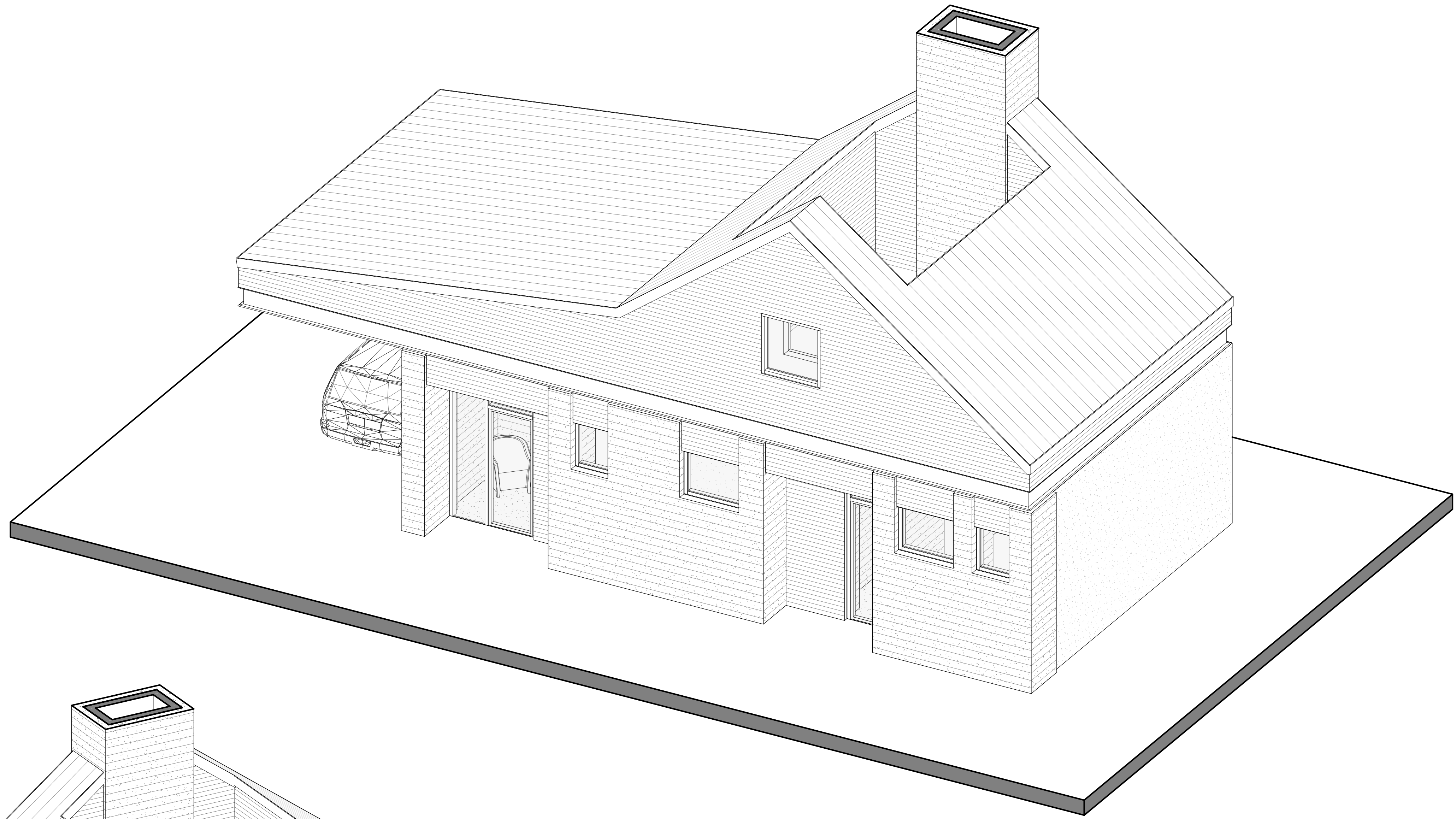
MINTURN MIKVEH  
151 MAIN ST  
MINTURN, COLORADO

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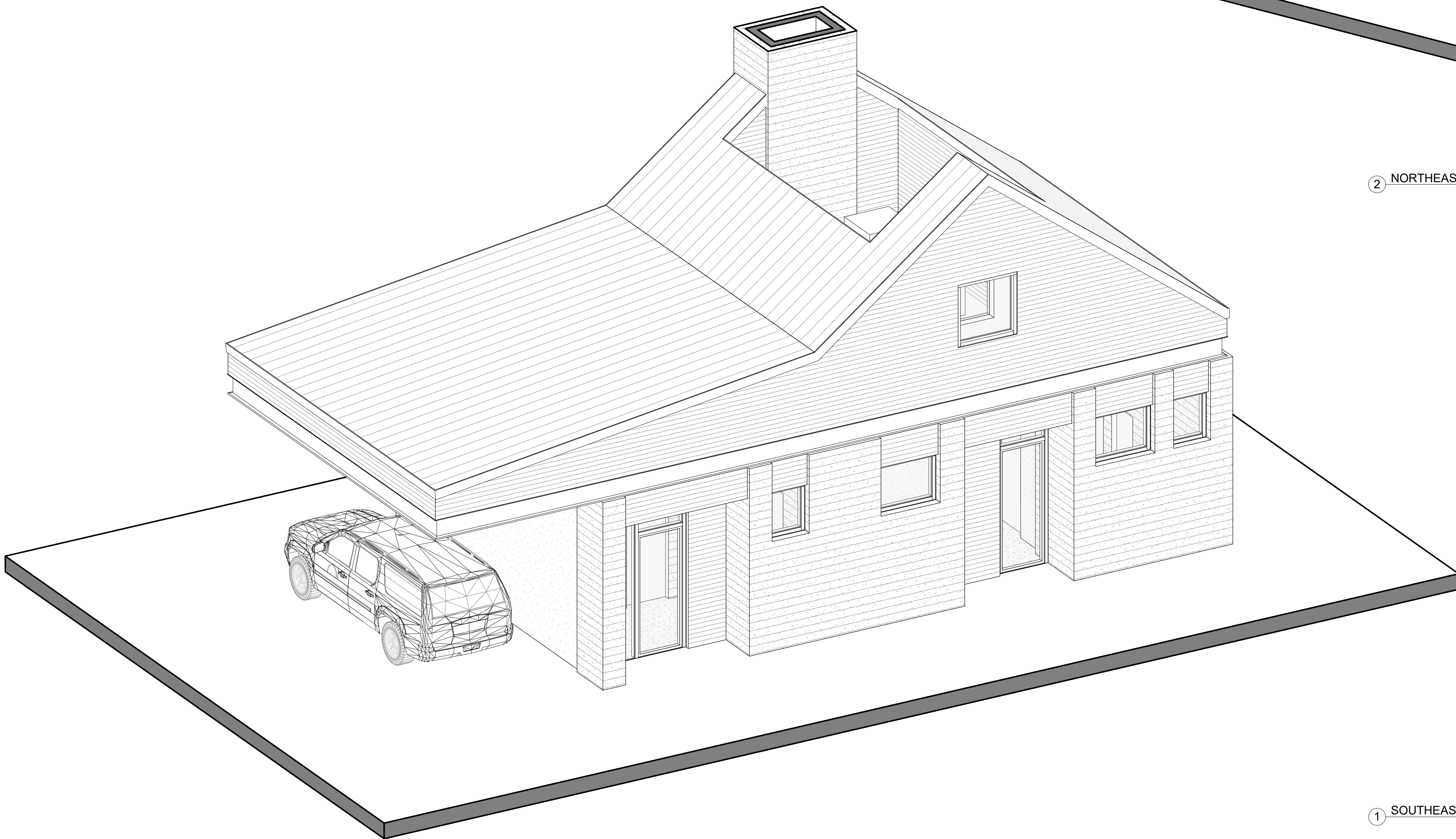
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PLANS

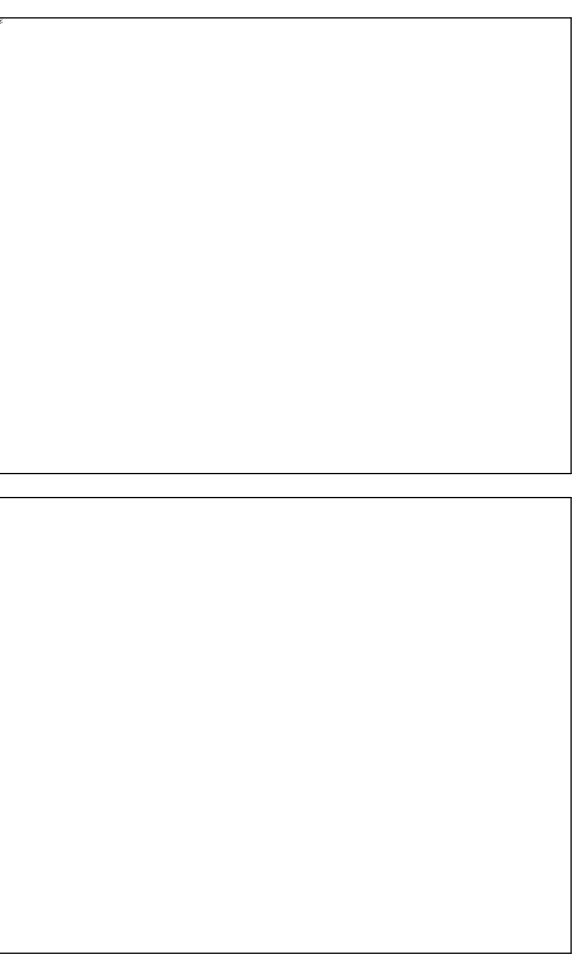
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2 NORTHEAST 3D



1 SOUTHEAST 3D



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 710 WEST LOUISHEAD CIRCLE  
 WAIL SPA CONDOMINIUMS UNIT A  
 WAIL COLORADO 81167

**MINTURN MIKVEH**  
 151 MAIN ST  
 MINTURN, COLORADO

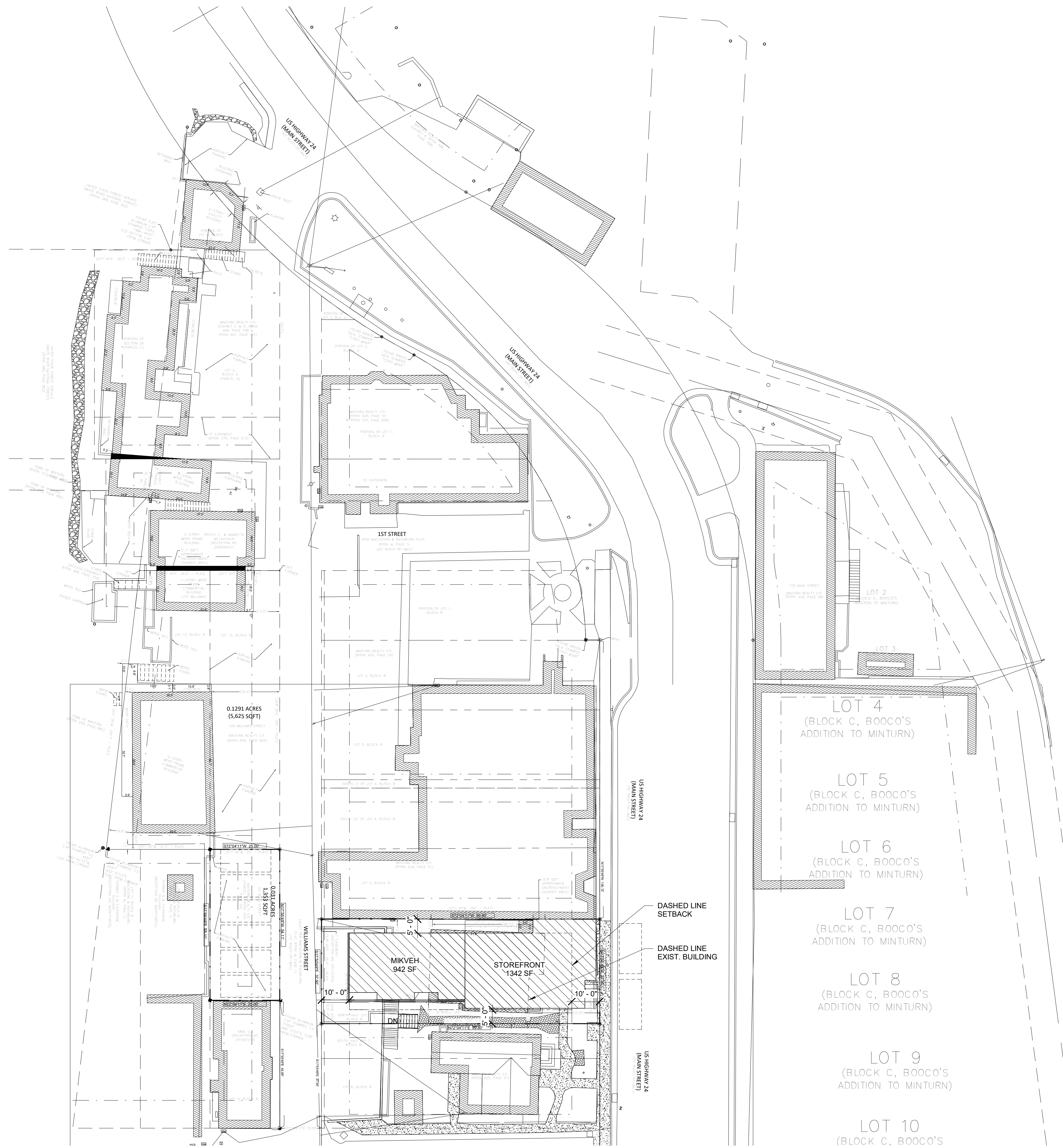
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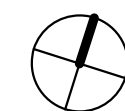
3D VIEWS

A900





① SITE PLAN OPTION 1  
1" = 20'-0"



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710 WEST LOUISHEAD CIRCLE  
WAIL SPA CONDOMINIUMS UNIT A  
WAIL, COLORADO 81657

MINTURN MIKVEH  
151 MAIN ST  
MINTURN, COLORADO

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SITE PLAN & ZONING

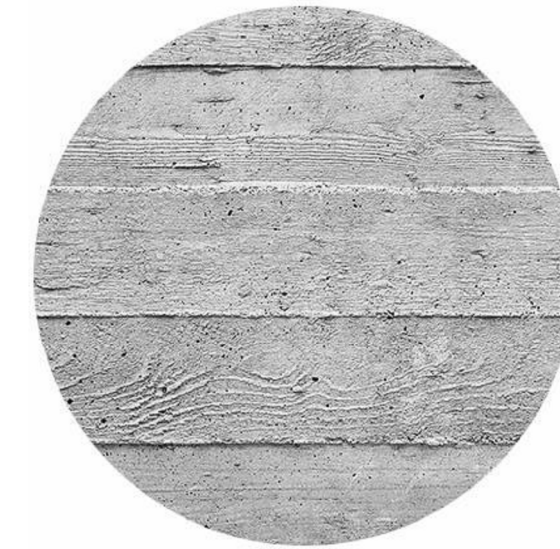
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STANDING SEAM METAL  
ROOF, TO MATCH  
WINDOWS



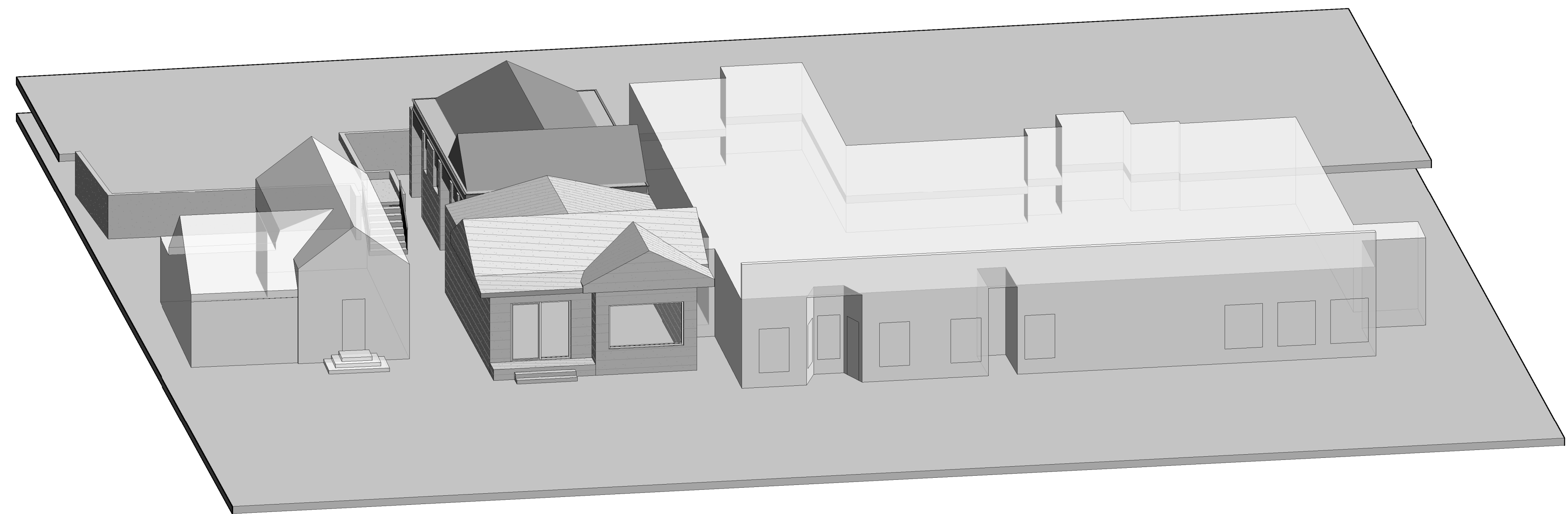
BOARD FORM CONCRETE



SHOU SUGI BAN  
WOOD SIDING



STEEL WINDOWS



③ 3D AERIAL **OPTION 2**



② 3D ALLEY VIEW



① 3D MAIN STREET VIEW

710 WEST LOUISHEAD CIRCLE  
MALL SPA CONDOMINIUMS UNIT A  
MALL COLORADO 81657

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MINTURN MIKVEH

151 MAIN ST  
MINTURN, COLORADO

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Project number  
Date 07.28.21  
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3D VIEWS & EXTERIOR  
MATERIALS

**A900**