



**Wednesday, September 7, 2022
Regular Session – 5:30pm**

AGENDA

**Town Council Meeting Minturn Town Hall / Council Chambers
302 Pine St Minturn, CO**

The agenda is subject to change, including the addition of items 24 hours in advance or the deletion of items at any time. The order of agenda items listed are approximate. This agenda and meetings can be viewed at www.minturn.org.

MEETING ACCESS INFORMATION AND PUBLIC PARTICIPATION

This will be an in-person meeting with access for the public to attend in person or via the Zoom link included. Zoom Link: <https://us02web.zoom.us/j/86090625058>

Zoom Call-In Information: **1 651 372 8299 or 1 301 715 8592 Webinar ID: 860 9062 5058**

Please note: all virtual participants are muted. In order to be called upon an unmuted, you will need to use the “raise hand” feature in the Zoom platform. When it’s your turn to speak, the moderator will unmute your line and you will have five (5) minutes for public comment.

PUBLIC COMMENTS: If you are unable to attend, public comments regarding any items on the agenda can be submitted to Jay Brunvand, Town Clerk, prior to the meeting and will be included as part of the record.

- 1. CALL TO ORDER**
- 2. ROLL CALL & PLEDGE OF ALLEGIANCE**
- 3. APPROVAL OF CONSENT AGENDA** *Consent agenda items are routine Town business, items that have received clear direction previously from the council, final land-use file documents after the public hearing has been closed, or which do not require council deliberation.*
 - 3.1 August 17, 2022 Meeting Minutes Pg 4
- 4. APPROVAL OF REGULAR AGENDA** *Opportunity for amendment or deletions to the agenda.*

5. DECLARATION OF CONFLICTS OF INTEREST

6. PUBLIC COMMENT *Citizens are invited to comment on any item on the Consent Agenda, or not on the regular Agenda subject to a public hearing. Please limit your comments to five (5) minutes per person unless arrangements have been made for a presentation with the Town Clerk. Those who are speaking are requested to state their name and address for the record.*

7. SPECIAL PRESENTATIONS *Presentations are limited to 5 minutes. Invited presentations are limited to 10 minutes if prior arrangements are made with the Town Clerk.*

7.1 Eagle Valley Community Foundation – Melina Valsecia, Director, EVCF Pg 14

8. COUNCIL COMMENTS & COMMITTEE REPORTS

9. BUSINESS ITEMS *Items and/or Public Hearings are listed under Business may be old or new and may require review or action by the Council.*

9.1 Resolution 34 – Series 2022 A Resolution supporting Minturn’s participation in the Eagle County Energy Conservation Code Cohort Pg 31

9.2 Ordinance 11 – Series 2022 (Second Reading) An Ordinance creating 100 Block Design Guidelines Pg 35

10. DISCUSSION / DIRECTION ITEMS

11. STAFF REPORTS

11.1 Town Manager Update Pg 100

12. FUTURE AGENDA ITEMS Pg 109

13. EXECUTIVE SESSION: An Executive Session pursuant to C.R.S. 24-6-402(3)(b) for the purposes of receiving legal advice and C.R.S. 24-6-402(4)(e) for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for negotiations, and/or instructing negotiators.

13.1 Battle Mountain – Plaskov

14. ADJOURN

INFORMATIONAL ONLY ITEMS

Upcoming Council Meetings:

- September 21, 2022 – Joint Session with Planning Commission
- October 5, 2022 – Joint Session with Historic Preservation Commission

- October 19, 2022
- November 2, 2022



Wednesday, August 17, 2022

Work Session – 3:30pm

Executive Session – 4:30pm

Regular Session – 5:30pm

OFFICIAL MINUTES

**Town Council Meeting Minturn Town Hall / Council Chambers
302 Pine St Minturn, CO**

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MEETING ACCESS INFORMATION AND PUBLIC PARTICIPATION

This will be an in-person meeting with access for the public to attend in person or via the Zoom link included. Zoom Link: <https://us02web.zoom.us/j/87054433644>

Zoom Call-In Information: **1 651 372 8299 or 1 301 715 8592 Webinar ID: 870 5443 3644**

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PUBLIC COMMENTS: If you are unable to attend, public comments regarding any items on the agenda can be submitted to Jay Brunvand, Town Clerk, prior to the meeting and will be included as part of the record.

1. WORK SESSION

Note: The Council entered the Work Session at 3:40pm.

1.1 100 Block Design Guidelines Review

The Minturn Town Council will open the Regular Meeting at 4:30pm for the purpose of convening into Executive Session. At (approximately) 5:30pm the Council will convene into Regular Session for the remainder of the meeting.

2. CALL TO ORDER

Note: The meeting was called to order by Mayor Earle Bidez at 4:31pm for the Executive Session portion only.

3. EXECUTIVE SESSION: An Executive Session pursuant to C.R.S. 24-6-402(3)(b) for the purposes of receiving legal advice and C.R.S. 24-6-402(4)(e) for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for negotiations, and/or instructing negotiators.

- 3.1 Battle Mountain – Sawyer
- 3.2 Conference with water counsel regarding Eagle River augmentation requirements – Siltanen
- 3.3 Conference with water counsel regarding litigation in Case No. 20CW3136 – Siltanen
- 3.4 Eagle River Surface Diversion Acquisition – Peterson-Cremer
- 3.5 Mountain top easement acquisition – Sawyer
- 3.6 Conditional Use Permits for housing in the 100 Block

Motion by George B., second by Gusty K., to convene in Executive Session pursuant to C.R.S. 24-6-402(3)(b) for the purposes of receiving legal advice and C.R.S. 24-6-402(4)(e) for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for negotiations, and/or instructing negotiators. Motion passed 7-0.

Those present in the Executive Session included: Mayor Earle Bidez, Mayor Pro Tem Terry Armistead, and Town Council members George Brodin, Lynn Feiger, Gusty Kanakis, Tom Sullivan, and Kate Schifani.

Staff present: Town Manager Michelle Metter, Attorneys Michael Sawyer, Justin Plaskov (zoom), Rob Marsh (zoom), Nick Poppe (zoom), and Water Engineer Kristy Radabaugh.

Direction given as a result of the Executive Session: No direction was given.

It was noted the Executive Session was recessed and will reconvene at the end of the Regular Session.

4. ROLL CALL & PLEDGE OF ALLEGIANCE

The meeting convened into Regular session at 5:45pm.

Those present in the Executive Session included: Mayor Earle Bidez, Mayor Pro Tem Terry Armistead, and Town Council members George Brodin, Lynn Feiger, Gusty Kanakis, Tom Sullivan, and Kate Schifani.

Staff present: Town Manager Michelle Metter, Attorney Michael Sawyer, Town Planners Scot Hunn (zoom) and Madison Harris, and Town Clerk/Treasurer Jay Brunvand

5. **APPROVAL OF CONSENT AGENDA** *Consent agenda items are routine Town business, items that have received clear direction previously from the council, final land-use file documents after the public hearing has been closed, or which do not require council deliberation.*

- 5.1 August 3, 2022 Meeting Minutes
- 5.2 Letter to United States Postal Service
- 5.3 Letter Supporting USFS Land Grant Application

Motion by Terry A., second by Tom S., to approve the Consent Agenda of August 17, 2022 as presented. Motion passed 7-0.

6. **APPROVAL OF REGULAR AGENDA** *Opportunity for amendment or deletions to the agenda.*

Add: continuation of the scheduled Executive Session at the end of the Regular Meeting

Motion by George B., second by Gusty K., to approve the Agenda of August 17, 2022 as amended. Motion passed 7-0.

7. **DECLARATION OF CONFLICTS OF INTEREST**

8. **PUBLIC COMMENT** *Citizens are invited to comment on any item on the Consent Agenda, or not on the regular Agenda subject to a public hearing. Please limit your comments to five (5) minutes per person unless arrangements have been made for a presentation with the Town Clerk. Those who are speaking are requested to state their name and address for the record.*

9. **SPECIAL PRESENTATIONS** *Presentations are limited to 5 minutes. Invited presentations are limited to 10 minutes if prior arrangements are made with the Town Clerk.*

10. **COUNCIL COMMENTS & COMMITTEE REPORTS**

George B. updated on the water tank construction.

11. **BUSINESS ITEMS** *Items and/or Public Hearings are listed under Business may be old or new and may require review or action by the Council.*

- 11.1 Ordinance 11 – Series 2022 (Second Reading) An Ordinance creating 100 Block Design Guidelines

This item was discussed in the Council Work Session held prior to this public meeting. Following are concepts of what has been discussed over the past few meetings:

Michelle M. opened the discussion with a brief history of the past proceedings and reviewed items still needed decision.

During the meeting on August 3rd, Council provided direction on the following:

- No residential allowed on ground level; all upper-floor residential is by conditional use permit only
- All structures in the 100 Block to be capped at 35' building height
- Consider parking by negotiation

Additional updates made to the 100 Block Design Standards & Guidelines include:

- Update to page 4 map simplifying the images to show only Blocks A vs Blocks B
- Updated 100 Block Framework Map on page 7 to show the town does not own the identified pedestrian passages and small gathering spaces
- Page 10 – added Minturn Guiding Principals
- Page 12 – updated the “current lot layout map” to identify the original platting of 25' wide lots with the current parcels overlaid
- Page 13 – Updated Eagle River Street dimension, text and graphic (no sidewalk)
- Page 15 – changed minimum setback for parking to 40' off of Main Street
- Page 15 – Added depictions of parking layout options to better explain how projects could park and activate the rear

Minturn 100 Block Chapter 16 Updates:

- No constrained lots
- Table 16-A: changed to require 5' side setbacks for all lots
- Table 16-6-70: All residential uses allowed on upper floors only (no ground floor) and by condition use only. Business and office services by conditional use only.
- Chapter 16; Article 16: Off Street Parking and Loading: Three options are being presented to the Council for consideration. These options will be discussed in depth during the work session and regular session. No tandem parking allowed
- Note: If Council decides to go with the newest parking option Table 16-A would need to be updated to reflect the 25' rear setback requirement

Ms. Cheney Bostic (zoom) gave a brief outline of current direction on parking onsite and still maintaining the desired view corridors.

Traffic Flow

Jeff Spanel, Minturn civil engineer, Inter-Mountain Engineering, submitted the conceptual traffic flow diagram to CDOT Region 3 Representatives and although we are yet to receive feedback, Jeff has indicated the following:

- A one-way heading north on Eagle River Street will most likely be denied due to the complex intersection at Eagle River Street & Main Street (next to Bellm Bridge)
- An additional vehicle access point (one way outlet) south of the Colorado Mattress building will most likely be denied given CDOT's requirements to have a minimum distance between access points for safety purposes
- A one-way heading north on Williams Street with a right turn only onto Main Street has the possibility of being approved. This would require a “no delivery truck” policy on Williams Street and a “Delivery Drop-Off Zone” on Main Street would be utilized at the northernmost west side of the parallel parking on Main Street (next to the Minturn

Country Club). Delivery drivers would then deliver their goods via a dolly (similar to every other mountain town delivery system) system.

Direction from Council is still required for the following items:

- Approve or modify recommended setback – 5ft setbacks and additional setback for onsite back parking.
- Determine a parking option
- Approve or deny turning Williams Street into a one-way heading north to a right turn only on Main Street including the Delivery Drop-Off Zone for delivery trucks
- Incentive Floor in the A Block – does Council want to keep the incentive requirements for Block A? This still allows a maximum of 35’ building height but requires a developer to provide the town incentives to obtain approval for the 35’.

Tom S. confirmed the rear setback on the east side of Williams would be a 5ft set back with onsite parking.

Discussion ensued that Williams St. would be changed to a North Bound one-way with right turn only at Williams and Main, Eagle River St would continue to be south bound. Williams would no longer be used for delivery trucks.

Parking option #1 was recommended. It was felt reduced parking was preferable to Cash in Lieu parking on the west side of Williams.

Step back of 3rd floor from 2nd floor to allow 25ft from Main St. Council was in favor of the step back option incentive.

Public Hearing Opened

Mr. Bill Pierce, 172 Main, asked about the diagonal spaces on Eagle St, they would belong to the property. He asked of Williams St, with the step back on south Williams, it would be 90degree. He discussed the desire for covered parking by extending the 2nd level over the parking spaces. It was explained that covered parking causes a potential undesirable feel but a deck could extend over the parking. Mr. Pierce confirmed there will be no covered parking; correct, other than a potential balcony.

Ms. Krista Bristol, 115 Nelson, spoke in support of no covered.

Mr. Hawkeye Flaherty, 160 Main St, showed several pictures and spoke against the type of buildings that would be allowed to be built. He felt his home would be in a shadow from the next-door developments. He noted the highest parking dates are on the high holidays during the winter and spoke that parking should be required onsite and requirements not reduced.

Mr. Darrel Wiegert, 1716 Main St., asked about delivery trucks and it was discussed. Asked about cash in lieu parking, it would be a one-time fee, it was noted that proposal was not included with the parking plan being considered.

Ms. Lynn Teach, 155 Main St., spoke against the Main St delivery truck option and stated it would further reduce parking.

Mr. Ron Levin (zoom), 172 Main St, discussed the rear setback on Eagle St. and parking concerns.

Mr. Hawkeye Flaherty, 160 Main St, asked about removing a house in that area. It would have to be built back as commercial. He contended the dates of the parking study was not on the right dates and felt the data was incorrect.

Tom S. stated he did not feel the changes proposed are not that different from existing code other than the commercial parking option.

Earle B. discussed a parking survey that was conducted during a recent summer market. It was high use but parking was available within a reasonable distance.

Mr. Devan Bristol, 115 Nelson, asked about if the fire dept had seen the proposals and their comments. Michelle M. stated they had not expressed any hard concerns including on zero setbacks.

Public hearing closed.

Gusty K. asked about Cash In lieu, this is not currently an option.

Terry A. stated very little has changed on the 100 block. She stated this is a plan that can be utilized and it has been studied in-depth. She stated the Council has worked hard to provide options for development that does not also destroy that which we have cherished.

Tom S. desired to see the allowable 1st floor square footage to be vetted out further to better address the desired outcome. Ms. Bostic stated the use tables show use but not a mandated size. She stated this could be reviewed. Discussion ensued as to a reasonable amount to exceed 50% or more, active use, and how that might or might not achieve the desired outcome.

Direction given was:

- For 100 Block A go with Option C – “Woonturn” Option
- For 100 Block B use the reduced parking table with no Cash in Lieu
- Allow 8’ max overhang of open-air decks into the rear setback of 100 Block A. Must be 2nd floor and above.
- Change rear setback in 100 Block A to 25’, 30’ for the incentive floor
- Clarify delivery parking
- Require 70% of ground floor space to be commercial in 100 Block A, with 100% of the frontage being commercial. Look into adding an active use definition to help accomplish this.
- Ensure that with the changes made here, any ripple effects are addressed.

Motion by Terry A., second by Tom S., to continue to September 7 Ordinance 11 - Series 2022 (Second Reading) an ordinance amending Chapter 16 of the Minturn Municipal Code to amend

and replace certain land use, development, and design regulations and standards applicable to the Old Town 100 Block Commercial Zone District. Motion passed 5-2. Note: Lynn F. and Earle B. voted Nay.

11.2 Ordinance 14 – Series 2022 An Emergency Ordinance Extending the Design Review Guidelines Moratorium – Sawyer

As Council continues to discuss the 100 Block Design Guidelines Ordinance, we have brought forward an emergency ordinance to extend the existing 100 Block Moratorium for 90 days, or November 15, 2022. Under Minturn Charter Sec. 11.7, an emergency ordinance will become effective immediately, but only for 90 days. Passage must be unanimous.

Sec. 11.7 also prohibits extension of an emergency ordinance by a subsequent emergency ordinance. We have concluded that this emergency ordinance is allowed because, although the original 100 Block moratorium was enacted by emergency ordinance, the subsequent moratorium was enacted through regular order and two readings of an ordinance. As such, that moratorium can be extended by emergency ordinance.

Discussion ensued as to the best date to bring this back to Council for consideration. Direction was given to extend the moratorium through September 23, 2022.

Public Hearing Opened
No Public Comment
Public Hearing Closed

Motion by George B., second by Tom S., to approve Ordinance 14 – Series 2022 An Emergency Ordinance Extending the Design Review Guidelines Moratorium through September 23, 2022 as presented. Motion passed 7-0. Note: An Emergency Ordinance must pass unanimously.

11.3 Ordinance 12 – Series 2022 (Second Reading) an Ordinance modifying Article 19 regarding Sign codes – Harris

The proposed text amendment below this memo attempts to conform the code with Supreme Court precedent regarding regulation of signs. Most relevant is the 2015 case, *Reed v. Town of Gilbert, AZ* that invalidated regulations distinguishing between signs based on their content. The rule of thumb coming out of this case is that if the code enforcement officer must read the sign to enforce the regulation, then it is probably unconstitutional. Minturn’s Code continues to contain some content-based regulations on signs, such as reference to “political signs” and “picketing” and applying different standards for size and duration based on these regulations. Even reference to real estate signs is conceivably illegal under *Reed*.

The Court recently declined to extend *Reed* to apply to commercial off-premise signs – advertising for a business not located on the property where the sign is located. However, the Code actually allows off-premise signs, which is something Council requested more information on. At the August 3 meeting, Council directed staff to revise the ordinance to allow off-premise signs to be allowed in locations on Main Street within 250 feet of the business being advertised. That

amendment is contained in Sec. 16-19-110 (3)(d).

Public Hearing Opened
No Public Comment
Public Hearing Closed

Motion by Tom S., second by Kate S., to approve Ordinance 12 – Series 2022 an Ordinance modifying Article 19 regarding Political Sign codes. Motion passed 7-0.

11.4 Resolution 33 – Series 2022 A Resolution Approving the Regional Transit Authority IGA - Metteer

The Council has held two public hearings, has spoken in several public meetings in support of this effort, and approved Resolution 19 – 2022 a Resolution in support of and approving an Intergovernmental Agreement for the Regional Transit Authority Formation Committee. This Resolution modifies the original IGA to incorporate critical input received at public hearings and through additional stakeholder engagements. This Resolution includes. The proposed modified IGA to be considered via Resolution 33 – Series 2022 continues to support County wide needs for transit services, increase ridership and efficiency across the valley’s existing transit agencies, provide affordable or free transit to the valley’s visitors and employee base, strengthen the connection between the valley’s different communities, and advance local climate action goals by reducing car trip s and increasing the use of low or zero emission public transportation.

In the November 2022 County Election, the town will be asking the Minturn voters to support this RTA and to adopt a county wide sales tax of .005, no other funding source is being proposed.

Earle B. presented how this would affect sales tax county wide. He noted this is .005cent tax and does

Motion by Terry A., second by Gusty K., to approve Resolution 33 – Series 2022 a Resolution approving the Regional Transit Authority Inter Governmental Agreement as presented. Motion passed 7-0.

12. DISCUSSION / DIRECTION ITEMS

13. STAFF REPORTS

13.1 Town Manager Update

Minturn Market

I have been acutely familiar with the Minturn Market since I started working at the Town of Minturn in 2009, and although this is only (approximately) half of the market’s lifespan as this summer we’re celebrating its 25th season, I believe it is time to rethink the summer market. With both visitor and vendor numbers down for the last several seasons and competition from other (newer) markets in the area, Minturn should embrace the inevitable evolution of the summer market.

As with the original creation of the market, I recommend creating a community visioning committee to consider how the market might grow and evolve into a fresh and exciting new version of the Minturn Market. I look for Council to ask questions and provide feedback on this consideration.

Congressionally Directed Spending

Minturn applied for \$2,000,000 toward the new water treatment plant in the 2023 round of Congressionally directed spending applications. Unfortunately, we heard from Senator Bennet's office this week that the subcommittee in the House did not approve Minturn's or many other western slope projects for funding as they're looking to spread the funds around to new communities. The Town of Minturn will continue our efforts toward grants in support of the water treatment plant costs.

Historic Preservation – Public Notices

Minturn has executed three historic preservation public notices thus far. There is limited time to nominate a structure once the public notice is published. More information on historic preservation public notices can be found at: <https://www.minturn.org/home/news/historic-preservation-public-notice-pursuant-mmc-sec-19-9-10>.

CDPHE – Funding Water & Wastewater Improvements

I virtually attended a webinar on funding for water and wastewater projects presented by the Colorado Department of Public Health and Environment. It was beneficial in understanding the Town's chances in being awarded any construction grants (it won't be impossible but because Minturn is not a "Disadvantaged Community" it will be more difficult). We learned about additional opportunities through the Colorado Water Conservation Board which may be an option and worth exploring.

Bear/Wildlife Garbage Issues

Minturn continues to see too many violations of unlocked or broken trash bins. More care must be taken to protect the local wildlife. Fines will start being issued for violations in relation to the improper use of bear-proof containers.

14. FUTURE AGENDA ITEMS

14a: Motion by George B., second by Tom S to reconvene in Executive Session. (7:49pm)

The Council reconvened in Regular Session at 10:30pm.

Direction given as a result of the Executive Session: No direction was given.

15. ADJOURN

As there was no further business the meeting stood adjourned at 10:30pm.

Earle Bidez, Mayor

ATTEST:

Jay Brunvand, Town Clerk

INFORMATIONAL ONLY ITEMS

Council Meetings:

- September 7, 2022
- September 21, 2022
- October 5, 2022



passionate
people.



powerful
work.



Eagle Valley
Community
Foundation



proven
impact.



Melina Valsecia-Monreal
Executive Director

Dan Godec
Chairman of the Board of
Directors

Eagle Valley Community Foundation



OUR VISION

A community network that works together to build trust, hope, and wellness for all.

OUR MISSION

EVCF provides access to programs and resources that support wellness, healthier food, and other essential needs in our community while embracing individual dignity and a commitment to sustainability and quality.



Organizational Values

Boldness: We aspire to be courageous about issues of equity and inclusion. We challenge ourselves to continually expand our organizational impact.

Dignity: We believe that a just society can become a reality when it is based on the inherent value that's tied solely to each person's humanity.

Integrity: We carry out our work with responsibility, transparency and accountability.

Innovation: We embrace continuous improvement, bold creativity and openness to transformational change.

Collaboration: We believe in the value of working with others and achieving together what may not be possible alone.

Pillars of EVCF



Healthy Food Access and Sustainability

The Community Market is local healthy and sustainable food system that includes a combination Food Bank, multiple local mobile markets, and a comprehensive grocery rescue effort. The Community Market partners with existing hunger projects such as the Vail Valley Salvation Army Food Pantry and congregate meals through local churches and other service providers.



Health and Wellness

MIRA (Mobile Intercultural Resource Alliance) is an RV that travels to our local communities focused on reducing barriers to health for our most under-served community members. The resources and services on MIRA are provided through strong partnerships with Vail Health, Eagle County Public Health and local service providers.



Community Capacity Building

Nonprofit Leadership
Fiscal Sponsorship
Elevar Leadership & Impact Fund
Project NEXT

Eagle Valley Community Foundation
IMPACT BY THE NUMBERS



YOU donated
\$1,198,450
in 2021.



YOU volunteered
20,391
collective hours of
service.

YOU fed **14,115**
healthy hot meals to 300
children in support of families
trying to return to the
workforce post Covid-19.



YOU helped feed
2,750+
local people every
week through The
Community Market.



YOU ensured local restaurants
adversely affected by the
Covid-19 pandemic received
\$50,262
through our gift card program,
simultaneously helping to
feed 2,948 customers of
The Community Market who
received food from these
restaurants.



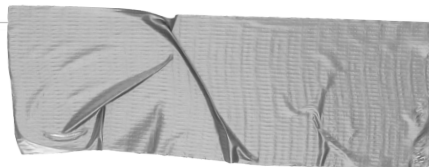
YOU made possible
8,761
MIRA connections
to health and
wellness resources.



YOU protected
6,570
people with Covid-19
vaccines through MIRA.

YOU paid school
lunch debt for
141
children in early
childhood
programs in the
school district.





Our Big Programs





A PROGRAM OF EAGLE VALLEY
COMMUNITY FOUNDATION

In Eagle County:

- o 8,700 residents struggle to put food on the table.
- o 30 percent of material sent to the landfill is food waste.
- The Community Market is turning excess into access.
- Working with local businesses to implement food rescue programs, The Community Market is reducing the amount of edible food sent to the landfill, by providing it to people who want to be part of the solution.
- The Community Market is committed to providing a dignified customer experience, promoting a healthy diet, and ultimately, helping to improve the overall health of community residents. The program is aimed at removing any stigma around food access and food security.
- We currently serve over 2,800 customers per week.
- 59% of the food distributed is fresh produce and 576,000 pounds of food was prevented from going to waste through our food rescue efforts.

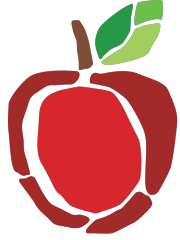


MOBILE INTERCULTURAL
RESOURCE ALLIANCE

ALIANZA MÓVIL DE
RECURSOS INTERCULTURALES

The goal of MIRA is to bring resources and services TO people.

- MIRA aims to serve as a single-stop, comprehensive, integrated and culturally relevant approach to resource connection and service delivery where people are.
- MIRA travels to neighborhoods, community sites and workplaces throughout Eagle County.
- Resources and services available on MIRA change based on community needs and priorities.
- Examples of available resources include basic health education and screenings, support in applying to public assistance programs, food resources, workforce development, and coordination with early childhood and physical activity programming, just to name a few.
- The resources and services are provided by a wide variety of local community organizations.
- MIRA is funded through a partnership with Vail Health and Eagle County Public Health. ²¹



The Community Market



HEALTHY PEOPLE

We believe that access to healthy food is a right, not a privilege.



This past year we served **2,750** customers weekly, up from pre-Covid numbers of **1,050** customers per week.

As the need has grown, we have:

- Increased our distribution of fresh produce sourced from grocery store donations, restaurants and regional Colorado farms to **59%**.
- Increased in-kind food product donations to **\$1,093,809**.
- Expanded grocery selection and capacity by relocating our Edwards storefront to a larger space at **429** Edwards Access Road in the Vail Health Edwards Community Health Center.

STRONG COMMUNITIES

We believe we are stronger when we work together.



We partnered with under-resourced communities in the valley to host pop-up mobile markets that are convenient and easily accessible. This participatory, community-centric environment allowed nearly **3,000** customers more autonomy in the food they receive. Plus, anyone can shop for free at our markets. No hoops to jump through – just good food and community bonding.

ENVIRONMENTAL SUSTAINABILITY

We believe reducing food waste plays a crucial role in protecting our environment.



The Community Market prevented over **576,000** pounds of food from going to waste through our local Food Rescue Program. That's almost 5x the amount of food we rescued in 2018! The program ensures food that is perfectly good for human consumption is rescued from local grocery retailers, farmers and food establishments and then properly distributed.



Unbelievable opportunity for people to get fresh food! Unbelievable opportunity to volunteer and donate to an organization that makes a huge difference in people's lives! — Peggy Bair

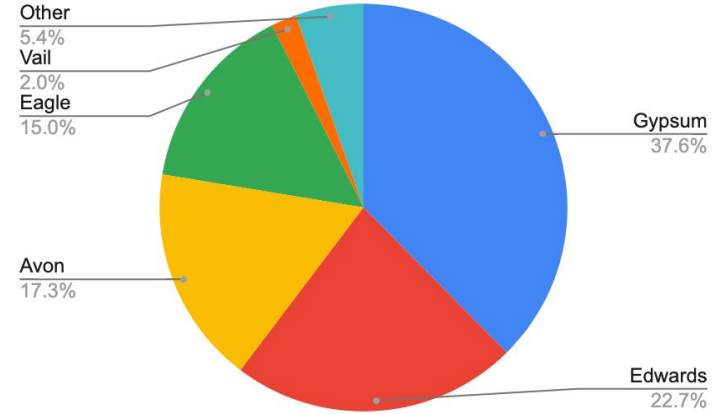
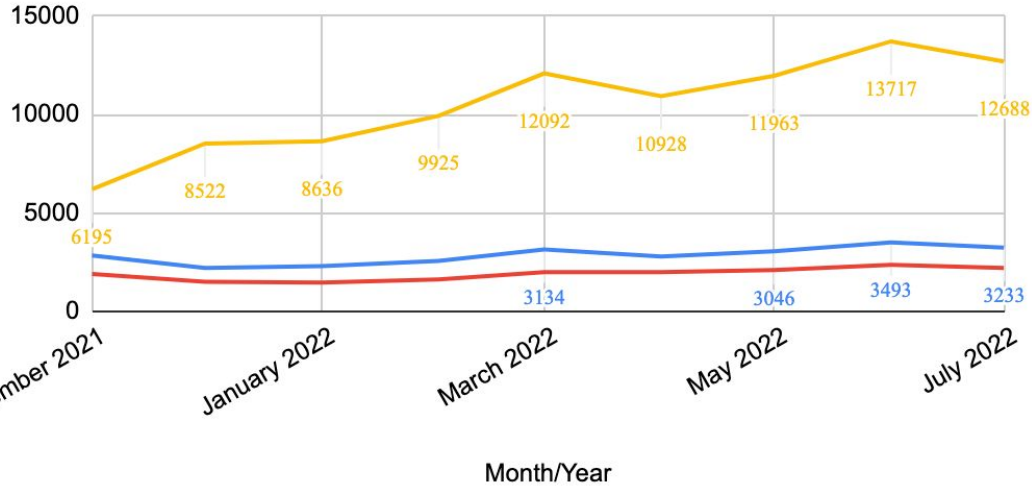


Eagle County Report



TCM Eagle County Report

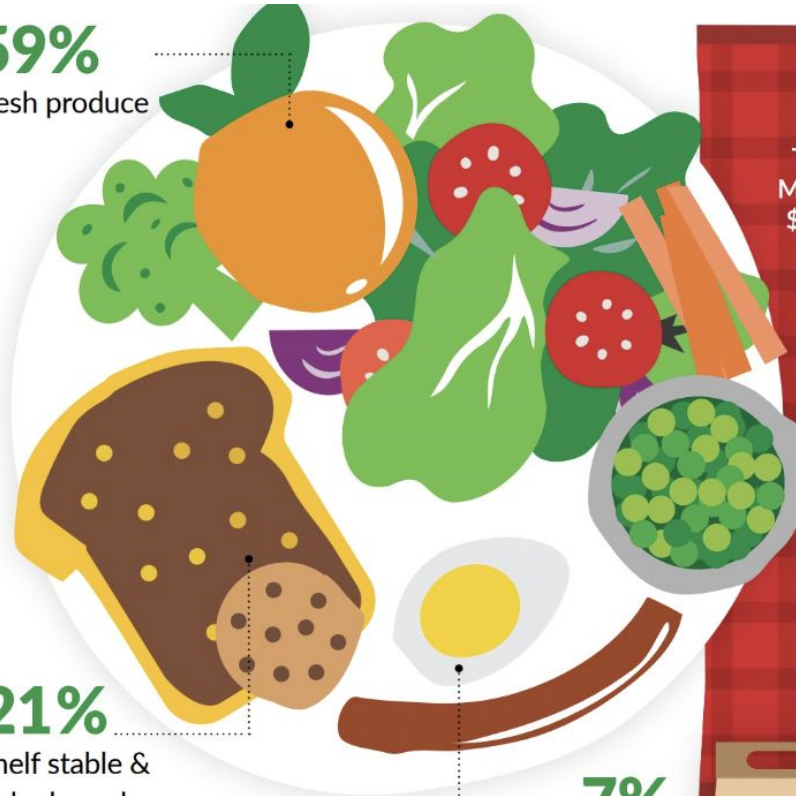
Visits One-time Visits Total People Fed





The Community Market

59%
fresh produce



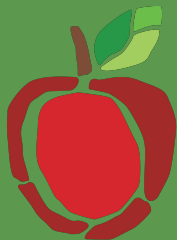
21%
shelf stable &
baked goods

13%
dairy & meats

7%
prepared meals

For every
\$1 donated,
The Community
Market distributes
\$7 worth of food.





We are
passionate people,
doing powerful work that
creates proven impact.





GOAL: Implement a mobile community resource center to improve systems of service delivery and increase accessibility of community resources. The mobile community resource center will enable government and community services, resources, and programming to go directly into neighborhoods, schools, community places and other gathering/event locations with culturally relevant branding, recognized and trusted leadership, and consistent data collection.

INITIAL KEY ORGANIZATIONS:

Eagle County: Public Health and Environment, Human Services, ECO Transit , IT, Innovation, GIS

Eagle Valley Community Foundation

Vail Health



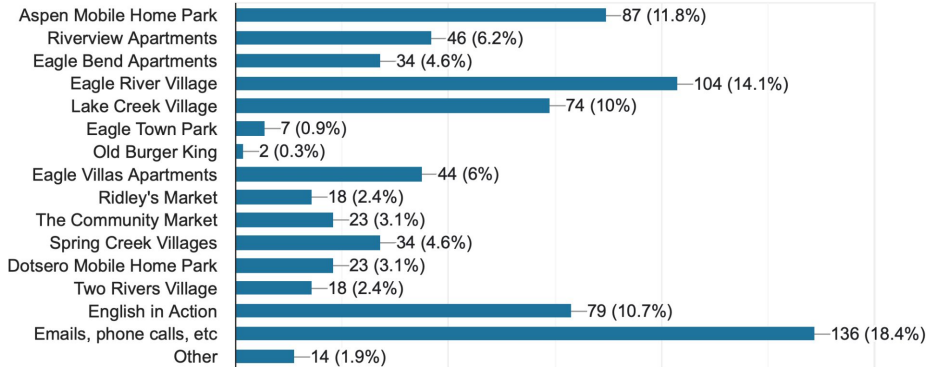
Barriers to Access

- Fear of government organizations
- Isolation and long distances
- Lack of cultural competence displayed by public serving organizations
- Lack of authentic community engagement



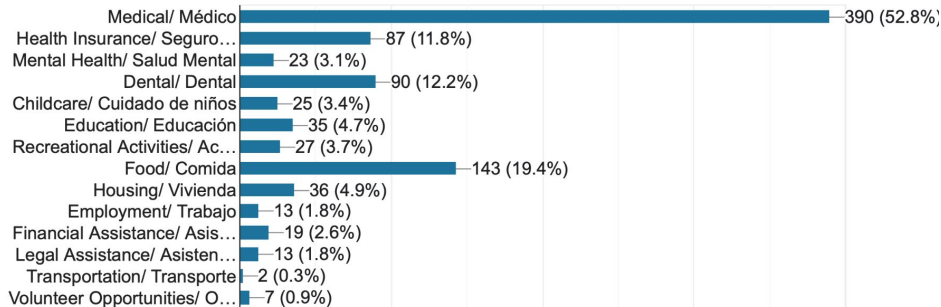
Bus Location/ Ubicación del Bus

739 responses



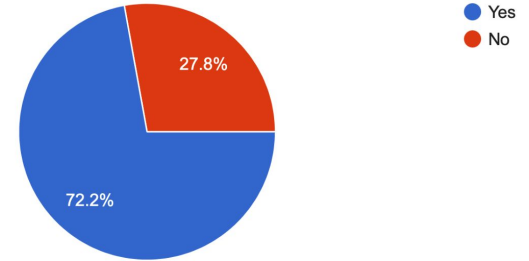
What resources are you interested in?/ ¿Qué recursos te interesan?

739 responses



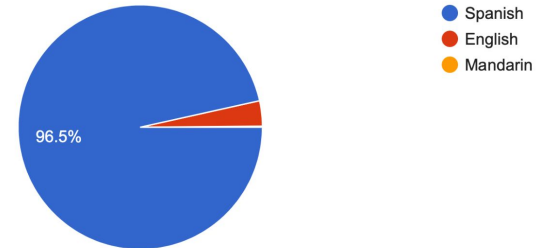
Returning Client/ Cliente que regresa

704 responses



Preferred Language/ Lenguaje de preferencia

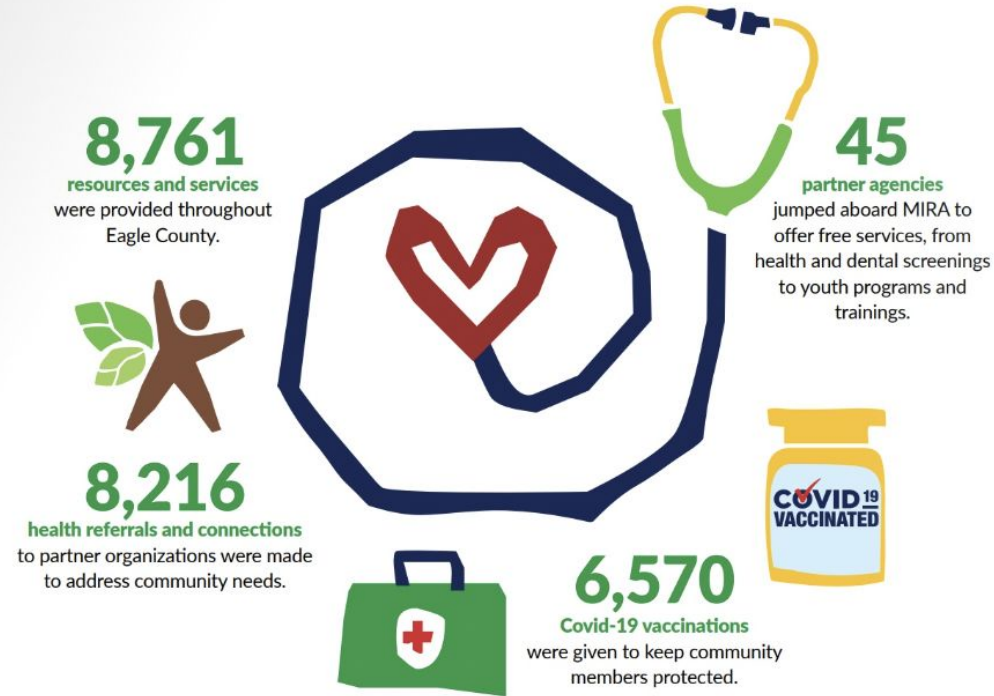
719 responses




Mobile Intercultural Resource Alliance



MIRA IS HELPING OUR NEIGHBORS ACCESS GREATER HEALTH AND WELLNESS:



 My hope is to offer women's health screening and adolescent contraceptive education to the population that MIRA Bus serves. I see MIRA as an essential part of reaching out to an underserved population, which is so necessary for the functioning of our county. — Dr. Elaine Huber

Passionate people.
Powerful work.
Proven impact.

Thank you!
Any questions?

Contact Information:

melina@eaglevalleycf.org

Executive Director





To: Mayor and Council
From: Jay Brunvand
Date: September 7, 2022
Agenda Item: Resolution 34 – Series 2022

REQUEST:

Staff is recommending Council to approve Resolution 34 – Series 2022 as presented.

INTRODUCTION:

The Town of Minturn adopted the Climate Action Plan for the Eagle County Community with Resolution 05-2017. The request tonight will allow the town to consider review of further guidelines of greenhouse emission reductions in the future.

ANALYSIS:

The Town of Minturn has partnered with the Climate Action Collaborative for Eagle County. This resolution allows the town to participate with the Collaborative in review of the 2021 International Energy Conservation Code using the Collaborative's assistance. This review will include possible amendments that would be specific to the Eagle County community and would allow collaborative participation rather than the town undertaking and funding our own review.

This Resolution in no way binds the Town to any future approval of the 2021 International Energy Conservation Code.

COMMUNITY INPUT:

N/A

BUDGET / STAFF IMPACT:

The cost or impact of the final adoption of the 2021 International Energy Conservation Code is unknown at this time. By participating in this collaborative review, it is anticipated impacts to both the town and developers will be identified.

STRATEGIC PLAN ALIGNMENT:

In accordance with Strategy #4 the Town will advance decisions/projects/initiatives that expand future opportunity and viability for Minturn.

RECOMMENDED ACTION OR PROPOSED MOTION:
Motion to approve Resolution 34 – Series 2022

Attachments:

- Resolution 34 – Series 2022

**TOWN OF MINTURN, COLORADO
RESOLUTION NO. 34 – SERIES 2022**

**A RESOLUTION OF THE TOWN OF MINTURN TO
WORK WITH EAGLE COUNTY COMMUNITIES TO
IMPLEMENT THE 2021 INTERNATIONAL ENERGY
CONSERVATION CODE AND TO IDENTIFY AND ADOPT
SUPPORTING AMENDMENTS THAT ALIGN WITH
STATE LAW AND SUPPORT THE IMPLEMENTATION
OF THE CLIMATE ACTION GOALS IDENTIFIED BY THE
CLIMATE ACTION COLLABORATIVE FOR THE EAGLE
COUNTY COMMUNITY.**

WHEREAS, the Town of Minturn recognizes that the 2021 International Energy Conservation Code will improve the energy performance and emissions of the built environment and will thereby promote the health, safety, resiliency, affordability, sustainability, and general welfare of our community;

WHEREAS, the Town is a partner in the Climate Action Collaborative for the Eagle County Community, which has a goal to reduce local greenhouse gas emissions 50% by 2030 and an associated target to immediately adopt above building code standards and implement net zero or all-electric new construction codes by 2030;

WHEREAS, Xcel Energy has committed to achieve 80% renewable electricity by 2030, and 100% by 2050, therefore continuing to reduce building electricity CO2 emissions into the future;

WHEREAS, the 2021 International Energy Conservation Code is estimated to improve building energy efficiency eight to nine percent over the 2015 International Energy Conservation Code, therefore reducing operational energy costs;

WHEREAS, the Town of Minturn recognizes that regional collaboration and consistency will streamline the construction process for builders and developers who work across the Eagle County communities, and will reduce the costs for our local government to adopt and enforce energy codes;

WHEREAS, the Colorado Energy Office provide will provide free technical assistance to a cohort of Eagle County communities working collaboratively to adopt of the 2021 International Energy Conservation Code plus supporting amendments;

WHEREAS, Lotus Engineering and Sustainability, the Southwest Energy Efficiency Project, and technical code experts have partnered with the Colorado Energy Office to provide technical assistance to cohorts of communities across Colorado to adopt the aforementioned new construction code and amendments;

WHEREAS, the State of Colorado passed HB22-1362 requiring all Colorado communities to adopt and enforce an energy code that achieves energy performance equivalent to or better than

the 2021 international energy conservation code, including electric and solar ready code language;

WHEREAS, Eagle County communities can play a leadership role in the state in adopting and implementing updated energy codes that work for our unique built environment and economy, thereby influencing the transition towards climate-friendly homes and buildings and accelerating the benefits to our citizens;

NOW, THEREFOE, BE IT RESOLVED by the Minturn Town Council of the Town of Minturn, that:

SECTION 1: The Town of Minturn hereby commits to participation in the Eagle County Code Cohort, the adoption of the 2021 International Energy Conservation Code, and consideration of consistent supporting amendments for this code cycle and all future code cycles.

INTRODUCED, READ, APPROVED, ADOPTED AND RESOLVED this 7th day of September, 2022.

TOWN OF MINTURN

By: _____
Mayor

ATTEST:

Jay Brunvand, Town Clerk



To: Mayor and Council
From: Madison Harris, Planner I
Date: September 2, 2022
Agenda Item: Ordinance 11 - Series 2022: 100 Block Standards and Guidelines

REQUEST:

Continue to October 19, 2022 Ordinance 11 - Series 2022 an ordinance amending Chapter 16 of the Minturn Municipal Code to amend and/or replace certain land use standards, development standards, and design standards applicable only to the Old Town 100 Block Commercial Zone District.

INTRODUCTION:

Ordinance 11 - Series 2022 was continued to September 7, 2022 at the August 24, 2022 Council meeting.

ANALYSIS:

Currently staff and consultants are working through the “ripple effect” of the changes decided upon by Council, and thus staff is requesting this ordinance be continued to the second meeting in October.

COMMUNITY INPUT:

Ongoing

BUDGET / STAFF IMPACT:

TBD.

STRATEGIC PLAN ALIGNMENT:

The Town Council’s review of ordinances and their actions aligns with the following key strategies:

PRACTICE FAIR, TRANSPARENT AND COMMUNICATIVE LOCAL GOVERNMENT

THE TOWN WILL SEEK TO MAKE INFORMED, DATA-BASED DECISIONS WITH A STANDARD OF “DOING IT RIGHT.” WITH AN HONEST APPROACH TO ALL ASPECTS OF LOCAL GOVERNMENT AND A FOCUS ON THE PUBLIC PROCESS, THE TOWN COUNCIL AND STAFF ARE COMMITTED TO SERVING MINTURN WITH THE HONESTY AND INTEGRITY EXPECTED OF A SMALL-TOWN GOVERNMENT.

ADVANCE DECISIONS/PROJECTS/INITIATIVES THAT EXPAND FUTURE OPPORTUNITY AND VIABILITY FOR MINTURN

The ability for Minturn to approach development as resilient, sustainable, creative and diverse will allow the town to continue embracing what has “made Minturn, Minturn.” The town can further leverage its crossroads location as a valley-wide benefit and competitive advantage.

RECOMMENDED ACTION OR PROPOSED MOTION:

Continue the following to October 19, 2022:

1. Ordinance 11 - Series 2022 an ordinance amending Chapter 16 of the Minturn Municipal Code to amend and replace certain land use, development, and design regulations and standards applicable to the Old Town 100 Block Commercial Zone District.

ATTACHMENTS:

- Questions from StudioSeed
- Ordinance 11 - Series 2022
- 100 Block Design Standards and Guidelines
- 100 Block Code Updates

Considerations and questions for Minturn Town Council as it relates to setback and parking changes:

1) Parking Configurations 30-degree vs. 90-degree:

- a. A 30-degree parking configuration will yield 4 parking spaces per 50' wide lot, but due to the angle, issues may arise (especially on Eagle River Drive where there are existing structures at a neighboring property line) where maneuvering space may not be available, which could reduce parking to 3 spaces.
- b. A 30-degree parking configuration requires a 14' backup space and 20' deep parking space (34' total), so with a 15' ROW and 25' setback (40' total) essentially would provide a 6-foot area between the parking and the building setback for a sidewalk.
- c. 90-degree parking yields more spaces (5 per 50' wide lot) and would not create issues with neighboring property lines.
- d. However, 90-degree parking requires a 24' back up space and an 18' deep parking space (40' total), so 15' ROW + 25' setback (40' total) would mean that a building could land right at the edge of the parking with no space for a walkway.
- e. ***Consideration/Question: should 90-degree parking be allowed in addition or in lieu of 30-degree parking?***
 - i. An additional requirement could state that a min. of 5' must be provided between the edge of parking and a building for 90-degree parking, but this further reduces the ground floor usable space and overall program of a building.
 - ii. South of Nelson, this configuration will be required because it is required to remain a two-way street (no outlet to Toledo.)

2) Parking Requirements:

- a. Is it desired to use the **reduced parking table and no in lieu fee** for 100 Block: A and still require some parking (including Main Street spaces)? Or should parking very simply state that ***only residential parking is required on-site*** at one space per unit (exempting all commercial, including lodging, office, and light industrial)? This would simplify the process and open up more possible land use configurations.

3) Setbacks:

- a. The **25-foot setback would also reduce upper floors** (5-feet previously). As we learned in economic studies, residential will be key to creating a mixed use project so loss of space on upper floors is a concern.
- b. It is recommended that a **10-foot maximum encroachment** be allowed for upper floors for the following reasons:
 - i. It provides a covered arcade/pedestrian space adjacent to the building on the ground floor, which ideally will have entrances on the rear side now that we are "activating" Williams and Eagle River Streets. 10-feet is considered an acceptable dimension without compromising the architectural scale (when Cheney was pulling out the 2nd floor in live modeling mode, we were looking at substantially more than 10-feet)
 - ii. Allowing the 2nd floor to protrude would add 400 SF (on a 50' wide lot) of additional programming and would also encourage 3rd floor terraces integrated into the architecture that take advantage of views (especially on Eagle River side).

- iii. The previous direction of 8-foot maximum “unroofed” balconies is a concern given the mountain climate and snow. This scenario would not preclude this scenario from happening though.

4) Risks to Consider:

- a. Without the public investment to create the “woonerf” street design on Williams and Eagle River Streets, **will commercial uses want to locate on these streets – or will they sit empty?** This concept of the shared and activated street – **may only work as planned if the Town is committed to spending the dollars** to invest in ROW improvements.
- b. **Will residential uses be viable without covered/enclosed garage spaces?** As we learned from the economic models, residential is almost certain to catalyze any commercial.

5) Allowable Uses:

- a. Even with public investment in Williams and Eagle River Streets, it may be necessary to **extend allowable uses** that front onto these corridors. These types of uses to consider include: **professional services/offices** – architecture office, design/build, interior design, photography studio, non-profit office space, live/work, accountant, etc. These uses are currently exempt from the ground floor and could risk activation if allowable commercial uses (retail/restaurant) choose not to locate on Williams/Eagle River Streets.

6) Trash and Deliveries:

- a. As currently written, trash enclosures would be required to meet the **same setbacks** as the building – is this desired, or should they be allowed to be **closer to the street/pick up zone?**
- b. Deliveries are noted to be rerouted to **loading zones on Main Street** to avoid large truck traffic and tight turn radii on Williams and Eagle River. Would this include trash pick up or **would trash trucks be able to pick up in the rear** (this would be desired for multiple reasons)?

TOWN OF MINTURN, COLORADO
ORDINANCE NO. 11 – SERIES 2022

AN ORDINANCE OF THE TOWN OF MINTURN,
COLORADO AMENDING CHAPTER 16 OF THE MINTURN
MUNICIPAL CODE TO AMEND AND REPLACE ZONING,
LAND USE, DEVELOPMENT, AND DESIGN REGULATIONS
AND STANDARDS APPLICABLE TO THE OLD TOWN 100
BLOCK COMMERCIAL ZONE DISTRICT

WHEREAS, the Town of Minturn (“Town”) is a Colorado home rule municipality organized pursuant to Article XX of the Colorado Constitution and with the authority of the Town of Minturn Home Rule Charter for which the Minturn Town Council (“Town Council”) is authorized to act; and

WHEREAS, the Town of Minturn 2021-2023 Strategic Plan (hereinafter the “Strategic Plan”) seeks to “foster the authentic small town character that is Minturn,” and to “Lead Minturn to long-term viability while preserving its unique character and genuine mountain town community,” through specific strategic plan goals and policies;

WHEREAS, the Strategic Plan contains four key strategies for implementation including “Practice fair, transparent and communicative local government,” “Long-term stewardship of the natural beauty and health of Minturn’s environment,” “Sustain and invest in the things that define Minturn as a proud, sturdy mountain town to “keep Minturn Minturn,”” and “Advance decisions/projects/initiatives that expand future opportunity and viability for Minturn;” and

WHEREAS, the Strategic Plan contains specific strategies in support of proposed amendments to Chapter 16, Zoning, Appendix B, Design Standards, such as “Sustain and Invest in the Things That Define Minturn as a Proud, Sturdy Mountain Town to ‘Keep Minturn Minturn,’” “Implement methods to recognize historic structures,” “Enforce ordinances aimed at maintaining the health, safety, welfare and aesthetic of the town – snow, trash, nuisance abatement and zoning/land use,” and

WHEREAS, the Town is experiencing development pressure in several zoning districts and neighborhoods, particularly in the “100 Block Commercial Zone District” (hereinafter the “100 Block”) and desires to address and update applicable zoning, land use, development and design regulations standards for the 100 Block; and

WHEREAS, in 2021, the Town commenced an update to the 2009 Community Plan (hereinafter “the Community Plan Update Project”); and

WHEREAS, a key objective of the Community Plan Update Project has been to review and update the Town’s zoning, land use, development and design regulations and standards, inclusive of certain chapters and sections of “Appendix B,” Town of Minturn Design Guidelines and Standards; and

WHEREAS, the Town has prioritized the updating of zoning, land use, development and design regulations and standards specific to the 100-Block;

WHEREAS, Town Council has directed staff to draft text amendments to Minturn Municipal Code Chapter 16, the Town Land Use Regulations, in accordance with the Town’s strategic plan and the Community Plan; and

WHEREAS, on June 22, 2022 the Minturn Planning Commission recommended approval of this ordinance; and

WHEREAS, the Minturn Planning Commission and Town Council have determined that the text amendments to the Land Use Regulations Chapter 16 as provided herein are necessary and proper.

NOW THEREFORE, BE IT ORDAINED BY THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO:

SECTION 1. The foregoing recitals are incorporated herein as if set forth in full.

SECTION 2. Chapter 16 of the Minturn Municipal Code is hereby amended read as follows, with additions shown in double underlined text and ~~strike through language~~ is deleted. Sections of Chapter 16 which are not expressly described in this Ordinance are deemed to continue to be in full effect without change.

ARTICLE 2 - Definitions, Illustrations and Lot Standards

* * *

Sec. 16-2-20. - Definitions.

Building height or height of building means the maximum vertical (plumb) distance measured at any point along the perimeter of the structure from the finished or natural grade (whichever is more restrictive) and a point on the roof depending on the roof type. No portion of any structure shall exceed the applicable building height limitation contained in this Code. (See Illustration No. 16-2.)

Height measurement points specified for the following types of roofs:

Flat roof: Height shall be measured to the highest point of the building, including parapet walls and rooftop appurtenances, but excluding architectural features and chimneys which may be permitted by Planning Director.

~~*Gable, hip, gambrel or shed roof:* Height shall be measured to the mean height level between the highest ridge or wall and its highest associated eave, provided, however, that if any parapet wall equals or exceeds the height of the highest ridge, then vertical distance shall be measured to the highest point of the parapet. Mansard roof height shall be measured to the decline of the roof.~~

See also [Section 16-3-30](#).

Pitched roof means a gable, hip, gambrel or shed roof where height shall be measured to the mean height level between the highest ridge or wall and its highest associated eave, provided, however, that if any parapet wall equals or exceeds the height of the highest ridge, then vertical distance shall be measured to the highest point of the parapet.
Mansard roof height shall be measured to the decline of the roof.

Half stories means habitable space that is located within the pitched roof portion of a structure or where the majority of the habitable space is below-grade. Habitable space within a half story shall have a minimum floor to ceiling height of 5 feet at the lowest point. See Sec. 16-2-65.

Incentive floor means an additional floor of development that is allowed only when additional requirements for the development are met. See Sec. 16-2-65.

Parking In lieu fee means a fee that is paid by a developer to the Town of Minturn in exchange for a reduced parking requirement. See Sec. 16-16-150.

Publicly accessible means a space such as a plaza or sidewalk on private property that is directly accessible from a public right-of-way and use by the general public is allowed.

* * *

Sec. 16-2-30. – Illustrations.

Illustration No. 16-6

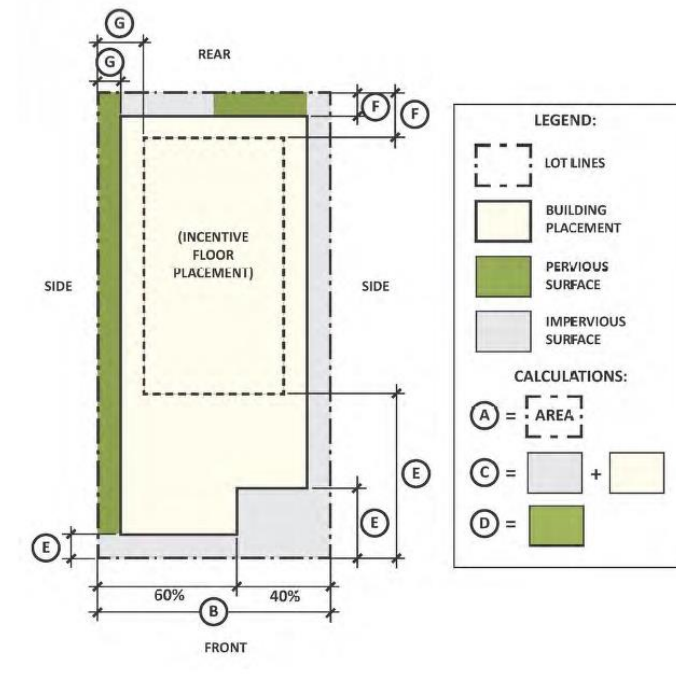
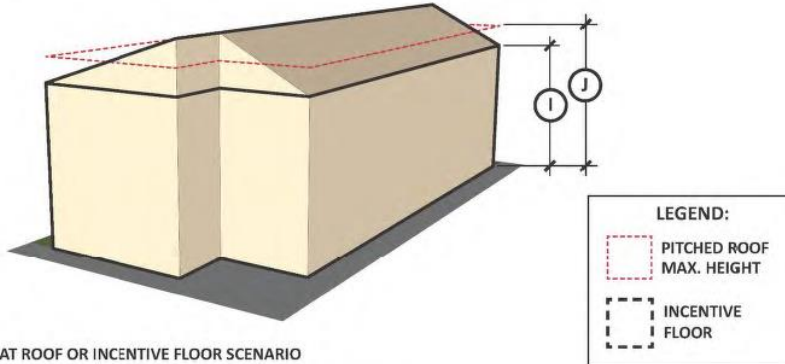
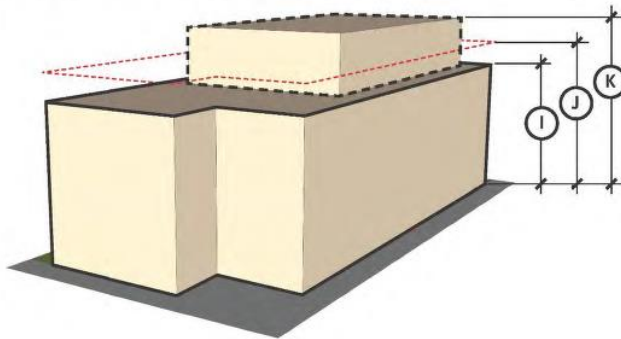


Illustration No. 16-7

PITCHED ROOF SCENARIO



FLAT ROOF OR INCENTIVE FLOOR SCENARIO



Sec. 16-2-40. – General lot requirements and dimensional standards.

Table 16-A
Dimensional Standards

| Character Area | Zones | <u>LOT REQUIREMENTS</u> | | | | <u>SETBACKS</u> | | | Live Stream Setback (ft) |
|-----------------|--------------------|--|---|--|--|---|------|------|-----------------------------------|
| | | <u>Min. Lot Area / Max. Lot Area</u> (sq. ft.) | <u>Min. Lot / Max. Lot Dimension</u> (feet) | <u>Maximum Building Lot Coverage</u> (%) | <u>Maximum Impervious Surface Area</u> (%) | <u>Minimum Setbacks (ft)</u> <u>^Incentive floor Setbacks (ft)</u> | | | |
| | | | | | | Front | Rear | Side | |
| Meadow Mountain | Fed. reg. land use | N/A | N/A | N/A | | Rec. 50 | N/A | N/A | Rec. 30 |
| Grouse Creek | Commercial | 5,000 | 50 | 70 | 80 | 20 | 10 | 10 | Eagle River=30 Grouse Creek=30 |

| | | | | | | | | | |
|----------|-----------------------------------|--|----------------|-----|-----|---|---|-----------------|----|
| Old Town | Recreation & open space | To be determined as part of conditional review | | | | | | | 30 |
| | Residential | 5,000 | 50 | 45 | 55 | 10 | 10 | 5 | |
| | 100 Block Commercial: <u>A</u> | 2,500 <u>/</u> 7,500 | 25 / <u>75</u> | 80% | 90% | 10 <u>5 min.</u> <u>15 min.</u> <u>40% lot frontage</u> <u>20 max.</u> <u>^25</u> | 10 <u>5</u> <u>^15</u> | 5 <u>^10</u> | |
| | <u>100 Block:</u> <u>B</u> | | | | | <u>5</u> | 10 <u>5</u> | 5 | |

* * *

Sec. 16-2-50 – Specific lot requirements and dimensional standards

(m) For structures two (2) or three (3) stories in height, a maximum roof length of sixty (60) feet in one (1) direction is allowed. After sixty (60) feet, the roof height needs to be lowered by a minimum of nine (9) feet for a minimum run of twenty (20) feet in length or change directions a minimum of ninety (90) degrees for a minimum length of twenty (20) feet.

(~~m~~) Sheds.

- (1) To constitute a shed that does not require a building permit, the structure shall be no larger than one hundred twenty (120) square feet in ground floor area and no higher than twelve (12) feet. Sheds in excess of one hundred twenty (120) square feet of ground floor area or twelve (12) feet in height require a building permit.
- (2) Sheds must be located in the rear or side yards, but such structures may not encroach into the side yard setbacks. A shed may encroach in the rear yard setback so long as it is a minimum of five (5) feet from all lot lines.
- (3) Sheds in all character areas and zones require a limited use review permit approved by the Planning Director. The denial of a limited use review permit by the Planning Director may be appealed to the Planning and Zoning Commission.
- (4) Sheds count toward the maximum impervious surface lot requirements.

(~~o~~) Greenhouses.

- (1) In all zones, a permanent greenhouse (which is not fully deconstructed and removed prior to winter) which exceeds one hundred twenty (120) square feet in the ground floor area or exceeds twelve (12) feet in height must receive a limited use review permit approved by the Planning Director and obtain a building permit. The denial of a limited

use review permit by the Planning Director may be appealed to the Planning and Zoning Commission.

- (2) Permanent greenhouses shall be counted toward maximum impervious lot coverage requirements.
- (3) Non-permanent greenhouses (which are fully deconstructed and removed prior to winter every year) do not count toward maximum impervious lot coverage requirements.

(p) Any residential structure constructed in a commercial zone district must comply with the applicable residential maximum building lot coverage limitation, the residential maximum impervious structure limitation, the residential building height limitation and residential setback requirements.

* * *

Sec. 16-2-60. – Building height limitations for all zone districts except 100 Block Commercial Zones.

* * *

Sec. 16-2-65. – 100 Block Commercial Zones building height limitations.

~~(a) In the 100 Block Commercial Zone, commercial buildings not fronting or adjacent to Highway 24 can have a maximum building height of thirty five (35) feet with a maximum angle of forty five (45) degree bulk plane from the street front setback or a maximum of twenty eight (28) feet for a flat roof.~~

~~(b) All buildings in the 100 Block Commercial Zone fronting or adjacent to Highway 24 located between Eagle Street and Williams Street can have a maximum building height of twenty eight (28) feet with a maximum angle of forty five (45) degree bulk plane from the street front setback or a maximum of twenty eight (28) feet for a flat roof.~~

~~(c) Slope of roof shall be a minimum of 4:12.~~

~~(d) Residential construction in the 100 Block Commercial Zone must follow residential building height limitations.~~

~~(See also Illustration No. 16-2 and Appendix B, Design Standards and Guidelines) The following Table 16-B contains the regulations for building height.~~

(a) New buildings in the 100 Block shall be allowed a maximum height as shown in Table 16-C.

(b) An incentive floor is allowed for 100 Block: A, following dimensional requirements listed in Tables 16-A and 16-C, and based on the provision of the following on-site improvements associated with the development:

i. An enhanced streetscape that provides a minimum of (1) street tree per 30 linear feet; (1) bench/seating area per 30 linear feet; and (1) bike rack accessible from a public right-of-way or passage; plus one of the following:

(1) A publicly accessible plaza or outdoor dining space (area shall be a minimum of 15% of total area of lot) that is visible from a public right-of-way.

(2) A publicly accessible passage (min. 4-foot wide pathway) is provided between Main Street and Williams Street or Main Street and Eagle River Street. A min. 8-foot side setback shall be required to accommodate this space. Public passages shall be a minimum of 150 feet apart, i.e. if one is already provided 100-feet away, this option shall not be used.

(3) An active use is provided along Williams or Eagle River Street.

Table 16-C

| | <u>I</u> | <u>J</u> | <u>K</u> |
|---------------------|--|---|---|
| | <u>100 Block Building Height Limitations</u> | | |
| | <u>Max. height / stories (flat roof)</u> | <u>Max. height / stories (pitched roof)</u> | <u>Incentive floor max. height / stories (any roof)</u> |
| <u>100 Block: A</u> | <u>28' / 2 stories</u> | <u>32' / 2.5 stories</u> | <u>35' / 3 stories</u> |
| <u>100 Block: B</u> | <u>35' / 3 stories</u> | <u>35' / 3 stories</u> | <u>N/A</u> |

* * *

Sec. 16-2-70. -- Specific front, side and rear yard setbacks requirements for 100 Block Commercial Zone.

- ~~(a) No parking spots allowed in the front or side setback on Highway 24 in the 100 Block Commercial Zone.~~
- ~~(b) No asphalt.~~
- ~~(c) Landscaping to include native trees, plants, tables, chairs, benches, decorative hardscape and planters for flowers.~~
- ~~(d) Pedestrian lights to be hung on building for pedestrian lighting. Meet dark sky requirements in Code Section 16-17-180, Exterior Illumination Standards.~~
- ~~(e) Must include irrigation.~~
- ~~(f) Must have exterior power outlets.~~
- ~~(g) All setbacks shall contain an area equal to five (5) percent of the total setback area for temporary snow storage (until snow can be removed off site).~~
- ~~(h) Ten (10) percent of each setback area must be landscaped area.~~
- ~~(i) Properties fronting Williams Street are encouraged to create an attractive, pedestrian friendly streetscape.~~

~~(j) Dumpsters, trash, and storage areas shall be properly screened.~~

~~(Ord. 5-2016 §2)~~

~~* * *~~

Sec. 16-2-80. -- Block and building site patterns and side yard setbacks for 100 Block Commercial Zone.

- ~~(a) Vacations of or building across originally platted lot lines shall not result in a building greater than seventy five (75) feet in width along Highway 24, Williams Street or Eagle Street.~~
- ~~(b) Building facades along Highway 24, Williams Street and Eagle Street shall not create the appearance of a single building that is wider than fifty (50) feet. Building facades must maintain the historic platted character of this area with building widths of twenty five (25) feet to fifty (50) feet.~~
- ~~(c) Maximum lot area (square feet): seven thousand five hundred (7,500).~~
- ~~(d) Minimum side yard setback is five (5) feet per twenty five (25) foot wide platted lot. For buildings fronting or adjacent to Highway 24 located between Eagle Street and Williams Street, the Planning Commission may require that the side yard setbacks be combined for the purpose of creating a ten (10) to fifteen (15) foot side yard corridor between two (2) buildings.~~
- ~~(e) Corner lots: ten (10) foot front yard setback facing Highway 24; five (5) foot setback on side facing any new or existing street connecting to Highway 24.~~

Historic Plat of 100 Block

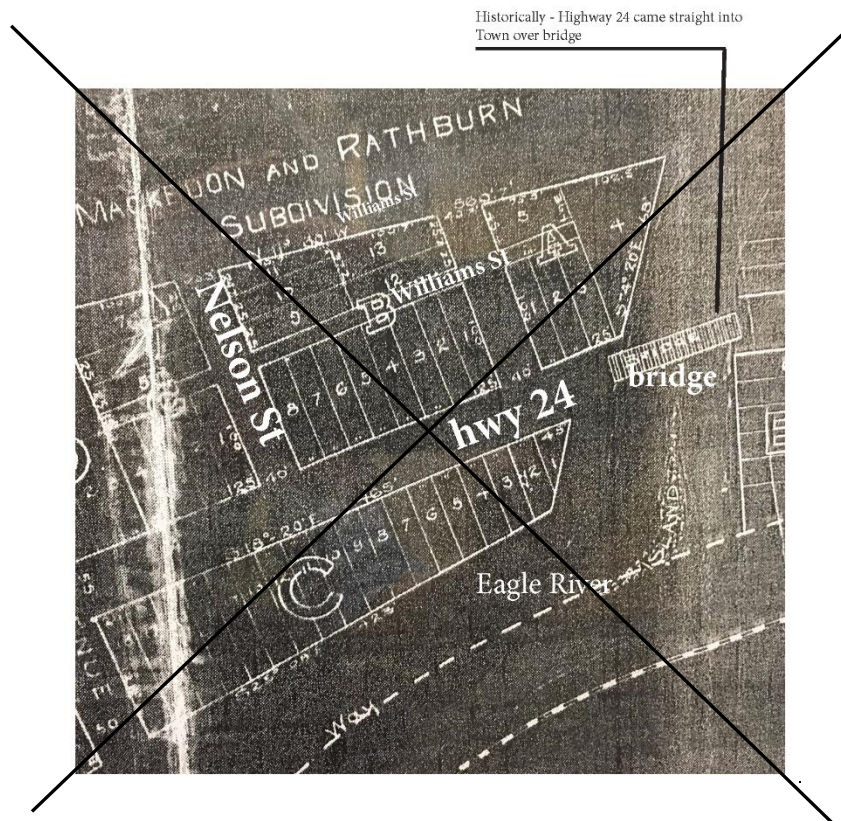
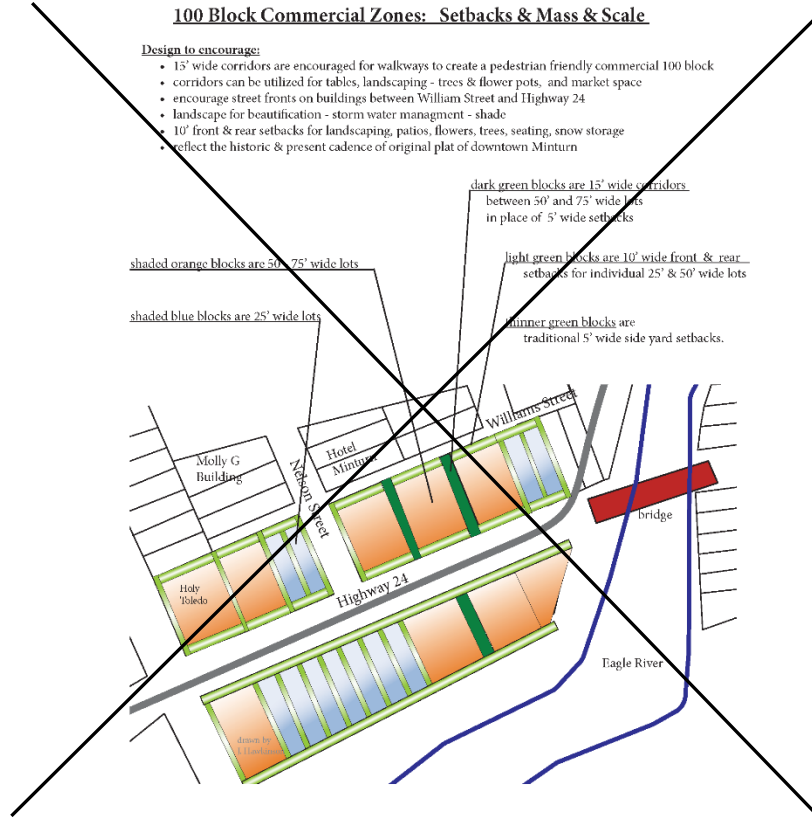


Diagram Example of:

100 Block Commercial Zones: Setbacks & Mass & Scale

Design to encourage:

- 15' wide corridors are encouraged for walkways to create a pedestrian friendly commercial 100 block
- corridors can be utilized for tables, landscaping - trees & flower pots, and market space
- encourage street fronts on buildings between William Street and Highway 24
- landscape for beautification - storm water management - shade
- 10' front & rear setbacks for landscaping, patios, flowers, trees, seating, snow storage
- reflect the historic & present cadence of original plat of downtown Minturn



* * *

ARTICLE 6 - Old Town Character Area

* * *

Sec. 16-6-35. - 100 Block Commercial Zones.

- The 100 Block Commercial Zones is characterized by a compact retail/commercial core area bisected by Highway 24. The area consists of businesses and residences with an identity of the historic commercial core that is distinct from other parts of the community.
- The purpose of this area is to provide convenient commercial services to residents and visitors and to promote the development of the Town's primary retail commercial district. Accommodation of sales tax-generating commercial uses and non-street level residential units can enhance Old Town vitality while maintaining the visual character and scale. Street level space within the 100 Block Commercial Zones shall be dedicated to retail uses. Non-street level space within the 100 Block Commercial Zones may be used for compatible retail, office, and residential uses.
- The 100 Block Commercial Zones should function as a pedestrian shopping corridor. Buildings ~~should be~~ shall oriented toward sidewalks and pedestrian areas with storefront windows and main entrance doors. The scale of buildings both in terms of height and width should encourage transparency, pedestrian engagement, and facilitate pedestrian movement not only along Highway 24 but also between Highway 24, Williams Street and Eagle Street and along all streets in the 100 Block.

(d) The small town historic main street character will be maintained by keeping in scale of with the original plat of twenty-five (25) foot by one hundred (100) foot lots and one (1) to two (2) story building heights-frontages along Highway 24. Large monolithic buildings and "strip" retail areas are discouraged-not allowed. New development in the 100 Block shall follow the standards and guidelines provided in Appendix B which seeks to ensure new structures maintain compatibility with historic structures while bringing new uses to increase vitality.

* * *

Sec. 16-6-70. – Old Town Character Area use table.

| Use | All Residential Zones | Commercial Zones | 100 Block Commercial Zone: <u>A</u> | <u>100 Block: B</u> | All Mixed-Use Zones | All Recreation & Open Space Zones | All Federally Regulated Zones | Light Industry and Public Facilities Zone | PUD Holding Zone | Railroad Right-of-Way/Transportation Zone |
|---|-----------------------|------------------|-------------------------------------|---------------------|---------------------|-----------------------------------|-------------------------------|---|------------------|---|
| R – Use by right C – Conditional use L – Limited use N – Not allowed | | | | | | | | | | |
| Accessory apartments (street level) | L | C | N | <u>N</u> | L | N | - | - | - | - |
| Accessory apartments (second floor or above) | L | C | <u>C*</u> | <u>C*</u> | L | N | - | - | - | - |
| Accessory dwellings (street level) | L | C | N | <u>N</u> | L | N | - | - | - | - |
| Accessory dwellings (second floor or above) | L | C | <u>C*</u> | <u>C*</u> | L | N | - | - | - | - |
| Automotive detail shops | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Automotive parts sales | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Bakeries and confectioneries | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Banks and financial institutions | N | R | N | <u>N</u> | R | N | - | - | - | - |

| | | | | | | | | | | |
|---|---|---|-----------|-----------|---|---|---|---|---|---|
| Barbershops | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Beauty Shops | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Business and office services | N | R | <u>NC</u> | <u>C</u> | R | N | - | - | - | - |
| Car washes | N | N | N | <u>N</u> | C | N | - | - | - | - |
| Cocktail lounges, taverns | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Club (street level) | N | N | N | <u>N</u> | N | N | - | - | - | - |
| Club (second floor or above) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Commercial accommodations | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Convenience stores | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Delicatessens and specialty food stores | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Drive-thru/up establishments | N | C | N | <u>N</u> | N | N | - | - | - | - |
| Drugstores and pharmacies | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Dry cleaners | N | R | N | <u>N</u> | N | N | - | - | - | - |
| Duplexes (street level) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Duplexes (second floor or above) | N | C | <u>C*</u> | <u>C*</u> | C | N | - | - | - | - |
| Garden landscaping supply and seed stores | N | R | N | <u>N</u> | R | N | - | - | - | - |
| Gas stations | N | N | N | <u>N</u> | C | N | - | - | - | - |
| Grocery stores | N | R | C | <u>C</u> | C | N | - | - | - | - |

| | | | | | | | | | | |
|---|---|---|-----------|-----------|---|---|---|---|---|---|
| Health/medical offices (street level) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Health/medical offices (second floor or above) | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Institutional uses (street level) | N | N | N | <u>N</u> | N | N | - | - | - | - |
| Institutional uses (second floor or above) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Laundries | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Laundromats | N | R | N | <u>N</u> | C | N | - | - | - | - |
| Liquor stores | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Manufacturing, light | N | C | C | <u>C</u> | N | N | - | - | - | - |
| Multi-family dwellings (street level) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Multi-family dwellings (second floor or above) | N | C | <u>C*</u> | <u>C*</u> | C | N | - | - | - | - |
| Pawn shops | N | R | C | <u>C</u> | C | N | - | - | - | - |
| Photographic studios | N | R | C | <u>C</u> | R | N | - | - | - | - |
| Professional activities (street level) | N | R | N | <u>N</u> | C | N | - | - | - | - |
| Professional activities (second floor or above) | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Professional offices, business offices | N | C | N | <u>N</u> | R | N | - | - | - | - |

| | | | | | | | | | | |
|--|---|---|---|----------|---|---|---|---|---|---|
| and studios (street level) | | | | | | | | | | |
| Professional offices, business offices and studios (second floor or above) | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Radio and television stores and repair shops | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Restaurant | N | R | R | <u>R</u> | C | N | - | - | - | - |
| Single-family residential dwellings | R | C | N | <u>N</u> | R | N | - | - | - | - |
| Retail stores including: Apparel stores; art supply stores and galleries; bookstores; camera stores and photographic studios; candy stores; chinaware and glassware stores; florists; gift stores; hobby stores; household appliance stores; jewelry stores; leather goods stores; luggage stores; music and record stores; newsstands and tobacco stores; sporting goods stores; stationery stores; toy | N | R | R | <u>R</u> | R | N | - | - | - | - |

| | | | | | | | | | | |
|--|---|---|---|----------|---|---|---|---|---|---|
| stores; variety stores; yardage and dry goods stores | | | | | | | | | | |
| Small appliance repair shops, excluding furniture repair | N | R | C | <u>C</u> | R | N | - | - | - | - |
| Tailors and dressmakers (street level) | N | R | N | <u>N</u> | R | N | - | - | - | - |
| Tailors and dressmakers (second level floor or above) | N | R | R | <u>R</u> | R | N | - | - | - | - |
| Theaters | N | R | N | <u>N</u> | C | N | - | - | - | - |
| Theaters, meeting rooms and convention centers (<u>street level</u>) | N | N | N | <u>N</u> | N | N | - | - | - | - |
| Theaters, meeting rooms and convention centers (second floor or above) | N | C | N | <u>N</u> | C | N | - | - | - | - |
| Travel and ticket agencies | N | R | N | <u>N</u> | R | N | - | - | - | - |

* All residential uses in the 100 Block shall be conditional based on their ability to provide sufficient access to parking (if applicable) which may require additional setbacks along Williams and Eagle River Streets due to these streets being extremely narrow. Refer to Appendix B 100 Block Design Standards and Guidelines for additional guidance.

* * *

ARTICLE 16 - Off-street parking and loading

* * *

Sec. 16-16-150. – 100 Block Incentive-Based Parking Program. (OPTION 1 – FEE IN LIEU + REDUCED SITE PARKING)

In order to incentivize sales-tax producing uses and the reuse of existing structures, new development in the 100 Block Zones may opt to reduce parking standards by providing the following:

- (a) Development shall calculate the total number of spaces required by the code for the proposed program by using Table 16-10, 16-11, and 16-12. The resulting number of spaces = A.
- (b) Development shall calculate the total number of spaces required for the proposed program using Table 16-14. The resulting number of spaces = B.

Table 16-14

| <u>Use</u> | <u>Parking Standard</u> |
|--|------------------------------------|
| <u>Sales-tax generating use (commercial, retail, restaurant, etc.)</u> | <u>0</u> |
| <u>Any use inside an existing structure (adaptive reuse/historic preservation)</u> | <u>0</u> |
| <u>Office and non-sales-tax generating commercial use</u> | <u>1 space per 500 sq. ft.</u> |
| <u>Light industrial</u> | <u>1 space per 1,000 sq. ft.</u> |
| <u>Residential (up to 2-bedroom max.)</u> | <u>1 space per unit</u> |
| <u>Lodging, hotel/motel, bed and breakfast</u> | <u>0.5 space per rentable room</u> |

- (c) Conduct the following calculation:
 - (i) A (minus) B (minus) allowable on-street/off-site spaces (Sec. 16-16-60) = # of spaces required for in lieu fee. The resulting number of spaces = C.
- (d) Developer shall pay in lieu fee to the Town of Minturn at a rate of C (times) \$10,000.
- (e) Developer would be exempt from in lieu fee if the following is true:
 - (i) Development meets parking requirements from Tables 16-10, 16-11, and 16-12; and
 - (ii) Development allows the additional parking (C) on-site to be used for public or shared parking for other uses in the 100 Block.
- (f) Tandem parking shall not be allowed.

Sec. 16-16-150. – 100 Block Incentive-Based Parking Program. (OPTION 2 – FEE IN LIEU + RESIDENTIAL PARKING AS-IS)

In order to incentivize sales-tax producing uses and the reuse of existing structures, new development in the 100 Block Zones may opt to reduce parking standards by providing the following:

- (g) Development shall calculate the total number of spaces required by the code for the proposed program by using Table 16-10, 16-11, and 16-12. The resulting number of spaces = A.
- (h) Development shall calculate the total number of spaces required for the proposed program using Table 16-14. The resulting number of spaces = B.

Table 16-14

| <u>Use</u> | <u>Parking Standard</u> |
|--|------------------------------------|
| <u>Sales-tax generating use (commercial, retail, restaurant, etc.)</u> | <u>0</u> |
| <u>Any use inside an existing structure (adaptive reuse/historic preservation)</u> | <u>0</u> |
| <u>Office and non-sales-tax generating commercial use</u> | <u>1 space per 500 sq. ft.</u> |
| <u>Light industrial</u> | <u>1 space per 1,000 sq. ft.</u> |
| <u>Residential</u> | <u>Use Table 16-10</u> |
| <u>Lodging, hotel/motel, bed and breakfast</u> | <u>0.5 space per rentable room</u> |

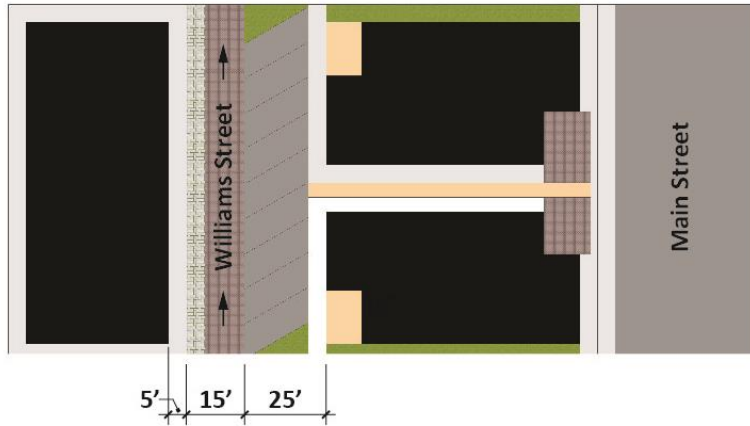
- (i) Conduct the following calculation:
 - (i) A (minus) B (minus) allowable on-street/off-site spaces (Sec. 16-16-60) = # of spaces required for in lieu fee. The resulting number of spaces = C.
- (j) Developer shall pay in lieu fee to the Town of Minturn at a rate of C (times) \$10,000.
- (k) Developer would be exempt from in lieu fee if the following is true:
 - (i) Development meets parking requirements from Tables 16-10, 16-11, and 16-12; and
 - (ii) Development allows the additional parking (C) on-site to be used for public or shared parking for other uses in the 100 Block.
- (l) Tandem parking shall not be allowed.

Sec. 16-16-150. – 100 Block Incentive-Based Parking Program. (OPTION 3 – SHARED STREET PARKING CONCEPT)

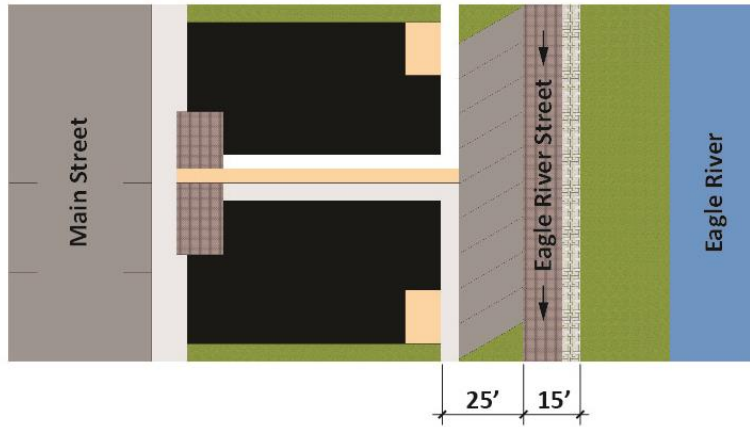
In order to incentivize sales-tax producing uses, the reuse of existing structures, activation of ground floor spaces, and a unique urban design concept of shared streets, parking requirements for any new uses on the 100 Block shall be exempt, except for the following:

- (a) 100-Block A properties shall provide 30-degree angled parking for the width of their lot along Williams and Eagle River Drives. From the 15-foot wide public right-of-way, a setback of 25-feet shall be provided and within the 25-feet the developer shall provide a 17' deep bay of 30-degree angled parking stalls that are 8.5-feet wide minimum. Refer to Diagram.

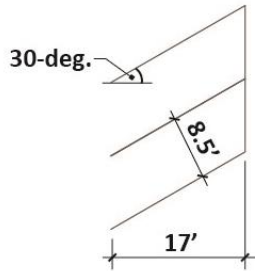
Williams Street Condition



Eagle River Street Condition



Parking Stall Dimensions



APPENDIX B - Design Standards and Guidelines

Chapter I, Section B - Goals, and Objectives, and Guiding Principles

In order to preserve and enhance the built environment, and to further the character of the community, Minturn has adopted design standards and guidelines for development. The Minturn Community Plan contains the community's vision for the future, a future that is grounded in the

character of the past. This character is based on a respect for both the history and environment of the region, a respect that is evident in the existing architectural styles and layout of the Town. The preservation and enhancement of landforms, vegetation, and wildlife habitat are consistent with these objectives.

These standards and guidelines are to be used as a supplement to the Town of Minturn Zoning Code. Review of compliance with these design standards and guidelines will occur for all new development, as well as major restoration and/or renovation of existing structures or developments within the Town boundaries.

New projects should be designed with the following guiding principles in mind:

1. Minturn is Rich with History – all designs should respect and reflect our rich history and help keep our stories alive for future generations.
2. Minturn is Close to Nature – every building in Minturn is steps from nature – we respect and appreciate our environment in all aspects of design.
3. Minturn is Colorful – Minturn is not dull – look around and you will see colorful buildings, roofs, people, and landscapes. New designs should reflect our diversity by implementing a colorful, not dull, palette.
4. Minturn is Authentic – we have never tried to be like other communities – we are uniquely and proudly the one and only “Minturn.” New designs should contribute to our meaning and authenticity.

* * *

Chapter III - Character Areas


* * *

Section 4 - 100 Block ~~Commercial~~ Zones

[This section replaced and amended in its entirety with Exhibit A – see exhibit ‘A’ attached hereto]


INTRODUCED, READ BY TITLE, APPROVED ON THE FIRST READING AND ORDERED PUBLISHED BY TITLE ONLY AND POSTED IN FULL ON THE OFFICIAL TOWN WEBSITE THE 6th DAY OF JULY 2022. A PUBLIC HEARING ON THIS ORDINANCE SHALL BE HELD AT THE REGULAR MEETING OF THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO ON THE 3rd DAY OF AUGUST 2022 AT 5:30 p.m. AT THE MINTURN TOWN HALL 302 PINE STREET, MINTURN COLORADO 81645.

TOWN OF MINTURN, COLORADO



Earle Bidez, Mayor

ATTEST:

By: 

Jay Brunvand, Town Clerk



THE TOWN OF MINTURN, COLORADO, ORDAINS THIS ORDINANCE ENACTED ON SECOND READING AND ORDERED PUBLISHED BY TITLE ONLY AND POSTED IN FULL ON THE OFFICIAL TOWN WEBSITE THIS 17th DAY OF AUGUST 2022.

TOWN OF MINTURN, COLORADO

Earle Bidez, Mayor

ATTEST:

By: _____
Jay Brunvand, Town Clerk

Exhibit A



100 BLOCK

Design Standards and Guidelines

APPENDIX B INSERT



August 2022 - FINAL DRAFT

CONTENTS

This document is organized into three chapters, as described below.

1 100 Block Framework

This chapter provides a framework for the 100 Block, presenting a quick history, key elements and principles for all new development. It provides guidance for how a project should respond to its surroundings and accent 100 Block assets to work toward becoming a cohesive, historic commercial core.

2 Site Design

This chapter provides specific design standards and guidelines as they relate to an individual site. These include the design and treatment of setbacks and other open space on a site as well as parking, screening, landscaping, and lighting.

3 Building Design

This chapter provides specific design standards and guidelines as they relate to an individual building. These include the appropriate scaling of a building and architectural elements, activation of edges, materiality, and roof design.

STRUCTURE OF DOCUMENT

The standards and guidelines herein are structured so that they can be easily followed and referenced. They are written as follows:

XX.1 THIS IS A TOPIC OR PRINCIPLE.

A. This is a standard (rule).

» *This is a guideline (advice) or supplementary information.*

A **topic or principle** explains what the standards and guidelines are about. A **standard** is a rule that shall be met if it applies to a particular site. Standards provide clear expectations for all projects to follow. A **guideline** is advice for a project - it is intended to give ideas for how a standard may be applied.

PURPOSE

This document replaces the former 100 Block Character Area from **Appendix B** of the Minturn Municipal Code. It seeks to provide guidance to property owners, developers, elected officials, and commissioners so that the community's vision for the 100 Block can be realized over-time.

This document will be **used by the Planning Commission**, which serves as the design review body for the town, to review individual projects that are proposed in the 100 Block. All projects must also follow **Appendix B: General Design Standards, Chapter 16** sections of the municipal code that relate to the 100 Block were also updated at the same time so that dimensional standards and other zoning regulations match the standards and guidelines.

PROCESS

The standards and guidelines were developed using input from the community. Two community **design charrettes** were held (October 2020 and February 2022), as well as **multiple meetings** with stakeholders, the Planning Commission, and Town Council. The community agreed on many aspects of the vision. There was disagreement on the appropriate scale of buildings and whether or not the area (or individual buildings) should be designated historic. Ultimately, this document and the code update seeks to **find a balance** between opposing views while **implementing the desired vision** to create a 100 Block that is allowed to evolve while requiring that new projects contribute to a **sustainable and vibrant future**.

- ▼ APPENDIX B - MINTURN DESIGN STANDARDS AND GUIDELINES
 - I. INTRODUCTION
 - II. GENERAL DESIGN STANDARDS
 - ▼ III. - CHARACTER AREAS
 - 1. - GROUSE CREEK CHARACTER AREA
 - 2. - OLD TOWN CHARACTER AREA
 - 3. - SOUTH TOWN CHARACTER AREA
 - 4. - 100 BLOCK COMMERCIAL ZONE



Design Guidelines
100 Block Commercial Zone



'History meets Present'

This document replaces the former Design Guidelines for the 100 Block Commercial Zone with cover page shown above. Principles from the former guidelines were carried over, as applicable.



MINTURN 100 BLOCK
Design Workshop | February 15, 2022



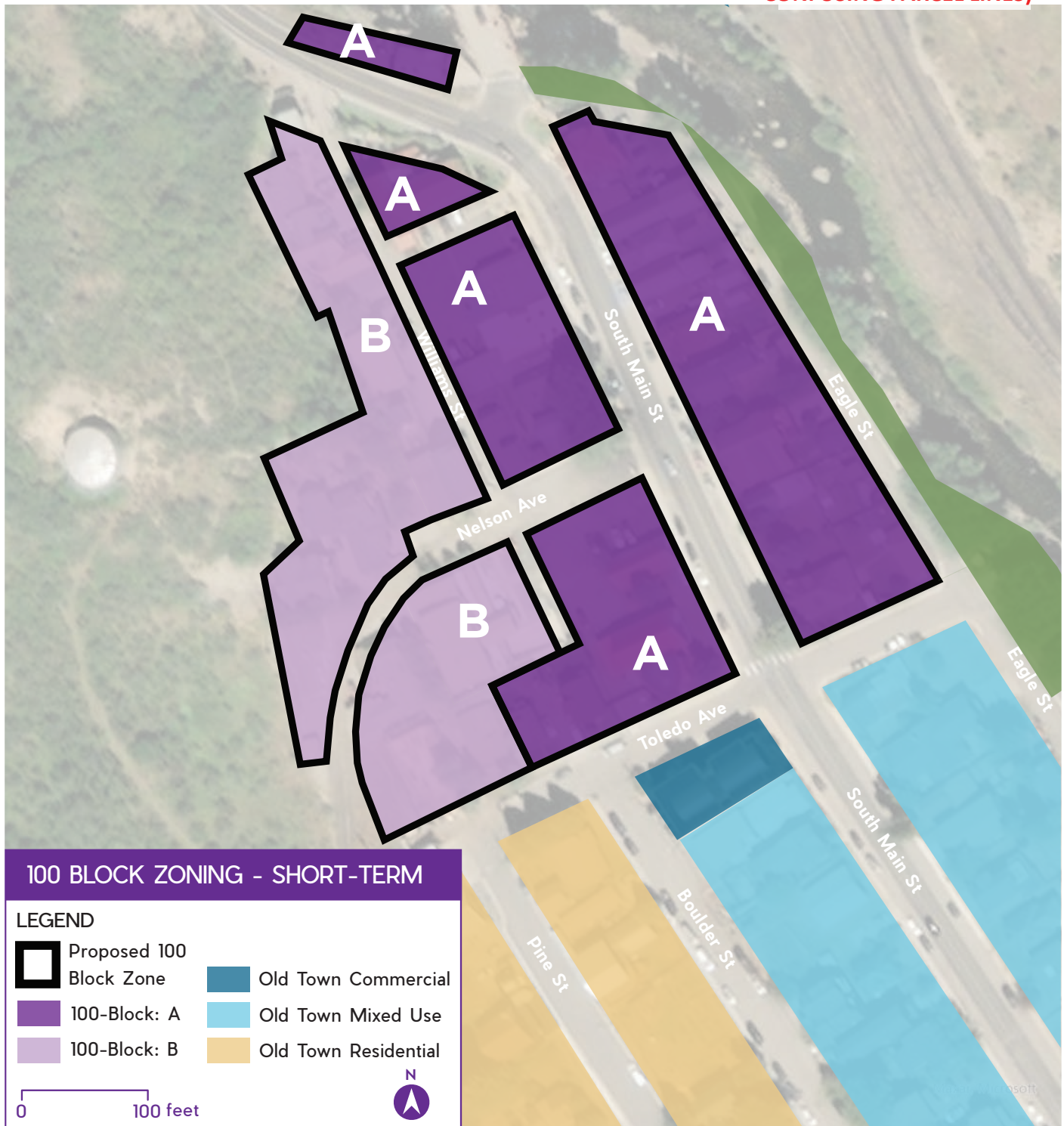
A Public Design Workshop was held on February 15, 2022 to discuss key elements of new development including: building height and frontage, site design and public realm, and parking. There were approximately 40 attendees.

APPLICABILITY

The design standards and guidelines herein apply to all properties outlined in heavy black line below. The new 100 Block Zone includes A and B subareas, which the standards and guidelines may reference if differences apply.

100 Block: A is essentially all properties that face Main Street and **100 Block: B** is all properties that face Williams Street and Nelson Avenue going up the hill.

SIMPLIFIED MAP (TOOK OUT CONFUSING PARCEL LINES)





100 BLOCK FRAMEWORK

1

The 100 Block is the **gateway to Minturn** and the heart of town. It is the **historic center and commercial core**, although over-time commercial uses have extended past the 100 Block along Main Street. The 100 Block should remain the commercial core into the future, where there is ample opportunity for small, local businesses to thrive as well as more opportunity for people to live and visit. It is important to the community that the historic assets be saved and the history and authenticity of Minturn be celebrated. **Main Street** will remain the primary thoroughfare, although public and private improvements will help activate and improve the experience. **Williams Street and Eagle River Street** will become quaint public spaces where pedestrians and cars share the space and active uses breathe new life to these areas. Space between buildings will be used for **pedestrian passages, small gathering spaces and outdoor dining areas**. **Views** of the water tower, Eagle River, and hillsides will be preserved and celebrated.

QUICK HISTORY

Minturn was established in the late 1800s as a railroad town and incorporated in 1904. It originated on **mining, farming, and railroad** industries and jobs. It has maintained a feeling of a **quaint mountain village** amongst great change in Eagle County and surrounding areas with Vail and Beaver Creek resorts developing in the late 1960s to 1980s. Though tourism has picked up in recent years, Minturn remains a **local's place**.

Today, Minturn is still primarily a **working class town** with nearly 40 percent of employment in construction and related trades. Minturn has remained a local, authentic place over the years. It is entirely made up of **small, local businesses**, started from people that have invested their personal time and money to help Minturn prosper. It faces challenges related to growth and affordability as land values have escalated at high rates in recent years, as they have all over Colorado.



A two-story building with wood siding, sidewalks, and covered arcade includes a storefront and area where people can gather as they watch a carriage being pulled from a ditch on Main Street.



The two-and-a-half story Eagle River Hotel includes a simple architectural form with a gabled roof and dormer. A porch on the front defines the entrance.



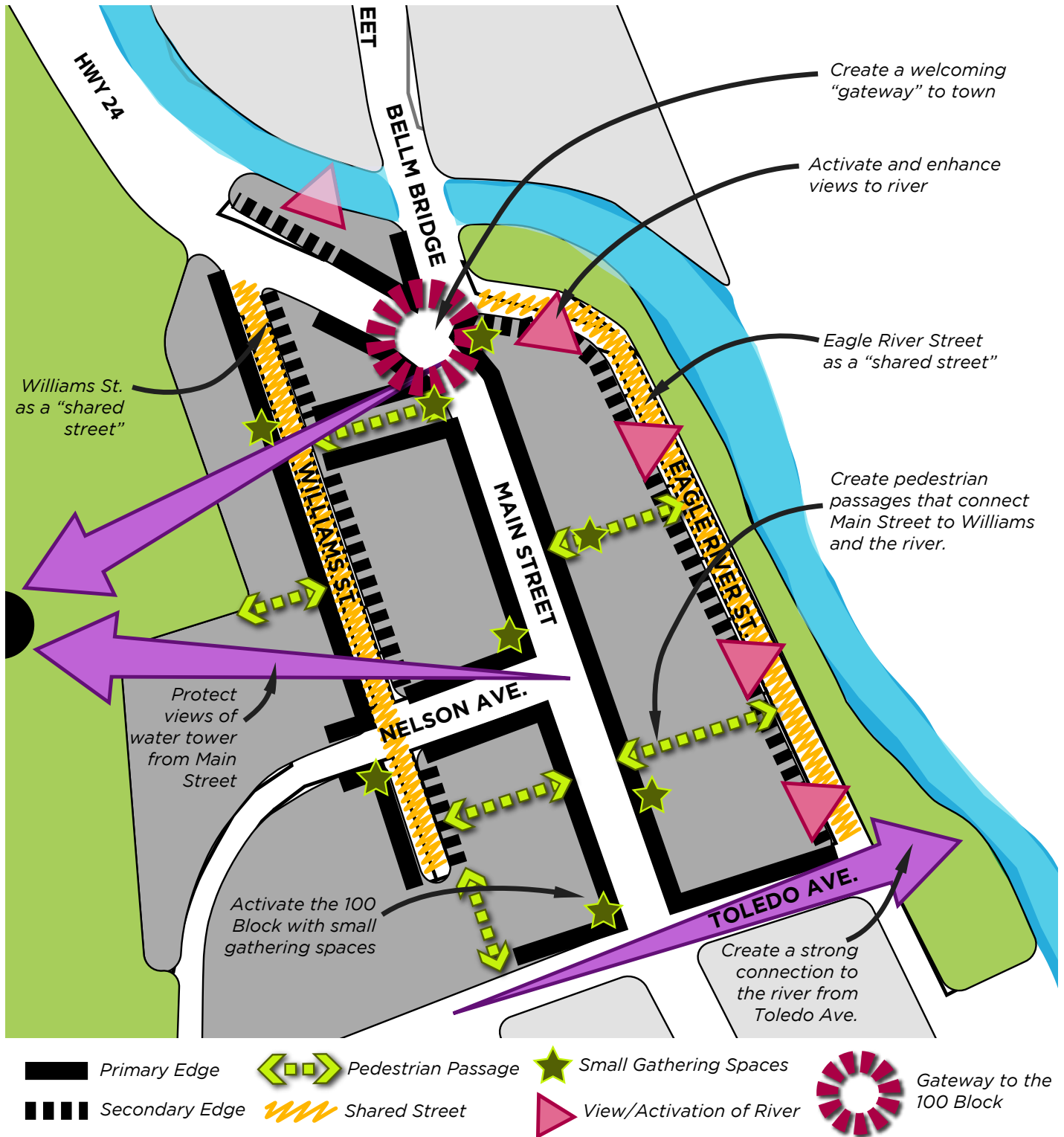
The Williams Cafe and Bar (now Magustos) is a wood and stucco structure that includes an arcade and storefront.



Main Street circa mid-1900s includes 2-story buildings at the sidewalk edge (zero setback) with narrow sidewalks.

100 BLOCK FRAMEWORK

This framework diagram explains key elements for the 100 Block that all projects should follow. Design principles that relate to the 100 Block Framework are provided on the following page.



100 Block Framework Diagram

***Note that pedestrian passages and small gathering spaces would be built on private property but be designed to be publicly accessible.**

FRAMEWORK PRINCIPLES

The following principles describe elements that will help implement the vision for the 100 Block. Each project should meet the following by providing a response to each principle in a project narrative

FP.1 CONTRIBUTE TO AN AUTHENTIC PLACE.

Minturn is known for being **authentic**. It's authenticity has been built over time, where layers of history are reflected in its people, places, and architecture. **What makes 100 Block authentic?**

- **It doesn't have a prescribed style of architecture** - each building seems to have it's own personality.
- **It has a rich history, but also has been built over time**, where buildings from different eras coexist and some have been added onto or modified to respond to new demands and uses.
- **It is made up of local businesses** - they are something you can only find in Minturn.
- **It is not another tourist mountain town** - though people love to visit, Minturn was not built *around* tourism. Design for locals first.

Each new project in Minturn's 100 Block should consider these aspects that make up its authenticity and contribute to it in similar ways, through unique architecture, a nod to history, and as a local's place first and foremost.

FP.2 DESIGN AT A HUMAN SCALE.

The 100 Block is **quaint**. Though new buildings may be larger than traditional buildings, their **scale should be compatible** to their surroundings. Monolithic buildings are not allowed.

Designing at a human scale means designing a building that is **comfortable** for someone to stand by and be in. It is made with materials that are small and modular - that can be carried by a person. They include details that add interest and delight to the human experience. They are designed to be experienced by pedestrians rather than cars.

Design new projects in the 100 Block with these elements in mind where each individual project contributes to a scale that is compatible and comfortable.



Designing at a human scale means designing a building that is comfortable for someone to stand by and be in.



Minturn's local businesses is one thing that contributes to its authenticity (and awesomeness.)



Human scale design is designed to be experienced by pedestrians rather than cars.

FP.3 ACTIVATE EDGES.

The vision of the 100 Block is to continue to be the commercial core and activity center of Minturn with active storefronts and sales tax-generating uses throughout. To create this desired activity, include **active uses**, e.g. businesses that are open the majority of the day and create activity on the street from people coming and going. This includes but is not limited to: coffee shops, cafes, restaurants/bars, retail stores, and ice cream/candy shops.

Primary edges (Main Street, Williams Street, and side streets) shall require active uses on the ground floor.

Secondary edges (east side of Williams and west side of Eagle River Street) should encourage active uses, but may also require access to parking or other back of house uses. Activation of these edges may include enhanced screening, landscaping, or public art - however, if possible, secondary entrances and active uses are also encouraged.



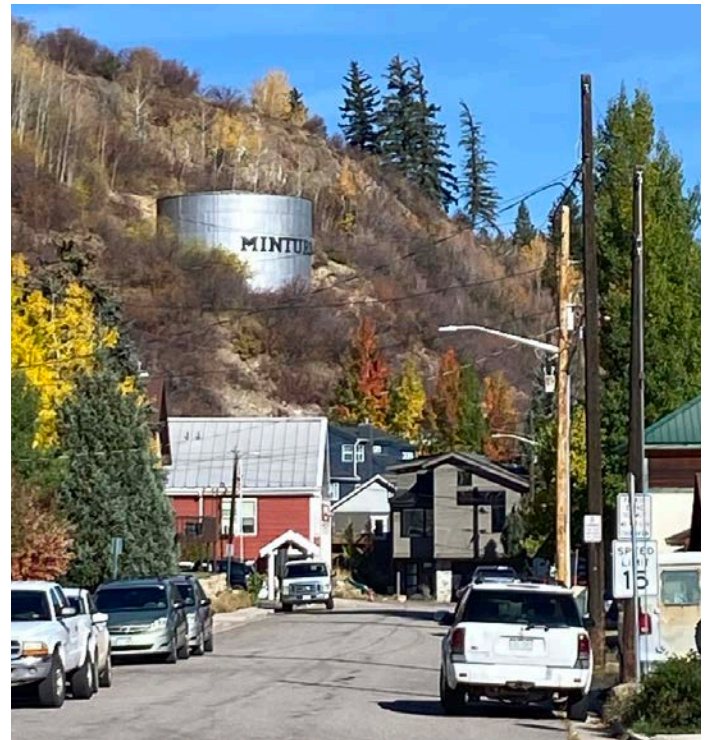
Active storefronts and tax-generating uses that engage the streets and sidewalks are desired.

FP.4 CELEBRATE AND ENHANCE IMPORTANT VIEWS

The **water tower** is iconic to Minturn. Views to the water tower from key intersections should be celebrated. A project at these locations should consider someone taking a picture - where the water tower is visible and part of the composition.

Views of the **Eagle River** are also important. Minturn would like to elevate the river as a key asset, so improving access and views to the river is a key component to achieving the vision.

Secondary uses to Main Street could be placed along Eagle River Street to activate it or **upper floor roof decks** could overlook the river. **Eagle River Street** should be a unique, quaint experience that encourages pedestrians to enjoy time walking along or resting at the edge of the river.



Views to the water tower from key intersections should be maintained.

NEW - APPLIES TO ALL DEVELOPMENT, BUT REINFORCED HERE AS WELL.

MINTURN GUIDING PRINCIPLES

In addition to the Framework Principles for the 100 Block, new projects should also follow Minturn's Guiding Principles:

***Minturn is Rich with History** – all designs should respect and reflect our rich history and help keep our stories alive for future generations.*

***Minturn is Close to Nature** – every building in Minturn is steps from nature – we respect and appreciate our environment in all aspects of design.*

***Minturn is Colorful** – Minturn is not dull – look around and you will see colorful buildings, roofs, people, and landscapes. New designs should reflect our diversity by implementing a colorful, not dull, palette.*

***Minturn is Authentic** – we have never tried to be like other communities – we are uniquely and proudly the one and only “Minturn.” New designs should contribute to our meaning and authenticity.*

FP.5 CREATE A WELCOMING PLACE.

The 100 Block is the “gateway” to Minturn and should appear **welcoming**. Activities should orient to Main Street, the public realm should be **well-designed and maintained** year-round, and the experience of navigating to destinations (businesses, parks, parking, etc.) should be very clear.

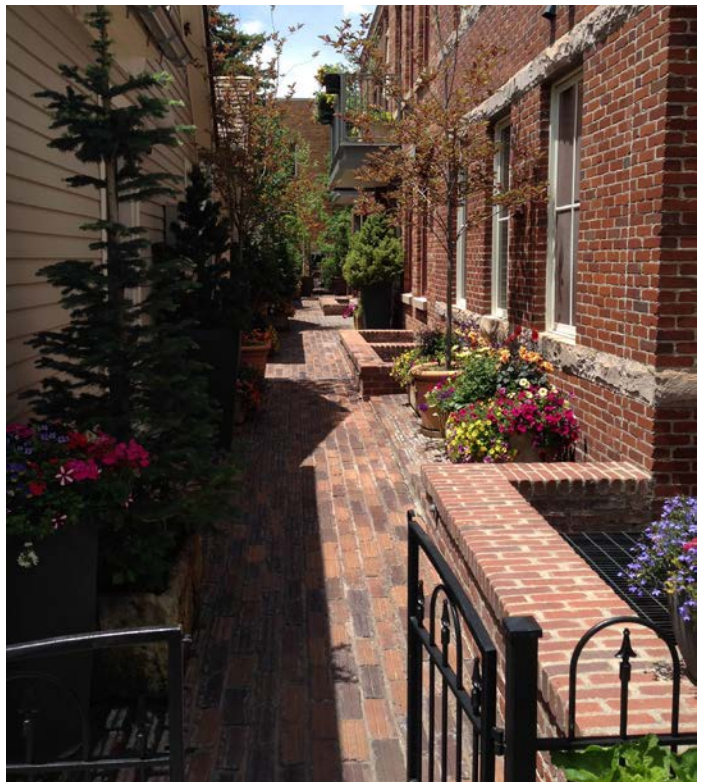
Projects located near the “gateway,” shall contribute to **provoking a sense of arrival** and an iconic and unique sense of “Minturn.” The public realm should be coordinated and consistent in design. And architecture should be unique to Minturn and of highest quality and attention to detail.

Throughout the 100 Block, each individual project should create **welcoming and inviting places**. This can be achieved by creating interactive places such as: small plazas, pocket parks, outdoor dining and retail, and public passages. These design elements tell people that they are welcome to navigate through the 100 Block with ease and delight.

Note that locations of passages and gathering spaces on the diagram are not exact, but indicate the general amount of these elements needed to create a successful network.



Interactive public art creates welcoming and inviting places, such as murals, statues, benches, and more.



Create welcoming and inviting places, such as this public passage with flowers and landscaping and windows overlooking the space.



SITE DESIGN

2

Site design refers to the arrangement and placement of buildings as well as other site features on a particular lot - such as landscape/hardscape, parking, trash, and snow. The goal is to improve the overall experience and appeal of the 100 Block by having well-organized and inviting spaces as well as clear rules to maintain them.

SITE DESIGN: DEFINING FEATURES

The original plat of 25-foot wide **lots** (right; above) and current ownership **parcels** (right; bottom) highlight how some historic lot patterns exist, but many of the original 25-foot lots were combined to create a larger parcel. The following are defining site features to consider in new designs:

25' LOT PATTERN

The original plat includes a series of lots that orient to Main Street with a 25-foot wide frontage. This allowed for multiple businesses to have visibility and access to customers and created a cadence along the street that changed properties (and therefore buildings) every 25-feet.

LINEAR NORTH-SOUTH ARRANGEMENT ALONG MAIN STREET

Due to surrounding topography, the arrangement of the 100 Block has remained linear in nature where the focus has always been on Main Street, and other roads are mainly used for access.

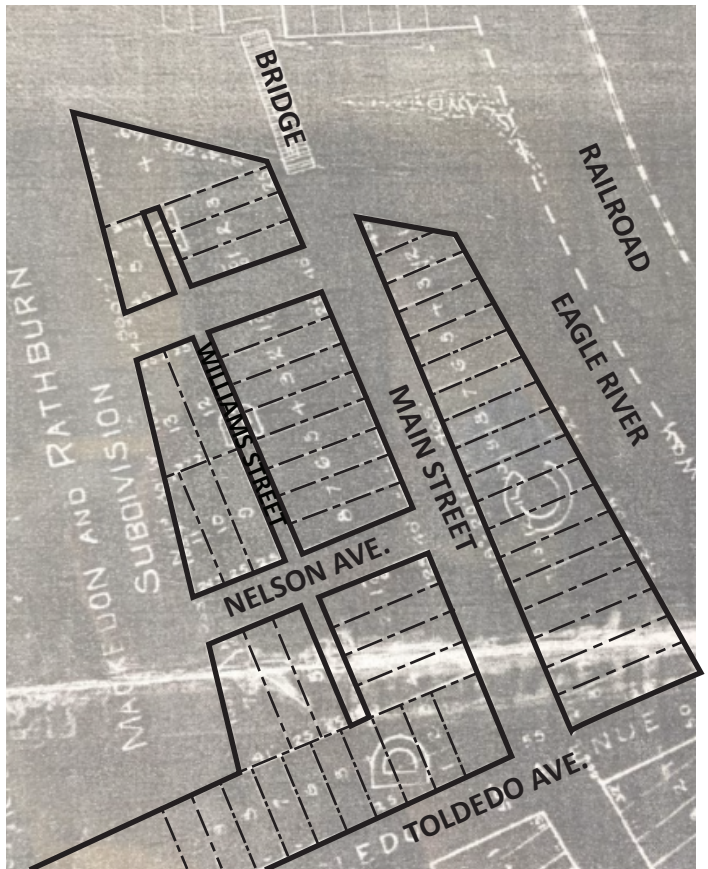
EAGLE RIVER AND RAILROAD

The river and railroad are consistent site features of the 100 Block that appear to be in their original alignment and location. Toledo Avenue has always terminated at the river's edge and the Bellm Bridge location is identical. Both elements contribute to the rich history of Minturn.

WATER TOWER

The Minturn water tower is situated above the 100 Block on the hillside to the west. This site feature is iconic to Minturn, as it has the "Minturn" name and can be seen from just about anywhere in the 100 Block due to its position high on the hillside.

NEW MAP AND DESCRIPTION TO EXPLAIN DIFFERENCE BETWEEN LOTS AND PARCELS



Above: historic platting of the 100 Block
Below: current lot layout of the 100 Block



SITE DESIGN: STANDARDS AND GUIDELINES

SD.1 STREET DESIGN

A. **Main Street and Toledo and Nelson Avenues** shall include a minimum **5-foot** public sidewalk adjacent to the curb. This space shall remain free and clear of objects at all times.

B. **Main Street and Toledo and Nelson Avenues** shall include a **5-foot** front setback adjacent to the sidewalk. The first **2-foot** of the setback shall contribute to widening the sidewalk to create a 7-foot clear walking path.

» *The additional 3-foot of setback may be used for objects such as: sandwich boards, planters, benches, sales racks or other outdoor display of goods, landscaping, cafe seating, etc.*

C. **Main Street** requires that **40%** of a property's frontage include a minimum **15-foot** setback. This area shall be accessible from a public sidewalk.

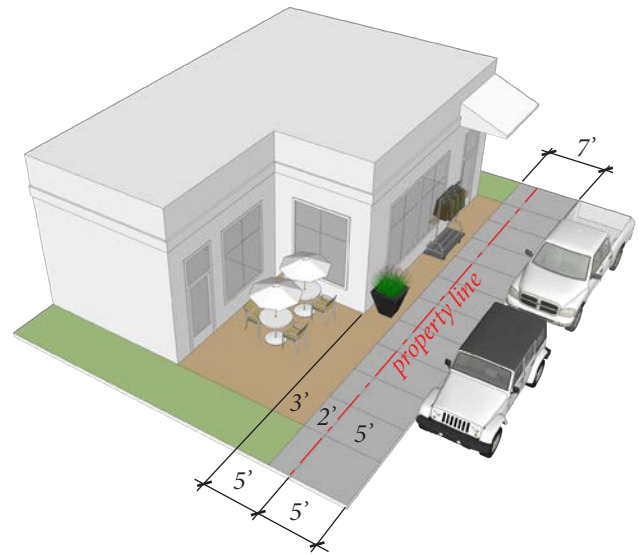
» *Design these spaces to be welcome and inviting. Consider ambient lighting, cafe seating, or other ways to activate the space.*

D. **Williams Street** shall include a **15-foot** clear public right-of-way (shared between vehicles and pedestrians) plus a minimum **5-foot** setback on either side for a total dimension of **25 feet** from building face to building face.

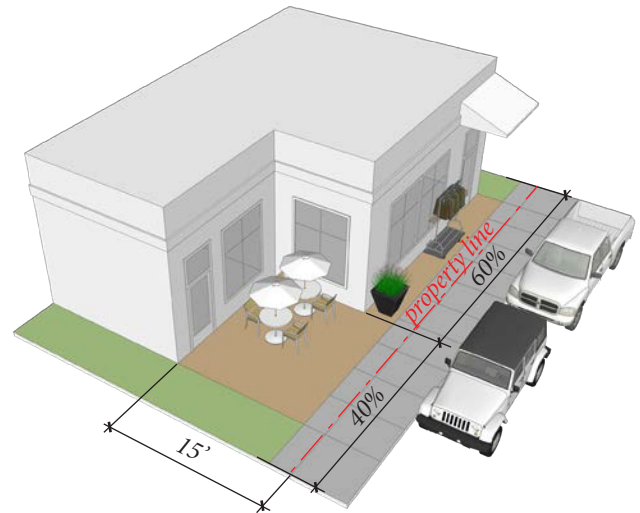
» *Consider changing the material of the setback area to differentiate it from the right-of-way.*

E. **Eagle River Street** shall include a **15-foot** clear public right-of-way (shared between vehicles and pedestrians) plus a minimum **5-foot** setback on the west side.

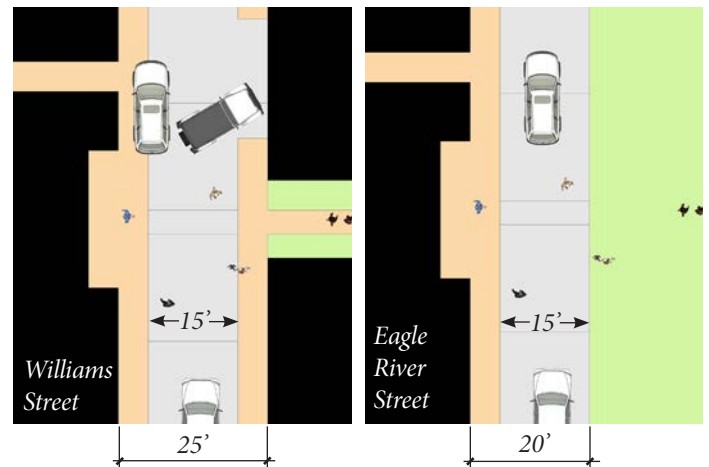
» *Consider changing the material of the setback area to differentiate it from the right-of-way.*



Main Street and Toledo and Nelson Avenues shall maintain a 5-foot sidewalk adjacent to the curb and dedicate an additional 2-foot of setback to the sidewalk to create a 7-foot wide sidewalk.



Along Main Street, 40% of a property's frontage shall be setback a minimum of 15-feet from the property line.



Williams and Eagle River Streets shall include a 15-foot clear public right-of-way (shared between vehicles and pedestrians) with a minimum setback of 5-feet.

**UPDATED EAGLE RIVER STREET DIMENSION,
TEXT, AND GRAPHIC (NO SIDEWALK)**

SD.2 ORIENTATION

- A. **Primary orientation**, meaning the “front(s)” of a building where the primary use(s) and entrance(s) are located shall be placed so that they face a primary edge (refer to Framework Diagram on page 9).
- B. **Secondary orientation** shall include entrances to secondary uses and upper floors and shall face secondary edges (refer to Framework Diagram on page 9).
- C. Where a public or semi-public space (plaza, park, sidewalk, street, etc.) is adjacent to a building, orient the building to **interact with and overlook** the outdoor space (i.e. include entrances and/or windows at eye-level.)
- D. Orient a building’s massing and design a site to **maximize sun exposure and minimize shading** on adjacent properties.
- » *Defined setbacks in Chapter 16 are designed to maximize sun exposure, but to the extent that a project can modify a design to create more sun access, it is desired.*
 - » *Utilize architectural features and/or landscaping to assist with seasonal sun control. For example, in the winter when the sun is lower it is important to allow sun deep into the building whereas in the summer when the sun is high, it is important to offer shading, such as through overhangs, awnings, deciduous trees, or other shading device.*



Where a public or semi-public space is adjacent to a building, orient the building to interact with and overlook the outdoor space.

SD.3 PUBLICLY ACCESSIBLE AND PRIVATE OPEN SPACE

- A. Required **publicly accessible open spaces** shall be **visible from and designed to activate** the adjacent right-of-way. If a new development is **adjacent to a low-scale traditional building**, the open space shall be placed toward that side of the property.
- » *Design these spaces to generate activity. Add elements like: places to sit, public art, dining area, lighting, and signage.*
 - » *If an enclosure is required for a dining space that serves alcohol, design the enclosure to be as open and transparent as possible.*
 - » *Consider the use of special paving to accent this space and differentiate it from the public sidewalk.*
 - » *Locate these spaces to maximize sun exposure.*
- B. A **publicly accessible passage** through a site shall include a minimum **4-foot** wide sidewalk. A minimum **8-foot** setback shall be required for the entire space. Provide **full cut-off light fixtures** to light the pathway.
- » *The entire space may be paved so long as the site’s pervious surface requirement has been met elsewhere.*
 - » *Pavers or other material may be approved by the design review commission.*
- C. **Private open space**, such as a patio, balcony, or roof terrace shall be a minimum of **5-foot** deep and **6-foot** wide.



Locate open spaces to maximize sun exposure and design them to be activated.

CHANGED SETBACK FOR PARKING TO 40- FEET TO ADDRESS SKINNY RETAIL FEAR.

SD.4 PARKING

- A. Parking **shall not be accessed from Main Street** and shall be set back a minimum of **40 feet** from Main Street. Parking shall be accessed from Nelson, Toledo, Williams, or Eagle River Street.
- B. If surface parking is located adjacent to a public street or publicly accessible open space, the side facing the public space **shall be screened** from public view a minimum of **36"** from the ground.
 - » *Use or combine the following methods: Landscaping, site walls, decorative fencing, public art, or other method that meets the intent of screening*
- C. **90-degree or angled parking** on Williams and/or Eagle River Streets shall be allowed, only if deemed appropriate by the Design Review Board.
 - » *Consider the amount of space available and use(s) of parking.*
- D. **Combine access** to two adjacent parking lots, wherever feasible to reduce the amount of driveways needed and to maximize parking counts.
- E. Design the **entrance** to any type of structured parking - podium, underground, or structured - to be clearly labeled with signage. If the structure includes a portion of public parking, a public parking sign shall be used and public spaces shall be clearly marked.
- F. **Integrate the design** of any type of structured parking into the architecture of the building. It shall not appear to be a separate structure or lack design detail because it is parking.



Screen public-facing parking from view by using landscaping, site walls, decorative fencing, public art, or other method.

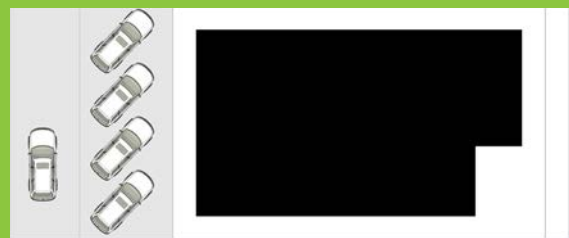
ADDED THIS TO BETTER EXPLAIN HOW PROJECTS COULD PARK AND ACTIVATE REAR.

CREATIVE PARKING SOLUTIONS

Depending on the amount of parking required for a project, consider parking solutions that allow for Williams and Eagle River Streets to be activated, while still accommodating parking. See some options below.



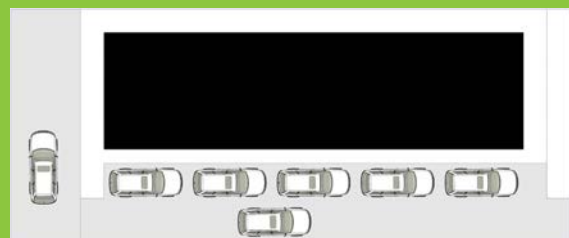
90-Degree Parking - This would require an increased rear setback and may require additional space to ensure cars backing out have enough space.



Angled Parking - Same as 90-degree, but requires less back up space.



Parallel Parking - Doesn't yield as much, but creates a more a commercial street feeling.



Side Parking Aisle - This would require vehicles to exit onto Main and should not be allowed everywhere, but could be a creative solution, if needed.

WILLIAMS OR EAGLE RIVER SIDE

MAIN STREET SIDE

ADDED THIS TO BETTER EXPLAIN VISION FOR WILLIAMS AND EAGLE RIVER.

“WOONERF” DESIGN

A “woonerf” is a Dutch term for a shared street. Invented in the Netherlands, but widely picked up and practiced throughout the world, this type of street is a narrow street that is shared between automobiles, pedestrians, and bicyclists. Typical design elements of a woonerf include:

- **Very narrow space (15-25 feet)** - typically only wide enough for cars to go one-way.
- **Slow speed design** - the space is designed to slow cars down by squeezing the space that a car can drive in. Typically a different material is used (often pavers) and bollards are used to direct cars and provide space for building entrances.
- **Activated** - uses front onto the street and provide activation.

This type of street character is desired for Williams and Eagle River Streets instead of widening them to be like any other street.



SD.5 SCREENING AND SNOW STORAGE

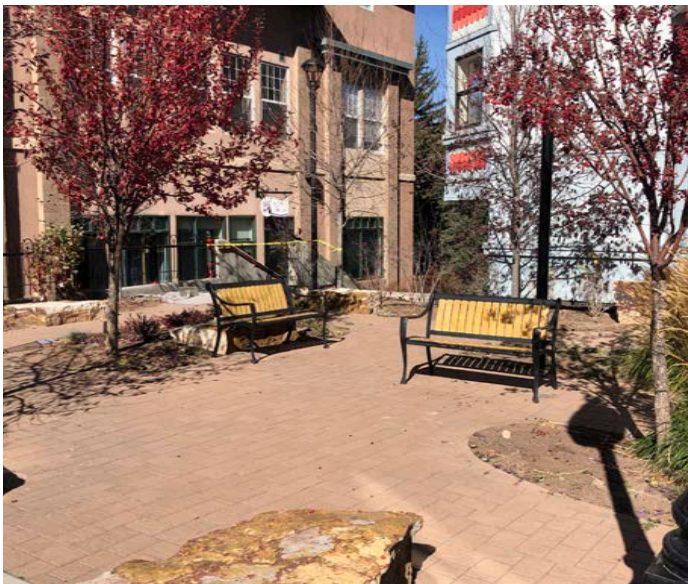
- A. A **designated area** for trash containers and snow storage shall be provided on a site plan.
 - » *These may be located in separate locations.*
- B. Refuse storage, general storage, loading and mechanical shall be **screened** when visible from a public right-of-way. Screening methods shall be incorporated as part of the landscape and architectural design - using **high-quality materials** that align with the design concept. Any permanent structure shall **meet the setbacks** within Chapter 16.
 - » *Coordination amongst adjacent property owners to share refuse containers and storage facilities is encouraged.*
- C. **Fences** in the front of a lot shall not exceed **3-feet** for opaque and **4-feet** for transparent. Fences in the rear shall not exceed **6-feet**. All refuse storage, no matter where the location shall be **6-feet** tall if detached from the primary structure.
 - » *Incorporating a permanent space within a covered or fully enclosed garage as part of the primary structure is encouraged.*
- D. **Snow shall be cleared** from public sidewalks by **8am** following a storm event and consistently maintained during typical business hours of 8am to 6pm.
 - » *Consider snow maintenance when designing the site plan for a project. Provide common areas where snow can be placed, ideally pervious locations to allow snow melt to be transferred underground.*



All refuse storage shall be 6-feet tall if detached from the primary structure and designed with high-quality materials.

SD.6 LANDSCAPE AND HARDSCAPE

- A. **Trees** placed within a setback facing Main Street shall be treated as “street trees” and be placed in a **tree grate** level with paving so that pedestrian space is maximized. These trees shall be **deciduous**.
- B. **Tree species** shall be proven as successful trees in Colorado high mountain climates with ample soil volume for deep root growth. Fruit bearing trees are not permitted.
 - » *Seasonal coloring of leaves is desired.*
- C. Landscape plans shall incorporate **drought-tolerant, low maintenance** plants and be coordinated with low impact development (LID) standards in the code.
 - » *Plants that need little to no irrigation are desired.*
 - » *Annuals in planters that require more water may be used in prominent locations such as along Main Street.*
 - » *Avoid using planters that may become used as or mistaken for trash containers.*
- D. **Hardscape** (pavers, gravel, mulch, decking) shall be selected so that it blends with the architecture and surrounding context.
 - » *Consider materials that were historically used in Minturn such as wood and stone.*
 - » *Consider maintenance and availability of materials when replacement is necessary.*



Hardscape shall be selected so that it blends in with the architecture and surrounding context. Seasonal coloring of leaves is desired.

SD.7 LIGHTING

- A. **Minimize light spill** onto adjacent properties and toward the sky.
 - » *Use a fixture that provides even lighting for a plaza, courtyard or patio area.*
 - » *Shield site lighting to minimize off-site glare.*
 - » *Orient fixtures to the ground.*
- B. Lighting shall be provided for all **publicly accessible open spaces** (plazas, pocket parks, passages) and facing **public streets**.
 - » *Small-scale fixtures with down-lighting or light bollards may be used to illuminate a pathway.*
 - » *Overhead lights should be pedestrian-scaled (approximately 12-15 feet).*
 - » *Lighting may be affixed to a building and directed toward public spaces as well.*
 - » *All lighting fixture designs should be of a style that is compatible with the architecture and site design.*
 - » *Overhead “twinkle” lights should be allowed.*



Lighting shall be provided for all publicly accessible open spaces.

SITE DESIGN: INSPIRATION



A private passage preserves a public view!



A semi-public courtyard space is accessible and visible from a public sidewalk.



A main street is activated with cafe seating that is comfortable to sit in with landscaping and shade.



Flowers in the summer add vibrancy and let people know a place is cared for.



Recycled bicycles attached to a stair railing adjacent to a public space provides interest and whimsy and celebrates the cycling lifestyle.



BUILDING DESIGN

3

Building design refers to the size, forms, and scale of a building as well as materiality and color, roof form, and architectural details. This chapter does not intend to dictate a particular “style” of architecture. Rather, it is intended to maintain a similar scale and form so that the historic character and charm of the 100 Block remains while allowing progress and change to occur over-time. Each new building should add to Minturn’s uniqueness. Creativity is encouraged though limited boundaries are necessary.

BUILDING DESIGN: DEFINING FEATURES

As shown in the Site Design chapter, the 100 Block was originally platted with 25-foot wide lots. However, buildings developed in various widths, some following the 25-foot pattern and others were larger - some even smaller - in width. This contributes to the eclectic nature of the 100 Block. Buildings were typically 1-2 stories with pitched roofs (some were one story with a pitched roof that had a faux facade to appear as a large, flat-roof structure.) They were built with simple forms and local materials. Ornate architecture was not common.

STAGGERED SETBACKS

There has always been a staggered appearance to Main Street, as some buildings were built right up to the street and others were set back. Other lots were used for open space, or not developed leaving “holes” in the streetscape.



A historic photo of Main Street shows the staggered setbacks of buildings along the street - some are at the sidewalk edge and others are set back further. Dimensions are estimations of approximate building widths to show the “pattern quilt.”

SIMPLE FORMS

Traditional buildings of Minturn were simple boxes of one to two stories and pitched roofs. Additive features like roof dormers and awnings, or bay windows and porches on residential structures, added more detail. Some included faux facades that looked like a two-story, flat roof structure from Main Street, but had a pitched roof behind. Some, like the Eagle River Hotel, was taller and had a flat roof, but that was not the norm.

NATURAL MATERIALS

Traditional building materials were primarily wood siding, although some stone masonry, brick and stucco were used as well. Roofs were primarily metal or asphalt shingled.

A PATTERN QUILT

Some original buildings remain, but have been updated and others have been demolished and replaced. Regardless, what is left today is a “pattern quilt” with unique “squares” that make up the eclectic spirit that people love and cherish.

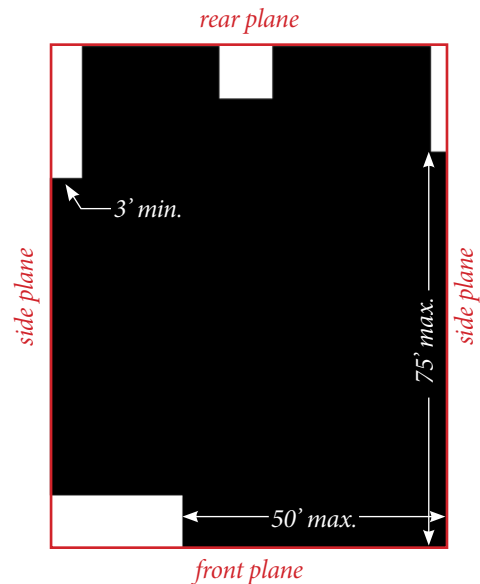


Artist rendering (credit: Beth Levine) of architectural styles in Minturn.

BUILDING DESIGN: STANDARDS AND GUIDELINES

BD.1 BUILDING FORM AND MASSING

- A. The **maximum length** of any wall on the same plane along the front and rear shall be **50-feet** and along the sides shall be **75-feet**. After this dimension, the building wall must shift on a horizontal plane a minimum of **3-feet**.
- B. **Main Street facades** shall be articulated every **25-feet** to reflect the historic platting of Main Street. Methods of articulation may include:
- » *A material change, plane change/wall offset, exposed structural detailing, or other architectural detail that provides a shadow line and breaks up the facade to maintain a cadence and scale of 25-foot intervals along Main Street.*
 - » *A change in height.*
- C. New structures shall **maintain a scale and compatibility** with nearby traditional buildings.
- » *Use similar building heights and/or datum lines (e.g. porch height, window sill/plate height, eave or ridge line, etc.) of adjacent structures.*
 - » *Align windows and use similar scale windows, where feasible.*
- D. Avoid forms that appear monolithic in comparison to the surroundings. **Break down the scale** of a building when adjacent to smaller structure(s). Consider the following:
- » *Transition a building height to be lower when adjacent to a one-story structure.*
 - » *Add elements like patios/porches, balconies, pergolas, awnings, etc.*
 - » *Use a lower floor-to-floor height or half-stories to maintain desired square footage without appearing monolithic.*
 - » *Also refer to BD.2 Street Level Design*
- E. **Express historic industries** of Mintun in the building form and massing: mining, railroad, farming.



Front and rear planes shall have a maximum wall dimension of 50-feet and side planes shall have a maximum of 75-feet. A 3-foot minimum offset is required to differentiate planes.



New structures shall maintain a scale and compatibility with nearby traditional structures.



Break down the scale of a building when adjacent to smaller structure(s).

BD.2 STREET LEVEL DESIGN

- A. The street level of a building **facing Main Street** shall include a minimum of **70%** transparency.
- B. **One entrance per 25 feet** is required along **Main Street**.
- C. Design each street level (ground floor of a building facing a public street) to **be active and engaging** and provide visual interest for pedestrians. A **storefront** shall be required along **Main Street**.
 - » *Reflect historic patterns and proportions for new storefront designs.*
 - » *If the use is not compatible with a storefront, an alternative may be approved by the Design Review Board.*
 - » *Alternative options for street-level design include: windows, doors, display windows, landscaping, wall art/mural.*
 - » *Activation is desired along Williams and Eagle River Streets.*

ADDED LAST BULLET - VISION INTENT IS TO ACTIVATE SECONDARY EDGES AS WELL, TO THE EXTENT FEASIBLE.



The street level of a building facing Main Street shall include a minimum of 70% transparency, have an entrance every 25-feet and be designed to provide visual interest.

BD.3 MATERIALS AND COLOR

- A. **Primary materials** shall be selected based upon historic use and compatibility with Minturn's historic industries and built environment.
 - » *Natural materials such as wood and stone are preferred primary materials. Contemporary interpretations of wood, such as concrete board may be appropriate.*
 - » *Brick is not a commonly found material in the 100 Block but may be applicable as a primary material due to its durability.*
 - » *Cementitious three-coat stucco may be used as a primary material, except facing Main Street.*
- A. **Secondary materials** shall add color and visual interest.
 - » *Consider use of metal or concrete boards or panels, stone veneer, stucco, or other contemporary material.*
- B. Extruded aluminum or vinyl siding and EIFS (exterior insulating finishing system) or other synthetic stucco shall **not be allowed**.
- C. **Color** is welcome in the 100 Block. Color selection shall take into consideration the context.
- D. Utilize materials that are **modules** - such as panels, siding, and masonry - to add human scale to a building.
- E. A **change of material** shall not be allowed on the same plane of a wall surface.
- F. Avoid using too many materials in a single design where a building appears too "busy."



Utilize materials that are modules to add human scale to a building. Color is welcome in the 100 Block.

BD.4 ROOFS

- A. Design **roofs** to **minimize the overall scale** of a building, be **compatible** with traditional forms, and **maximize sun access** to adjoining properties.
- B. **Pitched roofs** (gabled, hipped, and shed) are most common and preferred. **Alternative roof forms** such as mansard, gambrel, domed, curved, etc. are not allowed.
 - » *Utilize pitched roofs for additional indoor space, when feasible.*
 - » *Flat roofs and faux flat roofs are also present in traditional forms and should be permitted.*
- C. Roof shapes shall be made of **simple form(s) with additive features**. Do not over-complicate a roof form with too many forms.
- D. Be cognizant of where **snow** will shed. Do not place a public gathering space in a direct line of steep pitch. Place appropriate snow controls to avoid roof avalanches.
- E. Use **materials** commonly found in Minturn - metal (shingle or standing seam), shingle (asphalt or slate), or other material approved by the design review board.
 - » *Consider the color of a roof, especially when visible from a public street. Alternative colors (i.e. not black or gray) are welcome, so long as they are compatible with their surroundings.*
- F. In general, buildings and roof design should appear to **step up the hillside** when viewed from far away.



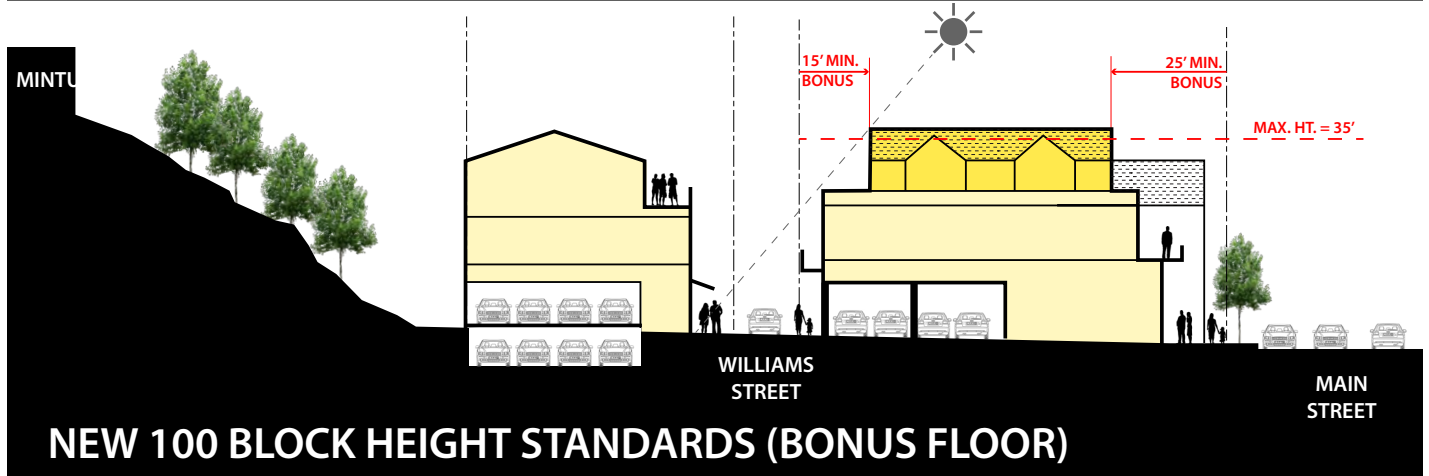
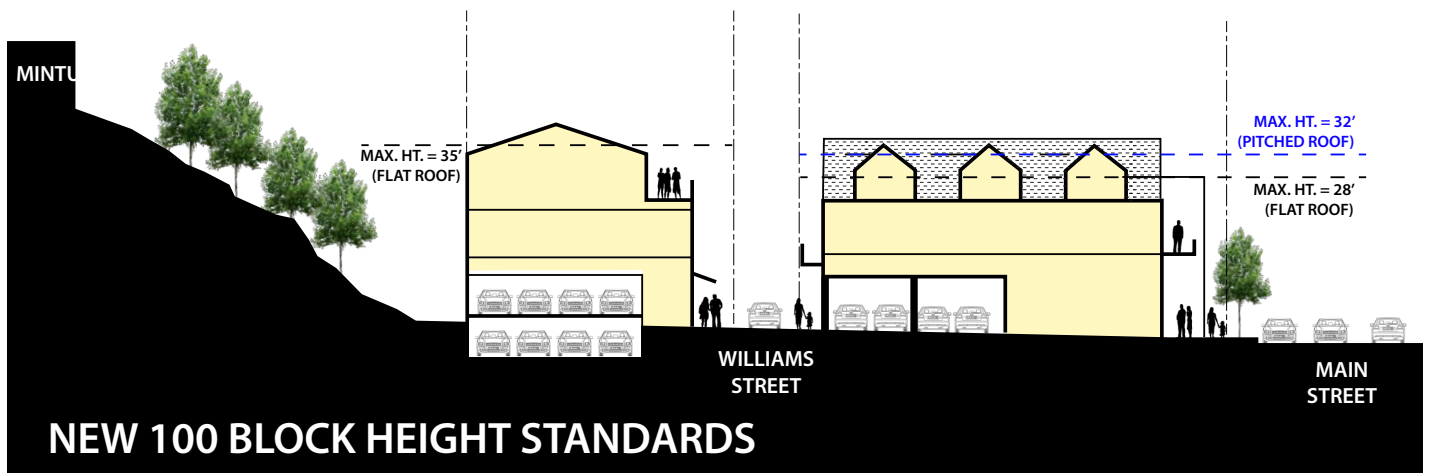
Pitched roofs are most common and preferred. Roof shapes should be made of simple forms with additive features.

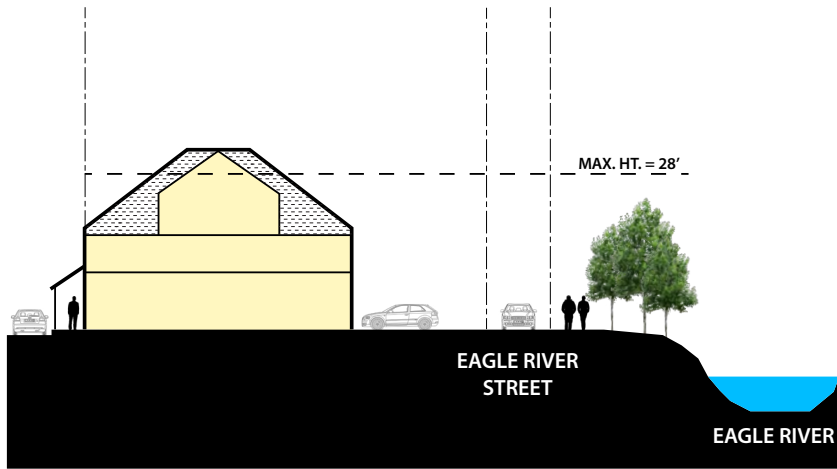


Buildings appear to step up a hillside.

BUILDING DESIGN: BUILDING HEIGHT STANDARDS

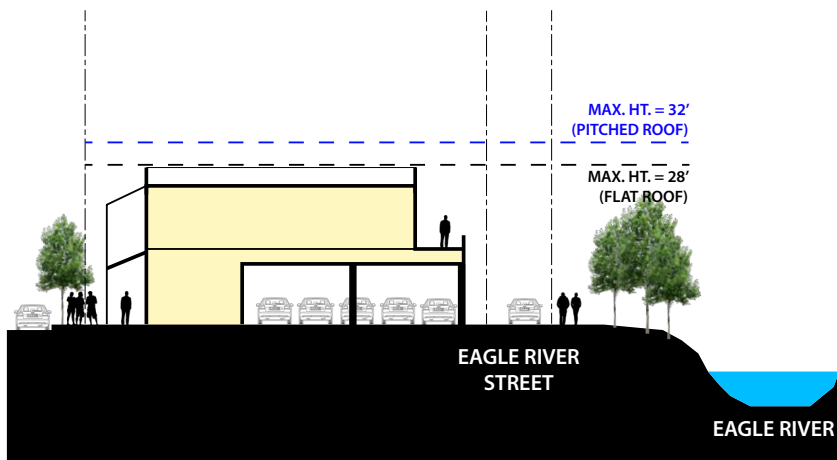
GRAPHIC HAS BEEN UPDATED TO REFLECT
CURRENT 35-FOOT MAX.





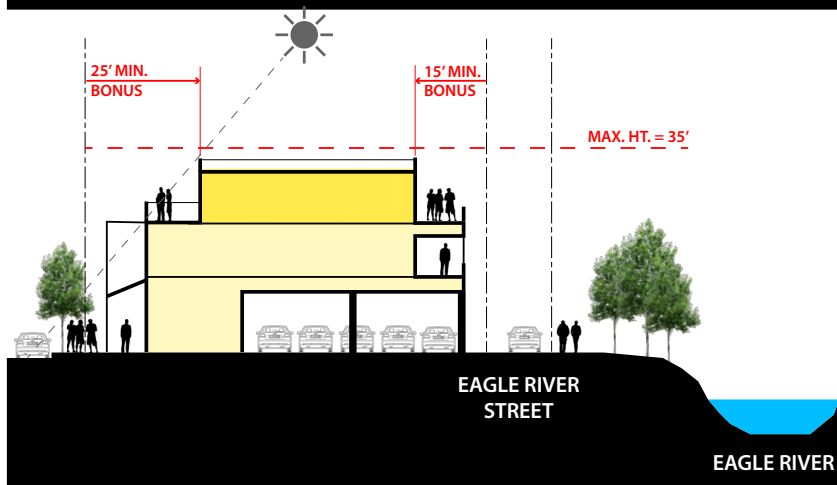
FORMER 100 BLOCK HEIGHT STANDARDS

This diagram shows the existing conditions and former height rules for the 100 Block which included a maximum of **28-feet** for properties with frontage along Main Street (now 100 Block: A) and **35-feet** for properties off of Main (now 100 Block: B.) There was also a bulk plane for Main Street properties that required a setback or pitched roof after 20-feet at the Main Street setback line.



NEW 100 BLOCK HEIGHT STANDARDS

100 Block: B (along Williams) maintains the same rules. 100 Block: A (along Main) keeps a maximum of **28-feet (2-stories) and 35-feet (3-stories)** for flat roof structures. **Pitched roof structures are encouraged** by allowing additional height: up to **32-feet (2.5 stories)**. The diagram to the left shows the potential scale of new structures under these rules.



NEW 100 BLOCK HEIGHT STANDARDS WITH A BONUS FLOOR

An additional floor/story may be added in 100 Block: A if it meets certain dimensional requirements as well as additional site improvement requirements. The maximum height for 100 Block: A is **35-feet (3-stories)**. This option will provide variety in scale and development options while maintaining the scale of traditional structures.

BUILDING DESIGN: INSPIRATION



The rear of a building is activated with doors, windows, product display and a brew kettle to accent its use and brand. The architecture pays homage to historic building types.



A building uses natural materials like wood and stone to blend into a natural setting with large windows to let daylight in.



Hotel Minturn is a local example of a taller-than-average building that fits in nicely due to the massing, roof design, and materials.

**REPLACED IMAGES THAT WERE NOT
INSPIRATIONAL - USED SOME LOCAL
EXAMPLES**



Though not new, the Fly Shop is a good example of something that celebrates Minturn's character - using natural materials and red accent color around the trim.



An transparent storefront spills light onto a sidewalk, making it inviting and welcoming.



A contemporary building uses wood and metal siding and activates the ground floor with a storefront, multiple entrances, and planters.



A modest 3-story mixed use building.



The “rear” of a building animates an alley by adding an active, tax-generating use. The scale of the building is reduced by stepping back upper floors to create a human scale from the ground.

ADDED ONE MORE MODEST MIXED USE BUILDING AS INSPIRATION

Minturn 100 Block

Chapter 16 Proposed Amendments

Chapter 16; Article 2: Definitions, Illustrations, and Lot Standards

Sec. 16-2-20 – Definitions.

Half stories means habitable space that is located within the pitched roof portion of a structure or where the majority of the habitable space is below-grade. Habitable space within a half story shall have a minimum floor to ceiling height of 5 feet at the lowest point. See Sec. 16-2-65.

Incentive floor means an additional floor of development that is allowed only when additional requirements for the development are met. See Sec. 16-2-65.

Parking In lieu fee means a fee that is paid by a developer to the Town of Minturn in exchange for a reduced parking requirement. See Sec. 16-16-150.

Pitched roof means a gable, hip, gambrel or shed roof where height shall be measured to the mean height level between the highest ridge or wall and its highest associated eave, provided, however, that if any parapet wall equals or exceeds the height of the highest ridge, then vertical distance shall be measured to the highest point of the parapet. Mansard roof height shall be measured to the decline of the roof.

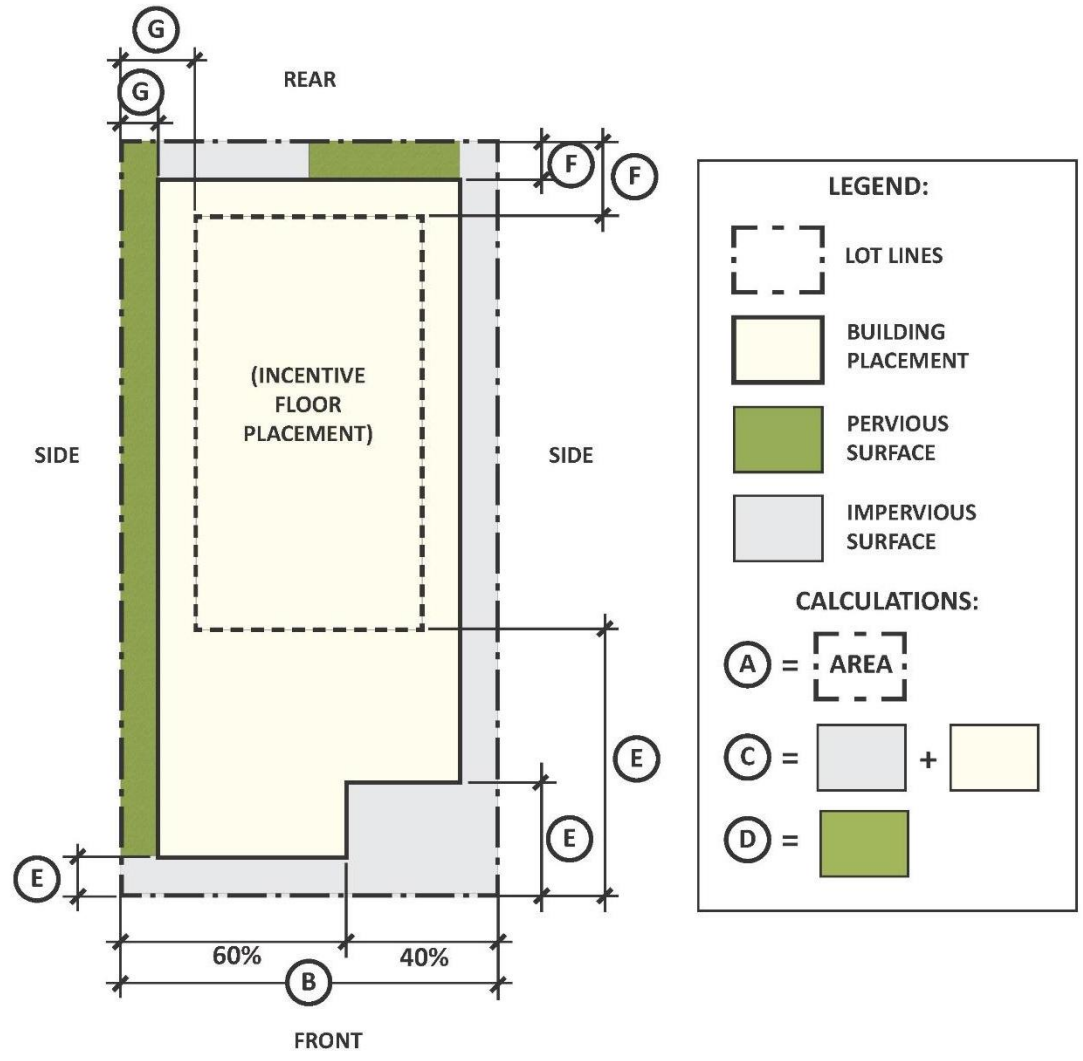
Publicly accessible means a space such as a plaza or sidewalk on private property that is directly accessible from a public right-of-way and use by the general public is allowed.

Sec. 16-2-40 – General lot requirements and dimensional standards.

TABLE 16-A
Dimensional Standards

| | | <u>A</u> | <u>B</u> | <u>C</u> | <u>D</u> | <u>E</u> | <u>F</u> | <u>G</u> | <u>H</u> |
|----------------|---------------------------------|--|---|-----------------------------------|-------------------------------------|---|---|-----------------|--------------------------|
| | | <u>LOT REQUIREMENTS</u> | | | | <u>SETBACKS</u> | | | |
| Character Area | Zones | Min. Lot Area / <u>Max. Lot Area</u> (sq. ft.) | Min. Lot / <u>Max. Lot</u> Dimension (ft) | Maximum Building Lot Coverage (%) | Maximum Impervious Surface Area (%) | *Minimum Setbacks (ft) <u>^Incentive Floor Setbacks (ft)</u> | | | Live Stream Setback (ft) |
| | | | | | | Front | Rear | Side | |
| Old Town | 100 Block Commercial : A | 2,500 / <u>7,500</u> | 25 / <u>75</u> | 80% | 90% | 10 <u>5 min.</u> <u>15 min. 40% lot frontage</u> <u>20 max.</u> <u>^25</u> | 10 <u>5</u> <u>^15</u> | 5 <u>^10</u> | 30 |
| | <u>100 Block: B</u> | | | | | <u>5</u> | 10 <u>5</u> | 5 | |

Illustration No. 16-6



Sec. 16-2-50 – Specific lot requirements and dimensional standards

~~(m) For structures two (2) or three (3) stories in height, a maximum roof length of sixty (60) feet in one (1) direction is allowed. After sixty (60) feet, the roof height needs to be lowered by a minimum of nine (9) feet for a minimum run of twenty (20) feet in length or change directions a minimum of ninety (90) degrees for a minimum length of twenty (20) feet.~~

Sec. 16-2-65 – 100 Block Commercial Zones building height limitations.

(a) In the 100 Block Commercial Zone, commercial buildings not fronting or adjacent to Highway 24 can have a maximum building height of thirty five (35) feet with a maximum angle of forty five (45) degree bulk plane from the street front setback or a maximum of twenty eight (28) feet for a flat roof.

(b) All buildings in the 100 Block Commercial Zone fronting or adjacent to Highway 24 located between Eagle Street and Williams Street can have a maximum building height of twenty eight (28) feet with a maximum angle of forty five (45) degree bulk plane from the street front setback or a maximum of twenty eight (28) feet for a flat roof.

(c) Slope of roof shall be a minimum of 4:12.

(d) Residential construction in the 100 Block Commercial Zone must follow residential building height limitations.

(See also Illustration No. 16-2 and Appendix B, Design Standards and Guidelines) The following Table 16-B contains the regulations for building height.

(a) New buildings in the 100 Block shall be allowed a maximum height as shown in Table 16-C.

(b) An incentive floor is allowed for 100 Block: A, following dimensional requirements listed in Tables 16-A and 16-C, and based on the provision of the following on-site improvements associated with the development:

a. An enhanced streetscape that provides a minimum of (1) street tree per 30 linear feet; (1) bench/seating area per 30 linear feet; and (1) bike rack accessible from a public right-of-way or passage; plus one of the following:

i. A publicly accessible plaza or outdoor dining space (area shall be a minimum of 15% of total area of lot) that is visible from a public right-of-way.

ii. A publicly accessible passage (min. 4-foot wide pathway) is provided between Main Street and Williams Street or Main Street and Eagle River Drive. A min. 8-foot side setback shall be required to accommodate this space. Public passages shall be a minimum of 150 feet apart, i.e. if one is already provided 100-feet away, this option shall not be used.

iii. Additional parking (min. 5 spaces extra) on-site for public use or that may be shared with other uses on the 100 Block. See Sec. 16-16-150.

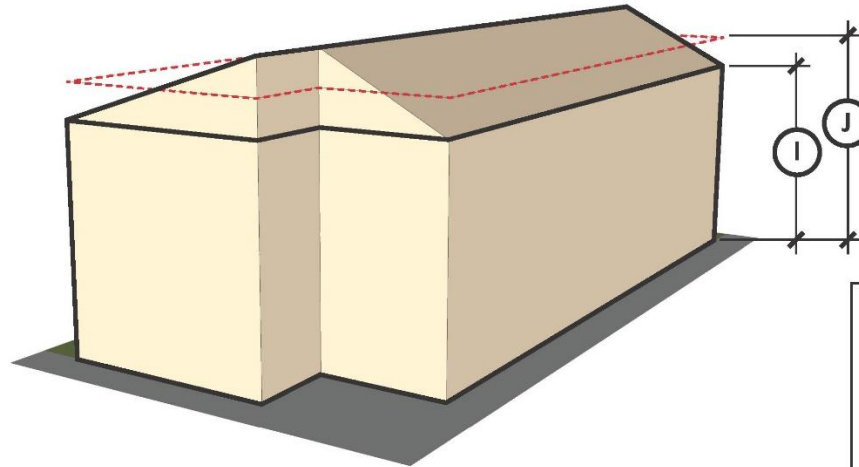
iv. An active use is provided along Williams or Eagle River Street.

Table 16-C



| | I | J | K |
|--------------|---------------------------------------|--------------------------------------|--|
| | 100 Block Building Height Limitations | | |
| | Max. height / stories (flat roof) | Max. height / stories (pitched roof) | Incentive floor max. height / stories (any roof) |
| 100 Block: A | 28' / 2 stories | 32' / 2.5 stories | 35' / 3 stories |
| 100 Block: B | 35' / 3 stories | 35' / 3 stories | N/A |

Illustration No. 16-7

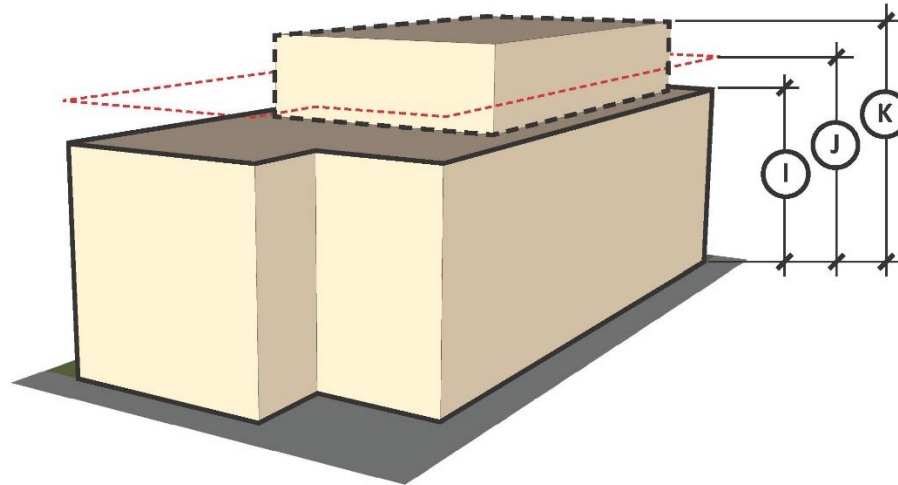
PITCHED ROOF SCENARIO



LEGEND:

-  PITCHED ROOF MAX. HEIGHT
-  INCENTIVE FLOOR

FLAT ROOF OR INCENTIVE FLOOR SCENARIO



~~Sec. 16-2-70. Specific front, side and rear yard setbacks requirements for 100 Block Commercial Zone.~~

- ~~(a) No parking spots allowed in the front or side setback on Highway 24 in the 100 Block Commercial Zone.~~
 - ~~(b) No asphalt.~~
 - ~~(c) Landscaping to include native trees, plants, tables, chairs, benches, decorative hardscape and planters for flowers.~~
 - ~~(d) Pedestrian lights to be hung on building for pedestrian lighting. Meet dark sky requirements in Code [Section 16-17-180](#), Exterior Illumination Standards.~~
 - ~~(e) Must include irrigation.~~
 - ~~(f) Must have exterior power outlets.~~
 - ~~(g) All setbacks shall contain an area equal to five (5) percent of the total setback area for temporary snow storage (until snow can be removed off-site).~~
 - ~~(h) Ten (10) percent of each setback area must be landscaped area.~~
 - ~~(i) Properties fronting Williams Street are encouraged to create an attractive, pedestrian friendly streetscape.~~
 - ~~(j) Dumpsters, trash, and storage areas shall be properly screened.~~
- ~~(Ord. [5-2016](#) §2)~~

~~Sec. 16-2-80. Block and building site patterns and side yard setbacks for 100 Block Commercial Zone.~~

- ~~(a) Vacations of or building across originally platted lot lines shall not result in a building greater than seventy-five (75) feet in width along Highway 24, Williams Street or Eagle Street.~~
- ~~(b) Building facades along Highway 24, Williams Street and Eagle Street shall not create the appearance of a single building that is wider than fifty (50) feet. Building facades must maintain the historic platted character of this area with building widths of twenty five (25) feet to fifty (50) feet.~~
- ~~(c) Maximum lot area (square feet): seven thousand five hundred (7,500).~~
- ~~(d) Minimum side yard setback is five (5) feet per twenty-five (25) foot wide platted lot. For buildings fronting or adjacent to Highway 24 located between Eagle Street and Williams Street, the Planning Commission may require that the side yard setbacks be combined for the purpose of creating a ten (10) to fifteen (15) foot side yard corridor between two (2) buildings.~~
- ~~(e) Corner lots: ten (10) foot front yard setback facing Highway 24; five (5) foot setback on side facing any new or existing street connecting to Highway 24.~~

Chapter 16; Article 6: Old Town Character Area

Sec. 16-6-35. - 100 Block Commercial Zone.

(a) The 100 Block Commercial Zone is characterized by a compact retail/commercial core area bisected by Highway 24. The area consists of businesses and residences with an identity of the historic commercial core that is distinct from other parts of the community.

(b) The purpose of this area is to provide convenient commercial services to residents and visitors and to promote the development of the Town's primary retail commercial district. Accommodation of sales tax-generating commercial uses and non-street level residential units can enhance Old Town vitality while maintaining the visual character and scale. Street level space within the 100 Block Commercial Zone shall be dedicated to retail uses. Non-street level space within the 100 Block Commercial Zone may be used for compatible retail, office, and residential uses.

(c) The 100 Block Commercial Zone should function as a pedestrian shopping corridor. Buildings ~~should be~~ **shall** oriented toward sidewalks and pedestrian areas with storefront windows and main entrance doors. The scale of buildings both in terms of height and width should encourage transparency, pedestrian engagement, and facilitate pedestrian movement not only along Highway 24 but also between Highway 24, Williams Street and Eagle Street **and along all streets in the 100 Block.**

(d) The small town **historic** main street character will be maintained by keeping in scale ~~of~~ **with the** original plat of twenty-five (25) foot by one hundred (100) foot lots and one (1) to two (2) story building heights **frontages along Highway 24**. Large monolithic buildings and "strip" retail areas are ~~discouraged~~ **not allowed**. **New development in the 100 Block shall follow the standards and guidelines provided in Appendix B which seeks to ensure new structures maintain compatibility with historic structures while bringing new uses to increase vitality.**

Sec. 16-6-70. – Old Town Character Area use table.

Note that this section notates only changes to the 100 Block section of the use table.

| | 100 Block: A | 100 Block: B |
|---|-----------------------|-----------------------|
| Accessory apartments (street level) | N | N |
| Accessory apartments (second floor or above) | C | C |
| Accessory dwellings (street level) | N | N |
| Accessory dwellings (second floor or above) | C | C |
| Business and office services | N C | N C |
| Multifamily dwellings (street level) | N | N |
| Multifamily dwellings (second floor or above) | C | C |
| Professional activities (street level) | N | N |
| Professional offices, business offices and studios (street level) | N | N |

***All residential uses in the 100 Block shall be conditional based on their ability to provide sufficient access to parking (if applicable) which may require additional setbacks along Williams and Eagle River Streets due to these streets being extremely narrow. Refer to Appendix B 100 Block Design Standards and Guidelines for additional guidance.**

What about allowing Professional offices – could imagine an architect’s office or similar design-related business on ground floor along Eagle River or Williams?? Discuss.

Chapter 16; Article 16: Off-Street Parking and Loading

We are presenting THREE FINAL OPTIONS to be considered for Council's final direction:

Option 1 is what Planning Commission approved; Option 2 increases residential parking requirements to match rest of town; Option 3 identifies a new solution to creating the desired urban design along Williams and Eagle River Streets as true, active, shared streets.

Sec. 16-16-150. – 100 Block Incentive-Based Parking Program (OPTION 1 – FEE IN LIEU + REDUCED SITE PARKING)

In order to incentivize sales-tax producing uses and the reuse of existing structures, new development in the 100 Block Zones may opt to reduce parking standards by providing the following:

- (a) Development shall calculate the total number of spaces required by the code for the proposed program by using Table 16-10, 16-11, and 16-12. The resulting number of spaces = A.
- (b) Development shall calculate the total number of spaces required for the proposed program using Table 16-14. The resulting number of spaces = B.

Table 16-14

| <u>Use</u> | <u>Parking Standard</u> |
|--|------------------------------------|
| <u>Sales-tax generating use (commercial, retail, restaurant, etc.)</u> | <u>0</u> |
| <u>Any use inside an existing structure (adaptive reuse/historic preservation)</u> | <u>0</u> |
| <u>Office and non-sales-tax generating commercial use</u> | <u>1 space per 500 sq. ft.</u> |
| <u>Light Industrial</u> | <u>1 space per 1,000 SF</u> |
| <u>Residential (up to 2-bedroom max.)</u> | <u>1 space per unit</u> |
| <u>Lodging, hotel/motel, bed and breakfast</u> | <u>0.5 space per rentable room</u> |

- (c) Conduct the following calculation:
 - a. A (minus) B (minus) allowable on-street/off-site spaces (Sec. 16-16-60) = # of spaces required for in lieu fee. The resulting number of spaces = C.
- (d) Developer shall pay in lieu fee to the Town of Minturn at a rate of C (times) \$10,000.
- (e) Developer would be exempt from in lieu fee if the following is true:
 - a. Development meets parking requirements from Tables 16-10, 16-11, and 16-12; and
 - b. Development allows the additional parking (C) on-site to be used for public or shared parking for other uses in the 100 Block.
- (f) Tandem parking shall not be allowed. (note: this was added by Council)**

Sec. 16-16-150. – 100 Block Incentive-Based Parking Program (OPTION 2 – FEE IN LIEU + RESIDENTIAL PARKING AS-IS)

In order to incentivize sales-tax producing uses and the reuse of existing structures, new development in the 100 Block Zones may opt to reduce parking standards by providing the following:

- (a) Development shall calculate the total number of spaces required by the code for the proposed program by using Table 16-10, 16-11, and 16-12. The resulting number of spaces = A.**
- (b) Development shall calculate the total number of spaces required for the proposed program using Table 16-14. The resulting number of spaces = B.**

Table 16-14

| <u>Use</u> | <u>Parking Standard</u> |
|--|------------------------------------|
| <u>Sales-tax generating use (commercial, retail, restaurant, etc.)</u> | <u>0</u> |
| <u>Any use inside an existing structure (adaptive reuse/historic preservation)</u> | <u>0</u> |
| <u>Office and non-sales-tax generating commercial use</u> | <u>1 space per 500 sq. ft.</u> |
| <u>Light Industrial</u> | <u>1 space per 1,000 SF</u> |
| <u>Residential</u> | <u>Use Table 16-10</u> |
| <u>Lodging, hotel/motel, bed and breakfast</u> | <u>0.5 space per rentable room</u> |

- (c) Conduct the following calculation:**
 - a. A (minus) B (minus) allowable on-street/off-site spaces (Sec. 16-16-60) = # of spaces required for in lieu fee. The resulting number of spaces = C.**
- (d) Developer shall pay in lieu fee to the Town of Minturn at a rate of C (times) \$10,000.**
- (e) Developer would be exempt from in lieu fee if the following is true:**
 - a. Development meets parking requirements from Tables 16-10, 16-11, and 16-12; and**
 - b. Development allows the additional parking (C) on-site to be used for public or shared parking for other uses in the 100 Block.**
- (f) Tandem parking shall not be allowed. (note: this was added by Council)**

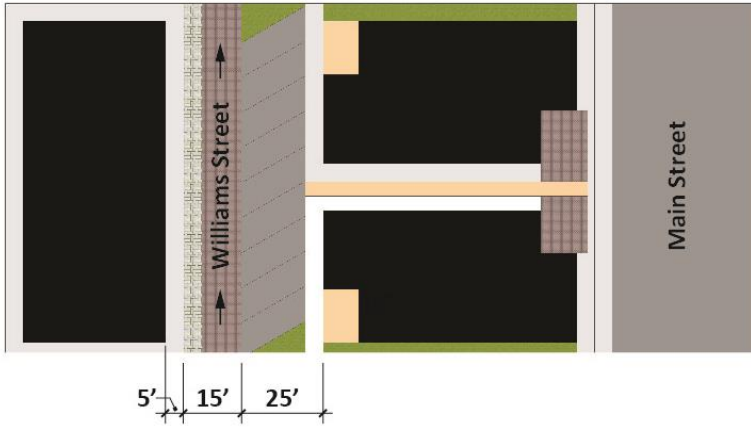
Sec. 16-16-150. – 100 Block Incentive-Based Parking Program (OPTION 3 – SHARED STREET PARKING CONCEPT)

In order to incentivize sales-tax producing uses, the reuse of existing structures, activation of ground floor spaces, and a unique urban design concept of shared streets, parking requirements for any new uses on the 100 Block shall be exempt, except for the following:

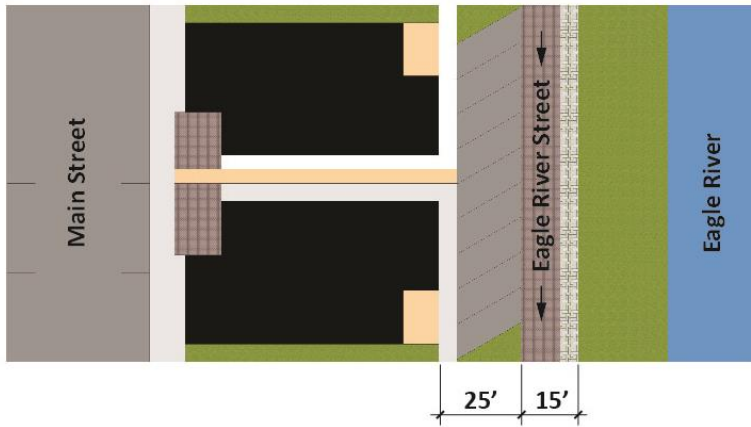
- (a) 100-Block A properties shall provide 30-degree angled parking for the width of their lot along Williams and Eagle River Streets. From the 15-foot wide public right-of-way, a setback of 25-feet shall be provided and within the 25-feet the developer shall provide a 17' deep bay of 30-degree angled parking stalls that are 8.5-feet wide minimum. Refer to Diagram on following page.

Note: Option 3 would require amending the rear setbacks of 100 Block: A from 5' to 25'.

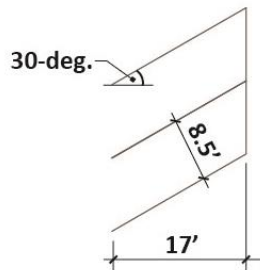
Williams Street Condition



Eagle River Street Condition



Parking Stall Dimensions



Appendix B – Minturn Design Standards and Guidelines

B. Goals, ~~and~~ Objectives, and Guiding Principles

In order to preserve and enhance the built environment, and to further the character of the community, Minturn has adopted design standards and guidelines for development. The Minturn Community Plan contains the community's vision for the future, a future that is grounded in the character of the past. This character is based on a respect for both the history and environment of the region, a respect that is evident in the existing architectural styles and layout of the Town. The preservation and enhancement of landforms, vegetation, and wildlife habitat are consistent with these objectives.

These standards and guidelines are to be used as a supplement to the Town of Minturn Zoning Code. Review of compliance with these design standards and guidelines will occur for all new development, as well as major restoration and/or renovation of existing structures or developments within the Town boundaries.

New projects should be designed with the following guiding principles in mind:

1. **Minturn is Rich with History** – all designs should respect and reflect our rich history and help keep our stories alive for future generations.
2. **Minturn is Close to Nature** – every building in Minturn is steps from nature – we respect and appreciate our environment in all aspects of design.
3. **Minturn is Colorful** – Minturn is not dull – look around and you will see colorful buildings, roofs, people, and landscapes. New designs should reflect our diversity by implementing a colorful, not dull, palette.
4. **Minturn is Authentic** – we have never tried to be like other communities – we are uniquely and proudly the one and only “Minturn.” New designs should contribute to our meaning and authenticity.

5. – 100 Block Commercial Zone
(see insert)



To: Minturn Town Council
From: Michelle Metteer
Date: September 7, 2022
RE: Town Manager Update

New Water Tank Update

See included *Construction Observation Report* by SGM.

New Water Treatment Plant - Construction Management at Risk Process

The process is underway for creating the Request for Proposal to acquire the construction firm that will assist with the building of a new water treatment plant. Bringing the contractor on at the beginning of the process will help identify bid/cost risks as well as supply chain issues. The goal is to have the project at 30% design by Q1 of 2023 with input from the contractor.

Water Meter Transmitter

Final installation of the WaterSmart meter transmitter is occurring by the end of this month. Minturn is now working to understand what additional steps must be taken to bring this program online and provide real-time water use to residents, businesses and property owners.

Gasoline Spill and Ongoing Mitigation Efforts

Cleanup efforts continue from the gasoline spill that occurred at the local Shop & Hop gas station over ten years ago. A new firm has been contracted by the State of Colorado to lead the cleanup effort. Work will be conducted later this fall and continued in the spring of 2023. The goal is to mitigate, as much as possible, the benzene plume in the groundwater table. Maps and benzene reads are provided with this update and a full public discussion on this matter will take place at the October 5, 2022 Council meeting. The Town encourages property owners, residents and businesses to take part in this discussion and cleanup effort.

For additional information on the spill please reference the [Shop & Hop Q2 Groundwater Monitoring Report](#). This is a large document that will not be included in the packet. Paper copies can be available at the Minturn Town Hall upon request.

Attachments:

- Water Tank Construction Observation Report
- Gasoline Spill Phase 1 & 2 Mitigation Plan



MEMORANDUM

TO: Michelle Metteer, Town Manager
FROM: Ryan Gordon, PE
DATE: August 30, 2022
SUBJECT: Construction Observation week ending August 126 2022

I was on-site on August 24th and 26th 2022 to check overall construction progress and to witness the concrete pours for the last of the tank walls and the tank foundation.

The reinforcement for the walls was installed per the contract documents including the reinforcement for the overflow pipe – which is embedded in the wall panel. The reinforcement for the tank foundation was checked along with the waterstops, post-tension cabling and piping. It was verified that the elements was installed per the plans.

The foundation was poured on Friday August 26th. The pour was approximately 107 cu. Yards or 11 truckloads. The concrete was placed in a single day. Concrete samples were taken on each truck to verify the mix design. Concrete formwork will be stripped on Saturday August 27th. Wall panels are scheduled to be placed on Tuesday September 6th.



Figure 1 – Wall panel pour



Figure 2 – Wall panel with reinforcement for overflow piping



Figure 3 – Tank floor rebar layout



Figure 4 – Tank waterstop



Figure 5 – Pouring concrete



Figure 6 - Finished Tank Floor

Michelle Metteer

From: Will Nabours <will.nabours@solitechnical.com>
Sent: Monday, August 8, 2022 8:44 AM
To: Michelle Metteer
Subject: Groundwater Monitoring Report
Attachments: Shop&Hop#12 Groundwater Monitoring Report - Q2 2022.pdf; Phase I.pdf; Phase II.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Hi Michelle,

We just wrapped up the most recent groundwater monitoring report following our Q2 2022 carbon injection pilot testing and groundwater sampling. The report is attached for your reference. The *Recent Activities* section of the narrative presents the work we've done since taking over the project from the previous consultant.

In summary, the carbon injection pilot test was successful in that we were able to inject our carbon fluid into the ground. We are still waiting to see the effects of the work on petroleum hydrocarbon concentrations in groundwater - the carbon takes several months to bind to soils and adsorb the hydrocarbons. As far as dissolved benzene concentrations go, not much has changed since the previous sampling event (Q3 2021). The Q2 2022 sampling event was performed on June 22, 2022. Dissolved petroleum hydrocarbon concentrations in samples collected from 13 site monitoring wells are presented in Figure 6 of the attached report. As you can see, most remaining contamination lies beneath Main Street.

As far as future work goes, the State will likely want us to perform two "full-scale" carbon injection events, pending Town of Minturn and CDOT approval. The first (Phase I) would include injecting at approximately 47 locations within the Norman Avenue cul-de-sac northeast of Main Street as well as partially down the shoulder of Main Street to the southeast of the intersection (see attached figure). The purpose would be to create a "barrier" between the benzene plume and the Eagle River. The second event would entail injections at approximately 41 locations along both shoulders of Main Street (Phase II) and would be contingent upon the results of the recently performed pilot test. The purpose of this would be to knock down benzene concentrations within the plume. All the injection points would be 10 feet apart and would look the same as those done on the 401 Main Street property (with a 3-to-6-inch core through the asphalt road surfacing). Upon completion, we would backfill the injection holes appropriately and restore the surface the same way as what was done during the pilot test.

Anyway, I'd like to hear your thoughts on the future work as I know we've talked a few times about trying to minimize further disturbance to Main Street.

I appreciate your time,

Will



Will Nabours, P.E.

Senior Engineer | Partner

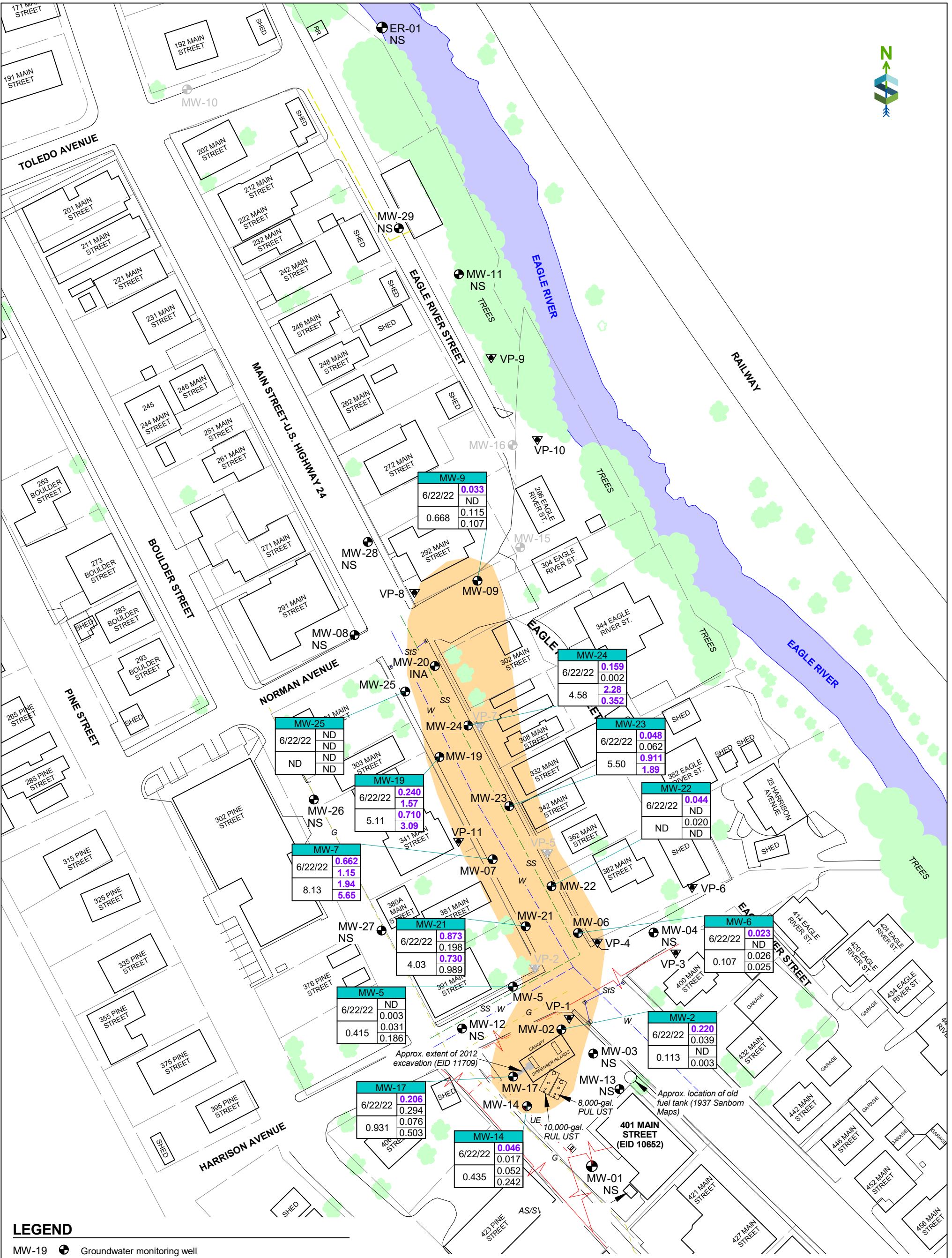
O: 720.507.3970 ext. 2

M: 303.946.7891

P.O. Box 7548, Denver, CO 80207

www.solitechnical.com

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LEGEND

- MW-19 Groundwater monitoring well
- VP-1 Soil vapor monitoring point
- MW-10 Abandoned/destroyed monitoring well
- VP-2 Abandoned/destroyed soil vapor monitoring point

| Well ID | 6/22/22 | 0.240 | Benzene |
|---------|---------|--------------|----------------|
| | | 1.57 | Toluene |
| | | 0.710 | Ethylbenzene |
| | | 3.09 | Xylenes, total |

Sample location and hydrocarbon concentrations in milligrams per liter (mg/L)
 ND = below laboratory detection limits
 INA = well inaccessible
 NS = well not sampled
0.240 Bold values exceed Tier I RBSLs

Estimated extent of dissolved benzene impacts > 0.005 mg/L

- UTILITIES:**
- W Water
 - G Natural gas
 - SS Sanitary sewer
 - StS Storm sewer
 - Overhead electric
 - Natural gas
 - Underground electric

Samples collected July 22, 2022

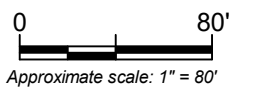
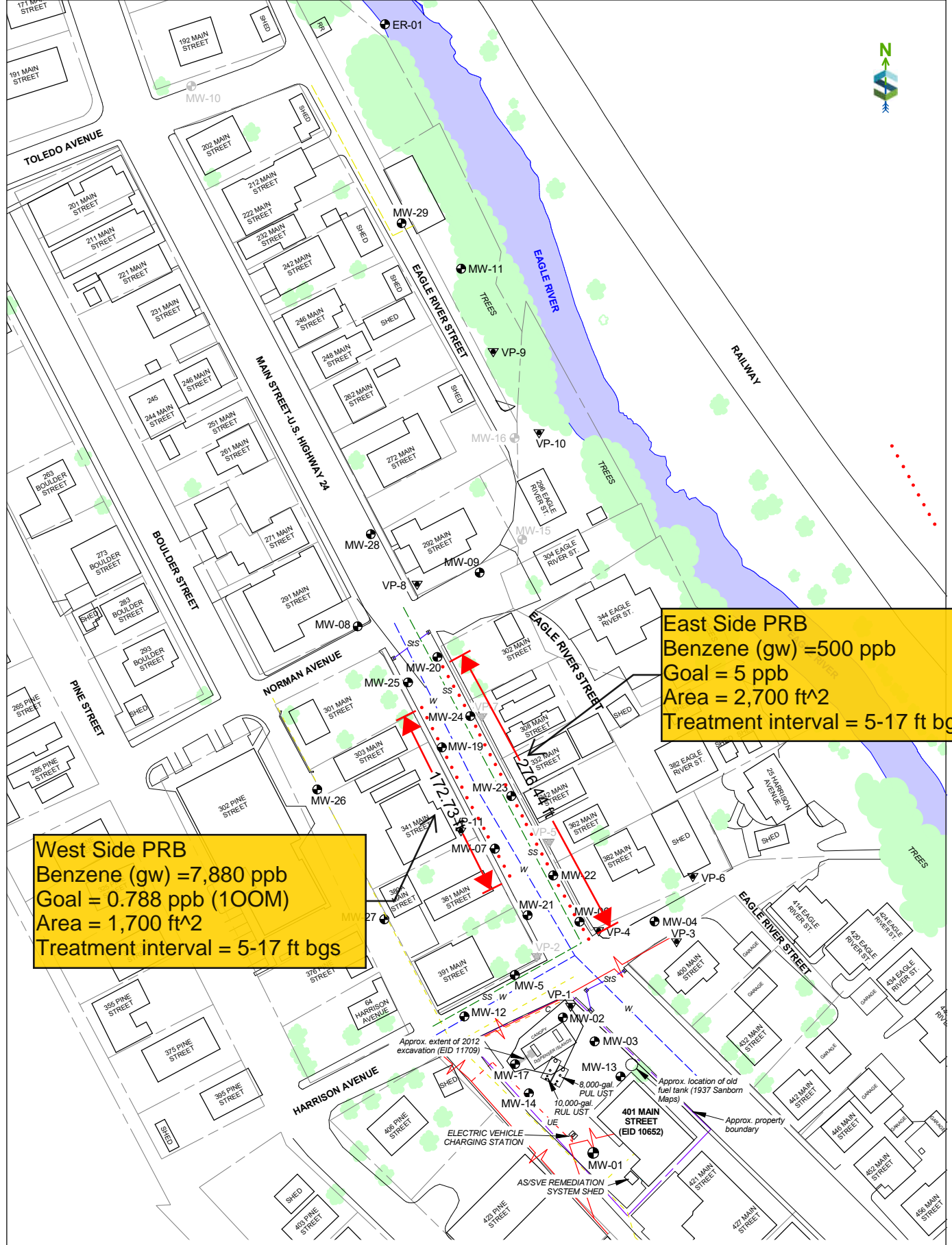


FIGURE 6 - Groundwater Sample Results

Shop & Hop #12
 401 Main Street
 Minturn, Colorado 81645
 Soli Project #1028





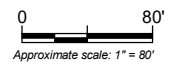
East Side PRB
 Benzene (gw) = 500 ppb
 Goal = 5 ppb
 Area = 2,700 ft²
 Treatment interval = 5-17 ft bgs

West Side PRB
 Benzene (gw) = 7,880 ppb
 Goal = 0.788 ppb (100M)
 Area = 1,700 ft²
 Treatment interval = 5-17 ft bgs

LEGEND

- MW-20 Groundwater monitoring well
- VP-1 Soil vapor monitoring point
- MW-10 Abandoned/destroyed monitoring well
- VP-2 Abandoned/destroyed soil vapor monitoring point
- Carbon slurry injection point (41 total on 10-foot spacings)

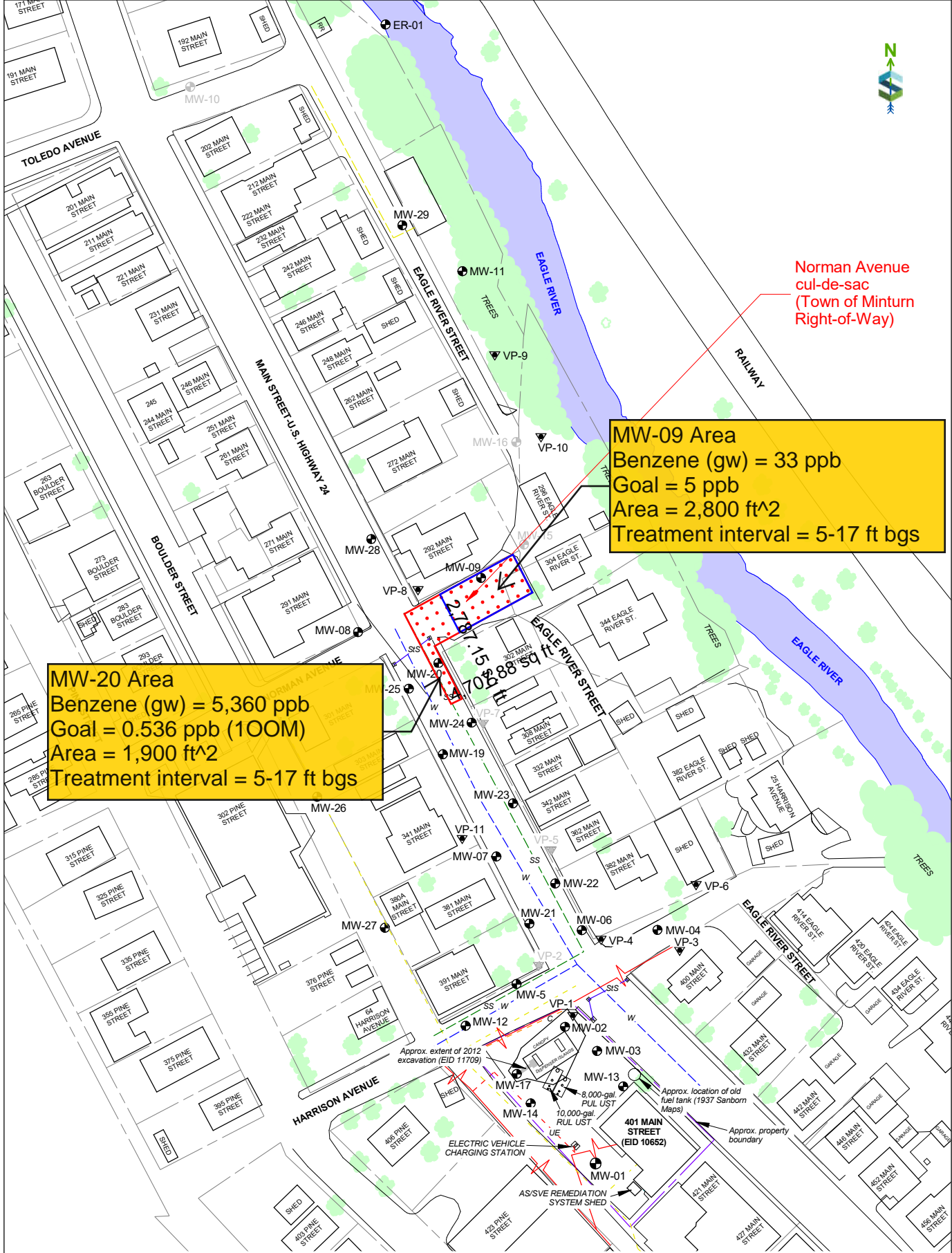
- UTILITIES:**
- Water
 - Natural gas
 - Sanitary sewer
 - Storm sewer
 - Overhead electric
 - Natural gas
 - Underground electric



RFQ Figure 1 - Phase II Injections

Shop & Hop #12
 401 Main Street
 Minturn, Colorado 81645
 Soli Project #1028





MW-09 Area
 Benzene (gw) = 33 ppb
 Goal = 5 ppb
 Area = 2,800 ft²
 Treatment interval = 5-17 ft bgs

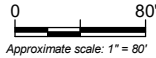
MW-20 Area
 Benzene (gw) = 5,360 ppb
 Goal = 0.536 ppb (100M)
 Area = 1,900 ft²
 Treatment interval = 5-17 ft bgs

LEGEND

- MW-20 Groundwater monitoring well
- VP-1 Soil vapor monitoring point
- MW-10 Abandoned/destroyed monitoring well
- VP-2 Abandoned/destroyed soil vapor monitoring point
- Carbon slurry injection point (47 total on 10-foot spacings)

UTILITIES:

- Water
- Natural gas
- Sanitary sewer
- Storm sewer
- Overhead electric
- Natural gas
- Underground electric



RFQ Figure 1 - Phase I Injections

Shop & Hop #12
 401 Main Street
 Minturn, Colorado 81645
 Soli Project #1028



Jay Brunvand
 Clerk/Treasurer
 301 Pine St #309 ♦ 302 Pine St
 Minturn, CO 81645
 970-827-5645 x1
treasurer@minturn.org
www.minturn.org



Town Council
 Mayor – Earle Bidez
 Mayor Pro Tem – Terry Armistead
 Council Members:
 George Brodin
 Lynn Feiger
 Gusty Kanakis
 Kate Schifani
 Tom Sullivan

Below reflects proposed topics to be scheduled at future Town Council meetings and is informational only. Dates and topics are subject to change.

| REGULAR TOWN COUNCIL MEETINGS |
|---|
| September 21, 2022 |
| Community Plan Draft Review – Joint Planning Commission/Council Discussion |
| Ordinance ___ - Series 2022 – An Ordinance extending the Design Review Moratorium |
| Executive Session – Battle Mountain |
| Discussion/Direction - Snow Removal Operations Plan |
| October 5, 2022 |
| Groundwater Cleanup update – Shop & Hop |
| Joint Session – Historic Preservation Commission/Minturn Town Council |
| Acceptance of the FY2023 Budget |
| Executive Session – Battle Mountain |
| October 19, 2022 |
| Minturn Shooting Range Discussion – Keystone Policy Center |
| Ordinance 11 – Series 2022 (Second Reading) An Ordinance creating 100 Block Design Guidelines – Hunn/Harris |
| Discussion/Direction - School District Cash in Lieu Fee |
| Executive Session – Battle Mountain |
| FY2023 Budget Discussion: Highlights and Special Projects |
| November 2, 2022 |
| Ordinance ___ - Series 2022 – Water Wise Guidelines |
| Executive Session – Battle Mountain |
| FY 2023 Budget – Public Hearing |
| November 16, 2022 |
| FY 2023 1 st reading of Budget Ordinances |
| December 7, 2022 |
| Fy 2023 2 ND reading of Budget Ordinances |

| |
|---|
| |
| |
| DATE TO BE DETERMINED |
| January 18, 2023 – Review Renewal of Ordinance 05 – Series 2020; An Ordinance Implementing a Water Tap Moratorium |
| Chapter 16 Zoning Amendments |
| Assessment of UPRR property and possible |
| Potential legislation regarding Mobile homes |