



**Wednesday, January 4, 2023**  
**Executive Session – 4:30pm**  
**Regular Session – 5:30pm**

## **AGENDA**

**Town Council Meeting Minturn Town Hall / Council Chambers**  
**302 Pine St Minturn, CO**

The agenda is subject to change, including the addition of items 24 hours in advance or the deletion of items at any time. The order of agenda items listed are approximate. This agenda and meetings can be viewed at [www.minturn.org](http://www.minturn.org).

### **MEETING ACCESS INFORMATION AND PUBLIC PARTICIPATION**

This will be an in-person meeting with access for the public to attend in person or via the Zoom link included. Zoom Link: <https://us02web.zoom.us/j/88281154612>

Zoom Call-In Information: **1 651 372 8299 or 1 301 715 8592 Webinar ID: 882 8115 4612**

Please note: all virtual participants are muted. In order to be called upon an unmuted, you will need to use the “raise hand” feature in the Zoom platform. When it’s your turn to speak, the moderator will unmute your line and you will have five (5) minutes for public comment.

**PUBLIC COMMENTS:** If you are unable to attend, public comments regarding any items on the agenda can be submitted to Jay Brunvand, Town Clerk, prior to the meeting and will be included as part of the record.

#### **1. CALL TO ORDER**

**The Minturn Town Council will open the Regular Meeting at 4:30pm for the purpose of convening into Executive Session. At (approximately) 5:30pm the Council will convene into Regular Session.**

#### **2. EXECUTIVE SESSION**

2.1 An Executive Session for the purposes of receiving legal advice on specific legal questions pursuant to C.R.S. 24-6-402(4)(b) and for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for

negotiations, and/or instructing negotiators pursuant to C.R.S. 24-6-402(4)(e) –  
• Battle Mountain – Sawyer/Plaskov

**3. ROLL CALL & PLEDGE OF ALLEGIANCE**

**4. APPROVAL OF CONSENT AGENDA** Consent agenda items are routine Town business, items that have received clear direction previously from the council, final land-use file documents after the public hearing has been closed, or which do not require council deliberation.

4.1 December 21, 2022 Meeting Minutes Pg 4

4.2 542 Main St LLC, DBA Sunrise annual renewal of a Hotel & Restaurant (City) Liquor license; 132 Main St, Douglas McAvity Owner/Manager Pg 11

4.3 Resolution 01 – Series 2022 a Resolution setting the official posing sites for the Town of Minturn Pg 14

**5. APPROVAL OF REGULAR AGENDA** Opportunity for amendment or deletions to the agenda.

**6. DECLARATION OF CONFLICTS OF INTEREST**

**7. PUBLIC COMMENT** Citizens are invited to comment on any item on the Consent Agenda, or not on the regular Agenda subject to a public hearing. Please limit your comments to five (5) minutes per person unless arrangements have been made for a presentation with the Town Clerk. Those who are speaking are requested to state their name and address for the record.

**8. SPECIAL PRESENTATIONS** Presentations are limited to 5 minutes. Invited presentations are limited to 10 minutes if prior arrangements are made with the Town Clerk.

- Ground Water Cleanup update – Will Nabours, SOLI Technical (Shop & Hop spill 2008 & 2012) Pg 16

**9. BUSINESS ITEMS** Items and/or Public Hearings are listed under Business may be old or new and may require review or action by the Council

9.1 Ordinance 01 – Series 2023 (First Reading) An Ordinance approving the historic landmark designation for the Saloon Building located at 146 N Main St. Pg 21

9.2 Resolution 02 – Series 2023 A Resolution approving the 2023-25 Minturn Strategic Plan Pg 46

9.3 Resolution 03 – Series 2023 A Resolution approving the Little Beach Park sub area Master Plan Pg 55

9.4 Resolution 04 – Series 2023 A Resolution approving the establishment of procedures and governing actions of commission in the general conduct of business. Pg 91

**10. DISCUSSION / DIRECTION ITEMS**

10.1 Review and Direction for the Minturn Community Plan Pg 101

11. **STAFF REPORTS** Pg 304

12. **COUNCIL COMMENTS & COMMITTEE REPORTS**

13. **FUTURE AGENDA ITEMS** Pg 305

14. **ADJOURN**

**INFORMATIONAL ONLY ITEMS**

Town Hall Closed:

- January 16 – MLK Day

Upcoming Council Meetings:

- January 18, 2023
- February 1, 2023
- February 15, 2023



**Wednesday, December 21, 2022**  
**Executive Session – 4:30pm**  
**Regular Session – 5:30pm**

**OFFICIAL MINUTES**

**Town Council Meeting Minturn Town Hall / Council Chambers**  
**302 Pine St Minturn, CO**

The agenda is subject to change, including the addition of items 24 hours in advance or the deletion of items at any time. The order of agenda items listed are approximate. This agenda and meetings can be viewed at [www.minturn.org](http://www.minturn.org).

**MEETING ACCESS INFORMATION AND PUBLIC PARTICIPATION**

This will be an in-person meeting with access for the public to attend in person or via the Zoom link included. Zoom Link: <https://us02web.zoom.us/j/81527493686>

Zoom Call-In Information: **1 651 372 8299 or 1 301 715 8592 Webinar ID: 815 2749 3686**

Please note: all virtual participants are muted. In order to be called upon an unmuted, you will need to use the “raise hand” feature in the Zoom platform. When it’s your turn to speak, the moderator will unmute your line and you will have five (5) minutes for public comment.

**PUBLIC COMMENTS:** If you are unable to attend, public comments regarding any items on the agenda can be submitted to Jay Brunvand, Town Clerk, prior to the meeting and will be included as part of the record.

**1. CALL TO ORDER**

**The Minturn Town Council will open the Regular Meeting at 4:30pm for the purpose of convening into Executive Session. At (approximately) 5:30pm the Council will convene into Regular Session.**

Mayor Earle Bidez called the meeting to order at 4:30pm.

Those present included: Mayor Earle Bidez, Mayor Pro Tem Terry Armistead (zoom), Town Council members George Brodin, Lynn Feiger, Gusty Kanakis (zoom), Kate Schifani, and Tom Sullivan (zoom).

Staff present: Attorneys Michael Sawyer, Justin Poppe, and Michelle Metteer.

## **2. EXECUTIVE SESSION**

2.1 An Executive Session for the purposes of receiving legal advice on specific legal questions pursuant to C.R.S. 24-6-402(4)(b) and for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for negotiations, and/or instructing negotiators pursuant to C.R.S. 24-6-402(4)(e) –

- Water Court Case 2020CW3136 - Holland & Hart – Siltanen/Ryan
- Belden Court Case CV300054 - Poppe/Sawyer

Motion by Terry A., second by George B., to convene in Executive Session pursuant to C.R.S. 24-6-402(3)(b) for the purposes of receiving legal advice and C.R.S. 24-6-402(4)(e) for the purpose of determining positions relative to matters that may be subject to negotiations, developing strategy for negotiations, and/or instructing negotiators – Water Court case 2020CW3136 and Belden Court Case CV300054. Motion passed 7-0.

Those present included: Mayor Earle Bidez, Mayor Pro Tem Terry Armistead (zoom), Town Council members George Brodin, Lynn Feiger, Gusty Kanakis (zoom), Kate Schifani, and Tom Sullivan (zoom).

Staff present: Attorneys Michael Sawyer and Justin Plaskov, and Town Manager Michelle Metteer.

No direction was given as a result of the Executive Session.

## **3. ROLL CALL & PLEDGE OF ALLEGIANCE**

The meeting convened in Regular Session at 5:55pm.

Those present included: Mayor Earle Bidez, Mayor Pro Tem Terry Armistead (zoom), Town Council members George Brodin, Lynn Feiger, Gusty Kanakis (zoom), Kate Schifani, and Tom Sullivan (zoom).

Staff present: Attorney Michael Sawyer (Zoom), Michelle Metteer, and Town Clerk/Treasurer Jay Brunvand (zoom).

**4. APPROVAL OF CONSENT AGENDA** Consent agenda items are routine Town business, items that have received clear direction previously from the council, final land-use file documents after the public hearing has been closed, or which do not require council deliberation.

4.1 December 7, 2022 Meeting Minutes

4.2 Rocky Mountain Taco annual renewal of a Fermented Malt Beverage (City) Liquor license; 291 Main St, Chris McGinnis Owner/Manager

Motion by George B., second by Kate S., to approve the Consent Agenda of December 21, 2022 as presented. Motion passed 7-0.

**5. APPROVAL OF REGULAR AGENDA** Opportunity for amendment or deletions to the agenda.

Motion by George B., second by Kate S., to approve the Agenda of December 21, 2022 as presented. Motion passed 7-0.

**6. DECLARATION OF CONFLICTS OF INTEREST**

**7. PUBLIC COMMENT** Citizens are invited to comment on any item on the Consent Agenda, or not on the regular Agenda subject to a public hearing. Please limit your comments to five (5) minutes per person unless arrangements have been made for a presentation with the Town Clerk. Those who are speaking are requested to state their name and address for the record.

**8. SPECIAL PRESENTATIONS** Presentations are limited to 5 minutes. Invited presentations are limited to 10 minutes if prior arrangements are made with the Town Clerk.

- Scholarship investments – Fraser Horn (Scholarship Committee Invited)

Mr. Fraser Horn presented an update and set up of the Scholarship Fund outlining the funds, funds available, and status of the annuities.

Discussion ensued as to why the funds were created as they were and how they work together to protect the initial investment.

Michelle M. noted how the annual payout works to benefit the annual scholarships awarded. She also stated that the town does not control the fund, the council can appoint up to two members of their direct choosing, in addition to citizens, and Battle Mtn representatives.

**9. BUSINESS ITEMS** Items and/or Public Hearings are listed under Business may be old or new and may require review or action by the Council.

9.1 Ordinance 22 – Series 2022 (Second Reading) An Ordinance approving an application for historic designation of the Downtown Minturn Water Tank

Michelle M. presented the Town of Minturn has initiated the nomination for Historic Landmark status of the Minturn Water Tank that is located on the hillside above the 100 Block. Adjacent properties were notified and a notice placed in the Vail Daily 10 days before the hearing. Staff believes that the water tank is an iconic piece of history for Minturn with ties to our railroad roots.

The Historic Preservation Commission (“HPC”) reviewed the nomination at their November 8, 2022 meeting. They discussed the eligibility criteria and how this structure conforms to certain aspects. They unanimously recommended approval of the application for nomination to the Town Council without conditions.

From the Nomination Application:

*“The Minturn Water Tank dates back to 1941, when it held water to serve the Town and feed steam engines on the railroad tracks to the east. It was a venture between Minturn and the Denver & Rio Grande Western Railroad. The railroad kept water along the tracks, but not enough to fill the 10,000 gallons of water needed to power each coal-fired steam engine. So the tank was built and a pipe carried water from the tank across the Eagle River under a bridge (presumed to be the current day Bellm Bridge, then the Main Street Bridge) to the rail yard. The tank served the railroad and the town for several years but was decommissioned in the early 1990s due to infrastructure challenges.”*

The Water Tank is over 75 years old, and staff and the HPC believes that 1) it has association with events that have made a significant contribution to history, 2) is a site of a historic event that had an effect upon society, 3) has geographic importance, and 4) enhances the sense of identity of the Town or community.

The process as laid out in Sec 19-4-30 of the MMC for review of designation applications is as follows:

- HPC shall review the application for conformance with the established criteria for designation and with the purposes of this Article.
- HPC shall recommend approval, approval with conditions, or denial of the application to the Town Council.
- This recommendation will be forwarded to the Town Council.
- If additional information is necessary for HPC to make a decision, the hearing may be continued.

Sec. 19-4-40 - Town Council proceedings.

- Within thirty (30) days after receipt of HPC's recommendation, the Town Council shall hold a public hearing to consider HPC's recommendation. Such notice and hearing shall be conducted in conformance with the procedures set forth in Section 19-4-20.
- The Town Council shall review the application for conformance with this Article 4.
- The Town Council shall deny, or, by ordinance, shall approve or approve with conditions, the proposed application.

The eligibility criteria to evaluate the application for designation are laid out in Sec. 19-3-20 and are as follows:

- a. Properties or historic districts shall be at least seventy-five (75) years old and meet one (1) or more of the following criteria in order to be considered for designation:
  - i. Association with events that have made a significant contribution to history;
    1. Is a site of a historic event that had an effect upon society; or
    2. Exemplifies cultural, political, economic, or ethnic heritage of the Town.
  - ii. Connection with persons significant in history.
  - iii. Distinctive characteristics of a type, period, method of construction, or artisan:
    1. Exemplifies specific elements of an architectural style or period;

2. Is an example of the work of an architect or builder who is recognized for expertise nationally, state-wide, regionally, or locally;
  3. Demonstrates superior craftsmanship or high artistic value;
  4. Represents a style that is particular to the Town;
  5. Represents an innovation in construction, materials, or design; or
  6. Represents a built environment of a group of people in an era of history.
- iv. Geographic importance:
1. Enhances the sense of identity of the Town or community; or
  2. Is an established and familiar natural setting or visual feature of the Town or community.
- v. Possibility to yield important information related to prehistory or history:
1. Addresses research questions or fills recognized data gaps;
  2. Embodies construction, development, or design adaptations; or
  3. Informs on the development of engineering systems.
- b. A property or historic district may be exempted from the age standard if the Town Council finds it to be exceptionally important in other criteria.

It was noted there were no changes from First Reading.

Public Hearing Opened  
No Public Comment  
Public Hearing Closed

Motion by George B., second by Kate S., to approve Ordinance 22 – Series 2022 (Second Reading) an Ordinance amending the Town’s Historical Register by adding the Minturn Water Tank – Parcel Number: 2103-263-00-009 as presented. Motion passed 7-0.

At this time Terry A. excused herself for the remainder of the meeting.

9.6 Resolution 41 – Series 2022 A Resolution approving an amendment to the Town Manager Contract

Michael S. updated the Council that this amends the exiting contract to allow for direction given from the Annual Review of the Town Manager.

Motion by Lynn F., second by Kate S., to approve Resolution 41 – Series 2022 A Resolution approving an amendment to the Town Manager Contract as presented. Motion passed 6-0. Note: Terry A. was excused absent.

9.7 Resolution 42 – Series 2022 A Resolution approving Eagle County Sheriff’s IGA Amendment #3

Michelle M. updated the Council on the proposed changes to the exiting IGA and that this amendment includes changes as agreed to with the ECSO.



Motion by Kate S., second by GeorgeB., to approve Resolution 42 – Series 2022 A Resolution approving Eagle County Sheriff’s IGA Amendment #3 as presented. Motion passed 6-0. Note: Terry A. was excused absent.

## **10. DISCUSSION / DIRECTION ITEMS**

### 10.1 Review and Direction for the 2023-2025 Minturn Strategic Plan

Michelle M. presented this is a Review draft 2023-25 Strategic Plan and gave staff feedback. No final approvals will be made during this discussion, only feedback. The Minturn Town Council is committed to the litigation of the Annexation documents for the Battle Mountain property. With this commitment in mind, staff, via feedback from the Council retreat in the spring of 2022, has created a draft 2023-25 Strategic Plan which emphasizes budget allocations toward the enforcement of the Annexation Agreement. The strategic plan is a critical document in outlining the goals of the Council for the next two years. This document will be heavily utilized by staff for direction toward action items to achieve the approved goals.

Michelle M. gave an update on the accomplishments and completions of items from the current Strategic Plan and Council was able to discuss and comment on the update. Tom S. felt we should review fees on driveway melt systems as they are more environmentally friendly than truck hauling snow plowed to stacking areas. It was noted that the businesses are hesitant to take a lead on the improvements in the possibility of a Minturn Downtown Development Authority. If businesses do not support the effort, the Town will most likely not pursue the opportunity. Michelle M. noted that the current direction is to support the efforts of enforcement of the Battle Mtn agreements. This is proving an expensive venture and some items will take a back seat while the agreements move forward.

Discussion ensued as to the Battle Mtn funds dedicated to the Little Beach Park development. Michelle M. noted the intent would be to use the dedicated funds for grant matching. Discussion on water and sewer to the east side of the river and direction was to move forward with design and engineering phase.

Michelle M. noted Phase II of the sidewalk will be in 2024 in order to best accommodate our present litigation costs.

## **11. STAFF REPORTS**

Michelle M. updated on the status of the Water Treatment Plant.

## **12. COUNCIL COMMENTS & COMMITTEE REPORTS**

- Eagle County Regional Transit Authority update

Earle B. updated on the first RTA meeting. He noted how the Board is structured and existing and necessary committees.

Earle B. updated on the Town accomplishments over 2022 to include Bolts Lake water rights and use of that area, multiple grants totaling approximately \$3,100,000 to over \$5m. He noted the Water Treatment Plant design work, new water meters town wide that will allow online consumption update of individual accounts. The creation of the Historic Preservation, the inclusion of the county wide RTA, the culmination of the recommendations from the Shooting Range Committee, near future adoption of the Community Plan and adoption of the 100 Block guidelines. Earle B. expressed this was proof of a job well done

George B. requested the Council Comments be moved back to the front of the meeting. This will be considered. He also felt the Manager's Report should be near the front of the meeting.

**13. FUTURE AGENDA ITEMS**

**14. ADJOURN**

Motion by George B., second by Kate S., to adjourn the meeting at 7:03 pm.

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Earle Bidez, Mayor

ATTEST:

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Jay Brunvand, Town Clerk

**INFORMATIONAL ONLY ITEMS**

Town Hall Closed:

- December 26<sup>th</sup> – Christmas
- January 2<sup>nd</sup> (2023) – New Year's

Upcoming Council Meetings:

- January 4, 2023
- January 18, 2023
- February 1, 2023



To: Mayor and Council  
From: Jay Brunvand  
Date: January 4, 2023  
Agenda Item: Annual renewal of Hotel and Restaurant Liquor License

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**REQUEST:**

Staff is requesting Council to review and approve the attached annual renewal of a Hotel and Restaurant Liquor License for the 542 Main St, dba Sunrise located at 132 Main St.

**INTRODUCTION:**

This establishment has an existing H&R License and this is the annual renewal.

**ANALYSIS:**

Not Applicable

**COMMUNITY INPUT:**

Not Applicable

**BUDGET / STAFF IMPACT:**

The applicant has submitted the required fee of \$125.00.

**STRATEGIC PLAN ALIGNMENT:**

In accordance with Strategy #4 the Town will advance decisions/projects/initiatives that expand future opportunity and viability for Minturn. Both as an employer and a sales tax contributor these businesses each help further Minturn.

**RECOMMENDED ACTION OR PROPOSED MOTION:**

This item is approved on the Consent Agenda, no separate motion is required.

**ATTACHMENTS:**

- Application and supporting documentation for the license renewal.

**Submit to Local Licensing Authority**

**SUNRISE  
 PO BOX 1285  
 Minturn CO 81645**

Fees Due	
Renewal Fee	550.00
Storage Permit \$100 X _____	\$
Sidewalk Service Area \$75.00	\$
Additional Optional Premise Hotel & Restaurant \$100 X _____	\$
Related Facility - Campus Liquor Complex \$160.00 per facility	\$
<b>Amount Due/Paid</b>	<b>\$ 550.00</b>

Make check payable to: Colorado Department of Revenue. The State may convert your check to a one-time electronic banking transaction. Your bank account may be debited as early as the same day received by the State. If converted, your check will not be returned. If your check is rejected due to insufficient or uncollected funds, the Department may collect the payment amount directly from your banking account electronically.

## Retail Liquor or Fermented Malt Beverage License Renewal Application

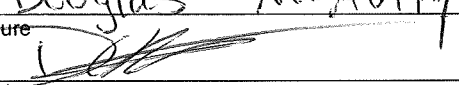
Please verify & update all information below

Return to city or county licensing authority by due date

Licensee Name 542 MAIN STREET LLC		Doing Business As Name (DBA) SUNRISE	
Liquor License # 03-09182	License Type Hotel & Restaurant (city)		
Sales Tax License Number 35294481	Expiration Date 02/13/2023	Due Date 12/30/2022	
Business Address 132 MAIN STREET Minturn CO 81645		Phone Number 9708275353	
Mailing Address PO BOX 1285 Minturn CO 81645		Email sunriseminturn@gmail.com	
Operating Manager Douglas McAulity	Date of Birth 4/14/83	Home Address 1133 main st minturn co 81645	Phone Number 9704458349
1. Do you have legal possession of the premises at the street address above? <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No Are the premises owned or rented? <input type="checkbox"/> Owned <input checked="" type="checkbox"/> Rented* *If rented, expiration date of lease <u>10/1/28</u>			
2. Are you renewing a storage permit, additional optional premises, sidewalk service area, or related facility? If yes, please see the table in upper right hand corner and include all fees due. <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			
3a. Are you renewing a takeout and/or delivery permit? (Note: must hold a qualifying license type and be authorized for takeout and/or delivery license privileges) <input checked="" type="checkbox"/> Yes <input checked="" type="checkbox"/> No			
3b. If so, which are you renewing? <input type="checkbox"/> Delivery <input type="checkbox"/> Takeout <input type="checkbox"/> Both Takeout and Delivery			
4a. Since the date of filing of the last application, has the applicant, including its manager, partners, officer, directors, stockholders, members (LLC), managing members (LLC), or any other person with a 10% or greater financial interest in the applicant, been found in final order of a tax agency to be delinquent in the payment of any state or local taxes, penalties, or interest related to a business? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			
4b. Since the date of filing of the last application, has the applicant, including its manager, partners, officer, directors, stockholders, members (LLC), managing members (LLC), or any other person with a 10% or greater financial interest in the applicant failed to pay any fees or surcharges imposed pursuant to section 44-3-503, C.R.S.? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			
5. Since the date of filing of the last application, has there been any change in financial interest (new notes, loans, owners, etc.) or organizational structure (addition or deletion of officers, directors, managing members or general partners)? If yes, explain in detail and attach a listing of all liquor businesses in which these new lenders, owners (other than licensed financial institutions), officers, directors, managing members, or general partners are materially interested. <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			
6. Since the date of filing of the last application, has the applicant or any of its agents, owners, managers, partners or lenders (other than licensed financial institutions) been convicted of a crime? If yes, attach a detailed explanation. <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No			

7. Since the date of filing of the last application, has the applicant or any of its agents, owners, managers, partners or lenders (other than licensed financial institutions) been denied an alcohol beverage license, had an alcohol beverage license suspended or revoked, or had interest in any entity that had an alcohol beverage license denied, suspended or revoked? If yes, attach a detailed explanation.  Yes  No

8. Does the applicant or any of its agents, owners, managers, partners or lenders (other than licensed financial institutions) have a direct or indirect interest in any other Colorado liquor license, including loans to or from any licensee or interest in a loan to any licensee? If yes, attach a detailed explanation.  Yes  No

<b>Affirmation &amp; Consent</b>		
I declare under penalty of perjury in the second degree that this application and all attachments are true, correct and complete to the best of my knowledge.		
Type or Print Name of Applicant/Authorized Agent of Business	Title	
Douglas McAuliffe	owner	
Signature	Date	
	10/27/22	
<b>Report &amp; Approval of City or County Licensing Authority</b>		
The foregoing application has been examined and the premises, business conducted and character of the applicant are satisfactory, and we do hereby report that such license, if granted, will comply with the provisions of Title 44, Articles 4 and 3, C.R.S., and Liquor Rules.		
<b>Therefore this application is approved.</b>		
Local Licensing Authority For	Date	
Town of Arroyo	1/9/2023	
Signature	Title	Attest



To: Mayor and Council  
From: Jay Brunvand  
Date: January 4, 2023  
Agenda Item: Resolution 01 – Series 2023

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**REQUEST:**

Council is asked to approve Resolution 01 – Series 2023.

**INTRODUCTION:**

This is an annual Resolution setting the posting sites for the community meeting public notices.

**ANALYSIS:**

This Resolution sets the public posting sites for official town meetings as required and as set forth in the Minturn Charter Section 4.8. Our Charter requires a minimum 24 hours' notice unless an emergency. This stipulation follows all state open meeting's laws and regulations.

**COMMUNITY INPUT:**

N/A

**BUDGET / STAFF IMPACT:**

N/A

**STRATEGIC PLAN ALIGNMENT:**

In accordance with Strategy #1 to practice fair, transparent, and communicative local government.

**RECOMMENDED ACTION OR PROPOSED MOTION:**

This Resolution is included in the Consent Agenda and does not require a separate motion.

**ATTACHMENTS:**

- Resolution 01 – Series 2023

**TOWN OF MINTURN**

**RESOLUTION NO. 01 – SERIES 2023**

**A RESOLUTION DESIGNATING TWO PUBLIC PLACES WITHIN THE TOWN OF MINTURN AND THE TOWN WEBSITE FOR THE POSTING OF PUBLIC NOTICES AND NOTICE FOR PUBLIC MEETINGS OF THE MINTURN TOWN COUNCIL, PLANNING AND ZONING COMMISSION, AND OTHER BOARDS, COMMISSIONS, AND AUTHORITIES OF THE TOWN OF MINTURN.**

**WHEREAS**, the Town of Minturn is a Home Rule Municipality under the Colorado Constitution; and

**WHEREAS**, the Town of Minturn Charter – Section 4.8 requires notices of public meetings be posted at two (2) public places and the Town website at least 24-hours prior thereto, and

**WHEREAS**, the Town of Minturn now wishes to designate two public places within its boundaries for posting of such full and timely notice to the public for public notices and notices for public meetings of the Minturn Town Council, Planning Commission, and other Boards, Commissions, and authorities of the Town of Minturn;

**NOW, THEREFORE, BE IT RESOLVED BY THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO THAT:**

- 1) The Town Council hereby designates the following as the public places for the posting of full and timely notice to the public as provided for in the Minturn Town Charter:
  - The bulletin board on the outside of the Town Center at 302 Pine St.
  - The bulletin board outside of the Town Center at 301 Boulder St.
  - The official Town website
  
- 2) This Resolution shall take effect immediately upon its passage.

**INTRODUCED, READ, APPROVED AND ADOPTED this 4<sup>th</sup> day of January 2023.**

\_\_\_\_\_  
**EARLE BIDEZ, MAYOR**

**ATTESTS:**

\_\_\_\_\_  
**JAY BRUNVAND, TOWN CLERK**



To: Minturn Town Council  
From: Michelle Metteer  
Date: January 4, 2023  
RE: Gasoline spill and mitigation efforts

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**Gasoline Spill and Ongoing Mitigation Efforts**

Cleanup efforts continue from the gasoline spill that occurred at the local Shop & Hop gas station over ten years ago. A new firm has been contracted by the State of Colorado to lead the cleanup effort. Work will be conducted in the spring of 2023. The goal is to mitigate, as much as possible, the benzene plume in the groundwater table and to keep the plume from reaching the Eagle River. Maps and benzene reads are provided with this update. The Town encourages property owners, residents and businesses to take part in this discussion and cleanup effort.

For additional information on the spill please reference the Shop & Hop Groundwater Monitoring Report at: <https://www.minturn.org/home/news/groundwater-monitoring-remediation-discussion-scheduled-january-4th>

This is a large document that will not be included in the packet. Paper copies can be available at the Minturn Town Hall upon request.

**Attachments:**

Email update  
Gasoline Spill Phase 1 & 2 Mitigation Plan



## Michelle Metteer

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**From:** Will Nabours <will.nabours@solitechnical.com>  
**Sent:** Monday, August 8, 2022 8:44 AM  
**To:** Michelle Metteer  
**Subject:** Groundwater Monitoring Report  
**Attachments:** Shop&Hop#12 Groundwater Monitoring Report - Q2 2022.pdf; Phase I.pdf; Phase II.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Hi Michelle,

We just wrapped up the most recent groundwater monitoring report following our Q2 2022 carbon injection pilot testing and groundwater sampling. The report is attached for your reference. The *Recent Activities* section of the narrative presents the work we've done since taking over the project from the previous consultant.

In summary, the carbon injection pilot test was successful in that we were able to inject our carbon fluid into the ground. We are still waiting to see the effects of the work on petroleum hydrocarbon concentrations in groundwater - the carbon takes several months to bind to soils and adsorb the hydrocarbons. As far as dissolved benzene concentrations go, not much has changed since the previous sampling event (Q3 2021). The Q2 2022 sampling event was performed on June 22, 2022. Dissolved petroleum hydrocarbon concentrations in samples collected from 13 site monitoring wells are presented in Figure 6 of the attached report. As you can see, most remaining contamination lies beneath Main Street.

As far as future work goes, the State will likely want us to perform two "full-scale" carbon injection events, pending Town of Minturn and CDOT approval. The first (Phase I) would include injecting at approximately 47 locations within the Norman Avenue cul-de-sac northeast of Main Street as well as partially down the shoulder of Main Street to the southeast of the intersection (see attached figure). The purpose would be to create a "barrier" between the benzene plume and the Eagle River. The second event would entail injections at approximately 41 locations along both shoulders of Main Street (Phase II) and would be contingent upon the results of the recently performed pilot test. The purpose of this would be to knock down benzene concentrations within the plume. All the injection points would be 10 feet apart and would look the same as those done on the 401 Main Street property (with a 3-to-6-inch core through the asphalt road surfacing). Upon completion, we would backfill the injection holes appropriately and restore the surface the same way as what was done during the pilot test.

Anyway, I'd like to hear your thoughts on the future work as I know we've talked a few times about trying to minimize further disturbance to Main Street.

I appreciate your time,

Will



**Will Nabours, P.E.**

Senior Engineer | Partner

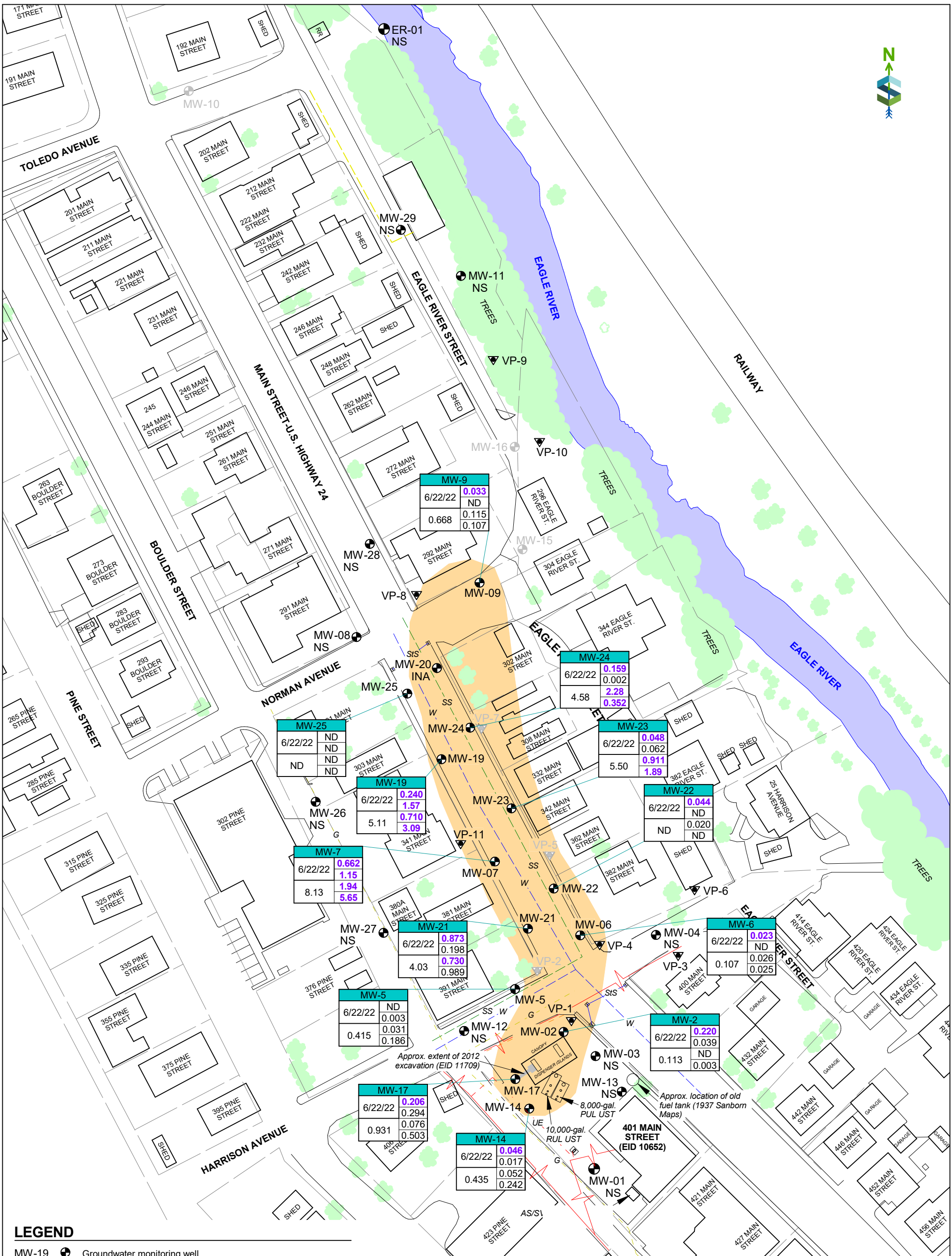
O: 720.507.3970 ext. 2

M: 303.946.7891

P.O. Box 7548, Denver, CO 80207

[www.solitechnical.com](http://www.solitechnical.com)

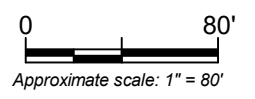
The information in this email is confidential and may be legally privileged. Access to this email by anyone other than the intended addressee is unauthorized. If you are not the intended recipient of this message, any review, disclosure, copying, distribution, retention, or any action taken or omitted to be taken in reliance on it is prohibited and may be unlawful. If you are not the intended recipient, please reply to or forward a copy of this message to the sender and delete the message, any attachments, and any copies thereof from your system.



**LEGEND**

- MW-19 Groundwater monitoring well
  - VP-1 Soil vapor monitoring point
  - MW-10 Abandoned/destroyed monitoring well
  - VP-2 Abandoned/destroyed soil vapor monitoring point
- | Well ID | 6/22/22 | 0.240 | Benzene        |
|---------|---------|-------|----------------|
|         |         | 1.57  | Toluene        |
|         |         | 0.710 | Ethylbenzene   |
|         |         | 3.09  | Xylenes, total |
- Sample location and hydrocarbon concentrations in milligrams per liter (mg/L)  
 ND = below laboratory detection limits  
 INA = well inaccessible  
 NS = well not sampled  
**0.240 Bold values exceed Tier I RBSLs**
- Estimated extent of dissolved benzene impacts > 0.005 mg/L
- UTILITIES:**
- W Water
  - G Natural gas
  - SS Sanitary sewer
  - StS Storm sewer
  - Overhead electric
  - C Natural gas
  - UE Underground electric

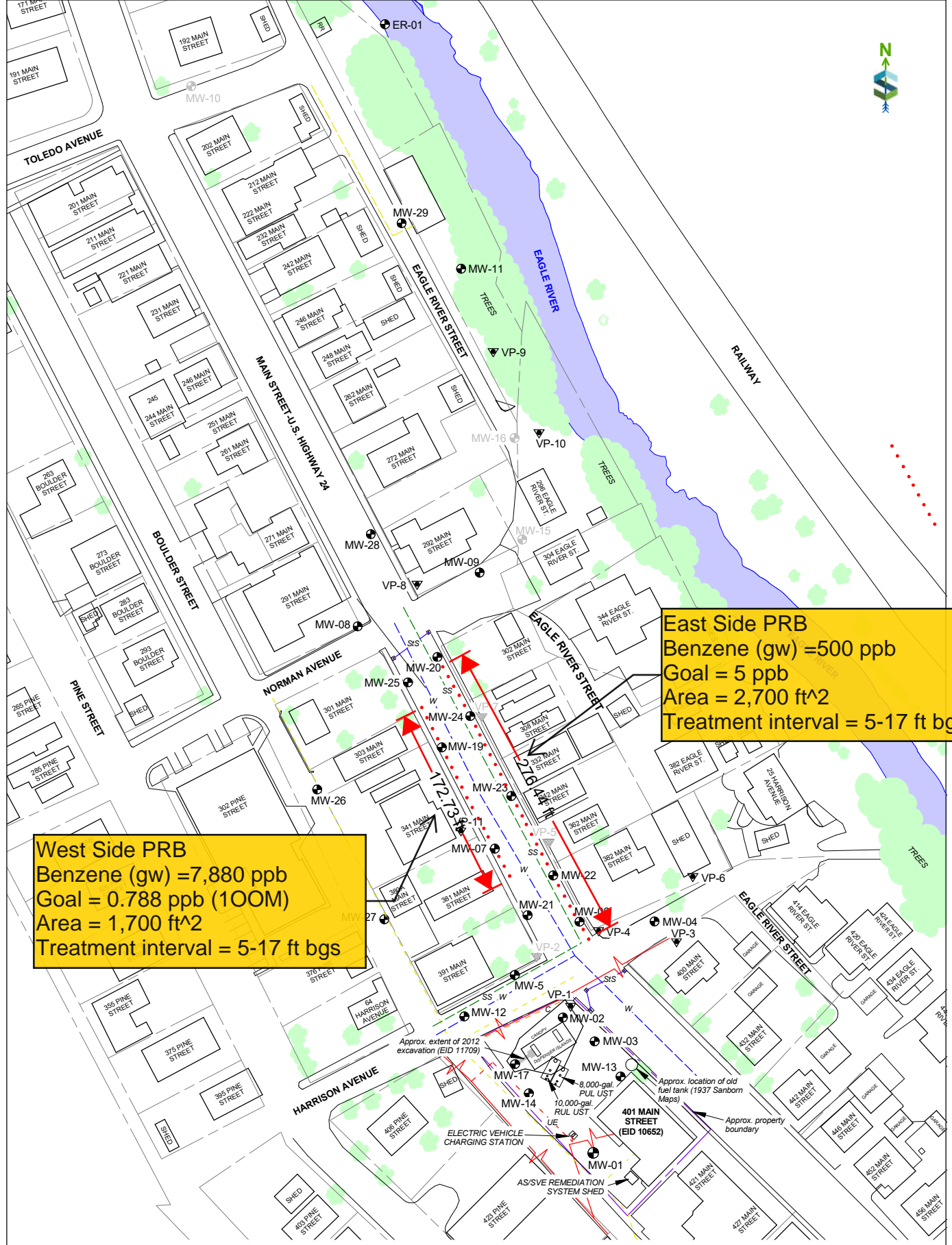
Samples collected July 22, 2022



**FIGURE 6 - Groundwater Sample Results**

Shop & Hop #12  
 401 Main Street  
 Minturn, Colorado 81645  
 Soli Project #1028  
 1028 Figure 6.dc (7/19/22-WN)





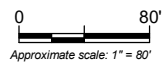
**East Side PRB**  
 Benzene (gw) = 500 ppb  
 Goal = 5 ppb  
 Area = 2,700 ft<sup>2</sup>  
 Treatment interval = 5-17 ft bgs

**West Side PRB**  
 Benzene (gw) = 7,880 ppb  
 Goal = 0.788 ppb (100M)  
 Area = 1,700 ft<sup>2</sup>  
 Treatment interval = 5-17 ft bgs

**LEGEND**

- MW-20 Groundwater monitoring well
- VP-1 Soil vapor monitoring point
- MW-10 Abandoned/destroyed monitoring well
- VP-2 Abandoned/destroyed soil vapor monitoring point
- Carbon slurry injection point (41 total on 10-foot spacings)

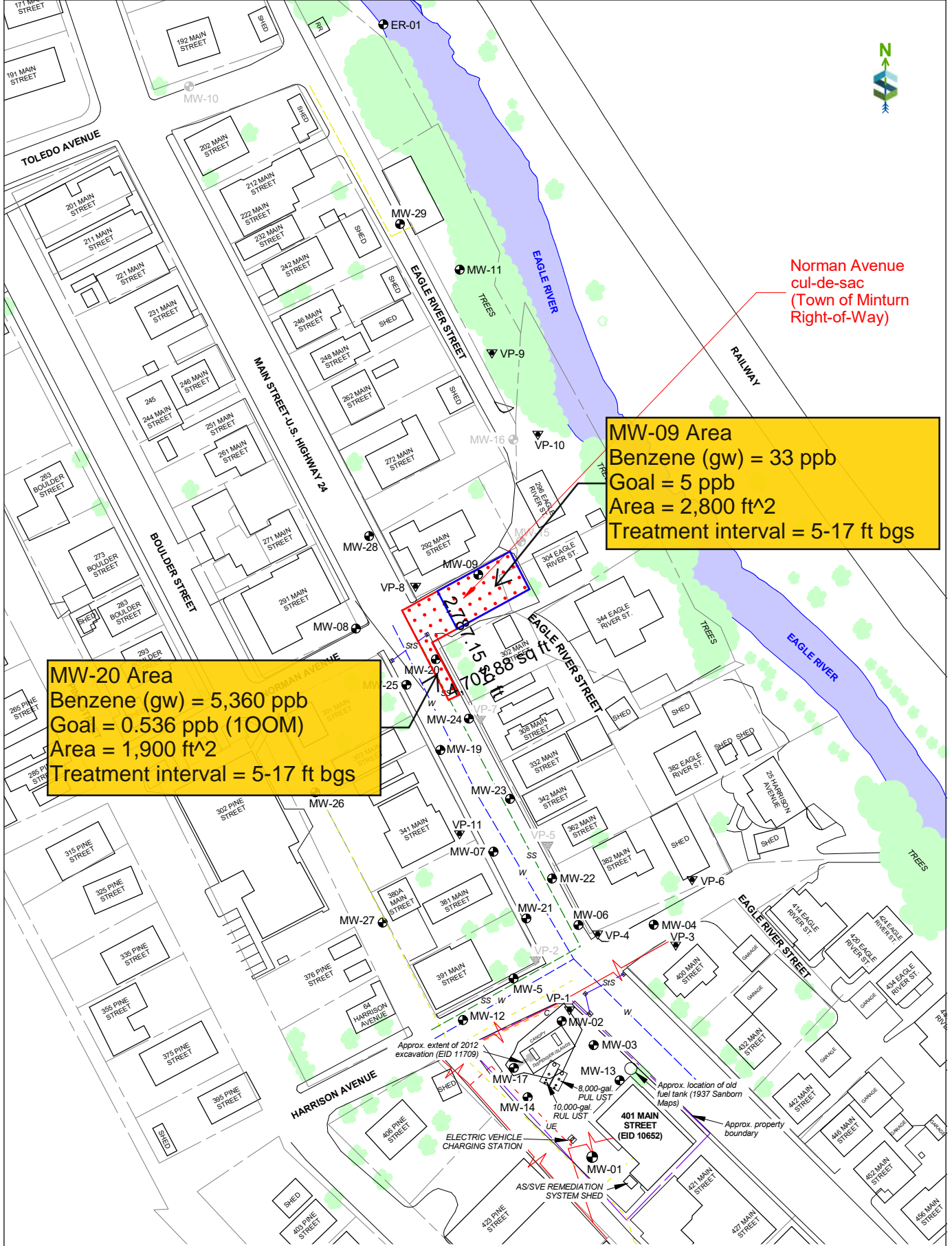
- UTILITIES:**
- Water
  - Natural gas
  - Sanitary sewer
  - Storm sewer
  - Overhead electric
  - Natural gas
  - Underground electric



**RFQ Figure 1 - Phase II Injections**

Shop & Hop #12  
 401 Main Street  
 Minturn, Colorado 81645  
 Soli Project #1028





**MW-09 Area**  
 Benzene (gw) = 33 ppb  
 Goal = 5 ppb  
 Area = 2,800 ft<sup>2</sup>  
 Treatment interval = 5-17 ft bgs

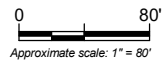
**MW-20 Area**  
 Benzene (gw) = 5,360 ppb  
 Goal = 0.536 ppb (100M)  
 Area = 1,900 ft<sup>2</sup>  
 Treatment interval = 5-17 ft bgs

**LEGEND**

- MW-20 Groundwater monitoring well
- VP-1 Soil vapor monitoring point
- MW-10 Abandoned/destroyed monitoring well
- VP-2 Abandoned/destroyed soil vapor monitoring point
- Carbon slurry injection point (47 total on 10-foot spacings)

**UTILITIES:**

- Water
- Natural gas
- Sanitary sewer
- Storm sewer
- Overhead electric
- Natural gas
- Underground electric



**RFQ Figure 1 - Phase I Injections**

Shop & Hop #12  
 401 Main Street  
 Minturn, Colorado 81645  
 Soli Project #1028



To: Minturn Town Council  
From: Michelle Metteer  
Date: January 4, 2023  
Agenda Item: Landmark Designation for 146 N Main Street (Minturn Saloon)



---

**REQUEST:**

Approve the landmark designation of 146 N Main Street (Commonly known as the Minturn Saloon) based on the criteria outlined in the Minturn Municipal Code Sec. 19-3-20, citing as findings of fact for this recommendation, public testimony and the December 20, 2022, Minturn Historic Preservation Commission unanimous recommendation for approval along with the staff report.

**Application:** #2022L – 002  
**Address:** 146 N Main Street  
**Zoning:** Old Town Character Area – Commercial Zone  
**Owner:** Los Amigos – Property Company LLC (locally Connie and Tony Mazza)  
**Applicant(s):** Krista Driscoll  
Lynn Teach

**CASE SUMMARY:**

Two independent nominations have been submitted for Historic Landmark status of the Saloon Building located at 146 N Main Street. Staff reviewed and deemed both applications complete and to meet the Minturn landmark designation criteria. As such, staff scheduled a public hearing for the Historic Preservation Commission meeting on December 20, 2022. Adjacent properties and the owner of 146 N Main Street were notified via certified mail, and a notice placed in the Vail Daily 10 days before the public hearing.

**DESIGNATION CRITERIA & EVALUATION; CHAPTER 19 MINTURN MUNICIPAL CODE:**

To meet landmark designation criteria, in accordance with Chapter 19, Minturn municipal Code, the application(s) must be complete, and the structure must meet the following criteria:

**MMC Sec. 19-3-20. Eligibility Criteria.**

- a. Properties or historic districts shall be at least seventy-five (75) years old and meet one (1) or more of the following criteria in order to be considered for designation:
  - 1) Association with events that have made a significant contribution to history
  - 2) Connection with persons significant in history
  - 3) Distinctive characteristics of a type, period, method of construction, or artisan
  - 4) Geographic importance
  - 5) Possibility to yield important information related to prehistory or history

**CRITERIA EVALUATION**

Town staff found that the application(s) for designation demonstrate that the structure meets the following criteria.

**A. Association with events that have made a significant contribution to history** (exemplifies cultural, political, economic, or ethnic heritage of the Town)

“By 1900, the town population had grown to 428 residents; still mostly miners, railroad workers, and their families. To accommodate for so many hard-working citizens coming home after a long day on the railroad or in the mines, W.H. Luby opened The Saloon, a local watering hole that still stands 115 years later and serves as the unofficial final destination of the Minturn Mile ski run...” (Baker, 2019)

From this original period forward the Saloon has been an unofficial landmark in Minturn. With several different iterations of ownership and name changes, the Saloon remains as it originally was established, a watering hole for residents and visitors alike.

**B. Distinctive characteristics of a type, period, method of construction or artisan**

The Minturn Saloon reflects the “false front” style of architecture that was reminiscent of buildings around the time of the California gold rush in the 1860’s and shortly thereafter. The preface of this architecture was to make simple sheds and gable roof buildings appear larger and more important. This also allowed for easier visualization of signs on storefronts as the false front would block the view of everything behind it.

False fronts can be seen on many of the buildings from Minturn’s original Main Street and is primarily utilized for commercial purposes but rarely seen on residential structures. The Saloon’s flat roof, decorative brickwork and flush entrance are all architectural examples from the early 20<sup>th</sup> century architectural era. (Walker, 2015)



Photo Credit: Vail Daily, December 2021

**C. Geographic Importance** (enhances the sense of identity of the Town or community)

“The Minturn Saloon building is one of the oldest structures in the town. The railroad and mining town of Minturn dates back to the 1880’s though this building was constructed in 1901. There was a fire in the downtown commercial section of Minturn in 1899 and this building is the result of the recovery effort.” (Minturn Saloon, 2022)

“The Minturn Saloon is at the celebratory endpoint of the Minturn Mile, where skiers and snowboarders can access the town from Vail Mountain via the out-of-bounds run. The Denver Post once called the Minturn Saloon one of the “most historic and iconic restaurants in Colorado,” and Esquire magazine has listed the Minturn Saloon as one of the best bars in

America every year since 2007.” (Vail Daily, 2021)

“By 1900, the town population had grown to 428 residents; still mostly miners, railroad workers, and their families. To accommodate for so many hard-working citizens coming home after a long day on the railroad or in the mines, W.H. Luby opened The Saloon, a local watering hole that still stands 115 years later and serves as the unofficial final destination of the Minturn Mile ski run...” (Baker, 2019)

The Minturn Saloon represents the northernmost point of Minturn’s commercial core. As a location immediately adjacent to the railyard and immediately north of the bridge from the commercial core, the Saloon holds an important geographic location for delineating the industrial railyard from commercial enterprises within town.

**PUBLIC REVIEW PROCESS:**

Town staff has met all posting and notification requirements delineated in Chapter 19, Sec 19-4-10 identifying legal notice procedures follow established guidelines as set forth in MMC. Sec. 16-21-610.

**PUBLIC COMMENTS:**

As of December 28, 2022, no public comments other than the applicant and owner representative, have been received regarding this application.

**RECOMMENDED ACTION OR PROPOSED MOTION:**

Recommend approval to designate the Saloon located at 146 N Main Street, as an historic landmark.

**ATTACHMENTS:**

- Ordinance 01- Series 2023
- Historic Landmark Nomination Applications for the Saloon located at 146 N Main Street
  - Krista Driscoll
  - Lynn Teach

**REFERENCES**

(2022). *Homepage*. Minturn Saloon. Available at: <http://minturnsaloon.com/>

Baker, Haley. (December 30, 2019). *Curious Nature, More than the Minturn Mile: Minturn’s History*. Walking Mountain’s Science Center. Available at: <https://blog.walkingmountains.org/curious-nature/2016/11/minturn-colorado-history>

Vail Daily. (December 18, 2021). *Minturn Saloon sold to Vail-area locals*. Available at: <https://www.vaildaily.com/news/minturn-saloon-sold-to-vail-valley-locals/>

Walker, Lester. (March 10, 2015). *American Homes: The Landmark Illustrated Encyclopedia of Domestic Architecture*. Black Dog & Leventhal.

**TOWN OF MINTURN, COLORADO  
ORDINANCE NO. 01 – SERIES 2023**

**AN ORDINANCE OF THE TOWN OF MINTURN,  
COLORADO AMENDING THE TOWN’S HISTORIC  
REGISTER ADDING THE SALOON BUILDING, PARCEL  
NUMBER: 2103-263-08-002**

**WHEREAS**, the Town of Minturn (“Town”) is a Colorado home rule municipality organized pursuant to Article XX of the Colorado Constitution and with the authority of the Town of Minturn Home Rule Charter for which the Minturn Town Council (“Town Council”) is authorized to act; and

**WHEREAS**, the Town of Minturn has established the Town of Minturn Register of Historic Places (the “Town’s Historic Register”) in Section 19-3-10 of the Minturn Municipal Code (“Code”); and

**WHEREAS**, pursuant to Code Sec. 19-4-30 the Historic Preservation Commission (“HPC”) has transmitted to the Town Council a recommendation to approve the designation of a structure for inclusion on the Town’s Historic Register; and

**WHEREAS**, based upon evidence received by the HPC at a hearing on December 20, 2022, the staff report and evidence received at the hearings before Town Council on January 4, 2023 and January 18, 2023 the Minturn Saloon structure, located at Parcel Number 2103-263-08-002 meets the criteria for designation as a structure for preservation as set out in Code Sec. 19-3-20 by:

- (a) Properties or historic districts shall be at least seventy-five (75) years old and meet one (1) or more of the following criteria in order to be considered for designation:*
  - (1) Association with events that have made a significant contribution to history;*
    - a. Is a site of a historic event that had an effect upon society; or ...*
  - (4) Geographic importance;*
    - a. Enhances the sense of identity of the Town or community;*

**WHEREAS**, the Nomination Application indicates that: “W.H. Luby first opened The Saloon in 1901 to accommodate Minturn’s population of 428 souls, as well as miners, railroad workers, a few gamblers and their families from the nearby towns of Red Cliff and Gilman”; and

**WHEREAS**, the Minturn Saloon is one of Minturn’s most iconic historic structures that is emblematic of the architecture of Minturn’s frontier heritage; and

**WHEREAS**, The Minturn Saloon is geographically important and has served as a central gathering place for the local community and visitors from far away destinations for over a century.



**WHEREAS**, the Minturn Saloon is part of the sense of community that makes up the Town of Minturn and provides an important bridge to the Town's past.

NOW THEREFORE, BE IT ORDAINED BY THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO:

SECTION 1. The foregoing recitals are incorporated herein as if set forth in full.

SECTION 2. That based upon the analysis reference above, and the evidence received at the public hearings, certain property herein called the Minturn Saloon at Parcel Number 2103-263-08-002, and legally described as follows, together with all improvements situated and located thereon, be and the same is hereby designated as a structure for preservation and inclusion on the Town's Historic Register.

SECTION 3. This ordinance shall be recorded among the records of the Clerk and Recorder of Eagle County.

**INTRODUCED, READ BY TITLE, APPROVED ON THE FIRST READING AND ORDERED PUBLISHED BY TITLE ONLY AND POSTED IN FULL ON THE OFFICIAL TOWN WEBSITE THE \_\_ DAY OF JANUARY 2023. A PUBLIC HEARING ON THIS ORDINANCE SHALL BE HELD AT THE REGULAR MEETING OF THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO ON THE 18 DAY OF JANUARY 2023 AT 5:30 p.m. AT THE MINTURN TOWN HALL 302 PINE STREET, MINTURN COLORADO 81645.**

TOWN OF MINTURN, COLORADO

\_\_\_\_\_  
Earle Bidez, Mayor

ATTEST:

By: \_\_\_\_\_  
Jay Brunvand, Town Clerk

**THE TOWN OF MINTURN, COLORADO, ORDAINS THIS ORDINANCE ENACTED ON SECOND READING AND ORDERED PUBLISHED BY TITLE ONLY AND POSTED IN FULL ON THE OFFICIAL TOWN WEBSITE THIS 18th DAY OF JANUARY, 2023.**

TOWN OF MINTURN, COLORADO

\_\_\_\_\_  
Earle Bidez, Mayor

ATTEST:

By: \_\_\_\_\_  
Jay Brunvand, Town Clerk



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 1 - Purpose:

The purpose of Historic Preservation is to enhance the Town of Minturn's local resources and to promote the public health, safety, and welfare through:

1. The protection and preservation of the Town's architecture, culture, and heritage as embodied in historic properties and historic districts, by appropriate regulations and incentives;
2. The stabilization of historic neighborhoods;
3. The establishment of the Town's Historic Register listing historic properties and historic districts;
4. The cultivation of civic pride in the art, architecture, and accomplishments of the past;
5. The encouragement of continued private ownership and utilization of such historic properties or historic districts now so owned and used;
6. The promotion of thoughtful community planning and design; and
7. The provision of educational opportunities to increase public appreciation of the Town's unique heritage.

## Section 2 - Intent:

The intention of Historic Preservation is to create a reasonable balance between private property rights and the public interest in preserving the Town's unique historic character through the nomination of buildings, structures, sites, objects, and historic districts for preservation.

## Section 3 - Eligibility Criteria:

Indicate how the property is significant to the Town of Minturn and its past by checking one or more of the following: Properties (*buildings, structures, sites, or objects*) or historic districts shall be at least 75 years old and meet 1 or more of the following criteria in order to be considered for designation:

1. Association with events that have made a significant contribution to history;
  - a. Is a site of a historic event that had an effect upon society; or
  - b. Exemplifies cultural, political, economic, or ethnic heritage of the Town.
2. Connection with persons significant in history.
3. Distinctive characteristics of a type, period, method of construction, or artisan:
  - a. Exemplifies specific elements of an architectural style or period;
  - b. Is an example of the work of an architect or builder who is recognized for expertise nationally, state-wide, regionally, or locally;
  - c. Demonstrates superior craftsmanship or high artistic value;
  - d. Represents a style that is particular to the Town;
  - e. Represents an innovation in construction, materials, or design; or
  - f. Represents a built environment of a group of people in an era of history.
4. Geographic importance:
  - a. Enhances the sense of identity of the Town or community; or
  - b. Is an established and familiar natural setting or visual feature of the Town or community.
5. Possibility to yield important information related to prehistory or history:
  - a. Addresses research questions or fills recognized data gaps;
  - b. Embodies construction, development, or design adaptations; or
  - c. Informs on the development of engineering systems.

A property or historic district may be exempted from the age standard if the Town Council finds it to be exceptionally important in other criteria.

- Sec. 19-3-20 MMC



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 4 - Basic Property Information:

Historic Name of Property (if applicable) and any other names:

--

### Address of Property

Street Address:
-----------------

Legal Description of Property:
--------------------------------

### Maps:

1. Include a location map on 8.5x11 inch page showing the property in relation to major streets
2. Include a site plan on 8.5x11 inch page showing the footprint of the building if applicable

### Property Owner Information:

Name:
-------

Mailing Address:
------------------

Email:	Phone:
--------	--------

### Applicant Information (if different):

Name:
-------

Mailing Address:
------------------

Email:	Phone:
--------	--------

### Is property listed on the National Register of Historic Places or the State Register of Historic Properties?

Yes      No

If yes, please provide date and registration number. Date: \_\_\_\_\_ Registration #: \_\_\_\_\_

### For Official Use:

Date Received: _____	Planner: _____
----------------------	----------------

Historic Preservation Commission -	Approval	Denial
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Comments:
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# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 5 - Category and Resources:

### Category of Nomination (check one):

- Building (any shelter or enclosure having a roof supported by columns or walls and intended for the shelter, housing or enclosure of any individual, animal, process, equipment, goods or materials of any kind.)
- Site (the location of a significant event; a prehistoric or historic occupation or activity; or a building, structure, or object, whether standing or vanished, where the location itself maintains historic or archeological value regardless of the value of any existing building, structure, or object.)
- Object (a material item of functional, aesthetic, cultural, historical, or scientific value that may be, by nature or design, movable yet related to a specific setting or environment.)
- District (a geographically definable area including a concentration, linkage, or continuity of properties within a specified period of significance and may include within its geographic boundaries one (1) or more contributing properties, which has been designated by the Town Council)
- Structure (anything constructed or erected, the use of which requires location on or in the ground, for purposes other than shelter of humans, animals, or chattels.)

### Narrative Statement of Significance:

Please describe how this property contributes to the history of Minturn. Why should the property be landmarked?



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 6 - Description:

### Architectural Description:

If property is a building, please describe the predominate architectural style such as Victorian or Mid-Century Modern.

### Narrative Description:

Please describe the current appearance of the property and any alterations that have occurred since its original construction.

### List of References:

Please cite the books, articles, personal contacts, and other sources used in preparing this application.

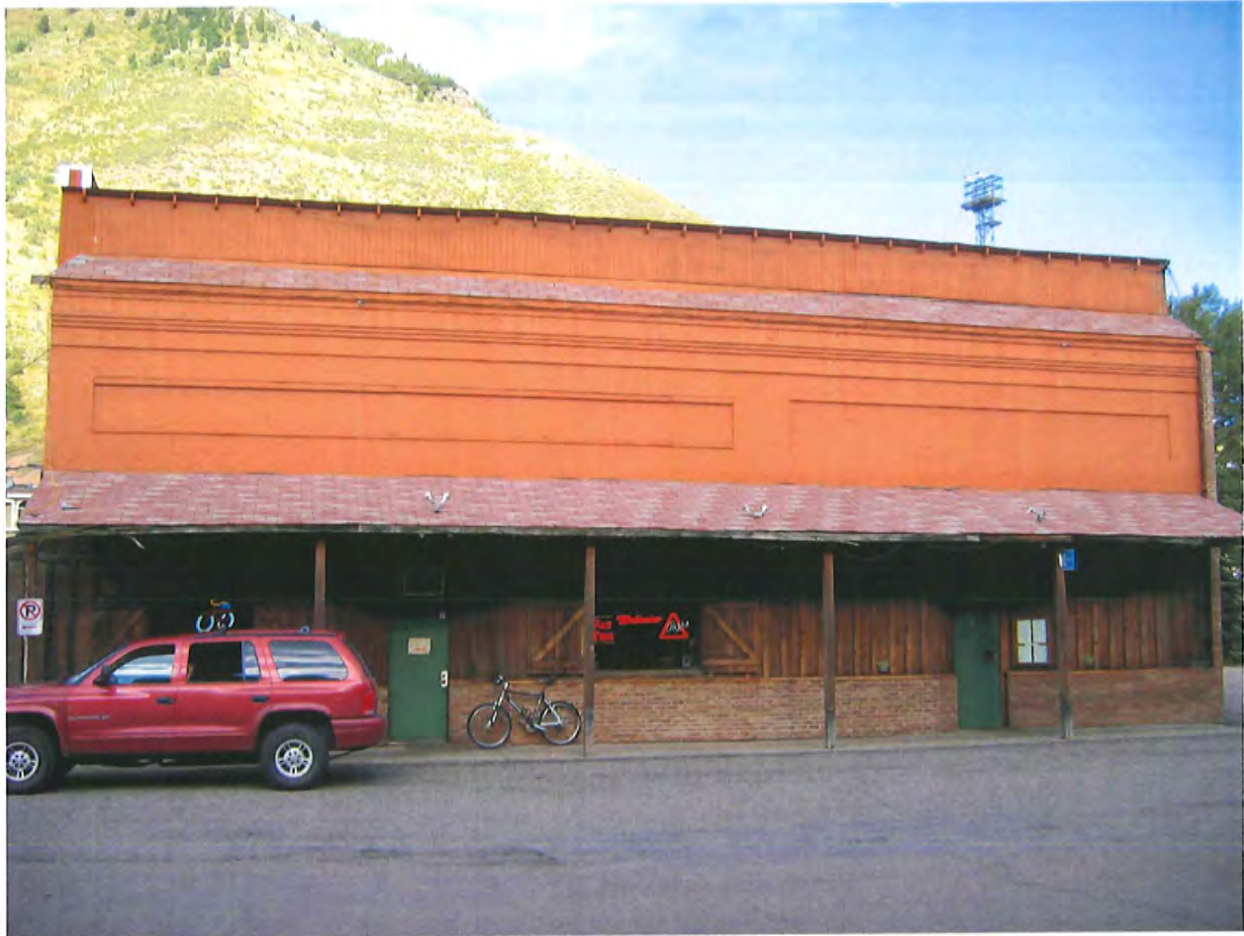
### Photographs:

Please attach current photographs of the property and, if available, copies of historic photographs. For building, structures, or objects, current photographs should show the property from all directions.

### Signature:

Krista Driscoll

If you are not the owner of the property that is being nominated, have you discussed this application with the owner(s)? This is encouraged, but not required. Yes                      No



**Address:** 146 Main Street  
**Name:** Minturn Saloon  
**Built:** 1901  
**Style:** Commercial  
**Characteristics:** Flat roof; flush entrance; decorative brickwork

# Property Record Card

Eagle

**LOS AMIGOS - PROPERTY COMPANY LLC**

245 PARK AVE FL. 44  
NEW YORK, NY 10167-4400

**Account: R003440**

Tax Area: 024 - MINTURN (TOWN) - 024

Acres: 0.208

**Parcel: 2103-263-08-002**

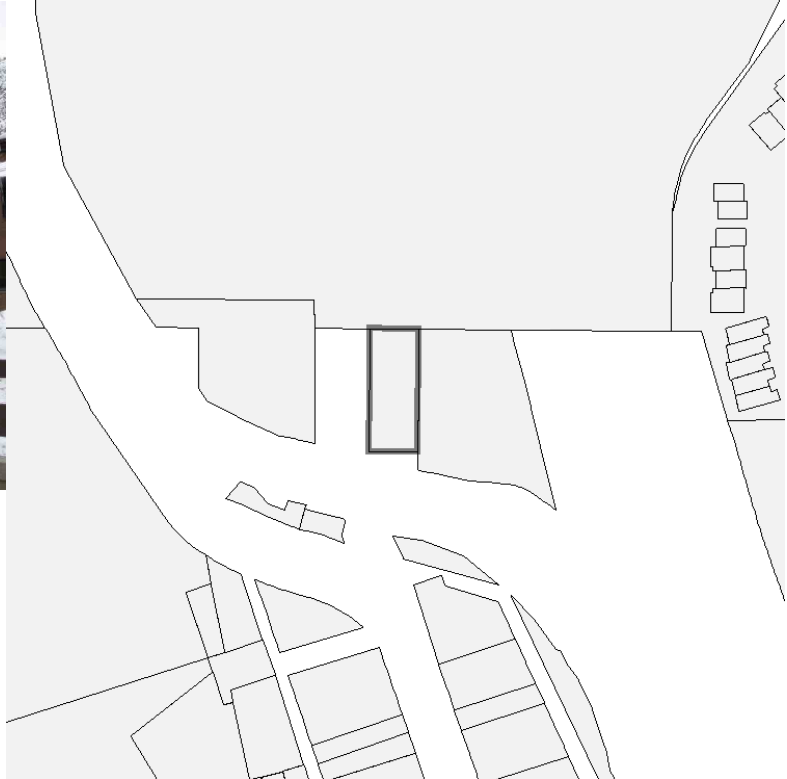
Situs Address:  
146 N MAIN ST  
MINTURN AREA, 0

## Value Summary

Value By:	Market	Override
Land (1)	\$231,230	N/A
Commercial (1)	\$167,720	N/A
<b>Total</b>	<b>\$398,950</b>	<b>\$398,950</b>

## Legal Description

Subdivision: BOOCO Block: B Lot: 5 AND:- Lot: 6, TOGETHER WITH 20' X 50' OF LAND ADJOINING SOUTH END OF LOTS 5 & 6



## Public Remarks

Entry Date	Model	Remark
12/19/2006	Commercial Occurrence 1	SALOON

## Sale Data

Doc. #	Sale Date	Deed Type	Validity	Verified	Sale Price	Ratio	Adj. Price	Ratio	Time Adj. Price	Ratio
202128007	12/17/2021	SWD	UV	Y	\$2,100,000	19.00	\$2,100,000	19.00	\$2,100,000	19.00
920775	06/15/2005	WD	QV	Y	\$550,000	72.54	\$550,000	72.54	\$550,000	72.54

## Land Occurrence 1

Abstract Code	2130 - SPEC.PURPOSE-LAND	Percentage	100
Use Code	2000 - COMMERCIAL	Neighborhood	20319 - MINTURN COMM
Land Code	5066 - MINTURN COMMERCIAL	Super Neighborhood	2300 - MINTURN / REDCLIFF COMMERCIAL
Size	0.208		

SubArea	ACTUAL	EFFECTIVE	HEATED	FOOTPRINT
---------	--------	-----------	--------	-----------



# Property Record Card

Eagle

## Land Occurrence 1

Land S		9068			
Total		9,068.00			
	<b>Value</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>
	\$231,230	25.50			

## Commercial Occurrence 1

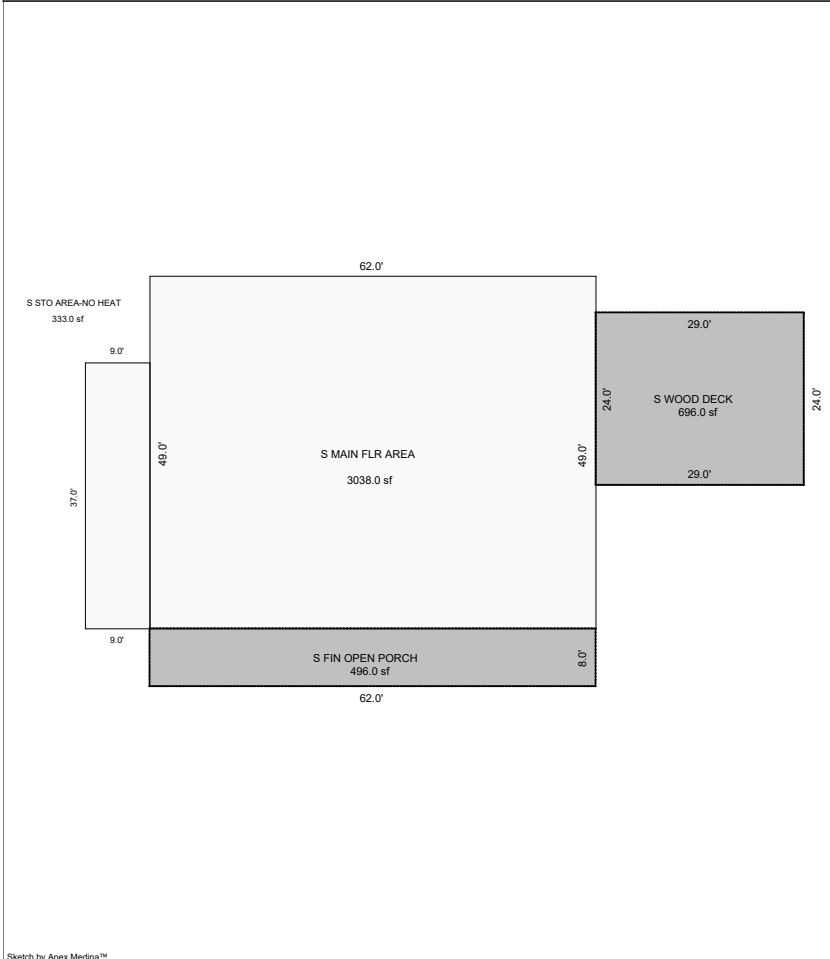
Abstract Code	2230 - SPEC.PURPOSE-IMPROVEMENTS	Percentage	100.0
Use Code	2000 - COMMERCIAL	Neighborhood	20319 - MINTURN COMM
Building Type	223000 - SPEC PURPOS	Arch Style	45 - RESTAURANT
Exterior Wall	14 - WOOD SIDNG 36 - BRICK/BRIC	Percentage	10.0 90.0
Roof Structure	2 - SHED	Interior Wall	5 - DRYWALL
Percentage	100.0	Floor Cover	5 - ASPH TILE
Percentage	100.0	Heating Fuel	3 - GAS
Heating Type	3 - FORCE AIR	Air Conditioning	4 - ROOF TOP
Actual Year Built	1901	Bedrooms	0
Bathrooms	3	Construction Quality	3 - FAIR
Effective Year Built	1970	Fixtures	12
Rooms	3	Units	1
Bathrooms	3	Bedrooms	0
Actual Year Built	1901	Effective Year Built	1970
Economic Obsolescence	77	Special Code	9 - COVID ADJ
Units	1	Stories	1 - STORIES 1.0
Use Code	2000 - COMMERCIAL		

SubArea	ACTUAL	EFFECTIVE	HEATED	FOOTPRINT
BAS	3038	3038	3038	3038
FM1				
FOP	496	149		496
UTL	330	149		330
WDD	696	35		696
Total	4,560.00	3,371.00	3,038.00	4,560.00
	<b>Value</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>
	\$167,720	36.78	49.75	55.21
			55.21	36.78

# Property Record Card

Eagle

## Commercial Occurrence 1



Sketch by Apex Medina™

### Abstract Summary

Code	Classification	Actual Value	Taxable Value	Actual Override	Taxable Override
2130	SPEC.PURPOSE-LAND	\$231,230	\$67,060	NA	NA
2230	SPEC.PURPOSE-IMPROVEMENTS	\$167,720	\$48,640	NA	NA
<b>Total</b>		<b>\$398,950</b>	<b>\$115,700</b>	<b>NA</b>	<b>NA</b>



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 1 - Purpose:

The purpose of Historic Preservation is to enhance the Town of Minturn's local resources and to promote the public health, safety, and welfare through:

1. The protection and preservation of the Town's architecture, culture, and heritage as embodied in historic properties and historic districts, by appropriate regulations and incentives;
2. The stabilization of historic neighborhoods;
3. The establishment of the Town's Historic Register listing historic properties and historic districts;
4. The cultivation of civic pride in the art, architecture, and accomplishments of the past;
5. The encouragement of continued private ownership and utilization of such historic properties or historic districts now so owned and used;
6. The promotion of thoughtful community planning and design; and
7. The provision of educational opportunities to increase public appreciation of the Town's unique heritage.

## Section 2 - Intent:

The intention of Historic Preservation is to create a reasonable balance between private property rights and the public interest in preserving the Town's unique historic character through the nomination of buildings, structures, sites, objects, and historic districts for preservation.

## Section 3 - Eligibility Criteria:

Indicate how the property is significant to the Town of Minturn and its past by checking one or more of the following: Properties (*buildings, structures, sites, or objects*) or historic districts shall be at least 75 years old and meet 1 or more of the following criteria in order to be considered for designation:

- Association with events that have made a significant contribution to history;
  - Is a site of a historic event that had an effect upon society; or
  - Exemplifies cultural, political, economic, or ethnic heritage of the Town.
- Connection with persons significant in history.
- Distinctive characteristics of a type, period, method of construction, or artisan:
  - Exemplifies specific elements of an architectural style or period;
  - Is an example of the work of an architect or builder who is recognized for expertise nationally, state-wide, regionally, or locally;
  - Demonstrates superior craftsmanship or high artistic value;
  - Represents a style that is particular to the Town;
  - Represents an innovation in construction, materials, or design; or
  - Represents a built environment of a group of people in an era of history.
- Geographic importance:
  - Enhances the sense of identity of the Town or community; or
  - Is an established and familiar natural setting or visual feature of the Town or community.
- Possibility to yield important information related to prehistory or history:
  - Addresses research questions or fills recognized data gaps;
  - Embodies construction, development, or design adaptations; or
  - Informs on the development of engineering systems.

A property or historic district may be exempted from the age standard if the Town Council finds it to be exceptionally important in other criteria.

- Sec. 19-3-20 MMC



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 4 - Basic Property Information:

Historic Name of Property (if applicable) and any other names:

MINTURN SALOON

Address of Property

Street Address:

146 MAIN ST.

Legal Description of Property: Subdivision BOOCO Block B Lots 5 AND - Lot 6 Together with 30'x50' LAND Adjoining South End of lots 5 & 6

Maps:

1. Include a location map on 8.5x11 inch page showing the property in relation to major streets
2. Include a site plan on 8.5x11 inch page showing the footprint of the building if applicable

Property Owner Information:

Name:

CONNIE & TONY MAZZA  
Los Amigos-PROPERTY COMPANY LLC  
NEW YORK, NY 10167-4400

Mailing Address:

245 PARK AVE. FR 44  
NEW YORK, NY 10167-4400

Email:

Phone:

Applicant Information (if different):

Name:

LEONARDA TEACH (MAMA)

Mailing Address:

PO 654 MINTURN CO 81645

Email:

Phone:

970-827-4133

Is property listed on the National Register of Historic Places or the State Register of Historic Properties?

Yes  No

If yes, please provide date and registration number. Date: \_\_\_\_\_ Registration #: \_\_\_\_\_

For Official Use:

Date Received: \_\_\_\_\_ Planner: \_\_\_\_\_

Historic Preservation Commission -  Approval  Denial

Comments:



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 5 - Category and Resources:

Category of Nomination (check one):

- Building (any shelter or enclosure having a roof supported by columns or walls and intended for the shelter, housing or enclosure of any individual, animal, process, equipment, goods or materials of any kind.)
- Site (the location of a significant event; a prehistoric or historic occupation or activity; or a building, structure, or object, whether standing or vanished, where the location itself maintains historic or archeological value regardless of the value of any existing building, structure, or object.)
- Object (a material item of functional, aesthetic, cultural, historical, or scientific value that may be, by nature or design, movable yet related to a specific setting or environment.)
- District (a geographically definable area including a concentration, linkage, or continuity of properties within a specified period of significance and may include within its geographic boundaries one (1) or more contributing properties, which has been designated by the Town Council)
- Structure (anything constructed or erected, the use of which requires location on or in the ground, for purposes other than shelter of humans, animals, or chattels.)

### Narrative Statement of Significance:

Please describe how this property contributes to the history of Minturn. Why should the property be landmarked?

The Saloon is one of the earliest commercial buildings in Minturn (1901)

It served as a gathering place for the miners and railroad workers who lived in Minturn.

It still serves as a loved gathering place for the people who call Minturn home and also those who visit our historic town



# HISTORIC LANDMARK NOMINATION

TOWN OF MINTURN PLANNING AND ZONING DEPARTMENT

P.O. Box 309/302 Pine Street Minturn, Colorado 81645-0309

970-827-5645 | planner1@minturn.org

## Section 6 - Description:

### Architectural Description:

If property is a building, please describe the predominate architectural style such as Victorian or Mid-Century Modern.

PLEASE SEE ATTACHED

### Narrative Description:

Please describe the current appearance of the property and any alterations that have occurred since its original construction.

ORIGINAL DECORATIVE BRICKWORK  
THE SOUTHSIDE OF BUILDING WAS A PARKING  
HOT AND IS NOW AN OUTDOOR PATIO.

### List of References:

Please cite the books, articles, personal contacts, and other sources used in preparing this application.

ELLA BURNET - HAS LIVED IN MINTURN FOR 24 YEARS  
PHYLLIS BUDER - " " " "  
LYNN KAVAKIS - 248 MAIN ST MINTURN APPX. 60-65 YRS  
HAS LIVED IN MINTURN  
IOA

Eagle County Property CARD

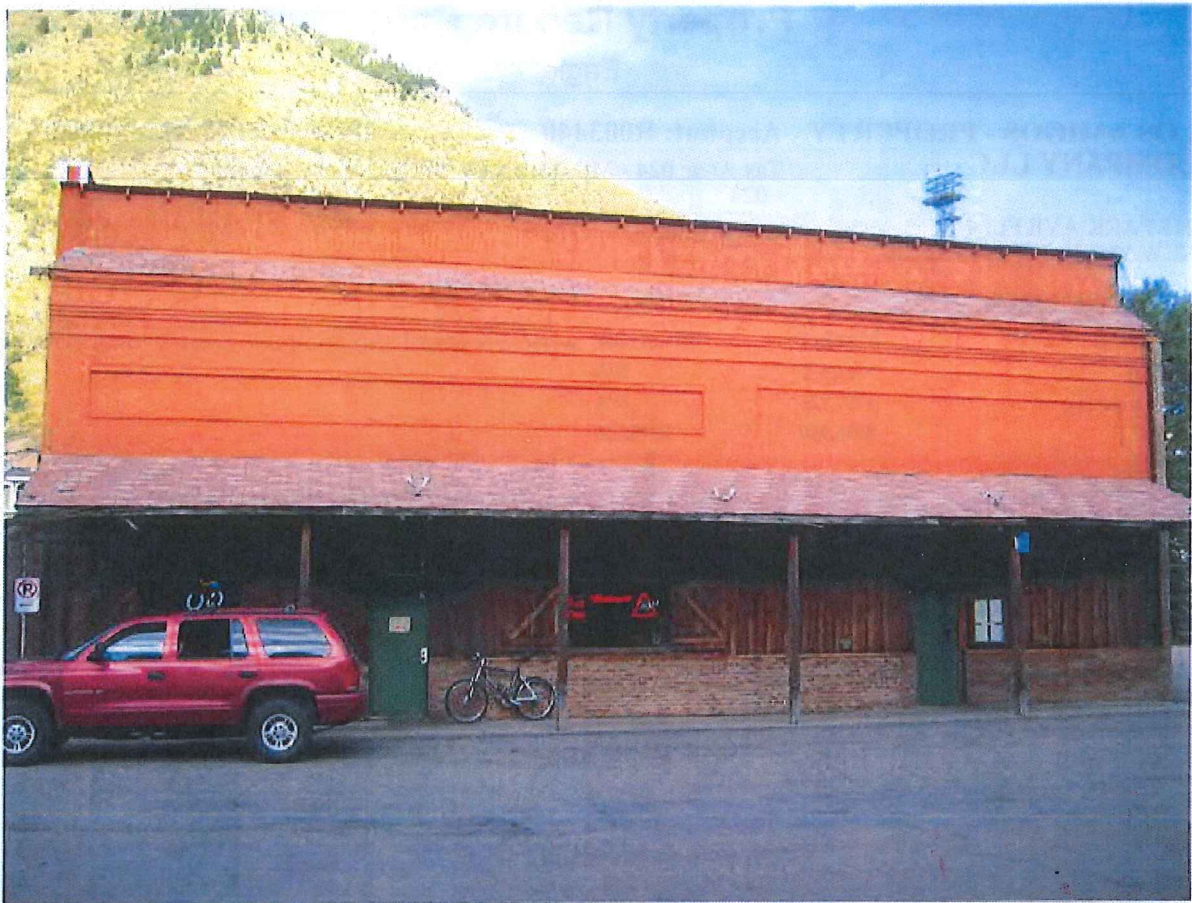
### Photographs:

Please attach current photographs of the property and, if available, copies of historic photographs. For building, structures, or objects, current photographs should show the property from all directions.

### Signature:

Leonardo Teach 11/10/22

If you are not the owner of the property that is being nominated, have you discussed this application with the owner(s)? This is encouraged, but not required. Yes  No



**Address:** 146 Main Street  
**Name:** Minturn Saloon  
**Built:** 1901  
**Style:** Commercial  
**Characteristics:** Flat roof; flush entrance; decorative brickwork

# Property Record Card

Eagle

**LOS AMIGOS - PROPERTY COMPANY LLC**

**Account: R003440**

**Parcel: 2103-263-08-002**

245 PARK AVE FL. 44  
NEW YORK, NY 10167-4400

Tax Area: 024 - MINTURN (TOWN)  
- 024

Situs Address:  
146 N MAIN ST  
MINTURN AREA, 0

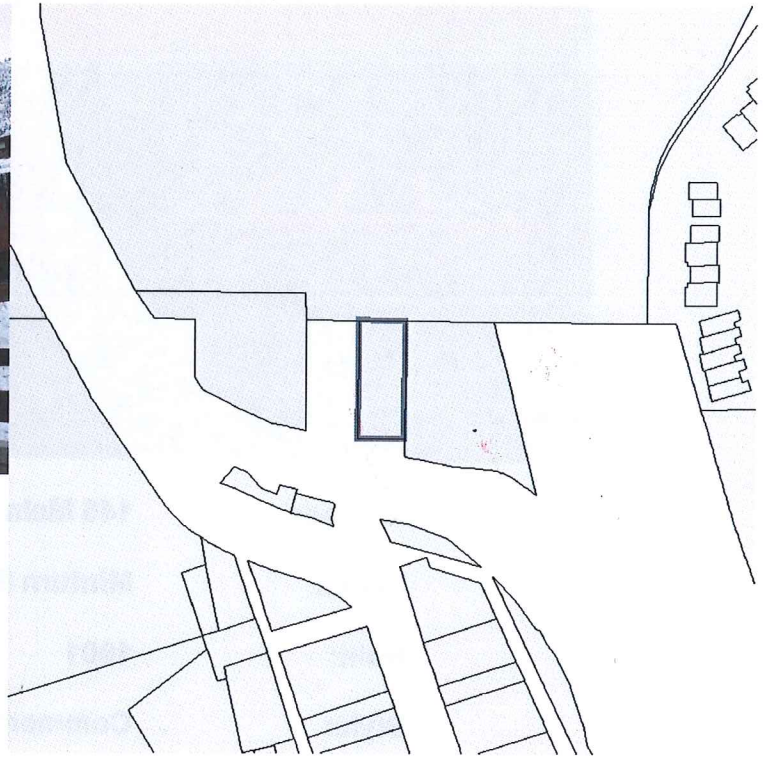
Acres: 0.208

## Value Summary

Value By:	Market	Override
Land (1)	\$231,230	N/A
Commercial (1)	\$167,720	N/A
<b>Total</b>	<b>\$398,950</b>	<b>\$398,950</b>

## Legal Description

Subdivision: BOOCO Block: B Lot: 5 AND:- Lot: 6, TOGETHER WITH 20' X 50' OF LAND ADJOINING SOUTH END OF LOTS 5 & 6



## Public Remarks

Entry Date	Model	Remark
12/19/2006	Commercial Occurrence 1	SALOON

## Sale Data

Doc. #	Sale Date	Deed Type	Validity	Verified	Sale Price	Ratio	Adj. Price	Ratio	Time Adj. Price	Ratio
202128007	12/17/2021	SWD	UV	Y	\$2,100,000	19.00	\$2,100,000	19.00	\$2,100,000	19.00
920775	06/15/2005	WD	QV	Y	\$550,000	72.54	\$550,000	72.54	\$550,000	72.54

## Land Occurrence 1

Abstract Code	2130 - SPEC.PURPOSE-LAND	Percentage	100
Use Code	2000 - COMMERCIAL	Neighborhood	20319 - MINTURN COMM
Land Code	5066 - MINTURN COMMERCIAL	Super Neighborhood	2300 - MINTURN / REDCLIFF COMMERCIAL
Size	0.208		

SubArea	ACTUAL	EFFECTIVE	HEATED	FOOTPRINT
---------	--------	-----------	--------	-----------



# Property Record Card

Eagle

## Land Occurrence 1

Land S		9068			
Total		9,068.00			
	<b>Value</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>
	\$231,230	25.50			

## Commercial Occurrence 1

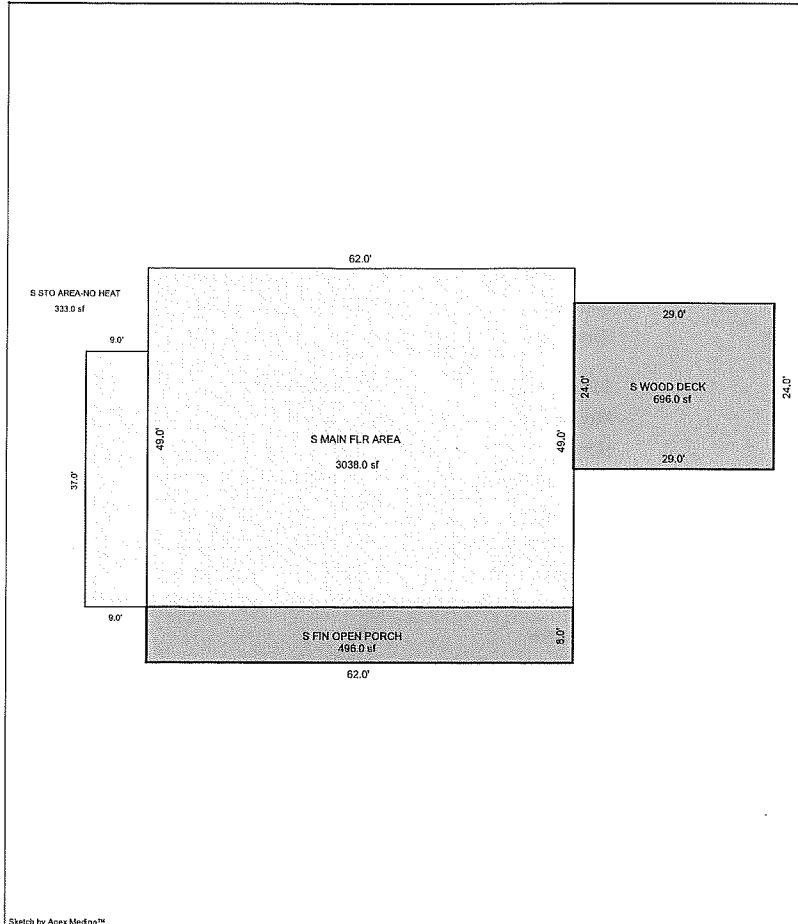
Abstract Code	2230 - SPEC.PURPOSE-IMPROVEMENTS	Percentage	100.0	
Use Code	2000 - COMMERCIAL	Neighborhood	20319 - MINTURN COMM	
Building Type	223000 - SPEC PURPOS	Arch Style	45 - RESTAURANT	
Exterior Wall	14 - WOOD SIDNG 36 - BRICK/BRIC	Percentage	10.0 90.0	
Roof Structure	2 - SHED	Interior Wall	5 - DRYWALL	
Percentage	100.0	Floor Cover	5 - ASPH TILE	
Percentage	100.0	Heating Fuel	3 - GAS	
Heating Type	3 - FORCE AIR	Air Conditioning	4 - ROOF TOP	
Actual Year Built	1901	Bedrooms	0	
Bathrooms	3	Construction Quality	3 - FAIR	
Effective Year Built	1970	Fixtures	12	
Rooms	3	Units	1	
Bathrooms	3	Bedrooms	0	
Actual Year Built	1901	Effective Year Built	1970	
Economic Obsolescence	77	Special Code	9 - COVID ADJ	
Units	1	Stories	1 - STORIES 1.0	
Use Code	2000 - COMMERCIAL			

SubArea	ACTUAL	EFFECTIVE	HEATED	FOOTPRINT
BAS	3038	3038	3038	3038
FM1				
FOP	496	149		496
UTL	330	149		330
WDD	696	35		696
Total	4,560.00	3,371.00	3,038.00	4,560.00
	<b>Value</b>	<b>Rate</b>	<b>Rate</b>	<b>Rate</b>
	\$167,720	36.78	55.21	36.78

# Property Record Card

Eagle

## Commercial Occurrence 1



### Abstract Summary

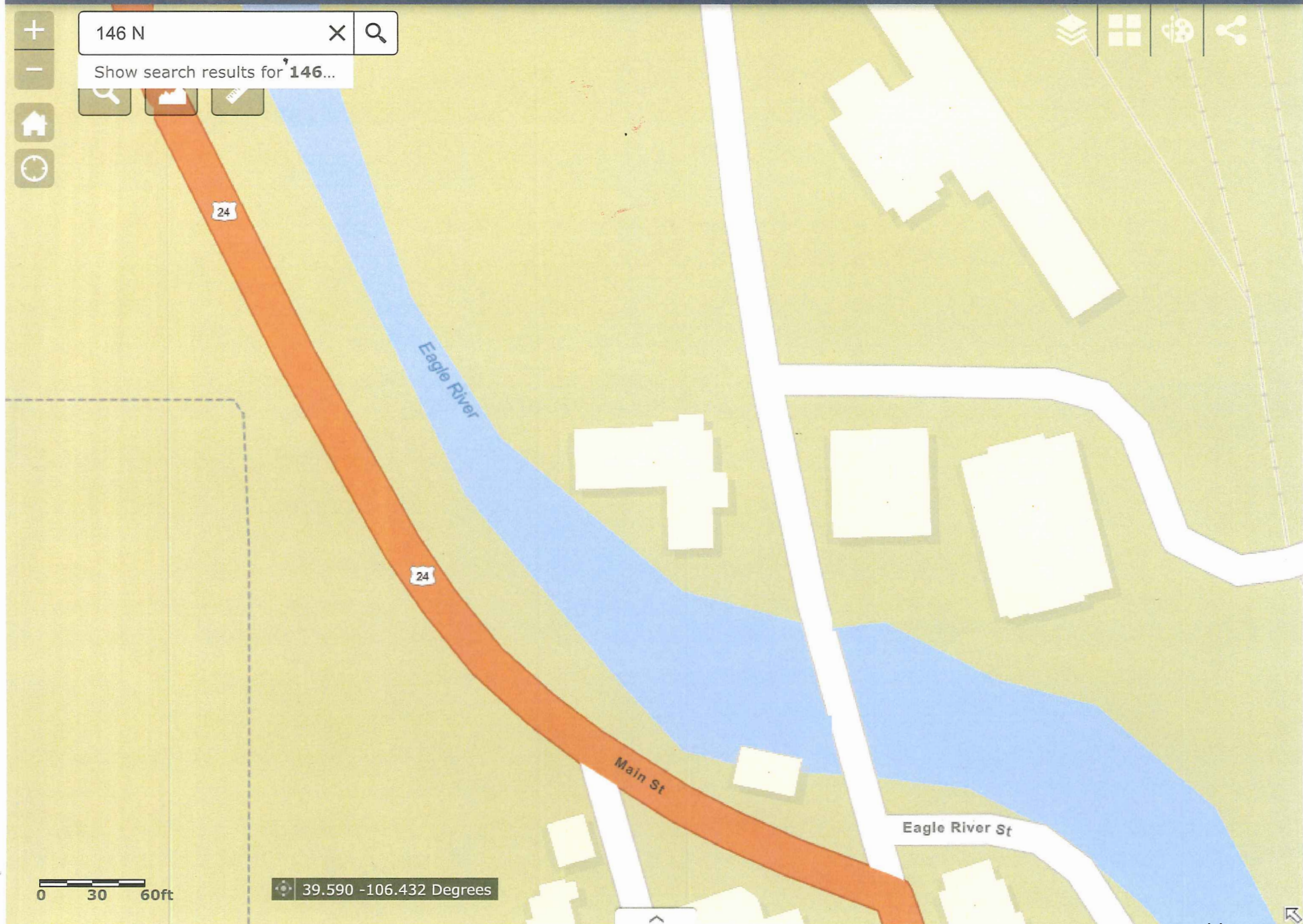
Code	Classification	Actual Value	Taxable Value	Actual Override	Taxable Override
2130	SPEC.PURPOSE-LAND	\$231,230	\$67,060	NA	NA
2230	SPEC.PURPOSE-IMPROVEMENTS	\$167,720	\$48,640	NA	NA
<b>Total</b>		<b>\$398,950</b>	<b>\$115,700</b>	<b>NA</b>	<b>NA</b>





146 N X Q

Show search results for 146...



0 30 60ft

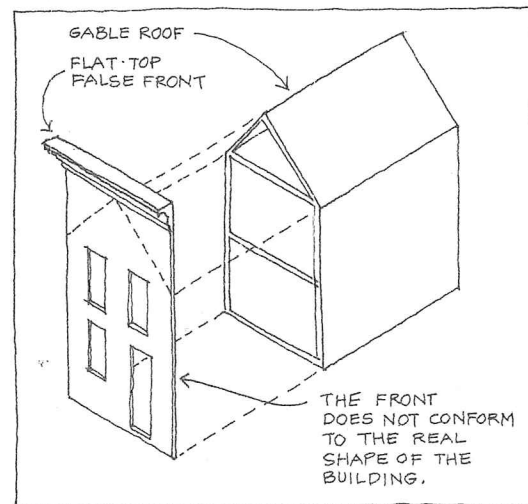
39.590 -106.432 Degrees

# False Front

Countrywide 1860

False front buildings first appeared in great numbers in the aftermath of the California Gold Rush of 1849. They were the result of small frontier town merchants attempting to make their simple shed and gable roof buildings appear larger, and more important, like the flat-roof buildings of eastern cities. The merchant wanted the casual shopper to see his sign (usually located on the false front), not the snow capped mountains behind his building. So the view was blocked and the false fronts dominated the street.

As the town's wealth increased, the use of the false front to create an urban atmosphere was no longer necessary. The false front, however, remained a significant commercial architectural form all over the country, in older, less prominent towns. The false front rarely was applied to residential architecture. The few houses on which they appeared were most often located on the main street and built to conform to the rest of the streetscape.

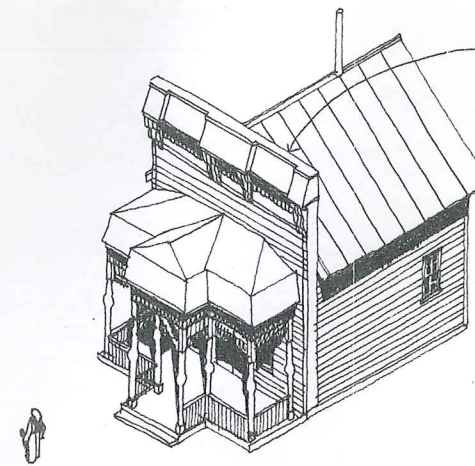


FALSE FRONT



1860 FALSE-FRONTED MAIN STREET OF EUREKA, COLORADO

Many false fronts were reminiscent of the Jacobean Style, illustrated on page 64 and the early Dutch Colonial stepped gable facade, as shown on page 58. Some historians think that the False Front Style was based on the Dutch Colonial Style.



AN ELABORATE FALSE FRONT MEANT TO SIMULATE THE MANSARD ROOF OF THE FRENCH SECOND EMPIRE STYLE (P. 148)

GALVANIZED SHEET METAL ROOF

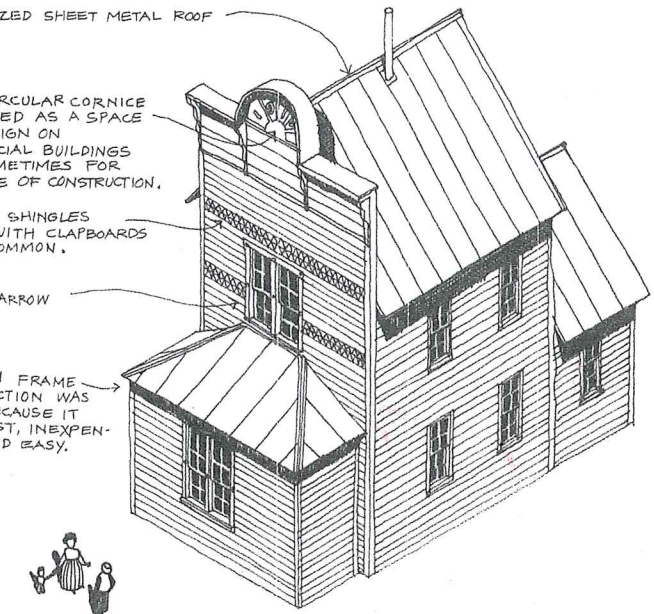
SEMICIRCULAR CORNICE WAS USED AS A SPACE FOR A SIGN ON COMMERCIAL BUILDINGS AND SOMETIMES FOR THE DATE OF CONSTRUCTION.

ROWS OF SHINGLES MIXED WITH CLAPBOARDS WERE COMMON.

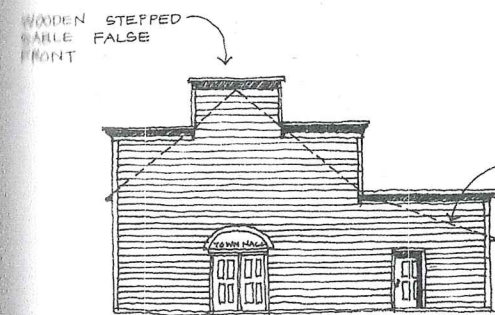
TALL NARROW WINDOWS

BALLOON FRAME CONSTRUCTION WAS USED BECAUSE IT WAS FAST, INEXPENSIVE, AND EASY.

1860 FALSE FRONT HOUSE, LEADVILLE, COLORADO



1874 FALSE FRONT HOUSE, CRESTED BUTTE, COLORADO



WOODEN JACOBAN STYLE FALSE FRONT

ROOF LINE BEHIND THE FACADE SHOWN DOTTED

1864 TOWN HALL, BENOA, NEVADA

GENERAL STORE, WOLF POINT, MONTANA



To: Mayor and Town Council  
From: Michelle Metteer  
Date: January 4, 2023  
Agenda Item: Minturn 2023-25 Strategic Plan

---

**REQUEST:**

Approve the 2023-25 Strategic Plan.

**INTRODUCTION:**

The Minturn Town Council is committed to the litigation of the Annexation documents for the Battle Mountain property. With this commitment in mind, staff, via feedback from the Council retreat in the spring of 2022, has created a 2023-25 Strategic Plan which emphasizes budget allocations toward the enforcement of the Annexation Agreement.

**ANALYSIS:**

The strategic plan is a critical document in outlining the goals of the Council for the next two years. This document will be heavily utilized by staff for direction toward action items to achieve the approved goals.

**COMMUNITY INPUT:**

Ongoing and critical

**BUDGET / STAFF IMPACT:**

TbD – But limited due to the commitment to litigation

**STRATEGIC PLAN ALIGNMENT:**

**ADVANCE DECISIONS/PROJECTS/INITIATIVES THAT EXPAND FUTURE OPPORTUNITY AND VIABILITY FOR MINTURN**

**CLEAR, TRANSPARENT AND COMMUNICATIVE LOCAL GOVERNMENT**

**RECOMMENDED ACTION OR PROPOSED MOTION:**

- Motion to approve Resolution 02 – Series 2023 a Resolution establishing Minturn’s 2023-25 Strategic Plan

**ATTACHMENTS:**

- Resolution 02 – Series 2023
- 2023 – 25 Minturn Strategic Plan

**TOWN OF MINTURN, COLORADO  
RESOLUTION NO. 02 – SERIES 2023**

**A RESOLUTION TO APPROVE THE TOWN OF MINTURN  
STRATEGIC PLAN FOR THE PERIOD 2021-2023**

**WHEREAS**, The Minturn Town Council has reviewed and approve the Town of Minturn 2023-2025 Strategic Plan; and,

**WHEREAS**, The Town Council encourages annual review and update of the Town of Minturn Strategic Plan on a bi-annual basis; and,

**NOW THEREFORE, BE IT RESOLVED BY THE TOWN COUNCIL OF  
THE TOWN OF MINTURN, COLORADO:**

1. The Minturn Town Council hereby approves and adopts the Town of Minturn Strategic Plan for the period 2023-2025 as documented in Attachment A of this Resolution.

**INTRODUCED, READ, APPROVED, ADOPTED AND RESOLVED this 4<sup>th</sup>  
day of January, 2023**

TOWN OF MINTURN

By: \_\_\_\_\_  
Earle Bidez, Mayor

ATTEST:

\_\_\_\_\_  
Jay Brunvand, Town Clerk

# TOWN OF MINTURN

## 2023 - 2025

### STRATEGIC PLAN

---



**2018-20 Strategic Plan Adopted by the Minturn Town Council**

09/05/2018 Resolution 14, Series 2018

**Amended by the Minturn Town Council**

07/01/2019 Resolution 27, Series 2019 – Vision Statement

07/01/2020 Resolution 20, Series 2020 – True North Statement

**2021-23 Strategic Plan Adopted by the Minturn Town Council**

10/21/2021 Resolution 28 - Series 2020

**2023-25 Strategic Plan Adopted by the Minturn Town Council**

01/04/2023 Resolution 02 – Series 2023



## STRATEGIC PLAN PURPOSE

---

The purpose of the Strategic Plan is to provide a cumulative roadmap for achieving the vision of the Minturn Community Plan. The 2023-25 Strategic Plan is the continuation of the first Strategic Plan created by the Town in 2018 and will set the tone for all forthcoming action items. With the Council and staff making continuous progress, an emphasis toward foundational work will be a consistent theme throughout this Plan.

The Strategic Plan is designed to work in coordination with the town budget and will be reviewed by the Council every year to adjust for changes as needed. Once approved, the Council and Minturn community will have the general outline for the work expected over the planning period.

The Strategic Plan will ensure that strategies and activities undertaken by the Town are consistent with the vision and values set forth by the Town Council and Minturn community. This plan will strive to achieve the following:

- Clearly communicate with the community the goals and objectives of the town over the next two years
- Establish a 2-year planning period and provide the platform for an ongoing discussion between the staff, Council and community regarding direction of the Town
- Identify challenges and opportunities expected to arise during the planning period and propose specific goals to address these issues
- Give a framework for town staff to set measurable objectives and timelines for execution

## STRATEGIC PLAN SUMMARY

---

Since its inception in 1904, Minturn has been a resilient and proud mountain town that full-time, working-class residents call home. With the closing of the Gilman mine and the decommissioning of the rail-lines, **Minturn has new opportunity within the mountain resort economy**. Today, **having an ideal location as a valley-wide intersection**, Minturn embraces the “basecamp position” geographically within the Eagle County region.

Core to Minturn’s values **is its strong identity as a small mountain-town community**. In the Colorado Rockies, sandwiched between two world-class resorts, this value requires maintaining **principles** and a **desire to lead by example**. Through a **willingness to confront growth and other issues** while **maintaining high standards in following the public process**, Minturn will work to stay true to this core values.

The Town is committed to providing a respectful and responsive government that follows a solutions-based approach rooted in the municipal code. To sustain community **trust**, we strive to follow a consistent **public process** in all areas of the municipal government.

Town of Minturn staff and elected officials will **support activities outlined in this strategic plan and work toward sustaining a great mountain town** by taking a **proactive approach** to municipal government. With an **engaged community** and a **safe, family-friendly environment**, Minturn endeavors to remain one of the **last great mountain towns**.

## MISSION STATEMENT

---

*“In collaboration with our community, foster the authentic small-town character that is Minturn”*

## VISION STATEMENT

---

*“Preserve Minturn’s long-term viability, its unique character and genuine mountain town community”*

**From the Vision Statement, four key strategies are identified to guide the Town efforts:**

- Practice fair, transparent and communicative local government
- Long-term stewardship of the natural beauty and health of Minturn’s environment
- Sustain and invest in the things that define Minturn as a proud, sturdy mountain town to “keep Minturn Minturn”
- Advance decisions/projects/initiatives that expand future opportunity and viability for Minturn

## TRUE NORTH STATEMENT

---

*“Leverage Minturn’s assets to benefit and maintain our funky, proud, uniquely classic mountain town where people grow roots through creating opportunities and community engagement.”\**

*\*Keeping in mind we are Minturn*

## STRATEGIC PLAN FISCAL YEARS 2023 - 2025

---

### PRACTICE FAIR, TRANSPARENT AND COMMUNICATIVE LOCAL GOVERNMENT

The Town will seek to make [ethical](#), [informed](#), [data-based decisions](#). With a focus on the [public process](#), the Town Council and staff are committed to serving Minturn with the honesty and integrity in all aspects of local government.

### STRATEGIES

- Support code enforcement through heartwarming messages of community togetherness
  - Wildlife protection and safety
- Commit to quarterly “Minturn Matters” community sessions by Town Council
- Utilize neighborhood-specific email blasts where able (this requires residents to sign up)
- Commit to semi-annual door hanger notices updating residents and businesses of seasonal changes in town operations, events, Council highlights and more
- Encourage resident sign-up to the EC Alert system

## STRATEGIC PLAN FISCAL YEARS 2023 - 2025

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### LONG-TERM STEWARDSHIP OF THE NATURAL BEAUTY AND HEALTH OF MINTURN'S ENVIRONMENT

Minturn will work to become a **leader in advancing renewable and sustainable initiatives**. With a forward-thinking approach and focus on **environmental stewardship** the town can responsibly utilize the remaining developable land in the Minturn valley.

#### STRATEGIES

- Incorporate “Firewise” guidelines in building and site-planning requirements
- Zero waste Minturn Market by 2023
- Commit to a 3-yr building code adoption cycle
  - Adopt 2021 building codes by 2023
- Work to advance renewable and sustainable initiatives
- If funding permits – support the installation of additional dog waste stations
- Implement Minturn’s 3–5-year Wildfire Mitigation Plan
- Implement Minturn’s waterwise landscaping regulations and initiatives

## STRATEGIC PLAN FISCAL YEARS 2023 - 2025

---

### SUSTAIN AND INVEST IN THE THINGS THAT DEFINE MINTURN AS A PROUD, STURDY MOUNTAIN TOWN TO “KEEP MINTURN MINTURN”

Minturn strongly values its **full-time residents** and welcomes **diversity**.

#### STRATEGIES

- Funding permitting - Complete the Minturn Municipal Code Chapter 16 rewrite for better communication regarding code expectations across all zoning areas
- Implement the 2022 Community Plan short-term goals and strategies while continuing to refine prioritization of medium- and long-term goals and strategies as funding allows
- Funding permitting - Complete an in-house update to the 2010 Minturn Three Mile Plan for Annexation
- Consider a Downtown Development Authority or similar entity to support implementation of downtown development and economic goals
- Apply for grants to support funding of the Minturn Water CIP
- Facilitate the creation and implementation of design guidelines for historic structures
- Create a clear and definable process for building permit applications and construction process guidelines
- Support local events through third party organizations
- Work with Visioning Committee to improve and grow the Minturn Market

## STRATEGIC PLAN FISCAL YEARS 2023 - 2025

---

### ADVANCE DECISIONS/PROJECTS/INITIATIVES THAT EXPAND FUTURE OPPORTUNITY AND VIABILITY FOR MINTURN

The ability for Minturn to approach development as **resilient, sustainable, creative and diverse** will allow the town to continue embracing what has “**made Minturn, Minturn.**” The town can further leverage its destination recreation location as a valley-wide benefit and **competitive advantage**.

### STRATEGIES

- Continue implementation of Minturn’s Water Capital Improvement Plan
- In conjunction with the 2022 Community Plan, develop a Capital Improvement Plan prioritizing upgrades to existing infrastructure including roads, public buildings, storm water drainage and pedestrian/bicycle amenities
- Encourage all modes of connectivity
- As part of the CTO Reimagine Destinations Program, Develop a Minturn / Red Cliff Outdoor Recreation Guide / Visitor Guide
- Participate / work closely with the Top of the Rockies Scenic Byway Committee to better utilize the byway for marketing and funding opportunities

**TOWN OF MINTURN, COLORADO  
RESOLUTION NO. 03 - SERIES 2023**

**A RESOLUTION OF THE TOWN OF MINTURN, COLORADO,  
ADOPTING THE LITTLE BEACH PARK SUB AREA MASTER PLAN**

**WHEREAS**, the Town of Minturn (“Minturn” or the “Town”) is a Home Rule Colorado municipality pursuant to Section XX of the Colorado Revised Statutes and with the authority set forth therein; and

**WHEREAS**, Town staff has determined that the adoption of the attached Little Beach Park sub area Master Plan will provide clear guidelines for Town development and expenditures under this plan; and

**WHEREAS**, the Town Council finds and determines that it is in the interest of the Town to adopt the Little Beach Park sub area Master Plan.

**NOW, THEREFORE, BE IT ORDAINED BY THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO THAT:**

**Section 1.** The foregoing recitals are incorporated herein as if set forth in full.

**Section 2.** The Little Beach Park sub area Master Plan is attached and inclusive of this Resolution.

**INTRODUCED, READ, APPROVED, ADOPTED AND RESOLVED this 4<sup>th</sup> day of January, 2023.**

TOWN OF MINTURN, COLORADO

By: \_\_\_\_\_  
Earle Bidez, Mayor

ATTEST:

\_\_\_\_\_  
Jay Brunvand, Town Clerk



# ***Town of Minturn Little Beach Recreation Area Plan***

*January 4, 2023*



*Prepared by:*







## TABLE OF CONTENTS

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2	Table of Contents
3	Introduction / Plan Purpose
4-11	Study Area
12-16	Plan Stakeholders and Facilities
17-18	Process
19-21	Summary of Input
22-34	Plan Recommendations
35	Conclusions



# I. Introduction / Plan Purpose

## Plan Purpose

The purpose of the Little Beach Recreation Area Plan is to create a recreation based plan for the area south of Minturn encompassing Little Beach Park and Amphitheater, the Riverview Cemetery, the Public Work Facility and Storage Yards, and the Minturn Bike Park and Mini-Mile Trail.

The plan proposes a long-term vision for the study area to recognize, protect, and promote it as the Town's major recreational and cultural hub. The plan seeks to unify the existing cultural, civic, and recreation facilities in the study area, which have evolved organically over time. With increased uses and greater intensity of community and recreation activity in the area, there is a need for a coordinated vision and framework to guide the future of the area. The plan makes recommendations for the study area for existing and new facilities based on input provided by stakeholders and residents.

A major goal of the plan is also to create a cohesive sense of place building on the exceptional character of the area and its existing assets and amenities. Greater connectivity between facilities in the area and with nearby residential areas of the town will help accomplish this goal, as expressed by the community.



The approach to the planning process included a **community and stakeholder engagement process** that engaged the residents and stakeholders of Minturn using a variety of methods including an open house, pop up events, and guided tours of the area. The process involved a **sequence of incremental planning steps** that have resulted in a conceptual framework plan for the area, identifying a variety of potential improvements suggested and vetted with members of the public and stakeholders.

The plan proposes to improve access, establish new connections to unify the various amenities, and to enhance Little Beach Park as a central hub, gathering spot, point of arrival and orientation for the community residents and other users.



## II. Study Area

The study area for the Little Beach Recreation Area Plan is located in lands within the Town of Minturn accessed by Cemetery Road. The area is uniquely situated in the valley and slopes between the Minturn Cliffs and the Eagle River. There are several civic uses that are located in this area, some of which have been developed and emerged as new uses over the past 20 years.

The study area includes the following existing facilities and uses:

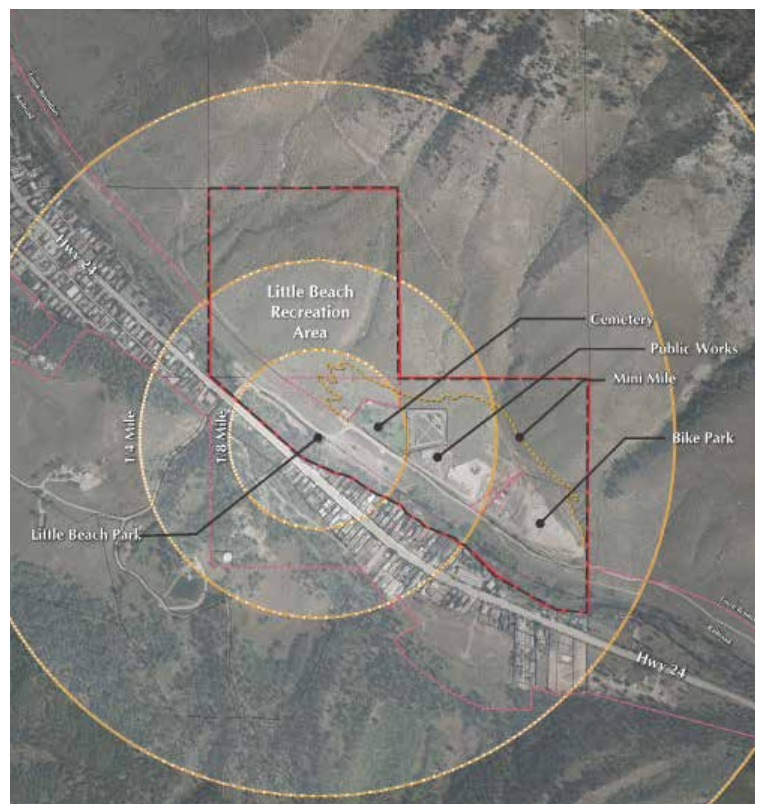
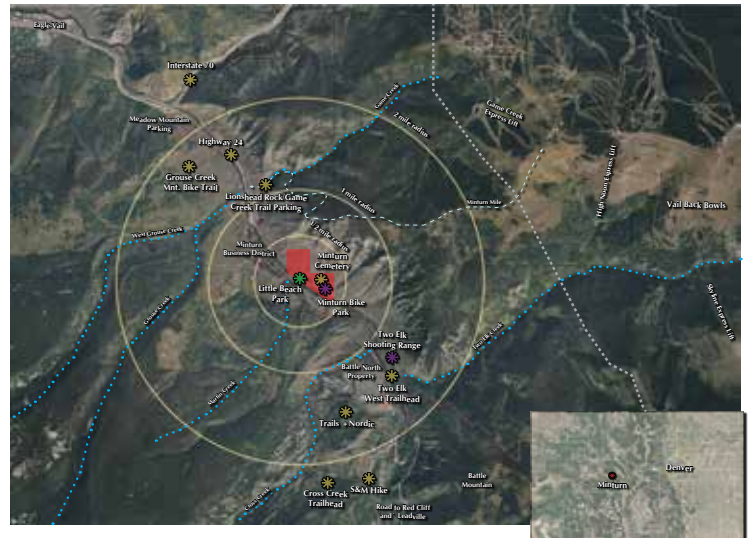
- **Little Beach Park**
- **Little Beach Amphitheater**
- **Minturn Riverview Cemetery**
- **Town Public Works Facility**
- **Storage Yards (on Town leased lots)**
- **Minturn Bike Park**
- **The Mini-Mile Trail**

Maps of the study area and of these facilities are provided in the next pages of this plan. These maps were developed specifically for the plan using Eagle County GIS information and were used for community meetings and site walks. A description of each facility and its relationship to the overall study area and other uses is provided in this section of the plan.

### Special Considerations

To the north of the study area lie the spectacular Minturn Cliffs, providing exceptional vistas and a dramatic backdrop that create a unique and identifiable sense of place with strong mountain character. The views afforded by the Cliffs are arguably one of the most valuable visual resources in the Eagle River Valley. Therefore, the basis of this plan is that the area must be treated sensitively and the views and natural character of the area must be preserved as part of the plan.

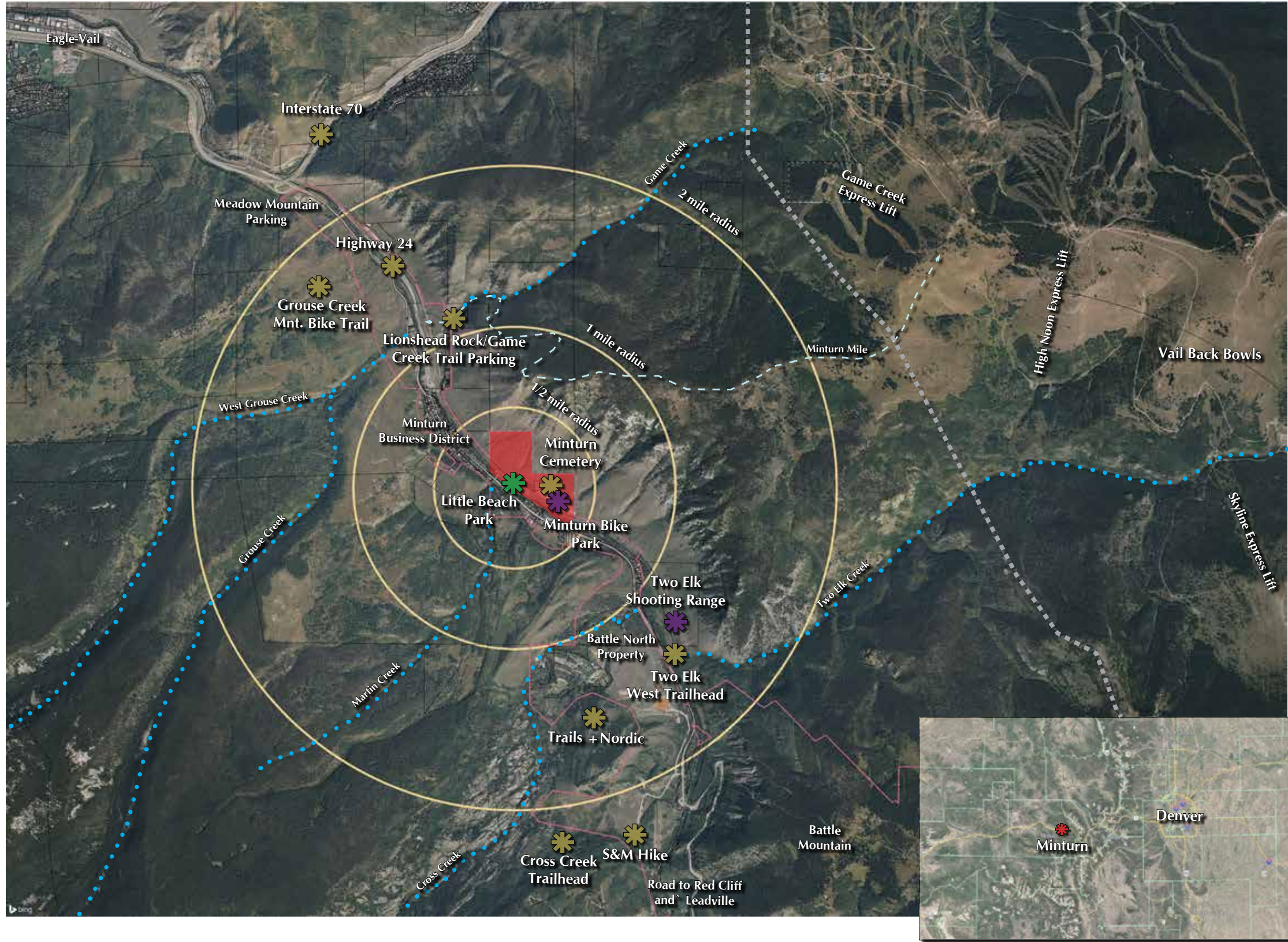
To the south of the study area are United States Forest Service (USFS) federally owned lands. Most notably, there is the Two Elk Trail and the Shooting Range, both popular destinations and long-time uses on federal lands in the vicinity of the study area.



These uses draw visitors to and through the study area through-out the seasons. Because of the 'off the beaten path' and tucked away location of these facilities, it is not uncommon for first time visitors to be confused or lost in trying to find the access to these facilities. Another basic premise of this plan is that greater wayfinding and orientation is needed in the study area as the use of the area has increased and some users have a difficult time finding their destination. This was confirmed during the planning process.



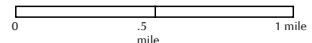
# II. Study Area



APRIL 7, 2022

LITTLE BEACH RECREATION AREA PLAN  
MINTURN, CO

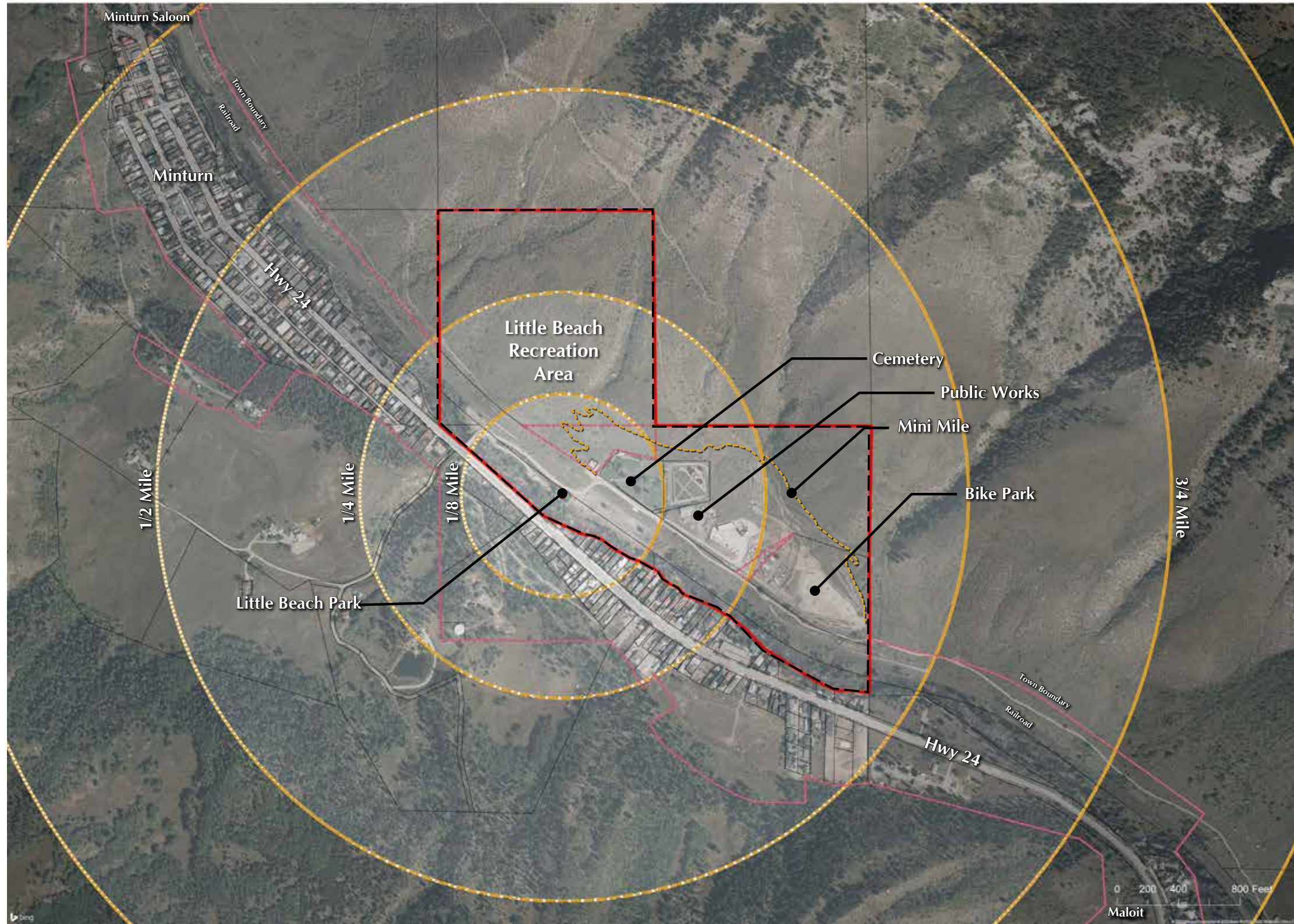
Overall Context Map



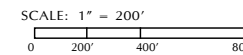
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## II. Study Area



### Little Beach Recreation Area



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# II. Study Area

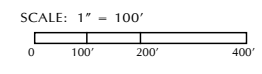


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MINTURN, CO



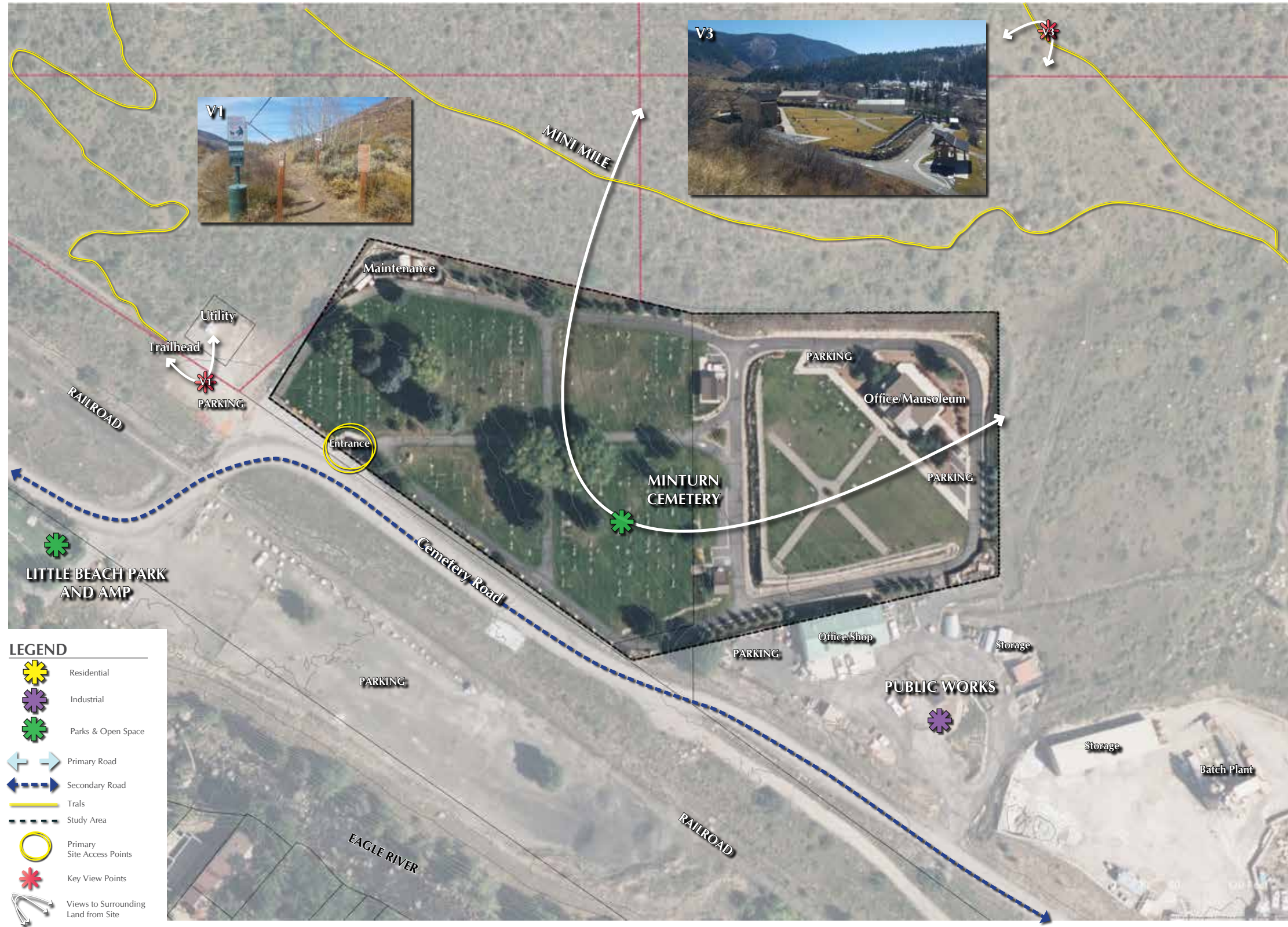
Minturn Little Beach Recreation Area



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## II. Study Area

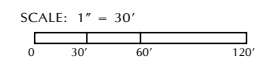


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### Minturn Cemetery



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### Study Area

## II. Study Area



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### Minturn Public Works Facility and Lease Lots



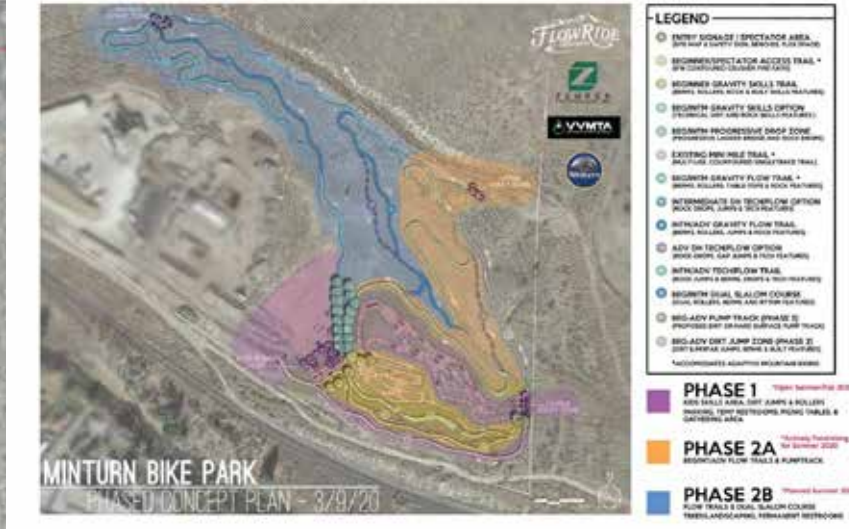
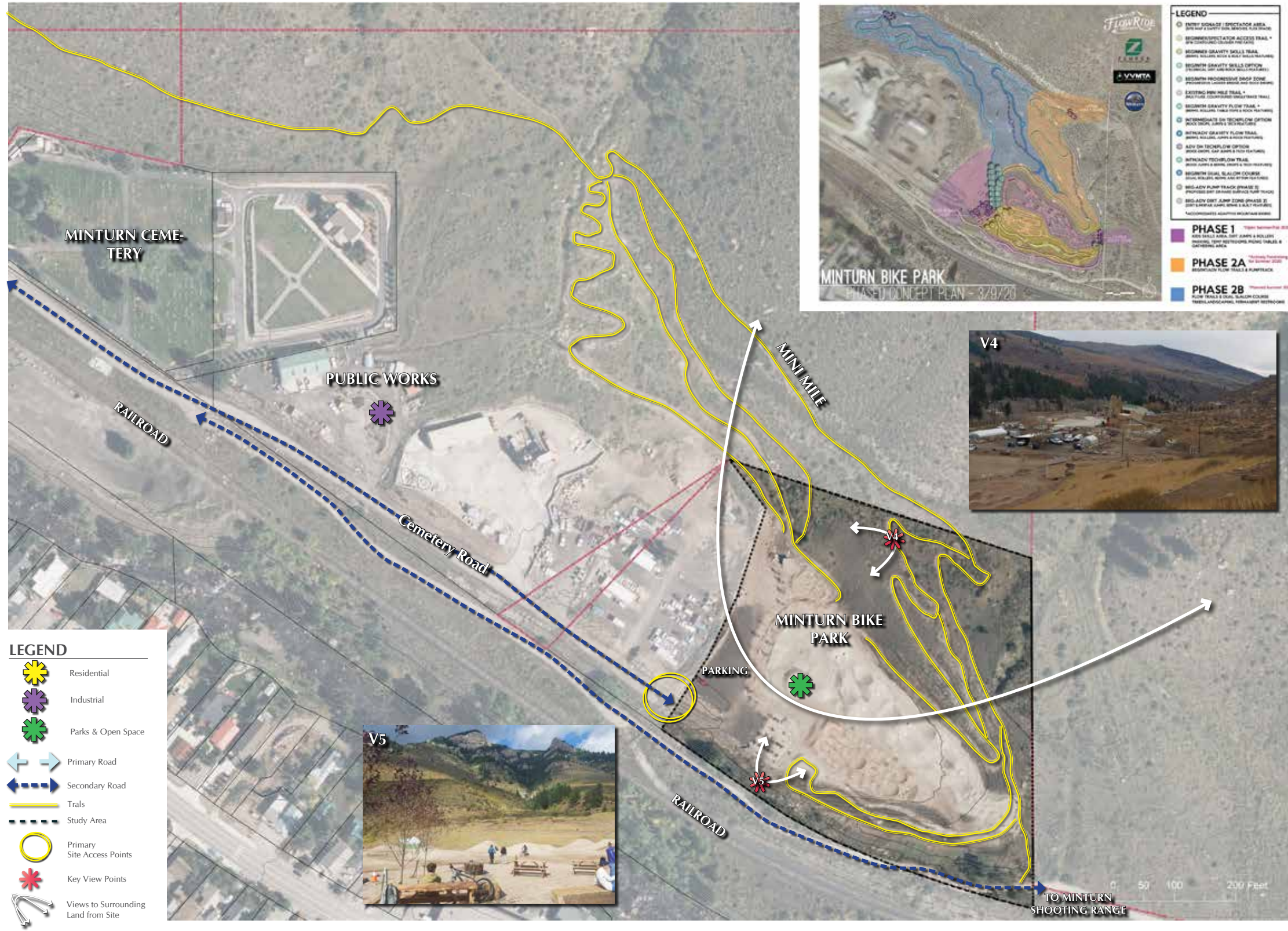
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Study Area



# II. Study Area

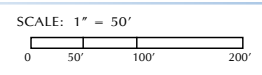


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Minturn Bike Park

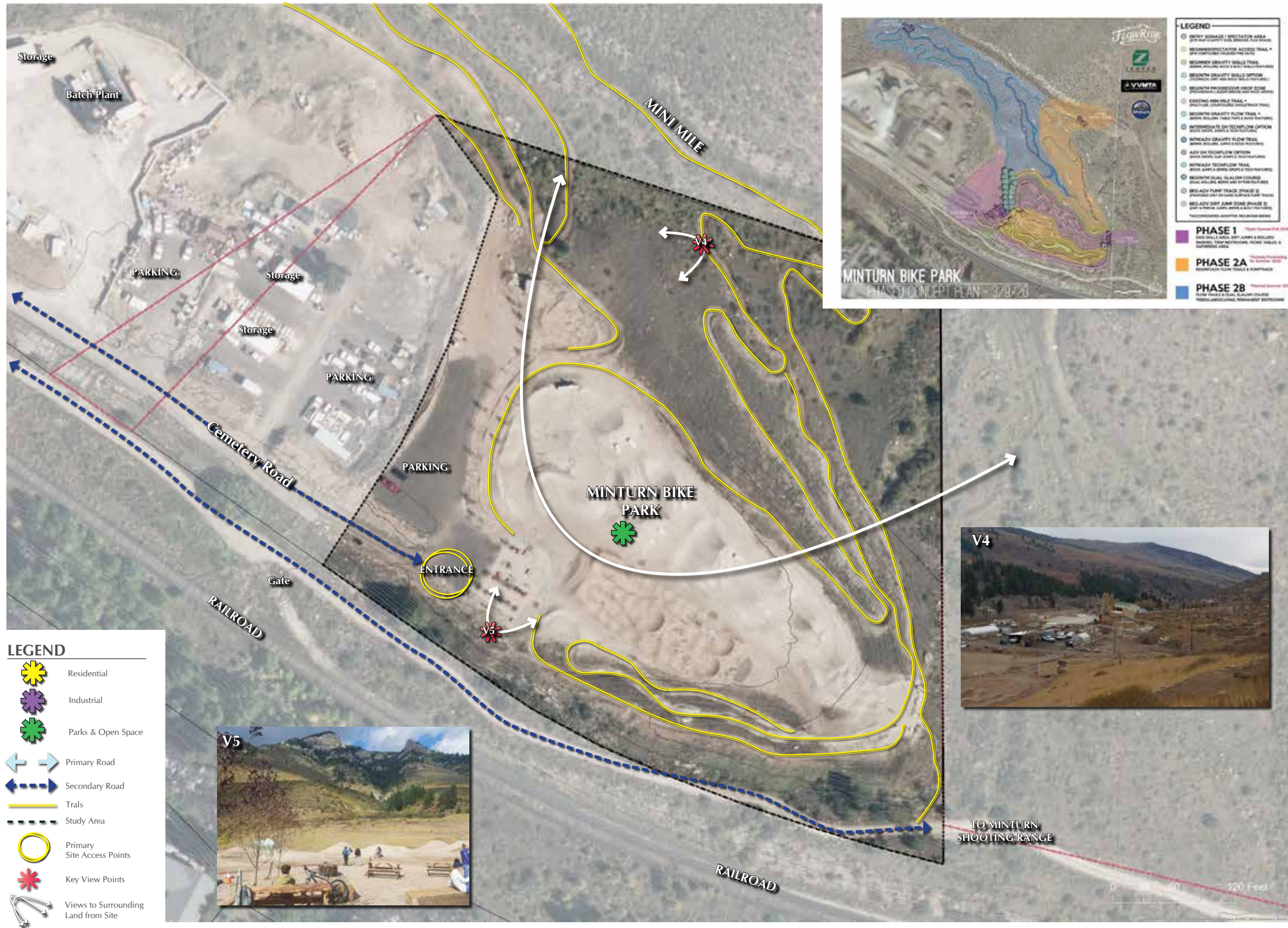


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Study Area

# II. Study Area

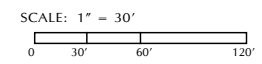


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LITTLE BEACH RECREATION AREA PLAN  
MINTURN, CO



## Minturn Bike Park



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## Study Area

### III. Plan Stakeholders and Facilities

#### Plan Stakeholders

The Stakeholders for the Little Beach Recreation Area Plan are the operators and or owners of the facilities located within the project study area, and of course, the community-at-large. These groups include the organizations that are listed below. The planning team engaged with representatives from each group during the planning process.

- **Community-at-Large**
- **Town of Minturn  
(Public Works, Events, and Administration)**
- **Minturn Riverview Cemetery District**
- **Vail Valley Mountain Trails Alliance**
- **Minturn Community Fund**
- **State of Colorado Parks and Wildlife**

These groups participated in the Minturn Community Plan open house on April 7, 2022, Little Beach Recreation Area site visits, or on individual phone calls. Representative(s) of each group expressed the needs for their respective facilities, and provided input on what is working well, what is in need of attention, and specific considerations related to future planning and improvements.

A description of each facility and its relationship to the overall study area and other uses is provided below. Maps and pictures of each facility are included for reference as well

#### Little Beach Park

Little Beach Park is the Town’s largest park within walking distance of the downtown commercial core. The Park is approximately 1.6 acres. It has a linear configuration stretching from the entrance at Cemetery Bridge Road and southwest along the Eagle River towards the Little Beach Amphitheater. The Park is located ‘down low’ along the river banks, and topographically removed from the rest of the planning area.



The Park has limited parking with 4 spaces accessed by a long driveway. Facilities within the park include a campground vault toilet, a small basketball court, and a playground with play elements for different age groups. Picnic tables and benches are located along the Eagle River under a dense canopy of cottonwood trees. The park is adjacent to and contiguous with the Little Beach Amphitheater, and connected by an asphalt path.



### III. Plan Stakeholders and Facilities (continued)

During special events, the asphalt path connects both facilities and park users are able to circulate between the playground and amphitheater, creating a special synergy between the facilities.

#### Little Beach Amphitheater

Little Beach Amphitheater is located adjacent to Little Beach Park. It is a Town-owned outdoor performance venue of approximately a half acre in size with a covered stage, a sloped lawn for seating, and nice well-maintained landscaping around the perimeter. The covered stage, a timber wood structure built circa 2005 is located close to the River, and faces north providing performers with views of the Minturn Cliffs. The location of the facility, is both unique and spectacular but is not without its challenges. Because the stage is so down low and close to the River, access from above presents a challenge for guests and for operations via a steep and narrow asphalt path and ramp.

The Little Beach Amphitheater is the venue for the Town's Summer Concert Series, as well as community celebrations and special events. At full capacity the venue can accommodate approximately 250 patrons. Due to its central location and ease of access, along with the Little Beach Park, the Amphitheater is the center piece of the recreation area. As such this plan goes into a bit more detail for the park and amphitheater, and also based on the level of stakeholder and community input received.

#### Minturn Cemetery

Perched on a hillside with breathtaking panoramic views under the Minturn Cliffs is the historical Minturn Riverview Cemetery. The Cemetery sits on a 4 1/2 acre lot located east of Minturn Little Beach Park on Cemetery Road. The Minturn Cemetery District owns and operates the Minturn Cemetery also known as River View Cemetery and Mausoleum. The Minturn Cemetery dates back to the late 1800s and serves the residents of Minturn and surrounding Eagle County communities. Many grave sites are over a century old.



### III. Plan Stakeholders and Facilities (continued)

#### Minturn Cemetery (continued)

The Minturn Cemetery is a public and non-denominational cemetery with both marked and unmarked graves of resident miners, railroad workers, ranchers and farmers, mainly of Hispanic or Scandinavian descent. The facility serves as a historic civic anchor for the study area. It is a calm and peaceful oasis providing opportunity for meditation, prayer, and grieving. The landscape and grounds are well-maintained and give off a sense of community pride and historical significance.

#### Public Works Facility

The Public Works facility sits on 1 1/2 acres located directly adjacent to the Minturn Riverview Cemetery on Cemetery Road. The Minturn Public Works is owned and operated by the Town of Minturn. The Minturn Public Works Department maintains municipal streets, operates the Town water system, services Town-owned buildings and maintenance of Town vehicles. This is an active maintenance

area for the town. Heavy equipment is stored and utilized from this site. Operations (voluntarily) cease when a memorial service at the adjacent cemetery is in progress. The Public Works Facility is centrally located to the core area of Town it serves. The staff takes great pride in the services provided to the Town and they serve as the year-round 'eyes and ears' with a constant civic presence in the study area.

#### Storage Yards

The Minturn Storage Yards are located directly to the East of the Minturn Public Works Facility on a parcel just over 4 acres in size. The Town of Minturn owns the land and leases the use of the land to various businesses for storage as well as the operations of the United Companies batch plant. The leasing of the Storage Yards and the concrete batch plant are an important source of revenue for the Town that is not easily replaced. Therefore, the future use of the yards must be carefully considered within the Town's economic framework. Other uses of this land will require replacing the revenue generated by the storage yards to keep the Town financially stable.



### III. Plan Stakeholders and Facilities (continued)

#### Minturn Bike Park

The Minturn Bike Park is located on Cemetery Road just over a quarter of a mile East of Little Beach Park. The Bike Park was built by the Vail Valley Mountain Trails Alliance and the Town of Minturn with donations and sponsorships from various community partners. The bike park was opened for the first time during the pandemic in August 2020. Some of the popular attractions include a slalom course, pump tract, tabletops, jump lines, and adults plying their wheels over rollers and bridges. The bike parks was built for all ages and abilities. It is open to the public daily from sunrise to sunset, weather depending, from early May to late October/early November. The bike park is closed for the winter season. It will remain closed and is under a seasonal wildlife closure from December 1 – May 1. In its first years of operation the Minturn Bike Park has proven to be a popular amenity and destination within Minturn and for the study area. Many of the park’s users have discovered the Little Beach Park and other facilities in the study area because of the Bike Park, and has catalyzed interest and increase use(s) within the study area.

#### Mini Mile Trail

The Minturn Mini Mile is a short beginner singletrack that starts on the west side of the Minturn Cemetery and ends on the east side of the Minturn Bike Park. The Mini Mile is a multi-use trail for both hiking and biking. The trail is designed as a loop and is great for beginners, families and children learning to ride off road and on classic single-track. The trail was built in 2017 and has around 150-200 feet of elevation change, moderate grades, with some bridge and rock features. The trail has a seasonal wildlife closure December 1 - May 1 when it is closed to all users. The Mini Mile is perhaps the facility within the study area that receives the greatest use on a sustained basis (other than during the winter) The planning team observed hikers and bikers every time while on site, and heard from others how much the trail is used and how important it is for the community.





Minturn Bike Park



Little Beach Amphitheater - Stage and Lawn



Little Beach Park Existing Bathrooms



Little Beach Park Large Parking Lot



Little Beach Park Playground



Little Beach Amphitheater - view from the Stage



Little Beach Amphitheater Stage



Little Beach Park Multi-Use Court and Parking



Minturn Bike Park



Minturn Cemetery viewed from the Mini Mile



Public Works Facility view from the Mini Mile



Little Beach Park Playground



## Existing Facilities and Amenities within the Study Area

## IV. Process

### Planning Process

The Little Beach Recreation Area Plan applied a traditional planning and community engagement process tailored to the Town of Minturn and project stakeholders. The process started in the late summer of 2021 and run through the summer of 2022 with an intended conclusion (in/by) the fall of 2022.

The planning process has paralleled the Town's Community Plan Update, and benefited of some overlap with the SE Group consultant team to align planning efforts. Ultimately, the Little Beach Recreation Area Plan will be a stand alone sub-area plan, with some of the major high-level recommendations referenced in the 'Quality of Life' chapter of the updated Community Plan.

### Community Engagement Events

The following engagement events were conducted as part of the outreach process to collect input from the community and from the major project stakeholders:

- May 5, 2021 - Pop-up at Maloit Park Bike Race
- May 19, 2021 - Pop-up at Mini Mile Bike Race
- June 4, 2021 Pop-up at Art Guild Event
- April 7, 2022 Open House (jointly with Community Plan team)
- May 5, 2022 Virtual Meeting Town Representatives
- May 19, 2022 Site Walk and Meeting
- June 25, 2022 Site Walk and Meeting
- August 2, 2022 Site Visit with Minturn Community Fund representatives
- August 2, 2022 - Minturn Cemetery Board Meeting
- August 4, 2022 - Little Beach Amphitheater Site Visit and meeting with Facility Sound Technician

- August 11, 2022 - Minturn Bike Park site visit and meeting with VVMTA representative
- August 19, 2022 - Virtual Meeting / SE Group Community Plan Update Team
- September 27, 2022 - Open House Share Preliminary Draft (Tentative).
- December 2022 Plan Adoption (Tentative)



Community members review maps of the Study Area





# Little Beach Recreation Area Plan



**TASK ONE**  
**Project Scoping, Start Up**  
**Mapping, Analysis, Inventory**  
 (May 2021 - April 2022)

1. Project Scoping / Start Up Meeting
2. Base-Mapping, Site Inventory and Analysis
  - Site Visits / Walks
  - Aerial Images
  - Photographic Inventory
3. Coordination Meetings / Scheduling

**TASK TWO**  
**Community & Stakeholder Engagement**  
 (April - August 2022)

1. Formal Open Houses / Presentations
2. Stakeholder Meeting(s) / Focused Interviews
3. Guided and self-guided tours of the project area
4. Monthly Town newsletter updates
5. Town Council Presentation(s)
6. Pop ups at Town Events (Concerts)

**TASK THREE**  
**Conceptual Master Planning**  
 (June - September 2022)

1. Preliminary Conceptual Master Plans
  - Hand drawn over aerial images
  - Diagrammatically express circulation, access, subareas
2. Refined/Final Conceptual Master Plan
  - Serves as basis for Sub Area Master Plan
3. Subarea Master Plan Narrative Text
4. Planning Commission Work Session
5. Town Council Work Session

**TASK FOUR**  
**Phase One Design**  
 (July - September 2022)

1. Conceptual Design for Little Beach Park Expansion

Tentative Work Plan - exact dates of meeting to be announced.

APRIL 7, 2022

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MINTURN, CO



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## V. Summary of Input

Through-out the course of the project, the planning team received significant input from multiple stakeholders and individuals. Most of this input was delivered via direct one-on-one engagement during the course of meetings, site visits and events conducted by the team at different stages in the process. In some cases, there was input delivered via written email, in follow-up to some of the public engagement events. This section of the plan summarizes general themes expressed by community members and stakeholders, and then specific input received for each of the key facilities and areas of the planning study.

The input received is then synthesized into the plan recommendations that are covered in Section VI of this report.

### General Themes

Following are some of the key themes from the input received during the course of the project:

1. Support for the vision for a recreation based plan to unify the uses in this area of Town.
2. Greater clarity on the location and primary access to each facility with maps and a way-finding system for orientation.
3. New connections and linkages between this area of town with other neighborhoods, and new connections between facilities within the study area.
4. Overall need to increase the level of safety of the area for pedestrians and cyclists as there is more activity in the area and sometimes overlapping activities.
5. Improvements to existing park and recreation facilities and new recreation facilities to enhance the overall user experience and create greater synergy between uses.
6. Celebrate the history of Town in the area via the Cemetery presence and the railroad historic use.
7. More emphasis on the Eagle River as a key natural amenity within the study area.
8. Bathroom facilities in a central location serving the area, to replace the existing bathrooms at the park.
9. Secondary access into the area for emergency service and additional connectivity.
10. Trail connection(s) to the Two Elk trail in addition to the existing road, to improve overall safety.
11. New pedestrian bridge across the Eagle River on south end of Town to connect to the recreation area.



Community members attend the Open House and provide input to the planning team



## V. Summary of Input (continued)

### Summary of Input on Specific Facilities

This section summarizes input received for each of the specific facilities and areas of use within the study boundary. The input is listed in no particular order of priority.

#### Little Beach Park

1. Eliminate the driveway and small parking lot on the north side of the Park. Convert these spaces to park uses.
2. Upgrade or replace the bathrooms with new ones.
3. Update the playground equipment.
4. Create a new multi-use court in the location of parking and the current bathrooms.
5. Add additional park amenities such as picnic tables, benches, and site furnishings.
6. Enhance access to Eagle River while promoting safety during high water times of year.
7. Confirm stability of timber retaining wall.
8. Integrate educational elements into the Park.

#### Little Beach Amphitheater

1. Expand and add more lawn seating area.
2. Re-grade the access path from the upper parking lot to the stage so it is not as steep for service, operations, and for guests. Make it ADA accessible.

3. Develop new bathroom and concessions facilities at the top, near the main parking lot.
4. Create a main entrance that is a clear point of arrival into the amphitheater.
5. Add a green room to the back of the stage that is covered and provides shade and cover from rain.
6. Add power to the large parking lot.
7. Add power plugs on the stage.
8. Consider a loading dock on the west side of the stage.
9. Add vehicle access from the west for loading and unloading.
10. Enhance and formalize the main upper parking lot serving the Amphitheater.

#### Minturn Cemetery

1. Need for expansion of the Cemetery, to the west or to the east for future burial areas, based on limited inventory of gravesites.
2. Use Cemetery as vehicle to promote and educate community on Town's and region's history.
3. Pave the road in front of the Cemetery.
4. Screen between Cemetery and the Public Works facility to address compatibility of facilities.
5. Enhance the character of the Mini Mile Trailhead and Public Utilities shed at the north end of the Cemetery.
6. Build new maintenance shed to protect maintenance and burial equipment during winter conditions.



View of the Cemetery in the foreground and Public Works in the background from the Mini Mile Trail



## V. Summary of Input (continued)

### Public Works Facility

1. Keep the facility where it is currently located, due to the central location and proximity to areas of Town it serves.
2. Bunker the Public Works Facility into the hillside and conceal it from view as much as possible.
3. Potential Public Works Facility expansion to the East or South for additional storage and operations.
4. Screen the facility from adjacent uses, especially the Cemetery.
5. As a long-term goal, consider relocating some of the operational uses elsewhere in Town, if there is a viable possibility that presents itself that makes sense.

### Storage Yards / Batch Plant

1. Over time, reclaim some or all of the leased storage yards as future park uses compatible with the Bike Park, Mini-Mile trail and other recreational activities in the area.
2. Create a large green park space for recreation multi-use compatible with the bike park and to satisfy a community need as it does not exist elsewhere within walking distance from the major residential neighborhoods.

3. When feasible economically, phase out the leasing of storage yards and convert the lands to recreation uses more compatible with the surrounding land-uses.

### Minturn Bike Park

1. Park is largely built out and not planning to expand.
2. Add more shade into the future with greater use of the park.
3. Enhance the entrance into the park.
4. Organize and formalize the parking lot so it is more efficient.
5. Create a large lawn green space next to the Park.

### Mini-Mile Trail

1. Improve signage at the northern trailhead.
2. Place trail on way-finding maps of area.
3. Connect trail to future trails, especially to the south toward Two Elk.
4. Ensure wildlife closures are obeyed in order to protect local wildlife, especially elk.



Storage Yards on Town of Minturn leased lots and the concrete batch plant and Public Works in the background.

## VI. Plan Recommendations

This section of the plan summarizes the plan recommendations based on the input received. The recommendations for the overall study area are expressed graphically and spatially in the Conceptual Framework Plan for the study on the following page, and the Concept Plan for the Park and Amphitheater.

### General Plan Recommendations

#### 1. Wayfinding and Directional Signs and Information

**Kiosk(s)** for the area, with a central orientation point at the parking area above Little Beach Amphitheater.

2. Centrally located **year-round bathrooms** located at the top of the Little Beach Amphitheater to serve visitors to the area and support events at the Amphitheater.

3. New pedestrian access from the Cemetery Bridge via a **Riverside Trail** to access the park and playground, and amphitheater.

4. A **new trail connection** between the Amphitheater continuing south to the Cemetery and Bike Park, and to give users an alternative to walking on the road alongside vehicular traffic.

5. A **secondary access into the study area from Hwy 24**, with a bridge across the Eagle River and connecting into the project area. This is intended to provide additional access for emergency response and a new pedestrian connection. Ideally this crossing would be established near the Eagle County Boneyard Open Space and the Maloit Park road to provide connectivity between existing open space and park uses.

6. **Expansion of the Little Beach Amphitheater** area to the south (reference additional detail and discussion).

7. **Future park and recreation facilities adjacent to the Bike Park**, at a future time when the leasing of the storage yards is no longer necessary.

8. **Coordinated expansions of the Minturn Riverview Cemetery and Public Works Facility.** Stakeholders from both facilities expressed the importance of their location and facility within the study area and the need for expansion into the future. The Town owns ample land to the east of the Cemetery, and to the north and east of the Public Works Facility. If the Batch Plant goes away, it would be a natural area for expansion of the Public Works Facility, given the adjacency and gentle slopes of the site. The area north of Public Works, and east of the Cemetery is steeper terrain that would lend itself to expansion of the Cemetery, given the current layout of plots for gravesites and adjacency in place. This plan does not recommend a particular expansion for either facility, and Town Council will ultimately decide if any expansions will occur in the future. However, the plan acknowledges that both facilities expressed the need for future expansion and the Town owns land in the area that appears to be suitable for expansion of both facilities in a coordinated manner.

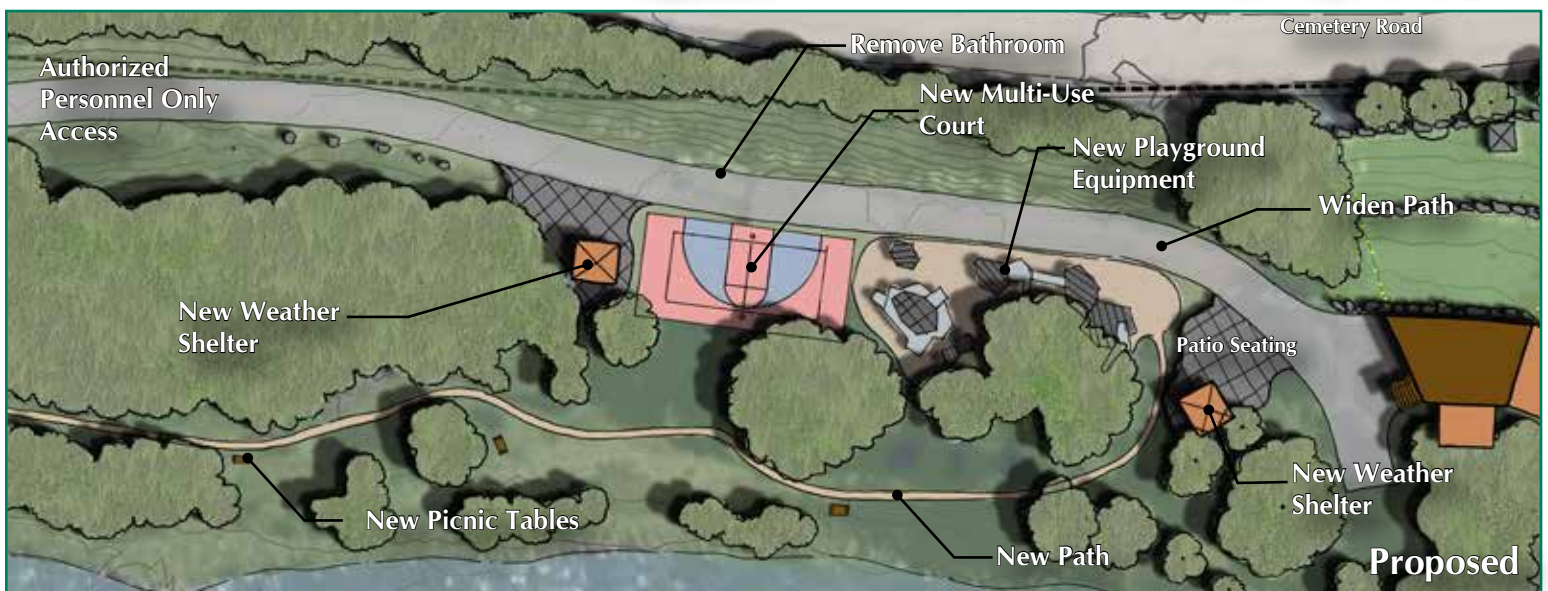


# VI. Plan Recommendations

## Plan Recommendations for Specific Facilities

### Little Beach Park

1. Eliminate the driveway and small parking lot on the north side of the Park. Convert these spaces to park uses.
2. Upgrade or replace the bathrooms with new ones.
3. Update the playground equipment.
4. Create a new multi-use court in the location of parking and the current bathrooms.
5. Add additional park amenities such as picnic tables, benches, and site furnishings.
6. Enhance access to Eagle River while promoting safety during high water times of year.
7. Confirm stability of timber retaining wall.
8. Integrate educational elements into the Park.



# VI. Plan Recommendations

## Little Beach Amphitheater

1. Expand and add more lawn seating area.
2. Re-grade the access path from the upper parking lot to the stage so it is not as steep for service, operations, and for guests. Make it ADA accessible.
3. Develop new bathroom and concessions facilities at the top, near the main parking lot.
4. Create a main entrance that is a clear point of arrival into the amphitheater.
5. Add a green room to the back of the stage that is covered and provides shade and cover from rain.
6. Add power to the large parking lot.
7. Add power plugs on the stage.
8. Consider a loading dock on the west side of the stage.
9. Add vehicle access from the west for loading and unloading.
10. Enhance and formalize the main upper parking lot serving the Amphitheater.



# VI. Plan Recommendations



Existing Playground



Existing Bathrooms



Existing Path to Stage with Potential Realignment



Existing Stage with Potential New Loading Dock



Existing Path to Park



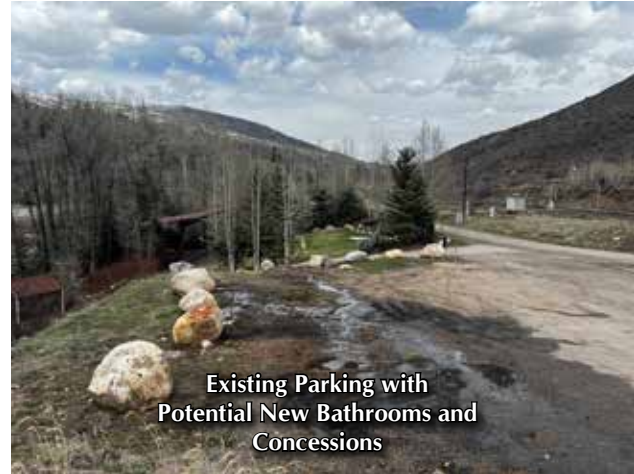
Existing Stage with Potential Expansion



Existing Back of Stage with Potential Greenroom



Existing Basketball Court



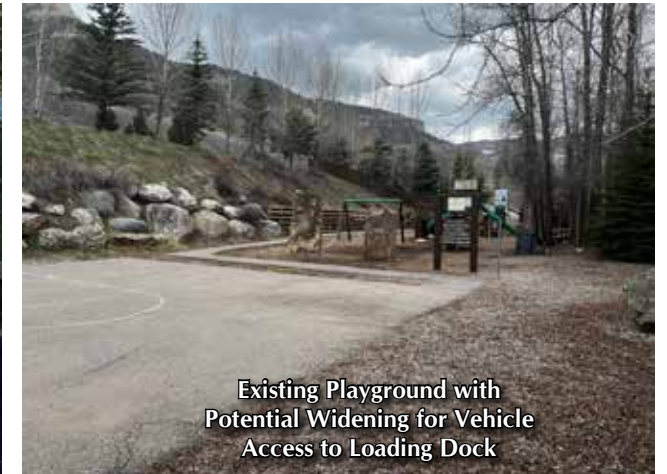
Existing Parking with Potential New Bathrooms and Concessions



Existing Lawn Seating



Existing Playground with Potential Widening for Vehicle Access to Loading Dock



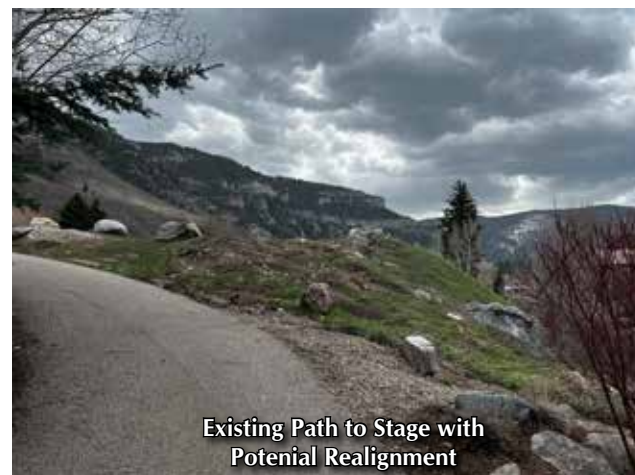
Existing Playground with Potential Widening for Vehicle Access to Loading Dock



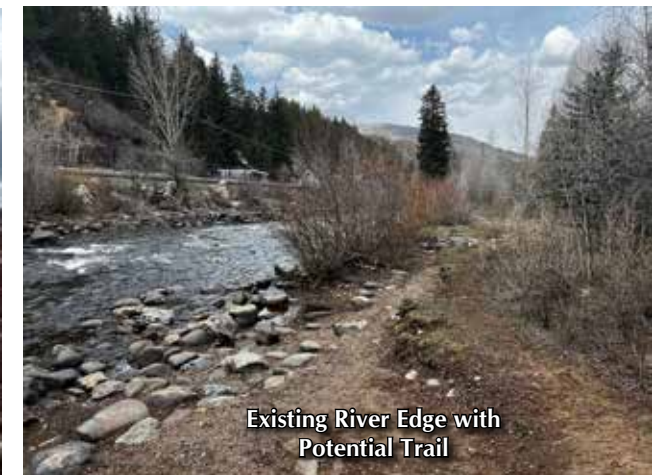
Existing Playground with Potential Widening for Vehicle Access to Loading Dock and New Multi-Use Court



Existing Playground



Existing Path to Stage with Potential Realignment



Existing River Edge with Potential Trail

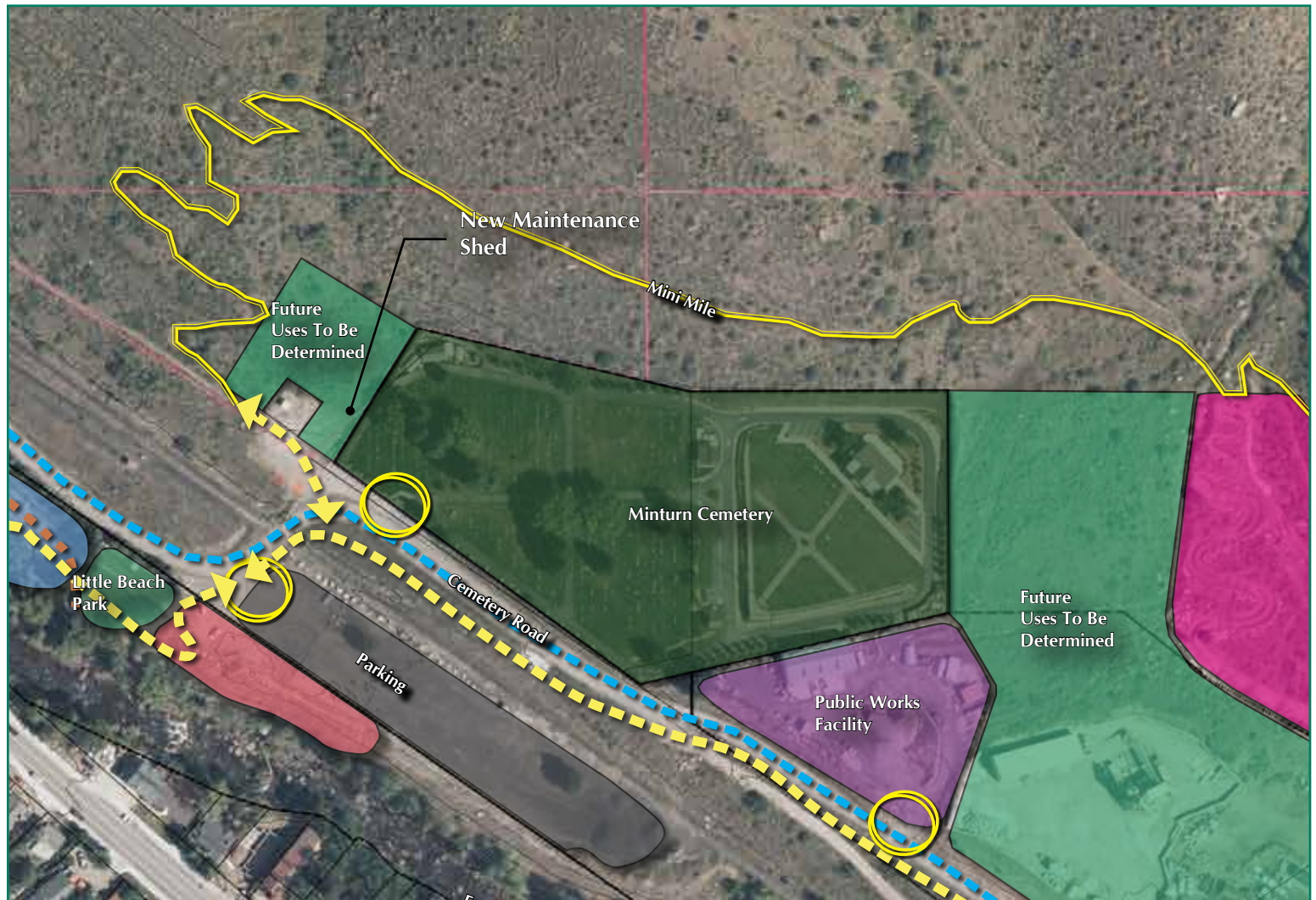




# VI. Plan Recommendations

## Minturn Cemetery

1. Potential expansion of the Cemetery to the east or to the west for future burial areas, based on limited inventory of gravesites. Would require the review and approval by Town Council and involve a sale or lease of the land.
2. Use Cemetery as vehicle to promote and educate community on Town's and region's history.
3. Pave the road in front of the Cemetery.
4. Screen between Cemetery and the Public Works facility to .
5. Enhance the character of the Mini Mile Trailhead and Public Utilities shed at the north end of the Cemetery.
6. Build new maintenance shed to protect maintenance and burial equipment during winter conditions.



Potential expansion areas for the Public Works Facility and the Minturn Cemetery



# VI. Plan Recommendations

## Public Works Facility

1. Keep the facility where it is currently located, due to the central location and proximity to areas of Town it serves.
2. Bunker the Public Works Facility into the hillside and conceal it from view as much as possible.
3. Potential Public Works Facility expansion to the East or North for additional storage and operations.
4. Screen the facility from adjacent uses, especially the Cemetery.
5. As a long-term goal, consider relocating some of the operational uses elsewhere in Town, if there is a viable possibility that presents itself that makes sense.

## Storage Yards / Batch Plant

1. Over time, reclaim some or all of the leased storage yards as future park uses compatible with the Bike Park, Mini-Mile trail and other recreational activities in the area.
2. Create a large green park space for recreation multi-use compatible with the bike park and to satisfy a community need as it does not exist elsewhere within walking distance from the major residential neighborhoods.
3. When feasible economically, phase out the leasing of storage yards and convert the lands to recreation uses more compatible with the surrounding land-uses.
4. Consider terminating the Batch Plan lease and use as a site for expansion of Public Works.



Potential expansion areas for the Public Works Facility and Long-term use of the Storage Yards



# VI. Plan Recommendations

## Minturn Bike Park

1. Park is largely built out and not planning to expand.
2. Add more shade into the future with greater use of the park.
3. Enhance the entrance into the park.
4. Organize and formalize the parking lot so it is more efficient.
5. Create a large lawn green space next to the Park.

## Mini Mile Trail

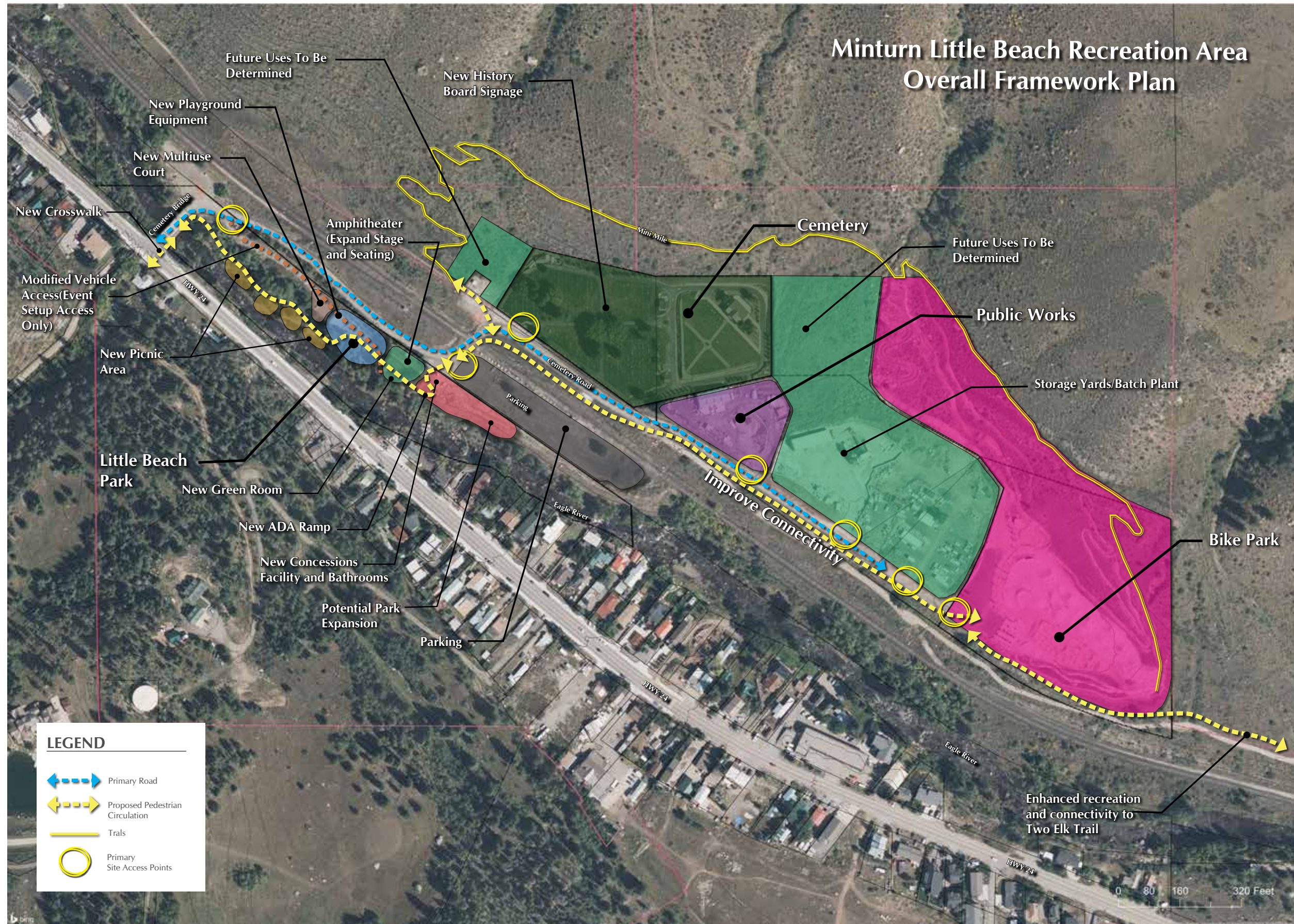
1. Improve signage at the northern trailhead.
2. Place trail on way-finding maps of area.
3. Connect trail to future trails, especially to the south toward Two Elk.
4. Ensure wildlife closures are obeyed in order to protect local wildlife, especially elk.



Potential Long-term Use of the Minturn Bike Park and Mini Mile Trail Improvements



# VI. Plan Recommendations



# VI. Plan Recommendations



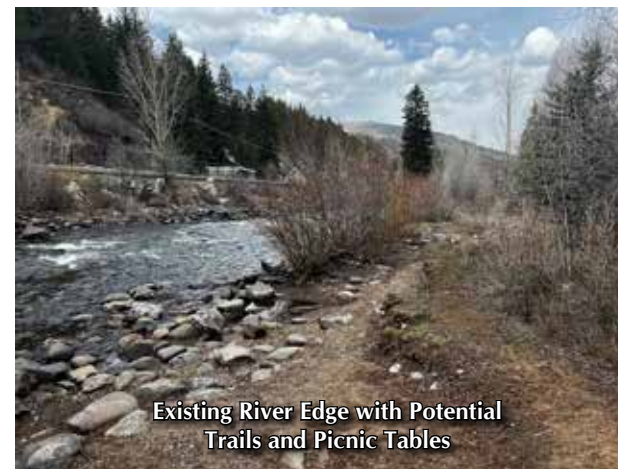
## Little Beach Park



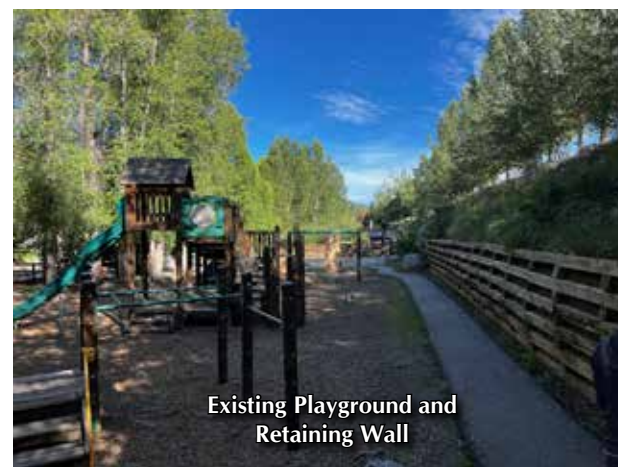
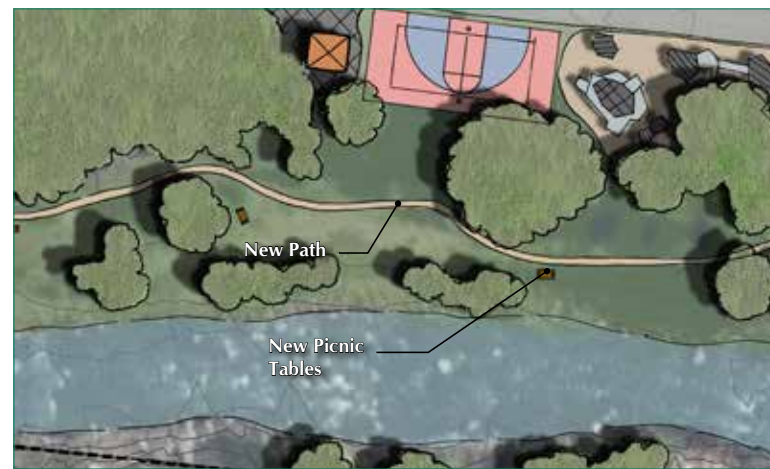
Existing Access Into Park



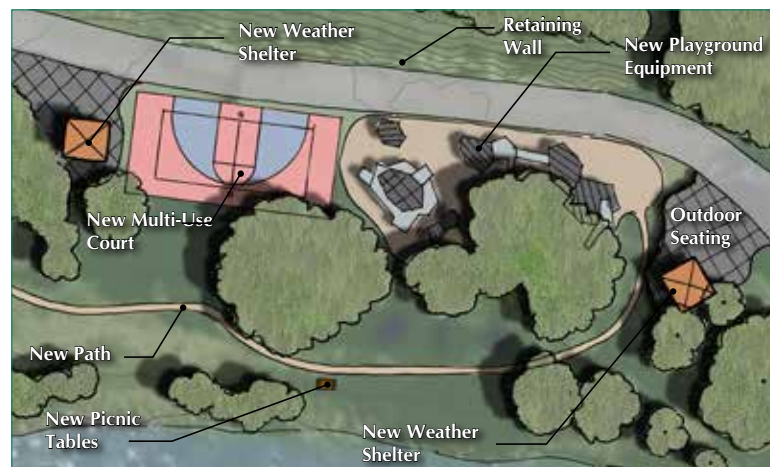
Existing Playground



Existing River Edge with Potential Trails and Picnic Tables



Existing Playground and Retaining Wall



- Eliminate the use of the small parking lot on the north side of the Park. Convert this space to park uses.
- Upgrade or replace the bathrooms with new ones.
- Create a new multi-use court in the location of the parking and the current bathrooms.

- Update the playground equipment and re-arrange the layout to allow a service drive connection to the back of the covered stage.

- Consider new spots for picnic tables along the driveway route and facing the River. Consider adding interpretive environmental education stations along the corridor to engage with the River.

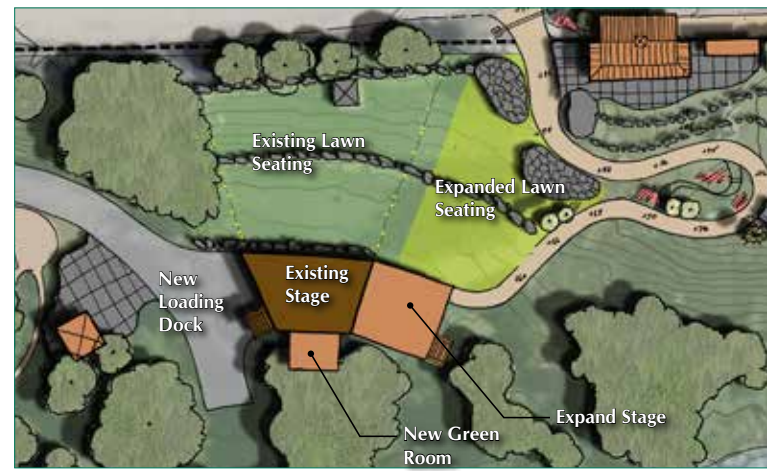
- Add additional park amenities such as picnic tables, benches, and site furnishings.
- Confirm stability of timber retaining wall.



## Little Beach Amphitheater



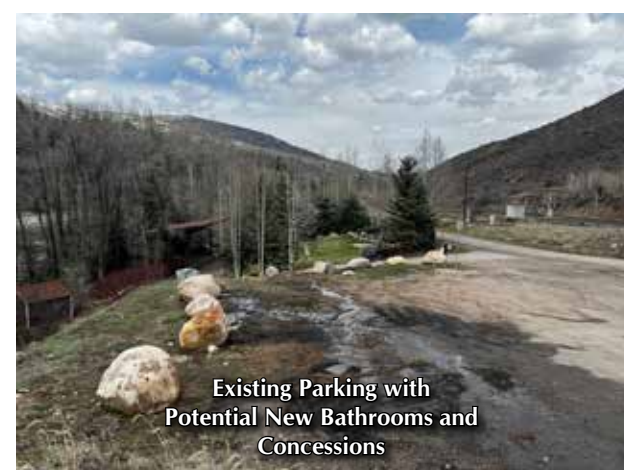
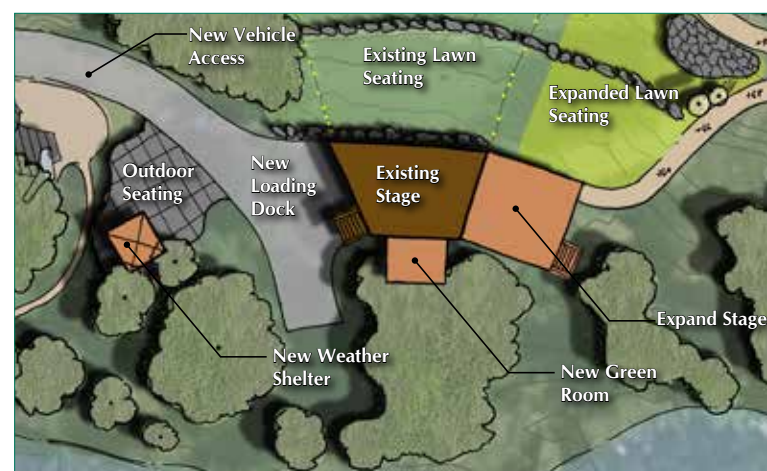
Existing Stage with Potential Expansion



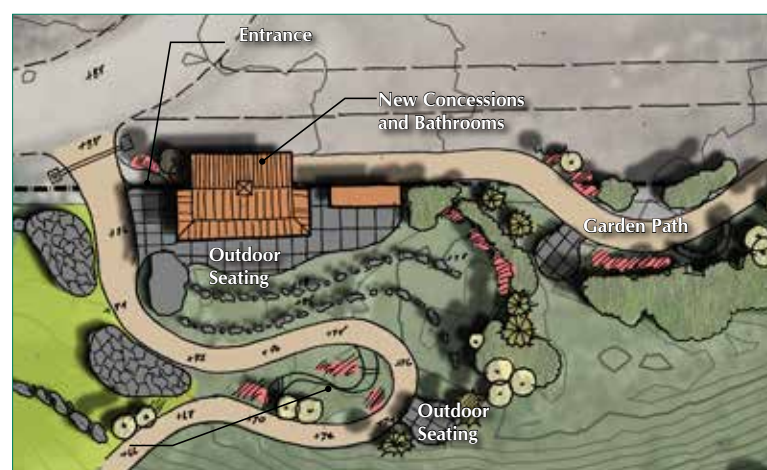
Existing Path to Stage with Potential Realignment



Existing Stage with Potential New Loading Dock



Existing Parking with Potential New Bathrooms and Concessions



- Expand and add more lawn seating area to the east by cutting into and terracing the existing hillside, similar to the existing lawn seating.
- Expand the stage to the south with an additional wing and platform.
- Re-grade the access path from the upper parking lot to the stage so it is not as steep for service, operations, and for guests. Make it ADA accessible.
- Develop new bathroom and concessions facilities at the top, near the main parking lot.
- Create a main entrance that is a clear point of arrival into the amphitheater and can serve as ticket check during ticketed events.
- Add a green room to the back of the stage that is covered and provides shade and cover from rain.
- Add power to the large parking lot.
- Add power plugs on the stage.
- Add vehicle access from the west for loading and unloading in coordination with the playground equipment replacement and a new layout.
- Enhance and formalize the main upper parking lot serving the Amphitheater.



# VI. Plan Recommendations





# VI. Plan Recommendations



## VII. Conclusion

The Little Beach Recreation Plan seeks to develop a more integrated and interconnected recreation area with the developed areas of Town for the use and enjoyment of residents and visitors. Ultimately, the plan seeks to protect the scenic and natural value of the area and leverage its potential as open space and recreation for the long-term environmental and economic sustainability of the Town.

The land ownership in the area is a mix of federal, private, and Town-owned lands. Therefore the implementation of the plan will require cooperation and coordination between these stakeholder entities to establish new trails, access points, and services throughout the area.

Multi-party easements and land use agreements are likely to be needed to 'pull-off' some of the big picture aspirations of the Town to create a unified plan for the Little Beach area. The benefit in doing so is a more cohesive framework for all land owners, improved access for use and maintenance and emergency services, and a more accessible and better connected experience for the community and its guests.

This plan is intended to start a new vision for this area of Town, and should be revisited and amended as necessary, based on changing conditions and to best accommodate and meet the needs of the Town's residents.

**- End of the Plan -**





To: Mayor and Council  
From: Madison Harris, Planner I  
Date: December 22, 2022  
Agenda Item: Resolution 4 - 2023 establishing procedures to govern action of commissions in the general conduct of business and to serve as a reference in settling parliamentary disputes

---

**REQUEST:**

Review and approve Resolution 4 - 2023 establishing procedures to govern action of commissions in the general conduct of business and to serve as a reference in settling parliamentary disputes.

**INTRODUCTION:**

As referenced in the memo from Karp Neu Hanlon that is attached, Council recently passed a resolution establishing procedures for conduct of meetings. As the Planning Commission and Historic Preservation Commission have different roles than the Town Council, it was deemed beneficial to amend Resolution 35 - 2022 to better reflect how the commissions should run meetings and how to address the business that comes before them.

**ANALYSIS:**

The Planning Commission reviewed this resolution at their December 14, 2022 regular meeting and forwarded a recommendation of approval to the Town Council. The Historic Preservation Commission reviewed this resolution at their December 20, 2022 regular meeting, and also forwarded a recommendation of approval to the Town Council.

**COMMUNITY INPUT:**

Ongoing

**BUDGET / STAFF IMPACT:**

TBD.

**STRATEGIC PLAN ALIGNMENT:**

**PRACTICE FAIR, TRANSPARENT AND COMMUNICATIVE LOCAL GOVERNMENT**

THE TOWN WILL SEEK TO MAKE INFORMED, DATA-BASED DECISIONS WITH A STANDARD OF “DOING IT RIGHT.” WITH AN HONEST APPROACH TO ALL ASPECTS OF LOCAL GOVERNMENT AND A FOCUS ON THE PUBLIC PROCESS, THE TOWN COUNCIL AND STAFF ARE COMMITTED TO SERVING MINTURN WITH THE HONESTY AND INTEGRITY EXPECTED OF A SMALL-TOWN GOVERNMENT.

**ADVANCE DECISIONS/PROJECTS/INITIATIVES THAT EXPAND FUTURE OPPORTUNITY AND VIABILITY FOR MINTURN**

The ability for Minturn to approach development as **resilient, sustainable, creative and diverse** will allow the town to continue embracing what has “**made Minturn, Minturn.**” The town can further leverage its crossroads location as a valley-wide benefit and **competitive advantage**.

**RECOMMENDED ACTION OR PROPOSED MOTION:**

1. Approve Resolution 4 - 2023 establishing procedures to govern action of commissions in the general conduct of business and to serve as a reference in settling parliamentary disputes.

**ATTACHMENTS:**

- Memo from Karp Neu Hanlon
- Resolution 4 - 2023 establishing procedures to govern action of commissions in the general conduct of business and to serve as a reference in settling parliamentary disputes.

Glenwood Springs – Main Office

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P. O. Drawer 2030  
Glenwood Springs, CO 81602

Aspen

0133 Prospector Rd.  
Suite 4102J  
Aspen, CO 81611

Montrose

1544 Oxbow Drive  
Suite 224  
Montrose, CO 81402

Office: 970.945.2261

Fax: 970.945.7336

*[\\*Direct Mail to Glenwood Springs](#)*

DATE: December 9, 2022  
TO: Minturn Planning Commission  
FROM: Karp Neu Hanlon, P.C.  
RE: Commission Conduct and Procedures Resolution

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Last month, Town Council adopted Resolution No. 2022 - 35, which replaced Resolution No. 2008 – 41 adopting procedures and standards of conduct for Council. The resolution in your packet comes in similar form and content to the Council resolution, but is tailored to the jurisdiction and practice of the commissions in the Town. It is intended as a code of conduct and procedural rules for handling the most common agenda items that commissions consider – namely applications for approvals, and text amendments to the municipal code. This resolution at this time would apply only to the Planning Commission and the newly constituted Historic Preservation Commission, but if others are created pursuant to Charter Article 7, this would apply to them as well.

The rules and guidelines in this document are intended to foster efficient and civil meetings that provide adequate due process to the public and applicants. Importantly, appropriate conduct in meetings is vital in protecting the Town from legal challenges to decisions. We will be discussing these issues further during the Commission meeting, including a review of public entity ethics and the distinction between legislative and quasi-judicial proceedings and decisions.

We are asking that Planning Commission consider recommending this resolution for adoption by Town Council.

TOWN OF MINTURN, COLORADO

RESOLUTION NO. 4 - SERIES 2023

A RESOLUTION ESTABLISHING PROCEDURES TO GOVERN ACTIONS OF COMMISSIONS FOR THE TOWN OF MINTURN IN THE GENERAL CONDUCT OF BUSINESS AND TO SERVE AS A REFERENCE IN SETTLING PARLIAMENTARY DISPUTES.

WHEREAS, the Home Rule Charter for the Town of Minturn provides at Section 4.8 that the Town Council shall determine the rules of procedure governing meetings; and

WHEREAS, by Resolution No. 35, Series 2022 the Town Council established Procedures to govern actions of the Town Council in the general conduct of business and as a reference in settling parliamentary disputes; and

WHEREAS, there exist within the Town government duly established commissions that make decisions and recommendations to the Town Council on their respective areas of expertise; and

WHEREAS, Town Council wishes to adopt a similar resolution on the appropriate conduct of Commission members; and

WHEREAS, the Town Council has determined that it is in the best interest of the Commissions of the Town of Minturn to have established procedures for the general conduct of its business and to serve as a reference to settle parliamentary disputes.

NOW THEREFORE, BE IT RESOLVED BY THE TOWN COUNCIL OF THE TOWN OF MINTURN, COLORADO:

**Section 1.** The foregoing recitals are incorporated herein as if set forth in full.

**Section 2.** Adoption of Policy.

**CONDUCT OF COUNCIL MEETINGS**

The Town Council hereby adopts the following procedures for conduct of business at Commission Meetings. For the purposes of this policy, "Commission" shall mean a board or commission duly constituted under Article 7 of the Town's Home Rule Charter. This procedure shall govern the conduct of all Commission Meetings, however, in handling routine business, the Commission may, by general consent, use a more informal procedure than that set forth in this procedure.

I. **Code of Conduct.** Commission Members shall follow this code of conduct to ensure safe, orderly, and professional atmosphere during all business conducted by the Town. Violation of this code of conduct may result in censure by the Town Council.

A. Commission Members shall at all times conduct themselves in a professional manner. Member comments shall be constructive and not be attacks on the Public, Staff, or other Commission Members. Members shall respect the floor, not interrupt, and follow reasonable and appropriate direction of the presiding officer during meetings.

B. All communications with staff, the Town Attorney, and Town consultants shall be coordinated with the Town Administrator.

C. Any meeting requested by a Commission Member with subordinate staff related to Town business shall be arranged through the Town Administrator.

II. **Presiding Officer.** The Chair is responsible for conducting meetings in an orderly and democratic manner and assuring that minority opinion may be expressed and that the majority is allowed to rule. At the same time, the Chair retains all of the prerogatives of a duly appointed commission member; the Chair may make and second motions and take part in discussions and must vote on all matters not involving the Chair's personal financial interest or the Chair's official conduct.

III. **Agenda.**

A. Items are placed on the agenda by the staff in consultation with the Chair. The order and priority of agenda items are determined in part by the processing of relevant applications and staff recommendations.

B. The general order of the agenda shall be generally as follows:

1. Call to order by the Chair.

2. Determination of quorum. Roll Call and Pledge of Allegiance.

3. Approval of the minutes of preceding meetings.

4. Approval of regular agenda. This provides opportunity to amend the agenda. No items may be added to the regular agenda that require public notice under the Code or the laws of the State of Colorado.

5. Declaration of Conflicts of Interest. Opportunity for Commission members to declare any known conflicts of interest pursuant to Charter Section 4.9 and Code Chapter 2 Article 4.

6. Public Comment (5 minute limit per person, on a first come first served basis).

Citizen participation is a time set aside for citizens to address the Commission concerning Commission related business not otherwise on the agenda. The Commission reviews citizen participation and assures that an appropriate response is given if the Commission determines that a response is required. Such review is usually immediately following the citizen participation. Staff and Commission responses are discouraged at the meeting, except for referral to the staff for further analysis and reports and ultimate Commission decisions on a future agenda. Chair may ask Commission if direction shall be given to staff.

7. Special Presentations. Government officials, community organizations, and similar interests may be invited to present on topics relevant to the Commission. These presentations are limited to 5 minutes unless prior arrangements are made with the relevant staff and approved by a majority of Commission.
8. Consideration of action items. Expected substantial public comment items pertaining to Commission actions are generally placed first on the agenda, but critical short items or items of extreme public interest may be placed first when deemed appropriate by staff and by the Chair.
9. Discussion/Direction. Commission may discuss items where staff is requesting direction that does not require a majority vote of the Commission.
10. Reports from Town Staff.
11. Commission Comments. At this point, any Commission member may place before the Commission matters which are not included in the formal agenda. This item is generally limited to responses to citizen participation, sharing of information, and requests for advice concerning matters pending before other bodies. No decision may be made based on a Commission Comment.

Matters requiring a formal Commission vote are normally placed on the agenda through the regular agenda review process, rather than dealt with under this item.

12. Concluding statement by Chair;
13. Future agenda items.
14. Adjournment. The Commission's goal is that all meetings be adjourned by 10:30 p.m. An agenda check will be conducted at or about 10:00 p.m. Generally, absent a deadline which the Commission cannot affect, no new substantial item will be addressed after 10:30 p.m. unless two-thirds of the Commission members in attendance at that time agree. All Commission meetings shall be adjourned at or before 11:00 p.m. unless two-thirds of the Commission members in attendance, by motion duly adopted, extend that deadline. Items not completed prior to adjournment will be continued to a date certain.



IV. **Rules of Speaking.**

- A. To obtain the floor, a Commission member or staff member addresses the Chair.
- B. To assign the floor, the Chair recognizes by calling out the person's name. Only one person may have the floor at a time. A person shall not speak while another has the floor. The Chair generally next recognizes the person who first asks for the floor after it has been relinquished.
- C. During citizen participation or public hearings, members of the public are recognized by the Chair. No person, other than staff, applicant, or invited presenter shall make a presentation (not including Council questions) longer than 5 minutes, unless given permission by majority direction of the Commission to extend the time limit.
- D. All Commission members, staff members and members of the public shall direct their remarks to the Commission action that they are requesting. Speakers engaging in personal attacks shall be gavelled out of order by the Chair. The staff and the Chair will attempt to focus discussion of agenda items in accordance with the materials which should contain a proposed outline of decisions.
- E. Staff presentations shall occur first and not be subject to any time limitations.
- F. Applicant presentations in a quasi-judicial proceeding shall occur after Staff presentation and shall not exceed 15 minutes unless provided for by majority vote of the Commission. Additional support from applicant's positions may come from additional witnesses during public comment.
- G. Commission Members will generally not speak during staff and applicant presentations. Commission Members may ask questions of the Staff and the Applicant after their presentations.
- H. The Chair shall open the public comment of the public hearing. Speakers shall not exceed 5 minutes each. All comments shall be directed to the Chair. Commission Members shall not engage directly with members of the public except to request clarification from members of the public.
  - 1. Permission may be granted by the Commission for members of the public to pool their time if the Commission determines that time can be saved and issues better addressed in order to facilitate public participation in Commission decision making. Speakers who desire to pool their time will not be granted the full pooled total, but a portion determined by the Commission in light of the complexity of the issues to be addressed and the projected time saved from the pooling. All persons wishing to pool their time must be present at the meeting in order for the Commission to recognize pooled time. No pooled time presentation will be permitted to exceed 20 minutes total.

- I. After public comment is closed, the Applicant shall have the opportunity to respond to points made during the public comment and shall not exceed 15 minutes.
- J. After the Applicant, Staff, and Public Comment are finished, Commission Members shall have the opportunity to present questions to the Applicant and Staff. Questioning shall be rotated so that to the extent practicable, a different Commission member goes first on each agenda item. Each Commission Member shall have 5 minutes during each turn.
- K. Commission members shall not debate matters prior to a motion being presented on the matter under consideration. The Chair may intervene to avoid extended debate prior a motion being presented. Commission members may also request intervention by making a point of order.
- L. Tabling or continuing motions are generally discussed before they are made, in order to allow for a reasonable amount of Commission discussion prior to making a nondebatable motion.
- M. The Chair may intervene in Commission debate in order to determine whether Commission wishes to postpone Commission action if more information or staff work appears warranted to facilitate a Commission decision.

V. **Procedure in Handling Motions.**

- A. A Commission member, after obtaining the floor, makes a motion. (If long or involved, the motions should be in writing). The Commission member may state reasons briefly before making the motion; but may argue the motion only after it has been seconded.
- B. Another Commission member seconds the motion. All motions require a second, to indicate that more than one member is interested in discussing the question. The person seconding the motion does not, however, have to favor the motion in order to second it, and may both speak and vote against it. If there is no second the Chair shall not recognize the motion.
- C. The Chair states the motion and asks for discussion.
- D. General debate and discussion follow, if desired. Each Commission Member shall have 5 minutes in which to make their remarks. The Commission Member who made the motion may not speak again until Commission Members who wish to be heard had the opportunity to speak. Commission Members, the Town Administrator, or the Town Attorney, when wishing to speak, follow the rules of speaking outlined above. Remarks should be addressed to the Chair.

- E. The Chair restates the motion and puts the question. Negative as well as affirmative votes are taken. In the case of a tie vote, the motion is lost.
- F. The Chair announces the result. The motion is not completed until the result is announced.

VI. **Voting.**

Voting ultimately decides all questions. A roll-call vote shall be taken upon the approval of all action items (i.e. proposed text amendments, resolutions, quasi-judicial proceedings). For other matters the Commission may use any one of the following methods of voting.

- A. Voice Vote: All in favor say “AYE”, and all opposed say “NO”. If the Chair is in doubt of the result of a voice vote the Chair may call for a roll call vote. If any Commission member is in doubt of the result of a voice vote, the Commission member may obtain a roll call vote by calling for it (without the need to be recognized by the Chair).
- B. Roll Call: The Chair (or Clerk at the Chair’s request) calls the roll of the Commission members and each member presents votes “aye” or “no” as each name is called.

VII. **Research and Study Sessions.**

- A. Information/Research Requests. Requests for information or for a briefing should be directed to the lead staff member assigned to the Commission, who shall direct the request to the appropriate staff member or the Town Attorney. A single commission member may request the appropriate lead staff member to provide available information or to answer any question concerning an agenda item. A majority vote of Commission members is required to assign a matter for research by staff, an attorney, or consultant.
- B. Work Sessions. Material for work sessions generally will be made available to the Commission and the public at least twenty-four hours before the date of the work session. Notice will be given as for other Commission meetings. Work Sessions are open to the public. Testimony of persons other than staff or invited guests is not permitted. The Commission will give direction to staff at work sessions for the presentation of action items at future regular Commission meetings. Summaries of work sessions are placed on the Commission agenda for approval, including the direction given, any remaining issues, and any staff reaction or proposed plan in response to the work session.

VIII. **Parliamentary Procedure.**

Except as otherwise provided herein, all matters of procedure are governed by Bob’s Rules of Order, Simplified Parliamentary Rules of Order for Colorado Local Governments

(2020), as amended.

IX. **Executive Sessions.**

All executive sessions shall comply with the provisions of the Colorado Open Meetings Law. Additionally, Commission Members shall each have the floor during an executive session for 5 minutes before the Chair calls on the next Member for comment. The time that a Commission Member may have the floor during an executive session may be enlarged by majority vote of the Commission.

INTRODUCED, READ, APPROVED AND ADOPTED this 4th day of January 2023.

TOWN OF MINTURN

By: \_\_\_\_\_  
Earle Bidez, Mayor

ATTEST:

\_\_\_\_\_  
Jay Brunvand, Town Clerk

# MINTURN



## COMMUNITY PLAN

*-Imagine Minturn-*

# MEMORANDUM

**TO:** Town of Minturn Town Council

**FROM:** Julia Randall, SE Group

**DATE:** 12/15/22

**RE:** 12/21/22 Meeting Information

This memo provides an overview of updates to the Minturn Community Plan draft following a joint meeting with Town Council and Planning & Zoning Commission on September 21<sup>st</sup> and two follow-up meetings with the Planning & Zoning Commission (October 12<sup>th</sup> and 26<sup>th</sup>). A full revised version of the plan is provided in the meeting packet. Changes to the plan since September 21<sup>st</sup> are noted in track changes. A “clean” version of the plan is also available at [this link](#). Recognizing that track changes can make a document more difficult to read, we have provided this clean version so that the Council can reference a more readable copy of the proposed plan text and graphics.

## About the Community Plan

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The Minturn Community Plan is a visioning document that synthesizes public input, technical research, mapping, and graphics. The document summarizes existing issues, strengths, and priorities for the Town and is intended to serve as a guide to policymaking in the Town of Minturn for the next 10+ years. The adoption of a Community Plan and its associated recommendations is not a rezoning process. Rather, the Planning Commission and Town Council should use the recommendations of the Community Plan to inform future amendments to zoning during the useful life of the plan.

## Overview of Feedback

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The project team has incorporated feedback from the Planning & Zoning Commission, Town Council, stakeholder organizations, Eagle County, Town staff, and the general public. The project team discussed major feedback with the Planning & Zoning Commission on October 12<sup>th</sup> and 26<sup>th</sup>. As noted in the tables below, the project team made several changes to the document at the direction of the Planning Commission. **These recommended changes are noted in purple, green, orange, and blue in track changes.** The difference in color in these edits is not significant – these different colors are associated with different members of the consultant team editing the document at different times. Some feedback that was received did not lead to proposed changes.

The project team would like to thank the Planning Commission and Town Council for their thoughtful feedback to date and looks forward to hearing the Council’s thoughts on the updated draft.

As described in the next section, Town staff conducted a final review of the draft plan and recommended changes to several chapters. **These recommended changes are noted in red in track changes.**

## Overview of Changes Recommended by Town Staff

---

Following meetings with the Planning & Zoning Commission on October 12<sup>th</sup> and 26<sup>th</sup>, Town staff conducted a thorough review of the document and recommended additional changes to the text. These changes are intended to incorporate more precise and up-to-date language about the status of PUDs, unit projections, and infrastructure capacity in Minturn. Town staff have also incorporated proposed text and a small set of Objectives and Actions in the Circulation & Mobility chapter that reflect current concerns and priorities regarding development along Highway 24 in south town. Town staff will present these changes to the Town Council at the December 21<sup>st</sup> meeting.

## Overview of Changes Recommended by the Planning Commission

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As described above, the changes to the Minturn Community Plan draft reflect feedback from Town Council, the Planning & Zoning Commission, Town staff, and stakeholders.

Key changes to the document include:

- Adding language describing the projected growth of the Town of Minturn to the Introduction and Transportation chapters
- Adding a new “Mixed Use 3” zoning district located in the 500 to 800 blocks. See below for a description of the district
- Changing the minimum lot size of the R1 and MU1 districts to better accommodate existing lot sizes in those areas
- Changing Martin Creek to MU2 and Maloit Park to MU3 to reflect the future goals for those districts
- Adjusting the language on non-conforming structures to build on previous work by the Planning & Zoning Commission
- Removing the lot coverage bonus recommendation for deed-restricted ADUs in R1; adding a goal to explore this policy in instances in which deed-restricted housing is provided
- Updates to the 100 Block chapter to reflect the final changes to the Appendix B insert and to summarize some of the initiatives that were proposed and rejected/accepted
- Updates to the Housing chapter and Water chapter to reflect most up-to-date information on the projected number of units in planned PUDs and Minturn’s water infrastructure
- The updated plan includes several new Actions that focus on regional collaboration, which was a major theme of discussion at the 9/21 meeting

## Feedback & Changes

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The following tables show feedback from the Town Council & Planning Commission (“Issue”), the proposed changes that SE Group suggested in response to feedback (“Proposed Change”), and the changes to the text approved by the Planning Commission at one or both of the October meetings (“Approved Changes”). Some changes have a description of the impact of the approved changes on the Town of Minturn (“Impact”).

*Note: The “Approved Changes” column contains summaries of the changes, not word-for-word copies of*

the changes.

## Zoning & Land Use

<u>Issue</u>	<u>Proposed Change</u>	<u>Approved Changes</u>	<u>Impact</u>
Desire to discuss/change boundaries and specifics of MU-1 /MU-2 zones + extend mixed-use allowances along Main St.	Add Transitional Residential zone for residential uses and low-impact commercial uses. Make Martin Creek MU-2. See attached Recommended zoning map.	Added MU3 zone and description.	This change allows for additional commercial development south of the 100 Block area in a more incremental way. The area proposed for the MU3 zone is predominantly single-family residential at present. MU3 is a better fit for this area than MU1 at this time.
Revisit zoning rec for Maloit Park - currently MU-2 which allows auto shops, etc. Maybe not best uses there.	No change recommended.	Changed Maloit Park to MU3.	Since Maloit Park will likely be developed as a PUD, its underlying zoning will likely be less consequential for its eventual development. However, MU3 is a better fit for the area than MU2.
Desire for additional discussion on lot subdivision allowances	Discussed with PC on 10/12	No change	

<p>Desire to add more discussion/acknowledgement of potential impacts of planned development (270-410 additional units).</p>	<p>To be added after description of planned PUD developments (Housing Development Conditions, Constraints, &amp; Opportunities heading): Sidebar          - "Balancing Growth &amp; Preservation:"          One of the most prominent challenges to planning &amp; community development is the balance between preserving what the community loves -- in this case small town character and way/pace of life -- with natural growth and associated demand for</p>	<p>Proposed text has been added.</p>	<p>Addresses the projected (though as yet not realized) growth of Minturn and discusses possible impacts on community character.</p>
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	<p>development to meet local and regional needs -- such as housing, jobs, services, etc. The development projects currently in Minturn's pipeline could eventually produce up to 400 units. This would be a significant increase for this small town and would inevitably impact the Town's and operations and physical conditions. However, it is important to note that these projects are a) still in planning phases and are subject to change, b) will be developed in phases over time (10+years) largely due to the Town's water capacity limitations, and c) that this plan puts forward a guiding path for the Town to strike an appropriate balance between growth and preservation -- to allow for much needed development</p>		
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	while also regulating that development in a strategic way to help preserve what makes Minturn “Minturn.”		
Desire for more detail/discussion on Objective 1.3.2 (allowances for non-conforming properties)	Discussed with PC on 10/12	<ul style="list-style-type: none"> <li>The non-conformities Action statement has been revised: <b>Action 1.2.3</b> Resume efforts to revisit the Article 22 nonconformities section to encourage flexibility and appropriate expansions. The goals of this update should be to maintain historic resources, incentivize reinvestment in older properties, increase accessibility of the property, and promote community character. Specific updates to nonconformity standards should be reviewed by the Town Council and Planning Commission.</li> </ul>	
Potential for public pushback on recommended zoning on/near Eagle Street.	This area is part of our new proposed	This area is now part of the MU3 district.	Addresses possible concerns about

Recommend keeping area S of Meek Ave residential.	Transitional Residential zone, which would preserve a substantial amount of residential.		changing to a primarily mixed-use district.
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Remaining Review/Questions for Town Council

- Review boundaries of the MU1 and MU3 districts be along Meek Ave. Review whether all side streets extending from Main Street should be zoned MU1.
- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

100 Block

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
Some Councilors and Commissioners were not in support of the cash-in-lieu parking recommendation	This proposal has been removed in the final Appendix B insert. We proposed removing the reference to it in the 100 Block chapter, or acknowledging that the idea was considered and rejected.	The chapter now contains an acknowledgement that the idea was considered and rejected.	
Add recommendation for specific signage to indicate pedestrian zone in 100-Block area	Add new Action: Implement a "Pedestrian Priority Zone" using signage in the 100-Block area to indicate that vehicles should travel cautiously and that pedestrians have the right-of-way.	This action has been added.	
Consider adding bollards/bump-outs to slow traffic, shorten crossings, increase ped visibility	Add to Action 2.4.2: Provide infrastructure and amenities that support walking and biking, such as sidewalks, <b>bollards and/or curb</b>	This action has been added.	

	<b>extensions</b> , paths, benches, bike racks, and a bike wash/repair station, within the 100 Block area.		
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Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

**An Economically Vibrant Community**

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
Would like to see some phasing or prioritization of these recommendations	No change proposed. See Implementation chapter for a discussion of prioritization of recommendations for all chapters (including Chapter 3: An Economically Vibrant Community)	No change proposed.	
Emphasize priority of Dowd Junction & Maloit Park projects	Discussed with PC on 10/12	No change proposed.	

Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

**Attainable Housing & Historic Character**

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
Speak more to affordable units in pipeline if possible	Based on earlier discussion with staff, these projects are shifting and unpredictable which	Updated for final draft for Council approval.	Provide an accurate picture of future units and density in Minturn.

	<p>makes it hard to pin down a solid number of expected deed-restricted units. SE Group will include the most up-to-date information in the final draft.</p>		
<p>Worry about lowering AMI threshold for affordable units – this has potential to increase costs of market units</p>	<p>Discussed with PC on 10/12</p>	<p>A new action recommends regional coordination related to AMI adjustments.</p>	
<p>Burden to create affordable housing should be on businesses (esp. big businesses regionally)</p>	<p>Add narrative to housing chapter: While many communities can rely on large businesses to generate affordable housing, Minturn does not have enough large employers to make this practice feasible. This is where regional collaboration with other large employers in the area and with County stakeholders becomes especially important.</p>	<p>Proposed text added to chapter.</p>	<p>Clarifies that this idea, while sound for many communities, is not feasible in Minturn.</p>
<p>Are there examples of one community buying down homes in a neighboring community - mutual benefit of jobs/housing for both?</p>	<p>No change to text proposed. Our team knows of no examples of this practice – towns like to invest in their own and would typically be wary of investing in housing in a neighboring municipality.</p>	<p>No change proposed.</p>	
<p>Questions about ADU's – how many can be reserved for LTR, guarding against STRs? Should the Town pursue a 15% lot coverage increase for ADUs as</p>	<p>Discussed with PC on 10/12 and 10/28</p>	<p>Removed lot coverage bonus as a specific zoning recommendation for residential districts, added a</p>	

<p>recommended in some residential districts in the recommended zoning? Should ADUs be specifically called out as an allowed use in MU zones?*</p> <p>*Note: This discussion is in the Smart Land Use &amp; Practical Zoning chapter, not the Housing chapter. We included it in the housing chapter discussion in order to combine discussions about ADUs.</p>		<p>recommendation to consider it in instances in which deed-restricted housing is provided.</p>	
<p>Add in something about potential for regional housing authority (similar to RTA)</p>	<p>New Action: Actively engage with regional partners to support the possibility of a new Regional Housing Authority</p>	<p>Proposed text added to chapter.</p>	<p>Highlights Minturn's willingness to work regionally to meet housing goals</p>
<p>Give better explanation of the relationship/process for deed-restricted housing in development. The issue is that developers want to profit (typically achieved through large-scale development), but the town doesn't want large-scale development that would fundamentally change its character.</p>	<p>Discussed with PC on 10/12</p>	<p>No change.</p>	

Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

**Water, Hazard Mitigation, & Sustainability**

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
<p>Would like the plan to recognize scarce water future, not just short-term issues</p>	<p>Text addition: Minturn and the surrounding region have been experiencing recent and extended periods of drought and reduced snowpack. Soils in the area are drier than they used to be, and there is less runoff because that water is staying in the soil. Elsewhere in the Colorado River Basin, downriver communities have overallocated water. Minturn and neighboring communities have started to adjust water usage, unlike some other communities in AZ, for example.</p>	<p>Proposed text added to chapter.</p>	<p>The proposed text addition addresses large-scale water trends that affect Minturn.</p>
<p>ERWSD owns and maintains sewer lines- clarify narrative &amp; recommendations to this end (not town of Minturn)</p>	<p>Wastewater Systems: ... repairs to older sanitary sewer mains may be needed to improve capacity in the pipe system. <b>The Town should coordinate with ERWSD to ensure that Minturn's wastewater systems are thoroughly assessed and needs are addressed.</b></p> <p>Action 5.1.3: <b>Advocate for wastewater systems upgrades as needed in Minturn through coordination with ERWSD, either through the ongoing Wastewater</b></p>	<p>Text added to chapter.</p>	<p>Addresses the need to collaborate with ERWSD on wastewater, I&amp;I studies.</p>

	<b>Collection System Master Plan or an Inflow &amp; Infiltration (I&amp;I) Study that covers Minturn, including a prioritized implementation plan.</b>		
Well rights along creek already ruled out due to pollution; waiting on rights for redundancy/diversion from Eagle River	Current text does not mention wells. Final text should mention surface water diversion (Eagle River) and/or a potential future interconnect with ERWSD	To revisit with most updated information for final draft for Council approval.	Addresses need for source redundancy and reflects updated findings from recent water investigations (ruling out wells)
Further address hazard mitigation and emergency management	Existing Conditions - Wildfire & Other Natural Hazards: Add: "Minturn is serviced by the Eagle River Fire District and is currently informed by Eagle County's Hazard Mitigation Plan."  Action 5.3.5 - Retain the services of an emergency preparedness consultant to assess, bolster, and test Minturn's emergency response systems; also to establish a natural hazard monitoring system to track local and regional events to accurately assess local risks and changing trends over time.  New Action: Continue to support, proactively participate in, and implement Eagle County's imminent update to the Community Wildfire Protection Plan	Proposed text added to chapter.	



Add recommendation for recycled water system/standards	Add discussion explaining that this is not feasible at this time. Minturn would need to have a major user to do this -- it requires a separate system, pumping equipment that would use gray water.	Proposed discussion added to chapter.	
Need to address fire mitigation on Federal Land/actively engage with Forest Service	New Action: Coordinate with US Forest Service to initiate planning and implement associated strategies for forest fuels health and fire mitigation on public lands surrounding Minturn.	Proposed text added to chapter.	Highlights potential to collaborate with USFS on this important issue
Bring back volunteer fire program	Add New Action: Continue to proactively coordinate with the Eagle River Fire District and consider reestablishing a volunteer fire safety program with associated training and education	Proposed text added to chapter.	Bolsters Minturn's emergency preparedness

Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

**Intuitive Mobility, Circulation, and Connectivity**

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
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Add discussion about general impact of regional growth and change on Hwy 24 & Minturn	Discussed with PC on 10/12	Added discussion to chapter.	
Add some discussion about management/enforcement of expanded 2-hour parking zone - How does this affect residents who rely on street parking? Desire for additional discussion about enforcing regulation that rely on signage.	Discussed with PC on 10/12  Move residential parking analysis from Appendix into the body of the chapter.	Added further detail on residential parking recommendations and a reference to the Appendix.	
Add recommendation for sharrows (paint that identifies that bikes share the road) on Hwy 24	Action 6.3.7 covers sharrows: "Partner with CDOT and advocate for the effort to add sharrows on Main Street through town to draw attention to people bicycling."	Proposed text added to chapter.	Encourage comfortable biking experiences along Hwy 24
Add rec to consider city/regional bike-share program	Add Action: Consider participating in a regional or neighboring bike share program, such as the ShiftBike program in Vail, Eagle Vail, and Avon; promote service at Dowd Junction.	Proposed text added to chapter.	Increase micromobility options in Minturn, encourage mobility options at Dowd Junction.
Emphasize non-res parking on edges of town, esp. with RTA circulation to get people into and out of downtown.	Discussed with PC on 10/12	No change.	
Add discussion or recommendation about conducting parking counts at additional times of day	Discussed with PC on 10/28	Added language to recommendation about continuing to collect parking	

		data; noted that early morning and afternoon have been identified as busy times for some local businesses.	
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Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

Quality of Life

<u>Issue</u>	<u>Proposed Change</u>	<u>Final Change</u>	<u>Impact</u>
Mention \$10 drop in fee at rec center	Add to Action 7.3.3: ...winter ice rink. <b>Increase community awareness of the \$10 drop in option and consider implementing other rewards programs or incentives that encourage its use and increase its accessibility to all residents.</b>	Proposed text added to chapter.	Highlight existing programs that encourage use of the rec center
Add rec to consider dark sky town designation/regs	New Action: As development increases so does light pollution. To preserve Minturn's unique position for starry-sky viewing, consider establishing Minturn as a Dark Sky Community per the International Dark Sky Association standards and designation process. Confer with other Dark Sky communities in Colorado, such as	Proposed text added to chapter.	

	Westcliffe and Silver Cliff as needed.		
Add Eagle River public access fishing points (2 ADA) to table & map	Complete this change as proposed	Proposed text added to chapter.	
Add rec for more rec development at Maloit Park	New action: Promote the inclusion of recreational amenities in redevelopment at Maloit Park, including playgrounds, trails, gathering areas, etc.	Proposed text added to chapter.	
Add note about Bolts Lake as rec opportunity in the pipeline	Proposed text to be added after last paragraph of existing conditions section: The Bolts Lake area offers potential opportunity for future recreational development. An asset now owned by ERWSD with intentions to pursue its revival as an acting reservoir, this project could include recreational amenities such as hiking, boating, and camping, similar to Homestake Reservoir near Leadville.	Proposed text added to chapter.	

Remaining Questions for Town Council

- Do the proposed changes adequately address the feedback raised by the Commission and Council for this section?

**Open House & Stakeholder Feedback**

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After the initial joint meeting on September 21<sup>st</sup>, the planning team held an open house on September 27<sup>th</sup>. The planning team also received feedback on the plan from stakeholders, including Eagle County.

**Community Feedback from the Open House**

Community feedback from the Open House has been incorporated into relevant chapters in the “Community Input” sections.

### Eagle County

In response to Eagle County’s request for further discussion of natural resources, new discussion has been added suggesting that the Town collaborate with CPW on future planning and monitoring efforts that protect the Town’s mountain environment.

### VVMTA

The revised plan incorporates actions suggested by VVMTA related to stewardship and collaboration on trails initiatives.

### Eagle Valley Land Trust

The revised plan incorporates additional references to collaboration with Eagle Valley Land Trust on stewardship initiatives, particularly related to the Boneyard Open Space.

## Zoning Chapter Graphics

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In the version of the plan that the Council discussed on September 21<sup>st</sup>, there was a note in the Zoning chapter (Chapter 1) that the planning team would add a graphic illustrating lot coverage to the final plan. Now that the proposed dimensional standards have been further reviewed and refined by the planning commission, the planning team was able to create these graphics. They are included **at the end of the zoning chapter**.

These graphics also help explain and show the three proposed mixed use districts (MU1, MU2, and MU3). As explained in the accompanying text for the graphics, the graphics show the differences in purpose and form between the three MU districts. The graphics show a hypothetical set of four lots of standard sizes (2500, 5000, 7500, and 10000 sf), each with a depth of 100’. Each graphic shows examples of the variety of allowed building forms for residential and commercial uses in each district. For example, the MU3 graphic shows a low-impact commercial use in a building that has been converted from a residential home. As a point of clarification, the 100 Block district is also recommended in this plan as a mixed-use area, reflecting recent Council decisions. These graphics focus on MU1, MU2, and MU3; they do not show dimensional standards for the 100 Block.

As noted in the accompanying text on the graphics, the lot coverage allowances were developed using Minturn’s current definition of lot coverage (copied below). This definition largely refers to building area, not just impervious surfaces.

*Building lot coverage* means the portion of a lot that is covered by buildings, including all interior space including garages, and all enclosed cantilevered portions of a building, covered porches, covered walkways and similar covered areas. Buildings are measured at the outside exterior wall. Additionally, fifty percent (50%) of the total area of second- and third-level decks shall be counted towards the allowable building lot coverage. Second- and third-level decks that cover a portion of a building that has already been counted towards the allowable lot coverage shall not count towards the total lot coverage. Second- and third-level decks that are structurally supported from the ground level shall have their entire area counted towards the allowable lot coverage. (See Illustration No. 16-3.)

In this chapter’s zoning recommendations, multi-family housing is currently listed as an allowed use in MU1, MU2, MU3 and R2. The mixed use graphics show a multi-family structure in each of the

mixed use districts. This multi-family prototype contains tuck-under garage spaces – this type of building form requires a lot coverage of 70%. The consultant team recommends allowing this type of multi-family housing, which provides parking for residents in an efficient manner and reduces potential pressure on street parking. A present, the recommendations for lot coverage in this chapter do not differentiate between single-family/duplex and multi-family structures. **In order to provide this type of multifamily housing, the consultant team suggests that the chapter include separate lot coverage provisions for single-family/duplex and multi-family structures (see table below).**

In addition, the current recommended dimensional standards for MU2 and MU3 do not specify allowable lot coverage for commercial/MU structures.

Our team recommends the following dimensional standards. Additions as mentioned above are noted in green. These additions are not currently reflected in the text of the zoning chapter; the consultant team will update the chapters to reflect Council’s opinion following 1/4/23.

District	Min. lot size SF & duplex	Min. lot size MF	Max. lot coverage commercial or mixed-use lot	Max. lot coverage residential (SF, duplex)	Max. lot coverage residential (MF)	Height limit
MU1	2500 sf	7500 sf	80%	50%	70-80%	28-35'
MU2	5000 sf	7500 sf	60-70%	60%	70%	28'
MU3	5000 sf	7500 sf	50-60%	60%	70%	28'

Note: In the state of Colorado, Comprehensive Plans/Community Plans are not legally required to provide dimensional standards for specific uses; those standards are typically ironed out in land use code update processes. The Minturn Community Plan takes a more detailed approach to zoning and land use and therefore recommends some dimensional standards. **The Council can decide whether to provide additional detail and specification regarding lot coverage for multifamily and commercial structures at this time or wait to do so in a future land use code update process.**

Remaining Questions for Town Council

- Would the Council like to add specific lot coverage recommendations for MF and commercial at this time or wait to do so in a future land use code update process?

# MINTURN



COMMUNITY PLAN  
-Imagine Minturn-

## INTRODUCTION

### About Minturn

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Minturn is an iconic Colorado mountain community. Surrounded by distinctive cliffs and intersected by the scenic Eagle River, the Town of Minturn is known for its stunning natural environment and excellent access to the White River National Forest. Residents choose to live in Minturn for its high quality of life, quaint downtown, strong sense of community, and inspiring setting.

### Minturn's History

The narrow valley that contains present-day Minturn was first inhabited by the Ute tribe, who used the area as summer hunting grounds. The Arapahos entered the area in 1849, and the two tribes fought on what is now known as Battle Mountain. In 1904, Minturn was incorporated as a railroad and mining town. Though the railroad and mine are currently inactive, several other industries have found success in Minturn's history. In the early 20<sup>th</sup> century, for example, Minturn became a top producer of greens and was known as the "Lettuce Capital of the United States."

Though the town's agricultural and industrial economies both cycled through periods of boom and bust, Minturn remained resilient. When old industries died, new ones always formed in their place. At the same time, the community sometimes had to deal with the unpleasant remnants of bygone industries. After the mines closed, the Eagle River ran orange, and the Town had to undergo significant environmental cleanups related to mining waste. Minturn's history as a railroad town is still honored and preserved in its name: The town itself is named after Robert Bowne Minturn, Jr., vice president of the Denver and Rio Grande Western Railroad. Other places throughout town also bear historical names. For example, "Everkrisp" was name of the company that grew lettuce in Minturn in the 1920s; today, it's the name of a popular mountain bike trail near town. Just as these names have persisted through time, several families have been in Minturn for generations, testaments to both the past and the future of the community.

The Minturn community takes immense pride in the unique character of their town compared to neighboring communities: unlike some other communities, which were incorporated only a few decades ago as skiing destinations, Minturn's past extends far beyond the history of ski resort development



*The Minturn Market is the oldest market in the valley and is still going strong.*

in Colorado. The Minturn Saloon, for example, has been in business for over a century. Minturn is also a hidden gem, tucked away in a steep and stunning valley. A person walking through Minturn's Old Town, with its historic facades, vibrantly colored buildings, and small-town charm, might never suspect that one of the world's most prominent mountain resorts and resort communities lies just a few miles away. Minturn offers residents and visitors a different experience from a master-planned ski village – an authentic place to dine, recreate, and attend community events.

### Minturn Today

Today, Minturn's largest industries are construction and retail. Minturn's local economy and character are decidedly different from those of its neighbors, which have a stronger emphasis on tourism and function as gateways to major ski resorts.

In recent years, the Town of Minturn has worked diligently to plan for the future of the community. Since 2009, when the last Community Plan was updated, the Town has produced several plans, including an Economic Development Strategic Plan, a Housing Action Plan, an Energy Action Plan, a Snow Removal Plan, Water Capital Improvement Plan, and a 2021-2023 Strategic Plan. At the time of writing, the Town is currently engaged in a Little Beach Park Area Plan and an effort to update design standards for the 100 Block. In addition to its investments in planning, the Town has also made several improvements to its recreational offerings. The newly opened Minturn Bike Park has provided an opportunity for residents and biking enthusiasts of all ages to practice their skills. The Bike Park complements Minturn's other recreational assets, including the Mini-Mile and Little Beach Park.

New construction in Minturn has been limited by several factors, including moratoriums on various forms of development. In 2020, in response to a lack of water resources in Minturn, the Town placed a moratorium on the issuance of new taps ~~requiring more than three single family equivalent units~~. In 2021, the Town placed two moratoriums on development in the 100 Block Commercial Zone District that prevented demolition while also limiting new development applications until such time that the Town could complete a comprehensive update of the 100-Block Commercial Design Guidelines and Standards and form a Historic Preservation Commission. These efforts were intended to maintain and enhance the Town's tax base and character in the "core" of the Town's downtown area. At the same time, Minturn's population has been stagnating and the town lacks sufficient housing inventory to meet demand. Median home prices and valuations have increased in recent years, with the average price per square foot of residential space in Minturn increasing 4.5% per year since 2015, or \$126 in total.

At the time of writing, there are active applications/proposals to create about 261<sup>[sh1]</sup> new units in Minturn. For a town of Minturn's size, this degree of change may be substantial, impacting transportation, economic activity, and social life in the town. Because many of these projects are still in preliminary planning phases, their exact number of units (and associated impacts) are subject to change. In addition, these projects will develop slowly and in phases, largely due to the Town's water capacity limitations. Though the exact impacts of the new units cannot be predicted in this plan, this plan offers several goals and actions that are intended to balance growth and preservation. Like all the goals and actions of this plan, recommendations related to growth and development have been informed by public input.

There is a lack of community consensus regarding future development in Minturn, specifically in the 100 Block. A segment of the Minturn community would like to limit future residential development and/or changes to development in the 100 Block. Others in the community are eager-open to providing new housing opportunities and see the 100 Block area as a critical opportunity to provide new retail spaces and residential units. The current water moratoriums on



development do not address these essential disagreements on development. This plan provides an overview of all viewpoints and offers an in-depth examination of the consequences of different land use policies for the Minturn community.

Minturn residents recognize the main strengths of their community, such as its small-town charm and superior access to the outdoors. Residents also recognize the primary issues within their community, including water scarcity and the urgent need to mitigate and adapt to climate change. This planning effort is intended to address Minturn's most pressing problems and provide a framework for future decision-making.

## About the Community Plan Update

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### The 2009 Minturn Community Plan

Minturn's previous Community Plan was adopted in 2009. The 2009 plan was developed by Town staff over a period of three years and was shaped by public input gathered during that period. This plan, *Imagine Minturn*, was initiated in 2021 and involved an interdisciplinary team of Colorado-based consultants.

### What is a Community Plan?

A community plan, also known as a comprehensive plan or general plan, is a document that guides land use policy for a community over several years. The community planning process involves a combination of public engagement and analysis. The process is designed to draw out public priorities for housing, transportation, economic development, and public works. The plan document represents the final phase of the planning process, after which the municipality may implement the recommendations of the plan.

### Legal Foundation

A comprehensive plan is described in the Colorado State Statutes Section 31-23-206 as an "advisory document to guide land development decisions." The statute provides that "it is the duty of the [planning] commission to make and adopt a master plan for the physical development of the municipality, including any areas outside its boundaries, subject to the approval of the governmental body having jurisdiction thereof [Town Council]." Following adoption of the community plan or comprehensive plan, it is the task of the Town Council to update zoning and other local policy to align with the adopted community plan.

### The Planning Process

To update the community plan, the planning team conducted a robust existing conditions assessment that included a review of existing planning, land use and zoning, transportation and connectivity, economic development, housing, and infrastructure. The planning process incorporated in-depth design and land use analysis of the 100 Block; the community plan process was coordinated alongside an effort to provide a set of design guidelines for the 100 Block.

The planning process involved a 17-member Steering Committee made up of Minturn residents who volunteered to guide the planning process. This group provided feedback on the vision, goals, and recommendations developed for this plan. The planning team also organized two open houses that solicited input on the concepts and recommendations of the plan.

**Minturn is an authentic mountain community known for its small-town charm and world-class recreational opportunities.**

**Residents and visitors appreciate what makes Minturn “Minturn,” including the Town’s vibrant and historic downtown and its well-connected system of multi-use paths and parks.**

**The Town prioritizes the environmental, economic, and social sustainability of the community through thoughtful new development and careful resource planning.**

The Vision Statement above was crafted in collaboration with the Community Plan Steering Committee. The vision statement is intended to guide the Community Plan and complement the Town of Minturn’s existing vision statement contained in the 2021-2023 Strategic Plan.

#### **Plan Themes & Structure**

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The Minturn Community Plan centers on eight main themes:

1. Smart Land Use & Practical Zoning
2. A Thriving 100-Block
3. An Economically Vibrant Community
4. Attainable Housing & Historic Character
5. Water Infrastructure, Hazard Mitigation & Sustainability
6. Intuitive Mobility, Circulation & Connectivity
7. Quality of Life, Recreation & An Inviting Public Realm
8. A Collaborative & Resilient Future

The following chapters of the plan focus on these eight themes. Each chapter contains a discussion of existing conditions, public input on the topic(s) discussed, central concepts and recommendations, and goal statements. The final themed chapter, **A Collaborative & Resilient Future**, provides a roadmap for implementation of the recommendations of the plan.

### **Sustainability in the Community Plan**

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Sustainability is a guiding principle of this plan and is woven into all chapters. “Sustainability” or “sustainable development” can be understood through three primary lenses: environmental sustainability, economic sustainability, and social sustainability. **Chapter 7: Water Infrastructure, Hazard Mitigation, & Sustainability** focuses on facets of *environmental sustainability*, including renewable energy, emissions reductions, low-impact building standards, wildfire mitigation, drought, water usage, and air quality. **Chapter 6: Intuitive Mobility, Circulation & Connectivity** and **Chapter 7: Quality of Life, Recreation & An Inviting Public Realm** also focus on environmental sustainability, particularly addressing the public health dimensions of sustainability. Chapter 6 provides several recommendations that support low-carbon and no-carbon transportation in Minturn, and Chapter 7 provides recommendations that support local food systems and environmental stewardship within Minturn. **Chapter 1: Smart Land Use & Practical Zoning**, **Chapter 2: A Thriving 100 Block**, **Chapter 3: An Economically Vibrant Community**, and **Chapter 4: Attainable Housing & Historic Character** all address social and economic sustainability in Minturn.

## Main Ideas

The following matrix summarizes the main ideas of each topic chapter:

Chapter	Main Ideas
Chapter 1: Smart Land Use & Practical Zoning	<ul style="list-style-type: none"> <li>• Provide a new zoning scheme that is clear and usable</li> <li>• Promote more housing types and mixed-use environments</li> <li>• Ensure that PUDs support community goals</li> </ul>
Chapter 2: A Thriving 100-Block	<ul style="list-style-type: none"> <li>• Summarize the community vision for the 100 Block</li> <li>• Ensure that future changes to the area remain compatible with the community's design priorities <u>as summarized in the Minturn Design Standards and Guidelines Appendix B insert (adopted in October 2022)</u></li> </ul>
Chapter 3: An Economically Vibrant Community	<ul style="list-style-type: none"> <li>• Provide a snapshot of industry and employment in Minturn</li> <li>• Discuss new economic development frameworks (e.g., DDA) and discuss the role of new mixed-use developments in Minturn's economy</li> </ul>
Chapter 4: Attainable Housing & Historic Character	<ul style="list-style-type: none"> <li>• Provide a snapshot of existing housing opportunities, PUDs, and historic character in Minturn</li> <li>• Discuss policy incentives for providing attainable housing throughout the Minturn community</li> </ul>
Chapter 5: Water Infrastructure, Hazard Mitigation & Sustainability	<ul style="list-style-type: none"> <li>• Provide an overview of Minturn's water infrastructure needs</li> <li>• Identify future planning and assessment needs for hazard mitigation, resiliency, and environmental sustainability</li> </ul>
Chapter 6: Intuitive Mobility, Circulation & Connectivity	<ul style="list-style-type: none"> <li>• Provide an overview of parking and transportation infrastructure in Minturn; discuss the findings of recent parking study</li> </ul>

	<ul style="list-style-type: none"> <li>• Identify multimodal transportation needs, particularly related to downtown and transit-oriented development at Dowd Junction</li> </ul>
<p>Chapter 7: Quality of Life, Recreation &amp; An Inviting Public Realm</p>	<ul style="list-style-type: none"> <li>• Discuss main quality of life assets in Minturn</li> <li>• Provide a set of recommendations to promote stewardship, public health, and a vibrant public realm in Minturn</li> </ul>

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# MINTURN



COMMUNITY PLAN  
*-Imagine Minturn-*

## CHAPTER 1

### SMART LAND USE AND PRACTICAL ZONING

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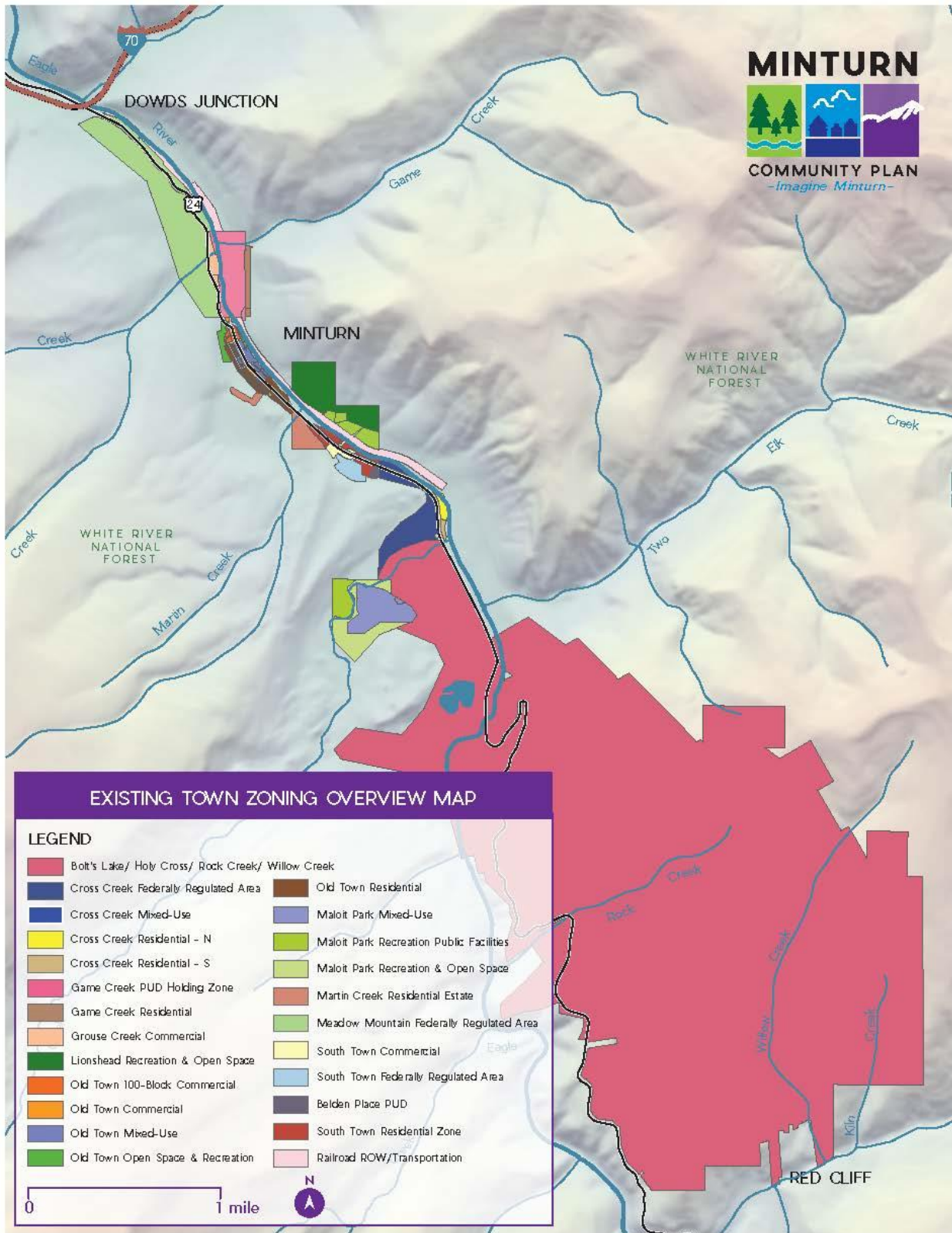
#### Introduction

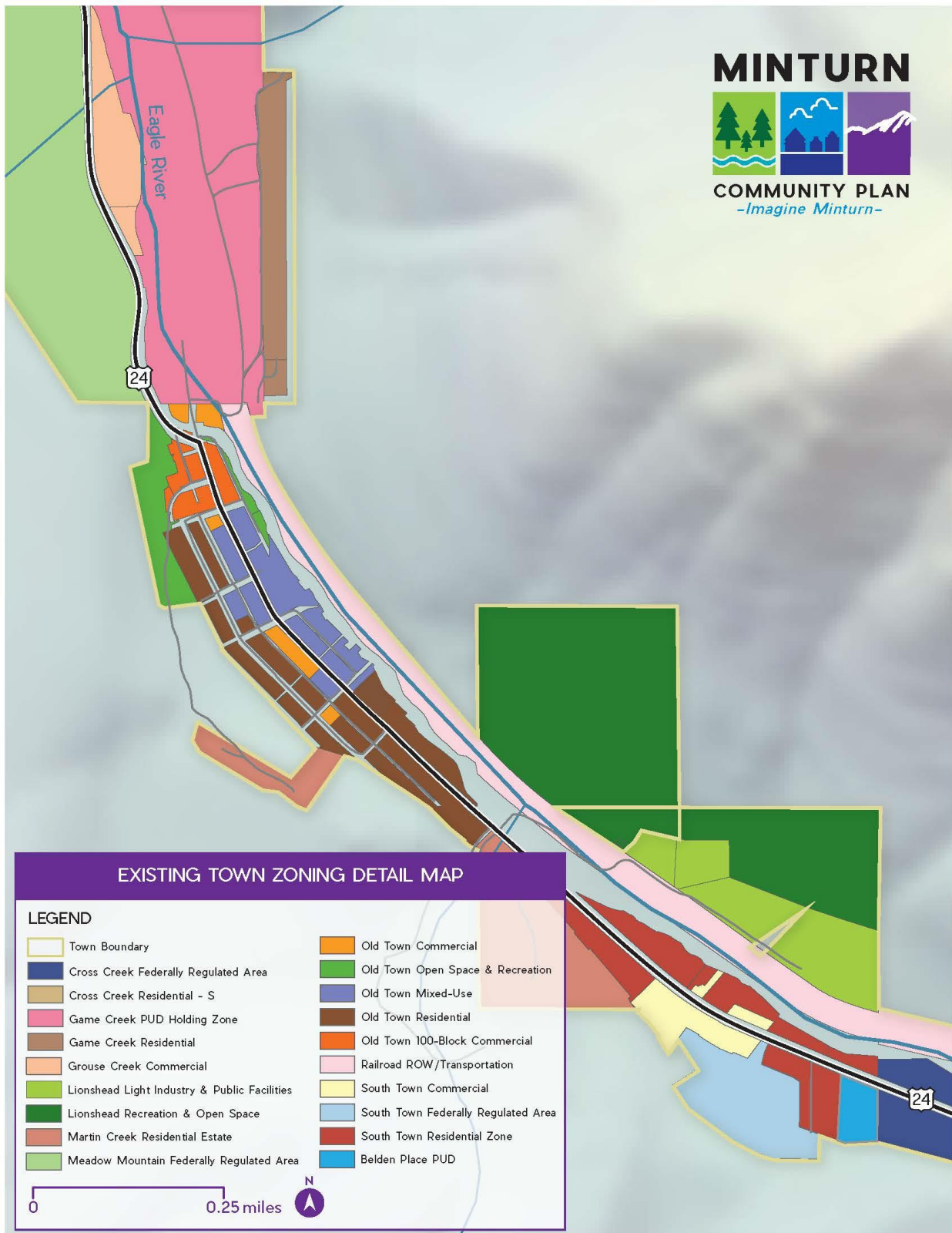
The Town of Minturn has an opportunity to shape its built environment through zoning and land use designations. Zoning and land use policies dictate where commercial, residential or other uses can be located, the size and location of buildings on a lot, and other specifications for parking and landscaping. These designations affect both vacant lots and those that are already occupied by a building or a use. On any lot, zoning can dictate whether one can subdivide a lot, expand a building footprint, add a story or unit, or add a new use. For properties that do not conform with the zoning, there can be limits to the property modifications allowed. It is important that zoning and land use changes reflect the development that currently exists and allow for the implementation of the desired future conditions.

This chapter makes recommendations about the Town's zoning map and districts, allowable uses, and dimensional standards. To take effect, an official rezoning and land use code update process to incorporate these changes is necessary. This would take place following the adoption of this plan and involve further evaluation and review of detailed elements.

#### Existing Conditions & Issues

At this time, the Town is relatively built-out with few vacant parcels and opportunities for large scale redevelopment beyond the PUDs currently under review or being discussed, including Minturn North, Midtown Village, Dowd Junction, and other Forest Service administrative parcels. Further, the Town is very geographically constrained by steep hillsides and federal land. Therefore, zoning within town boundaries and for existing lots must account for all of the development that the town needs to support community vibrancy and economic opportunity for years to come. The land use and zoning recommendations contained herein are critical for the Town to realize the vision of this plan.





**Character Areas and Complexity.** The existing zoning system is built around 12 “character areas,” designated areas with an intended design and list of uses, resulting in a unique set of zoning restrictions. Within each character area, there are one or more zoning designations that further describe allowed uses and dimensional standards. This results in 27 different zoning designations or unique development standards, a confusing configuration that is difficult to depict on a map



legible to residents and potential developers. Overall, this is a complicated system that has been hard for the Town Staff to interpret and administer. The original intention of this system was to promote the unique character of different areas of Town. This plan recommends a more traditional zoning system as well as policies and actions that promote community character in Minturn.

**Spot Zoning.** The existing zoning includes several instances that could be considered “spot zoning,” where different zoning is applied to a specific parcel or parcels within a larger area. There are several instances where a zoning designation only applies to a single parcel, or one parcel has been rezoned to a different designation than that of any surrounding parcel. Spot zoning is problematic as it is not legally defensible (as it is viewed as conferring a benefit to a particular property owner) and may not support broader land use goals.

**Existing Land Use Pattern.** Despite its relatively small size, Minturn has a full spectrum of existing land uses that are well-established. There are light industrial/small business park areas to the north, the historic small-lot Old Town area, and mixed commercial and residential areas to the south. There are few existing lots that are “mixed-use,” including commercial and residential uses on the same lot. These mixed-use buildings are not concentrated in any one area of Minturn. The plan recommendations should support these areas largely as they exist but allow flexibility to evolve and meet Town goals.

**Non-Conformities.** There are several instances of lots that do not conform to the uses or standards set out by the current zoning. Primarily, this involves structures built too close to the property line but also includes lots that have more units than allowed or lot coverages exceeding the allowance. The town’s existing rules regarding non-conformities can preclude the property owner from making desired upgrades. To address this, the Town can revise the zoning to bring more lots into conformance and/or establish policies to grant the property owner more flexibility to update non-conforming lots and structures.

**PUD.** In the Town’s current zoning, there are a few Planned Unit Development (PUD) holding zone districts. These areas effectively have no development standards at present; the dimensional standards and densities have to be determined during the PUD process. In this plan, zoning and land use recommendations will be made for each area of the town, which will help provide guardrails to future processes. This will also provide clarity on development standards for an area if PUD processes fail to reach implementation.

Sidebar: **Balancing Growth & Preservation:** One of the most prominent challenges to planning & community development is the balance between preserving what the community loves -- in this case small town character and way/pace of life -- with natural growth and associated demand for development to meet local and regional needs -- such as housing, jobs, services, etc. The development projects currently ~~in Minturn's pipeline~~ being discussed could eventually produce at least 250 units. This would be a significant increase for this small town and would inevitably impact the Town's and operations and physical conditions. However, it is important to note that these projects are a) still in planning phases and are subject to change, b) will be developed in phases over time (10+years) largely due to the Town's water capacity limitations, and c) that this plan puts forward a guiding path for the Town to strike an appropriate balance between growth and preservation -- to allow for much needed development while also regulating that development in a strategic way to help preserve what makes Minturn “Minturn.”

**Single-Family Homes.** Single-family homes are the predominant housing type in Minturn. This is consistent with the existing zoning, where many residential zones do not allow a housing type other than single-family homes. Single-family homes tend to carry a higher price point than other

housing types (i.e., duplexes, multi-family housing) and their proliferation may limit the Town's ability to provide attainable housing.



**Lot Sizes and Coverage.** The minimum lot size is the primary varying factor between different zone districts. Minimum lot sizes tend to reflect the character and development style (i.e., small

lots are allowed in Old Town, larger lots are allowed south of Old Town). At this point, with the town largely built out, minimum lot sizes primarily influence whether or not a lot can be subdivided. Subdividing existing lots can be an important tool to support additional units in town, but this practice can also have an impact of the character of the community. Lot coverages, or the percent of the lot that the structure occupies, vary by zone district but are sufficiently high in general. Of the smaller lots in town, few lots are built to their maximum coverage, even though property owners are maximizing their buildable area up to setbacks whenever possible. Relatedly, for many zone districts, there is a misalignment between the allowable lot size, coverage, and required setbacks. In these cases, it would be impossible to build to allowable coverage while abiding by setbacks on smaller lots. [WA1]

**Parking.** The Town has relatively ~~gratuitous~~ **generous** parking minimums for both residential and commercial development. For commercial development in particular, this policy may preclude a project from proceeding, as developers may not be able to find ample space for parking to make some projects meet the minimum requirement. Presently, parking minimums are related to uses, square footage, and units, and are otherwise consistent across zone districts.

**100 Block.** In recent years, the Town has recognized that the current allowable uses and dimensional standards of the 100 Block do not align with the community vision for the area. The Town has also recognized that the 100 Block area represents a critical opportunity area for the town to encourage mixed-use development to support the Town’s economic sustainability. The Town initiated a process to establish new zoning and design standards for that area, which is covered in [Chapter 2: A Thriving 100 Block](#).

**Past Rezoning Efforts.** In 2019, the Planning Commission began the process of updating the zoning map and consolidating districts. The recommended zoning map in this chapter builds off that effort. Further recommended revisions to the existing map are based on community input gathered during this plan process and are informed by the professional opinion of the planning team.

Further analysis of the existing zoning code and land use is included in [Appendix X](#).

### Community Input

This section describes community input around land use and zoning. Community input on topics such as housing and economic development has implications for zoning and land use. For example, community interest in attainable housing requires a multi-pronged approach that involves zoning to allow certain housing types.

During the plan public engagement process, residents and the steering committee highlighted the importance of ensuring the availability of housing for long-term residents and finding new sources of tax revenue through new or expanded commercial areas. With these points, residents stressed the importance of balancing commercial and residential uses and allowing a density that continues to support the character and quality of life of Minturn. Other topics that came up include providing resources for citizens to better understand zoning, incentivizing Accessory Dwelling Units (ADUs), appropriate uses of the Dowd Junction area, and continued short-term rental regulation.

Values	Concerns	Opportunities
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<ul style="list-style-type: none"> <li>• Small-town environment</li> <li>• Funkiness</li> <li>• providing housing for long-term residents</li> <li>• 100 Block as the heart of Minturn</li> <li>• Balance of residential and commercial uses</li> </ul>	<ul style="list-style-type: none"> <li>• Short-term rental and second homeowner proliferation</li> <li>• Loss of character</li> <li>• Density levels that could impact character</li> </ul>	<ul style="list-style-type: none"> <li>• Simplifying zoning</li> <li>• Commercial development in Dowd Junction</li> <li>• Greater flexibility around non-conformities</li> <li>• Incentivizing ADUs</li> </ul>
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The 2022 Town Community Survey, a statistically valid survey that is representative of the town’s population, highlighted some of the priorities to address with zoning and land use. Most important attributes of Minturn include sense of community, wildfire mitigation, small town atmosphere and appearance of town. Economic development and housing for locals were relatively modest priorities, but housing for locals was ranked as the focus area in which the Town is least meeting expectations. The Town has been challenged in providing and approving housing for residents, as there has been significant public pushback on several residential projects in recent years. The Town’s ~~current~~ zoning and water moratoriums have also ~~stymied~~ [precluded approval of large-scale](#) residential development.

At the [first](#) Community Plan Open House held as a part of this planning process, participants expressed interest in tools that would help them understand Minturn’s zoning, such as a Citizen’s Guide to Zoning in Minturn and/or an interactive map. [The second Community Plan Open House, which offered attendees the opportunity to comment on the draft zoning recommendations. Attendees offered several points of feedback: some attendees shared that the zoning recommendations should emphasize and support locally owned businesses. Some attendees stated that they liked that the recommendations solidified mixed use opportunities and promoted a “main street” feel. Some expressed a desire to place further limits on short-term rentals in Minturn and to emphasize housing for full-time residents. Some attendees expressed that the current zoning regulations achieve the vision articulated for this plan and Minturn and that the proposed regs do not support character of Minturn. There was also a desire to emphasize and incentivize improvements to existing buildings, not new land development.](#)

## Central Concepts

Zoning and land use – the mechanisms by which a municipality controls development – can have a critical role in shaping the character and economic development of a place. This section outlines the key changes to zoning that will promote land use practices that align with Minturn’s broader goals for vibrancy and economic sustainability.

### A NEED FOR CHANGE

Presently, Minturn’s zoning map and accompanying code are making it challenging for development and redevelopment to occur, even for projects that align with established community goals. The Town needs to have a legible and administrable map so that residents and developers can understand the rules that apply to them. The zones should promote fairness and equity within Minturn; for example, the town’s zoning districts should not “spot zone” particular parcels for the gain of a particular individual or group. Further, elements of the code preclude the Town from working towards its aims as they relate to attainable housing and economic development. These elements include allowing exclusively single-family homes in areas, high parking requirements, and limited allowance for residential/commercial multi-use buildings).

### CONSOLIDATE DISTRICTS

The Town has 27 zone districts under its current zoning, an unreasonable figure for a town of Minturn’s size. By reducing the number of zone districts and consolidating some districts by use and level of density, the Town has the opportunity to make the code increasingly legible and comprehensible, while also supporting some design flexibility and creativity. This approach will also reduce the need to review changes on a case-by-case basis (e.g. variances) and allow the Town to address development proposals more holistically and consistently. The Town will be able to review development proposals relative to the uses proposed (e.g. mixed-use development), not just the location in which it is proposed.

### ZONING THAT SUPPORTS THE TOWN’S VISION

Zoning is one of the most effective policy tools the Town has to implement its vision for the built environment and the community that lives and does business here. The recommendations outlined here set the Town on a proactive course, explicitly stating what the standards are for in each area of Town. Then, when developers approach the Town about a PUD or other project, the Town can share its vision that the developer must support, rather than the other way around.

### SUPPORT VIBRANCY

Minturn residents treasure the vibrancy of their community. Activity and social cohesion are important on neighborhood streets and in commercial areas, where people run into their neighbors and people are out and about supporting local businesses. Land use policies to address vibrancy can include short-term rental policies, and regulations for mixed-use buildings, connectivity and mobility, and residential density.

### TAKING NON-CONFORMITIES INTO ACCOUNT

Minturn, as a community with historic structures, invariably has many non-conforming uses and structures. At this time, under the current provisions, these non-conforming uses (important sources of community housing) could not be preserved should redevelopment or major property improvements occur, and many residents with non-conforming structures cannot make enhancements to their property. Further, some of these dimensional non-conformities support the character and sense of funkiness around Minturn. **The recommendations in this chapter support property owners in making sensible upgrades to their homes, which may be non-conforming structures. The intention of this recommendation is to preserve (at a minimum) existing residential density. Since much of Minturn’s housing stock is older, upgrades and**

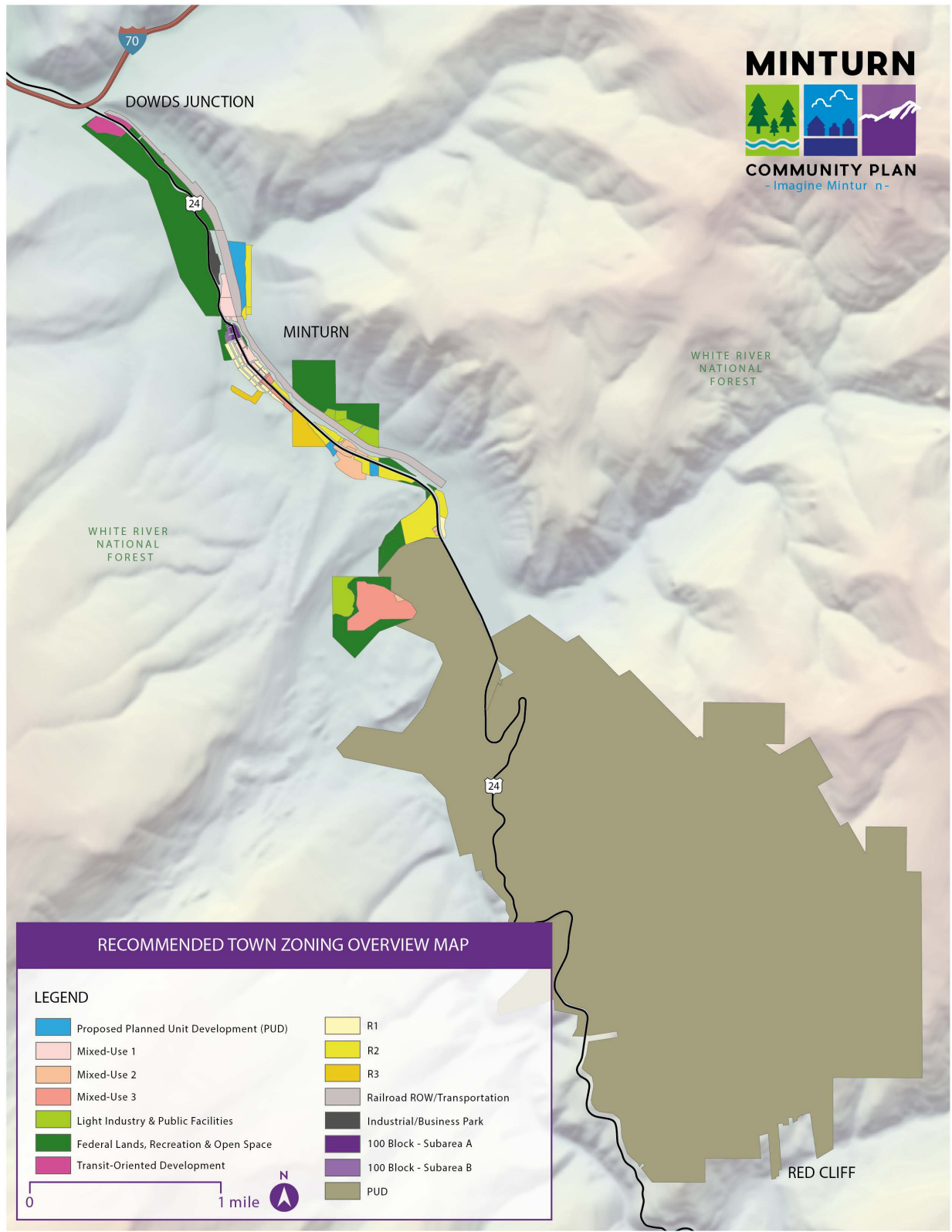
[maintenance are critical to preserving existing units.](#) A central theme throughout the recommendations is the need to preserve community character in Minturn, which is cherished by residents and visitors alike.

## Recommendations

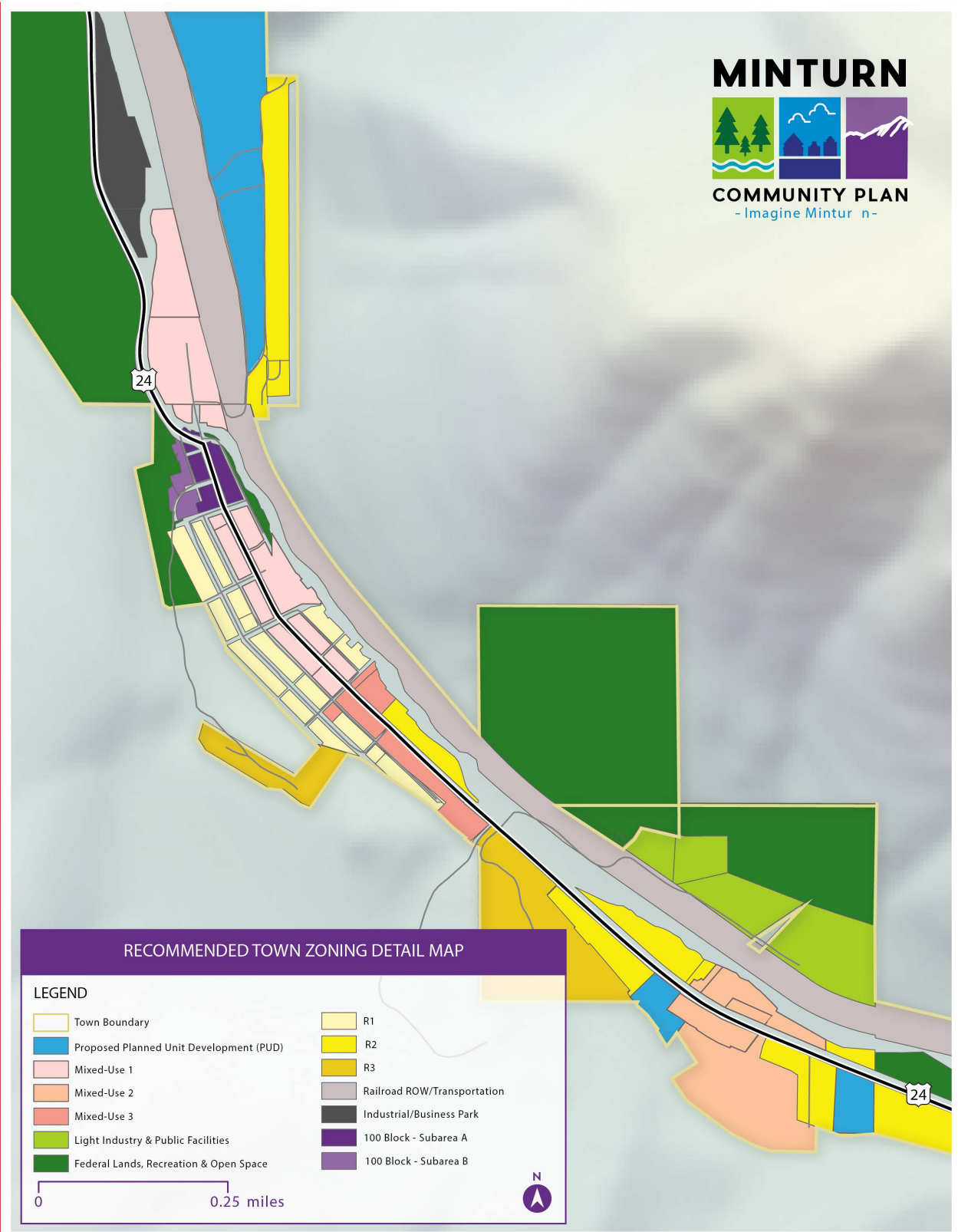
### OBJECTIVE 1.1 – PROVIDE AND FOLLOW A COHESIVE, FORWARD-THINKING LAND USE PLAN FOR THE TOWN.

The future zoning map ([Figure X](#)) consolidates districts, supports new attainable housing and economic development and preserves community character. While increases in unit count may be allowed in some areas, it is anticipated that any such changes would be relatively limited and occur slowly, as the town is nearing full build-out, particularly along Highway 24 from the 100 Block south. Should significant redevelopment occur and as the Town is approaching its water tap/SFE limit, the Town should update the allowable uses by right. It is recommended that the Town of Minturn expediently begin the rezoning and code update process.

- **Action 1.1.1** Adopt future zoning map and associated recommended zoning districts and requirements, as stated in this section.
- **Action 1.1.2** Adopt the **100 Block Design Standards and Guidelines Appendix B** insert.
- **Action 1.1.3** Consider establishing minimum and maximum setbacks in mixed-use and residential districts. This requirement is intended to encourage a visually pleasing development pattern with slightly varied setbacks and address a current misalignment between lot coverage and setback requirements on several lots in town.



[MH2]



[MH3]

The following new and existing zone districts are recommended. These recommendations get rid of character areas and are applied more broadly across Town, bringing the 27 existing zone districts down to 13. This figure and the categories are aspirational, sometimes ascribing desired development types rather than what currently exists.



**Mixed-Use 1.** This district is recommended along Highway 24 through the 700-400 Block (not including the 100 Block) and on Railroad land opposite downtown north of the river, across Bellm Bridge. The intent for this area is to contain a mix of residential and commercial uses, in a pedestrian-oriented environment in keeping with Minturn's historic character. Mixed-use development, such as commercial on the ground floor and residential on an upper floor, is encouraged within this area. Or alternatively, a block could contain some commercial uses next to residential uses. It is recommended that the commercial uses allowed as a use by right be in line with those of the existing Old Town Commercial District (i.e., restaurants, liquor stores, banks, accommodations, drugstores, specialty stores). Residentially, single family homes, duplexes, accessory dwelling units, or one to two units above commercial should be allowed on all lots, with multi-family only allowed on lots greater than 7,500 square feet. Minimum lot size in this zone should be 5,000-2,500 square feet, with 80% maximum lot coverage for mixed use and commercial structures, and 50% for purely residential structures. Building heights of up to 28'-35' feet should be allowed; property owners should be able to gain an additional half-floor by meeting identified requirements.

*See the graphic set at the end of this chapter to view a rendering of an example MU1 block.*

**Mixed-Use 2.** This district is recommended along Highway 24 starting at the 800 Block. This district is intended to combine residential and commercial uses similar to Mixed-Use 1, but this area would have larger lots and would not have the same consolidated feel of the Mixed-Use 1 district. It can have industrial/service-style uses that would not be appropriate in Mixed-Use 1 (i.e., automotive parts shops, appliance repair shops). Mixed-use buildings here could be live-work light industrial spaces. For residential uses, single-family homes, duplexes, and multi-family should be allowed. Minimum lot size should be 5,000 SF with up to 60% lot coverage allowed and maximum building height of 28'. Multifamily (3+ units) should be allowed on lots over 7,500 SF.

*See the graphic set at the end of this chapter to view a rendering of an example MU2 block.*

**Mixed-Use 3:** This district is recommended along Highway 24 between the 500 and 800 Blocks. This district is intended to provide for residential uses and low-impact commercial uses. Other commercial uses, such as restaurants, could be allowed as conditional uses. Residential uses in this area should consist of single-family homes, duplexes, and multi-family homes. Minimum lot size should be 5,000 SF, and multi-family housing should be allowed on lots over 7,500 SF. Lot coverage of up to 60% should be allowed with a maximum building height of 28'.

*See the graphic set at the end of this chapter to view a rendering of an example MU3 block.*

**R1.** This district is recommended in the Old Town area one block off of Highway 24 and portions of Cross Creek. The intent of this district is to support residential uses on small lots and support smaller scale residential uses on those lots. The only allowable uses would be single family homes, duplexes, accessory dwelling units, home occupations, and civic uses. Minimum lot size should be 2,500 SF, with up to 50% lot coverage allowed and a maximum building height of 28'. A lot coverage bonus of 15% should be allowed for the development of deed-restricted ADUs.

**R2.** This district is recommended in other residential areas of town with small to medium size lots. The intent of this district is to support the eclectic mix of housing types found in Minturn. Single family homes, duplexes, and accessory dwelling units should be allowed on all lots, with multi-family only allowed on lots greater than 7,500 square feet. Minimum lot size should be 5,000 SF, with up to 50% lot coverage allowed and a maximum building height of 28'.

**R3.** This district is recommended for large lots on the edge of Town. The intent of this district is to support low density residential in a few areas where that is the existing development pattern. Only single-family homes, duplexes, and accessory dwelling units would be allowed in this district, at a minimum lot size of 2 acres.

**100 Block.** This district was updated concurrent to the community plan update and includes two

west of Main along Williams). The intent of this district is to incentivize sales-tax generating uses and the reuse of existing structures, while new construction is required to fit in with the rest of the 100 Block through new design standards and guidelines.

**Transit-Oriented Development.** This district is recommended for Dowd Junction, to allow a mix of uses and higher densities oriented around ECO transit. TOD represents smart growth as it supports use of transit and the coordination of transportation and land use. This area should continue to support recreational access, allow for commercial uses, and support attainable housing for long-term residents.

**Light Industry and Public Facilities.** This zone district is recommended for the Lionshead and Maloit Park areas where it currently exists, accommodating a range of low-impact light industrial uses and public uses.

*Note: Civic uses are permitted in all districts.*

**Industrial/Business Park.** This district is recommended for the Grouse Creek area, allowing a broad range of commercial services. This district has been extended from its current footprint to include industrial uses along the railroad as well. Dimensional standards are recommended to remain consistent with that of the existing Grouse Creek Commercial Zone.

**Federal Lands, Recreation, and Open Space.** This district combines federal lands, recreation areas, and Town open space within the Town of Minturn boundary. Management of or development on lands in these areas should be in accordance with any conservation easements and direction of the land manager. In all such areas, use should be consistent with the need to protect life and property from flood damage.

**Railroad ROW/Transportation.** This district is recommended to remain unchanged from the current zoning. If an agreement can be reached in the future with property owners, this district has potential for public use as a multi-use trail, transit line, or other recreational amenity. The Rio Grande trail in Aspen is a popular rails-to-trails project that could serve as a case study for this process. However, it is important to note that Union Pacific Railroad policies currently opposed to rails-to-trails.

#### **OBJECTIVE 1.2: PROMOTE A FAIRER, SIMPLIFIED DEVELOPMENT PROCESS IN MINTURN.**

In the development process, it is important to limit unnecessary barriers while still undergoing a rigorous review. Barriers can be related to the process that the developer or landowner must undergo or site requirements.

- **Action 1.2.1** Across all zone districts where multi-family is allowed as a use by right, at or above 15 total units, the project would need to go through the PUD process. However, if the project is a redevelopment of a lot already at or above 15 total units, it should not need to go through the PUD process. This allows smaller projects in which local impacts would be minor to proceed efficiently and at a lower cost to the developer, which allows these projects to provide the housing at more attainable prices. With a higher threshold for the number of units, the PUD process would allow for greater design flexibility, assurance of property planning, and further scrutiny of potential impacts such as access and impacts to neighboring properties.
- **Action 1.2.2** Explore reducing parking requirements for both commercial and residential uses. Focus efforts on areas with good connectivity to transit or with ample on-street parking.
- **Action 1.2.3** Resume efforts to revisit the Article 22 nonconformities section to encourage flexibility and appropriate expansions. Update parts of the code that allow owners of nonconforming buildings to make changes to their properties. The goals of this update

should be to maintain historic resources, incentivize reinvestment in older properties, increase accessibility of the property, and promote community character. Specific updates to nonconformity standards should be reviewed by the Town Council and Planning Commission.

- [Action 1.2.4 Consider implementing a lot coverage bonus of 15% for the development of deed-restricted ADUs in residential and/or mixed-use districts.](#)

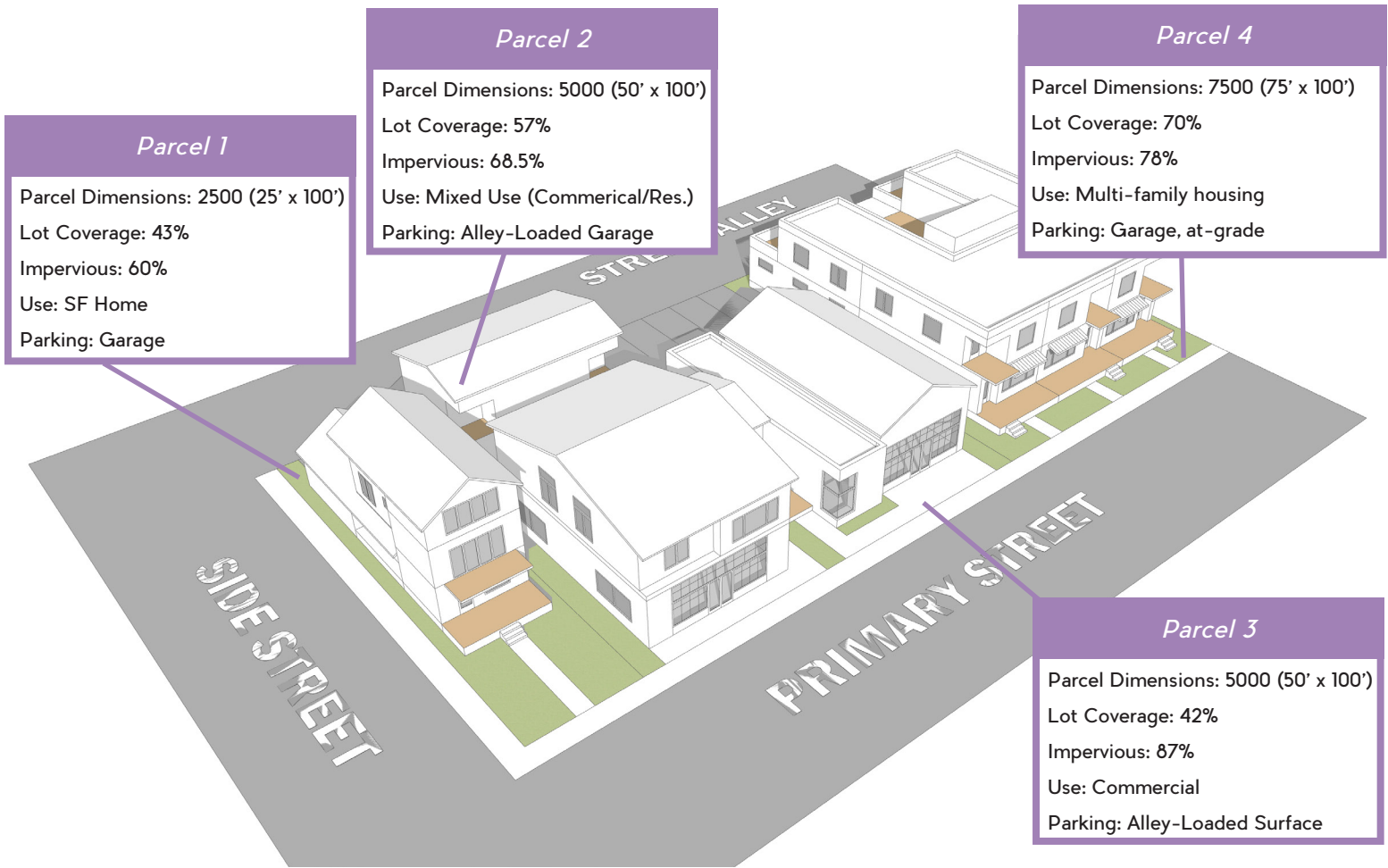
#### OBJECTIVE 1.3 – INCREASE RESIDENT ACCESSIBILITY TO ZONING AND LAND USE INFORMATION

It is important that the future code and zoning map be accessible to residents so that they understand what they are allowed to do.

- **Action 1.3.1** Provide clear and user-friendly resources for the public that explain Minturn’s zoning, such as a citizen’s guide or interactive zoning map.
- **Action 1.3.2** Clarify that residents are allowed to make upgrades to non-conforming structures, with limitations
- **Action 1.3.3** Increase access to and awareness of educational and financial resources (e.g., grants, tax credits) available to support historic preservation (federal and state).
- **Action 1.3.4** Continue to work with local fire mitigation entities and property owners to address any areas with increased fire risk; direct density to appropriate areas away from hazards such wildfires and flooding.
- **Action 1.3.5** Establish a walkable, cohesive development pattern that enables residents and visitors to access parks and the riverfront from Minturn’s Old Town and surrounding neighborhoods.

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# Mixed Use 1 (MU1)



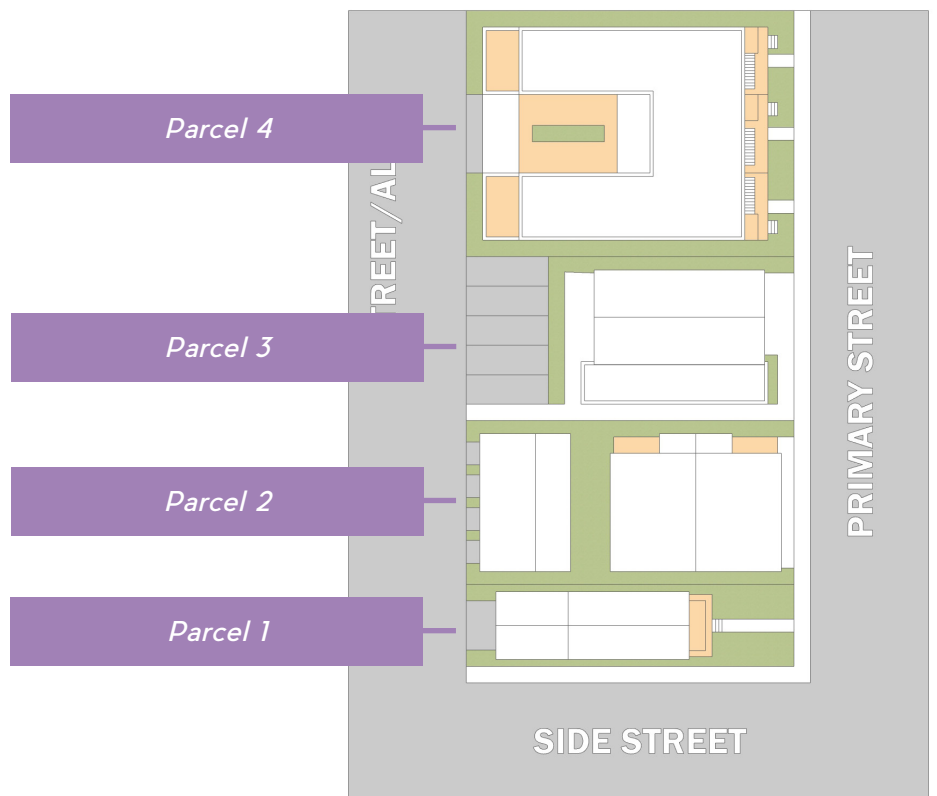
As shown in this massing, Mixed Use 1 (MU1) allows for a variety of uses, including single-family homes, duplexes, commercial uses, mixed-use commercial and residential buildings, and multifamily housing. The result is a dense, walkable neighborhood.

This massing assumes a minimum of 5' setbacks on all sides for all properties. This massing also assumes parking standards of 1 space per residential unit and 1 space per 450-500 sf of commercial space. The massing assumes a shared parking agreement for the mixed-use parcel.

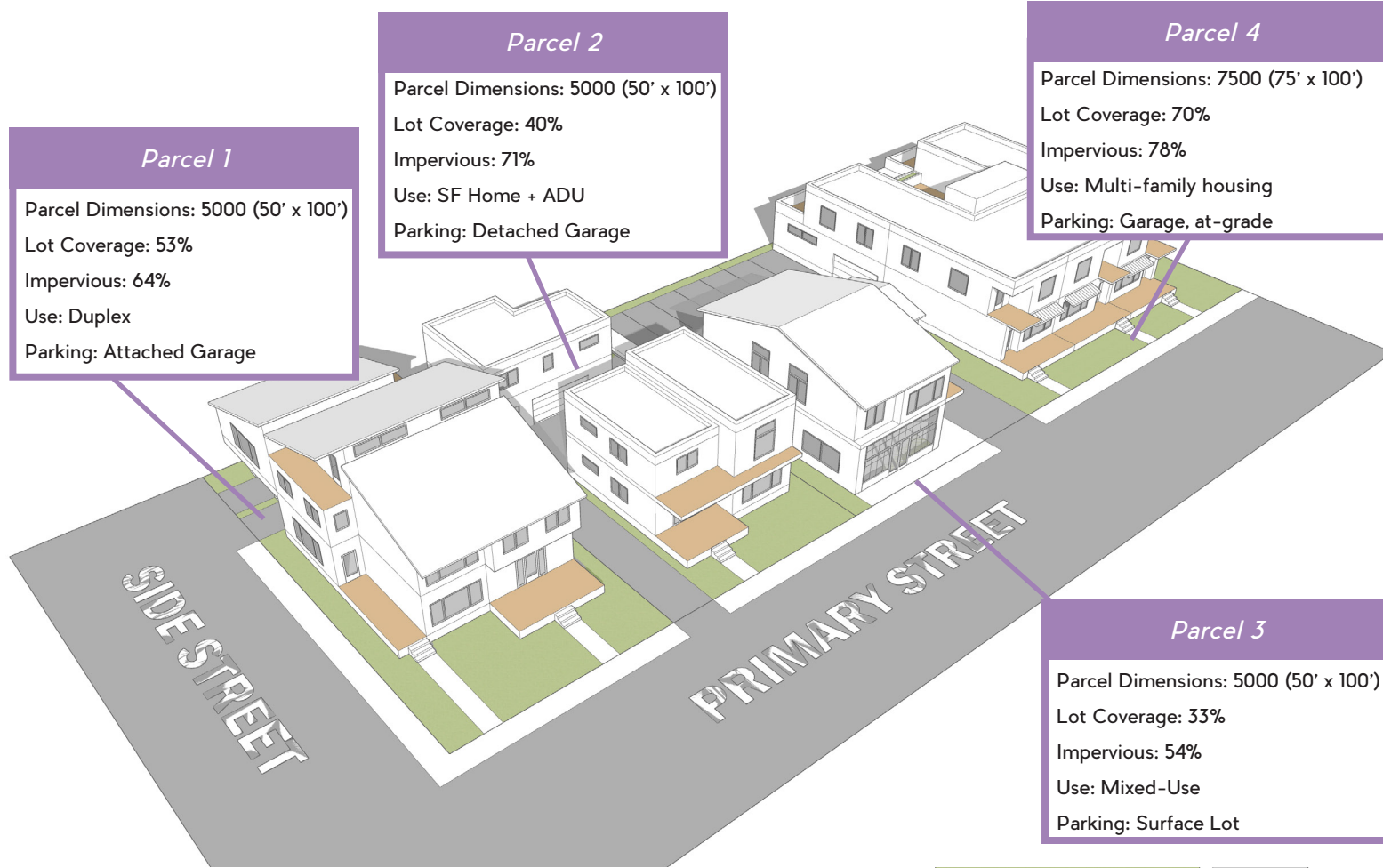
\*Note: This rendering considers lot coverage as it is currently defined in the Town Code. Minturn's definition of "building lot coverage" generally includes building footprints and not all impervious surfaces.

**Recommended District Standards:**

- Min. Lot Size (SF & Duplex): 2500 sf
- Min. Lot Size (MF): 7500 sf
- Max. Lot Coverage (Commercial or MU): 80%
- Max. Lot Coverage (Residential): 50%
- Height limit: 28-35'



## Mixed Use 2 (MU2)



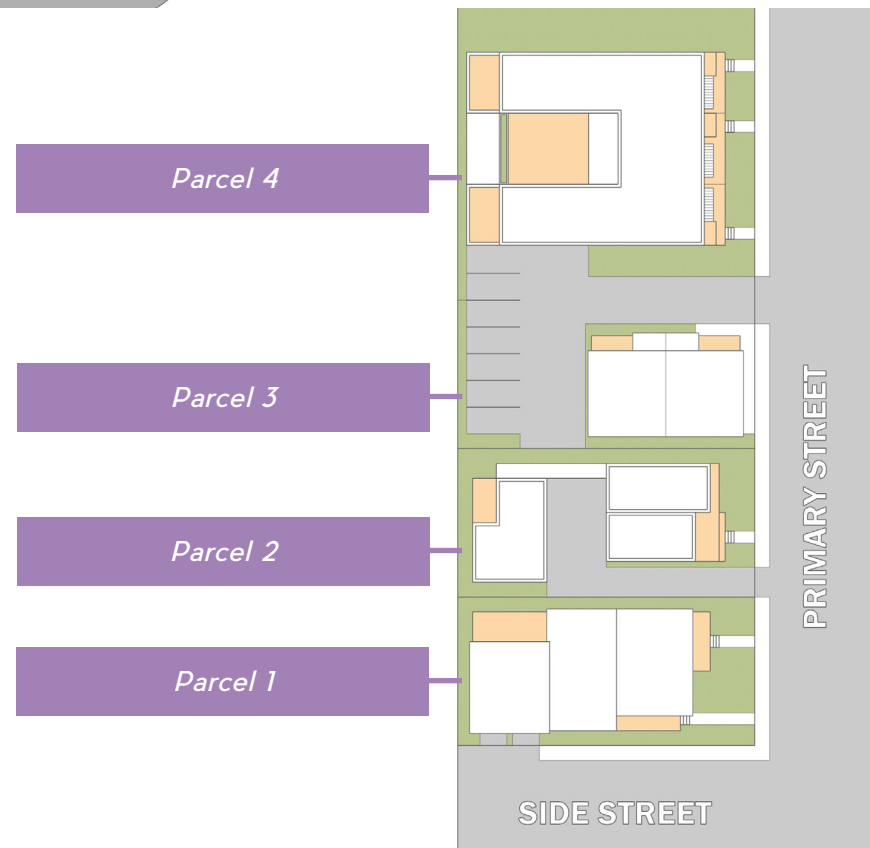
As shown in this massing, Mixed Use 2 (MU2) allows for a variety of uses, including single-family homes, duplexes, commercial uses, mixed-use buildings and lots, and multifamily housing. The intent of this district is to encourage mixed-use lots and more land-intensive commercial uses that may not be appropriate for the dense, walkable pattern intended for MU1.

This massing assumes a minimum of 5' setbacks on all sides for all properties. This massing also assumes parking standards of 1 space per residential unit and 1 space per 450-500 sf of commercial space. The massing assumes a shared parking agreement for the mixed-use parcel.

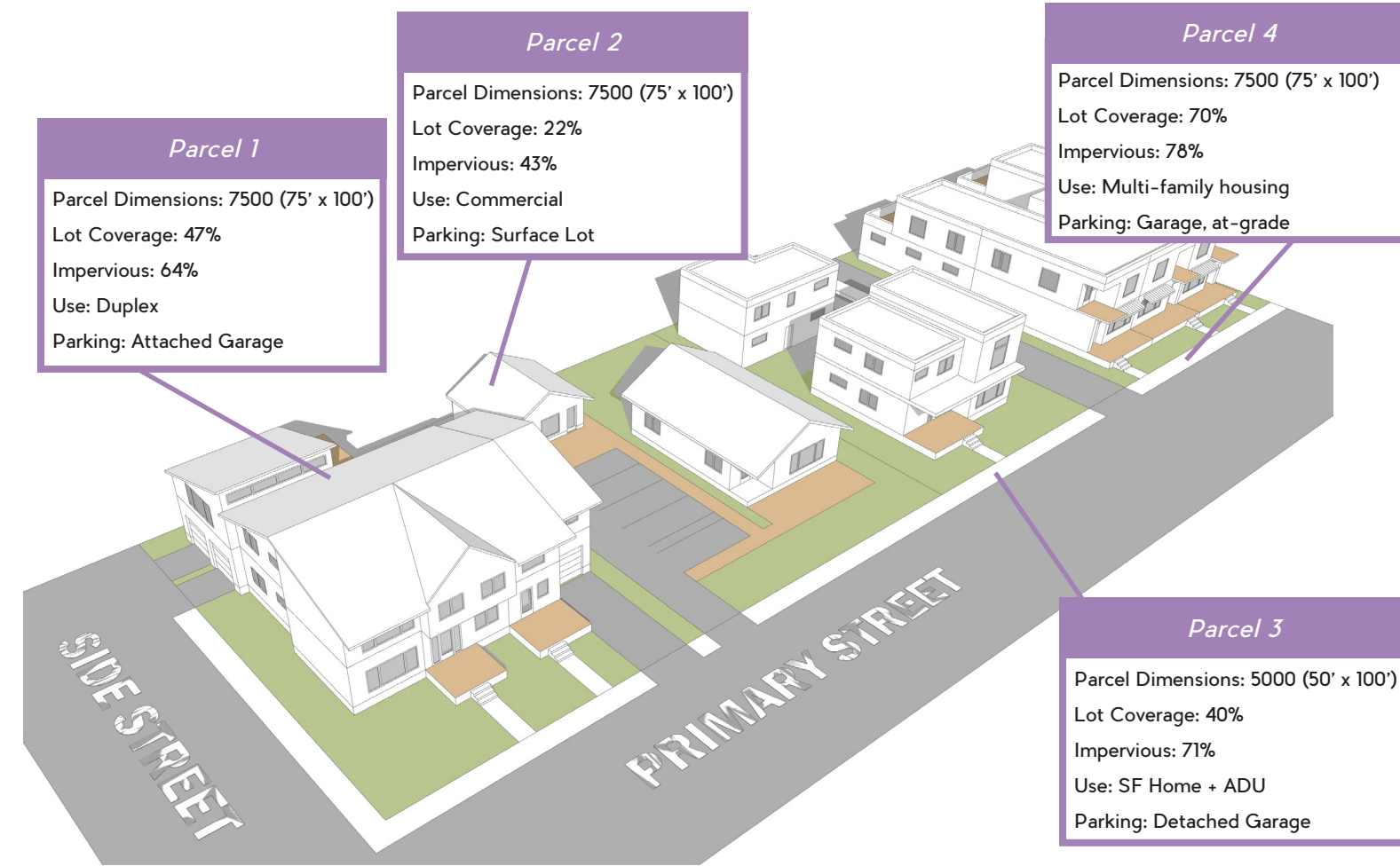
\*Note: This rendering considers lot coverage as it is currently defined in the Town Code. Minturn's definition of "building lot coverage" generally includes building footprints and not all impervious surfaces.

### Recommended District Standards:

- Min. Lot Size (SF & Duplex): 5000 sf
- Min. Lot Size (MF): 7500 sf
- Max. Lot Coverage (Residential): 60%
- Height limit: 28'



## Mixed Use 3 (MU3)



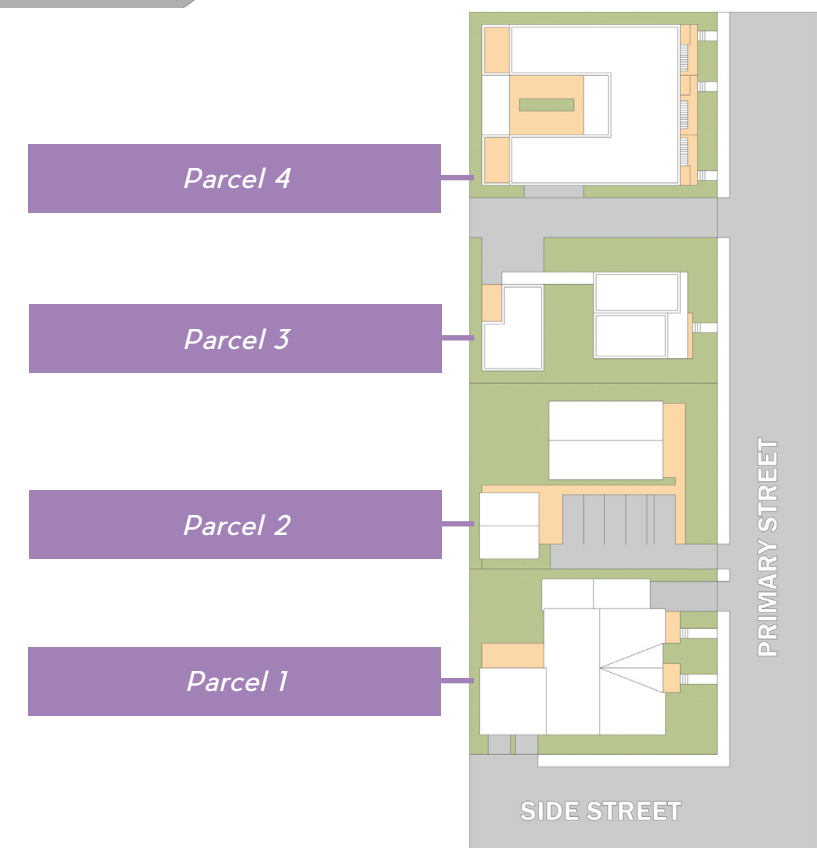
As shown in this massing, Mixed Use 3 (MU3) allows for a variety of uses, including single-family homes, duplexes, commercial uses, mixed-use commercial and residential buildings, and multifamily housing. The intent of this district is to include residential and low-impact commercial uses in an area that is currently residential in character.

This massing assumes 5' minimum setbacks on all sides for all properties. This massing also assumes parking standards of 1 space per residential unit and 1 space per 450-500 sf of commercial space. The massing assumes a shared parking agreement for the mixed-use parcel.

\*Note: This rendering considers lot coverage as it is currently defined in the Town Code. Minturn's definition of "building lot coverage" generally includes building footprints and not all impervious surfaces.

### Recommended District Standards:

- Min. Lot Size (SF & Duplex): 5000 sf
- Min. Lot Size (MF): 7500 sf
- Max. Lot Coverage (Residential): 60%
- Height limit: 28'



# MINTURN



COMMUNITY PLAN  
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## CHAPTER 2

### CHAPTER 2. A THRIVING 100 BLOCK

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#### Introduction

The 100 Block of Minturn is the town's historic commercial center. While it has always had a mix of residential and commercial uses, the vision is for the 100 Block to become more commercial and over time. The community has appreciated the recent growth and development addition of artistic businesses and gathering spaces, such as the Agora, and there is a desire to see the area become more commercial and vibrant with more shops and restaurants. The 100 Block has been built over time, resulting in an eclectic character with a variety of architectural styles. People like that it doesn't look like other downtowns – it is uniquely “Minturn.” However, there have been some disagreements about how to address redevelopment. Recent development proposals and the single-owner acquisition of multiple properties have spurred debate, leading to a series of design discussions which have resulted in new design standards and guidelines for the 100 Block. Some residents and business owners favored stricter limitations on development, whereas others viewed ed new retail spaces and second-floor residential units as key to Minturn's economic viability. Some viewed ed stringent design standards and historic designations as important protections for Minturn's downtown character and charm. While others valued the opportunity for architectural creativity that comes with looser regulations. There is also disagreement on whether the 100 Block should be primarily residential, commercial, or mixed-use in nature. There is solid agreement that circulation for both vehicles and pedestrians should be improved to increase safety and to enhance access to Minturn's natural assets, such as the Eagle River. There has also been some spirited debate around parking availability and relaxing parking regulations for commercial uses to incentivize commercial, sales tax-generating uses. A few recent development proposals and single-owner acquisition of multiple properties have spurred debate, leading the town to initiate a series of design charrettes and presentations focused on trying to find consensus and a clear direction forward. A 6-month moratorium on development was issued to provide time to work through 100 Block development debates.

Concurrent with the writing of this Community Plan, Town Council issued a 6-month moratoria on new development and demolition proposals in order to Minturn updated its the 100 Block Design Standards and Guidelines. These regulations can be found throughout Chapter 16 of the Minturn Municipal Code and in the “Appendix B Insert,” which was approved by the Town Council in August —October of 2022<sup>[JR1]</sup>. This chapter takes the prescriptive elements of the 100 Block Design Standards and Guidelines and contextualizes them within the plan's broader vision for Minturn. This future vision for the 100 Block reflects community input, identifies suitable residential and commercial uses and opportunities, and seeks to find a balance between opposing viewpoints. Critically, this vision recognizes and supports the 100 Block as the heart of Minturn.

## Existing Conditions & Issues

Minturn's 100 Block is bounded by Highway 24 (Main Street) to the north and Toledo Avenue to the south. The Eagle River forms the eastern edge, and the hillside with the "Minturn" water tank as a prominent feature forms the western edge [of the 100 Block area](#).

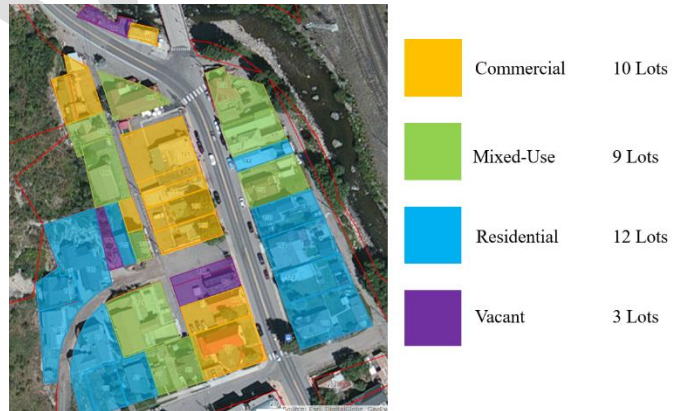
Vehicles access the 100 Block, as well as the broader Town of Minturn, via Highway 24/Main Street. There are two limited width streets that run parallel to Main Street – Williams Street to the west and Eagle River Street to the east. Each of these streets are only 15-foot wide and currently operate as one-way streets going south. On-street parking is available for both residents and businesses, and a municipal lot north of the 100 Block provides additional parking capacity for busy days.

Pedestrians can navigate the 100 Block via sidewalks and on Williams and Eagle River Streets, which function both as pedestrian and vehicular circulation routes. East-west access is primarily available via Toledo Avenue.

Most structures in the 100 Block are one to two stories tall. Williams Street contains two three-story buildings: the Molly G and the Minturn Hotel. The Molly G is the only building in downtown Minturn that provides underground parking.

Minturn's 100 Block has many assets, including boutique stores, eclectic restaurants and outdoor gathering spaces, inspiring scenery, and a successful farmers' market. From Minturn's downtown, individuals can access world-class outdoor recreation opportunities – this quality is rare even in Colorado mountain communities. While the 100 Block is effectively Minturn's "downtown," it is not the town's only retail commercial area, as Main Street includes other commercial and mixed-use developments into the 200-400 blocks. There are additional commercial buildings into the 900 Block and 1000s, including the Battle Mountain Trading Post, Rocky Mountain Adventure Rentals, and a new yoga studio. The town also contains a business park and several light industrial businesses. Dowd Junction and South Minturn present additional potential for commercial development and sales tax generation.

Over time, some commercial lots in the 100 Block area were redeveloped into single family homes. Today, there is a mix of 12 single-family residences, 10 strictly commercial uses, 9 mixed-use properties, and 3 vacant lots (see inset map). Historically, 100 Block businesses and residences have been owned by a diverse mix of Minturn residents. This has changed since 1985, when Minturn Realty acquired several properties in the 100 Block. At present, [31% of 100 Block properties](#) are owned by a single property owner – MR Minturn. The ownership of so much of the 100 Block by a single individual has exacerbated some residents' fears that loose development standards could enable a single architect to reshape the character of Minturn's downtown in a way that strips the 100 Block of its current charm.



The 100 Block is experiencing specific challenges, including perceived parking issues, limited pedestrian and vehicular circulation, a lack of coordinated signage and wayfinding, and a [need](#) [desire](#) [for](#) [to](#) connectivity to the nearby Eagle River. As described above, the 100 Block

has also been the subject of ongoing public debates regarding which zoning changes will most benefit Minturn in the future. During this planning process, residents expressed that Minturn is struggling to define its identity as distinct from development in the rest of the Vail Valley. The 100 Block, as a focal point of Minturn, is a key facet of this identity and community character.

Since the 100 Block area is varied in its character and scale, it has been challenging for the Town to develop a zoning designation that encompasses the variety of built forms in the area. Many buildings are non-conforming and historic in character. Many residents would like to preserve historietraditional structures, but the code does not currently permit some of them as constructed. This becomes challenging when these buildings need upgrades and changes beyond routine maintenance.

In 2021 and 2022, Minturn ~~recently created~~enacted two moratoria on demolition and new development, launched an historic preservation process, and initiated an update to its 100 Block Design Standards and Guidelines. In May of 2020, a moratorium on the allocation of water taps was passed, limiting new construction projects to no more than three single-family equivalents eachabove historic use on a property. In November of 2021, a moratorium on 100 Block building, alteration, and demolition permits was passed, preventing new development from occurring before Minturn’s land use and zoning code could be updated (since repealed). During January of 2022, Minturn began the process of creating a Historic Preservation Policy, which may be utilized to create a Historic Preservation District or identify Historic Landmarks throughout town. This policy was adopted in June of 2022. The Town has also been working with Downtown Colorado Inc. and the “Downtown Doers” Group on opportunities for the organization of the business community. The Town Council is currently considering forming a Downtown Development Authority (DDA).

Concurrently withto the Community Plan Update, Minturn began the process of updating its 100 Block Design Standards and Guidelines. Upon completion, tThese standards and guidelines, adopted in October of 2022, will provide a framework to guide design decisions and sets site design standards for properties in the designated 100 Block. The 100 Block Design Standards and Guidelines replace the former 100 Block Character Area from Appendix B of the Minturn Municipal Code, as well as update the sections of Chapter 16 that relate to the 100 Block for consistency.

### Community Input

The 100 Block has been the subject of extensive community engagement prior to and during this community planning process. In October 2020, the Town convened the first of two Design Charettes intended to draw out community priorities and design preferences for the 100 Block. At this event, residents were asked to rank their top priorities for the 100 Block from a list of improvements and design options. In general, participants favored improvements that support the Town’s quality of life and quaint character. The top priorities included dedicated spaces for arts and culture; improved circulation, connectivity, river access; biking amenities (bike racks, wash/repair station); landscaping (street trees, lawns, flower boxes); and public gathering spaces (plazas, pocket parks). Participants also noted their interest in preserving key views in Town, such as the view of the water tower.

In February 2022, the Town convened a second 100 Block Design Charette. This workshop was intended to confirm and refine the community priorities that were identified at the previous engagement event. Participants were again asked to rank their top design priorities for the 100 Block. The responses mirrored the results from the 2020 charette; however, parking and housing ranked among the top five priorities for 2022. This workshop helped identify two subareas of the 100 Block, which are the 100 Block: A and 100 Block: B subareas. These areas are distinct



portions of Minturn's 100 Block that warrant different design standards and guidelines. In general, the 100 Block: A encompasses the areas with frontage on Hwy 24/Main Street 100 Block: B generally includes the area west of Williams Street, which does not have frontage on Hwy 24. Participants in the workshop were generally supportive of the distinction between these two areas, as this framework will enable more targeted height requirements for different areas of the 100 Block.

At the second and final Plan Open House, which was held on September 27, 2022, community members were invited to rank several potential improvements to the 100 Block. Respondents expressed the most support for enhancing gathering spaces, establishing river access, and creating shared streets. They expressed the least support for creating a town gateway and downtown signage/wayfinding.

### Central Concepts

The Minturn community understands the rich potential of the 100 Block to serve as a community hub that is representative of the town's history and spirit. Though residents and community representatives disagree about some of the specific design standards that should apply to the 100 Block, the community is united in the belief that the 100 Block is a special place. It is distinct from the rest of Minturn's districts, and it is a unique environment compared to the downtowns of neighboring communities. The primary community values for the 100 Block include economic development, vibrancy, connectivity, and charm.

### THE IDENTITY

Minturn residents have a clear idea of what the character of the 100 Block *is* and *is not*. Throughout this planning process, residents expressed that Minturn is distinct from neighboring communities, which have extensive resort-adjacent development. In contrast, the 100 Block is quaint and historic in its character and offers a different vibe and charm.

The character of the 100 Block (and Minturn as a whole) has been called "quaint" and "funky." Several design features promote this "funky" character, including vibrant colors of downtown buildings, distinctive building facades, and variable roof heights and designs. The 100 Block Design Standards and Guidelines included in Appendix B of the Town code attempt to capture and promote these key design features.

### WHAT WILL CHANGE?

While change is often feared, especially in small communities, it is inevitable. Most of the structures in the 100 Block area are decades-old, and they will require updates and changes over time. Today, development pressure exists everywhere in Colorado, and Minturn is no exception. Many properties have changed hands in recent years, with several now held by the same owner. Therefore, it is important to prepare for change. The recommendations within this chapter and the 100 Block Design Standards and Guidelines aim to preserve the essence of the 100 Block area amidst future changes and redevelopments.

The concepts for the 100 Block attempt to connect Main Street to the area's nearby assets, such as the Eagle River, and celebrate its uniqueness and history while preparing for future change. As the historic centerpiece of town, the 100 Block has potential to serve more functionally as a "downtown" area with more complete transit opportunities, community gathering spaces and a mix of uses including more vibrant commercial uses like retail shops and restaurants. At present, the area primarily caters to car travel. A primary goal for the future of the 100 Block is to create a

more pedestrian, bike, and transit-friendly environment while also improving connectivity for car travel and accommodating deliveries to businesses.

#### Goals of the 100 Block Vision & Framework

- Create a vibrant and walkable “hub” for the community with a variety of shopping, dining, events, arts, and gathering opportunities.
- Celebrate the Eagle River and Minturn Water Tank as community amenities with views of and access to them.
- Complement the 100 Block’s distinctive historic character and development patterns.
- Improve connectivity and efficiently direct visitors to parking, prioritizing the municipal lot to the north.

#### THE FRAMEWORK

The Framework Diagram describes key principles for creating a cohesive 100 Block. Improvements will require both public and private sector investment to implement. Key concepts for improving the 100 Block include:



*Rendering depicting gateway features that would enhance downtown Minturn and improve circulation for people walking and driving.*

- 1) **Gateway** [GU3] – the intersection of Highway 24 with Main Street going north across the river is the primary entrance to Minturn. Recent improvements like the small plaza by Magustos, curb extensions with landscaping, gateway lighting elements, and a signalized pedestrian crossing are wonderful additions. To build on these improvements, the town should:

- a. Add a welcoming feature such as a “Welcome to Minturn” monument sign, public art feature, or special lighting.
- b. Add more pedestrian scaled lighting such as bollards or pedestrian lights that could help illuminate this area at night.
- b.c. Slow down auto traffic and provide more pedestrian crosswalks that designate where pedestrians can safely cross the street. A stop sign at the gateway intersection should be explored which would require vehicles to stop, allowing them to pause and consider turning to access the municipal lot while preventing auto-pedestrian conflicts.
- e.d. Increase the size of the parking signage to clearly direct cars to the municipal lot. Additionally, a sign further out notifying people about the public lot would be beneficial so drivers can think ahead and prepare to turn.
- e. Make pedestrian improvements to Bellm Bridge such as adding lighting to illuminate the bridge at night and create a more pleasant and safe walk between the 100 Block and municipal parking lot. Future improvements could also include widening the sidewalk area on the eastern edge of the bridge and include an expanded overlook area with seating.

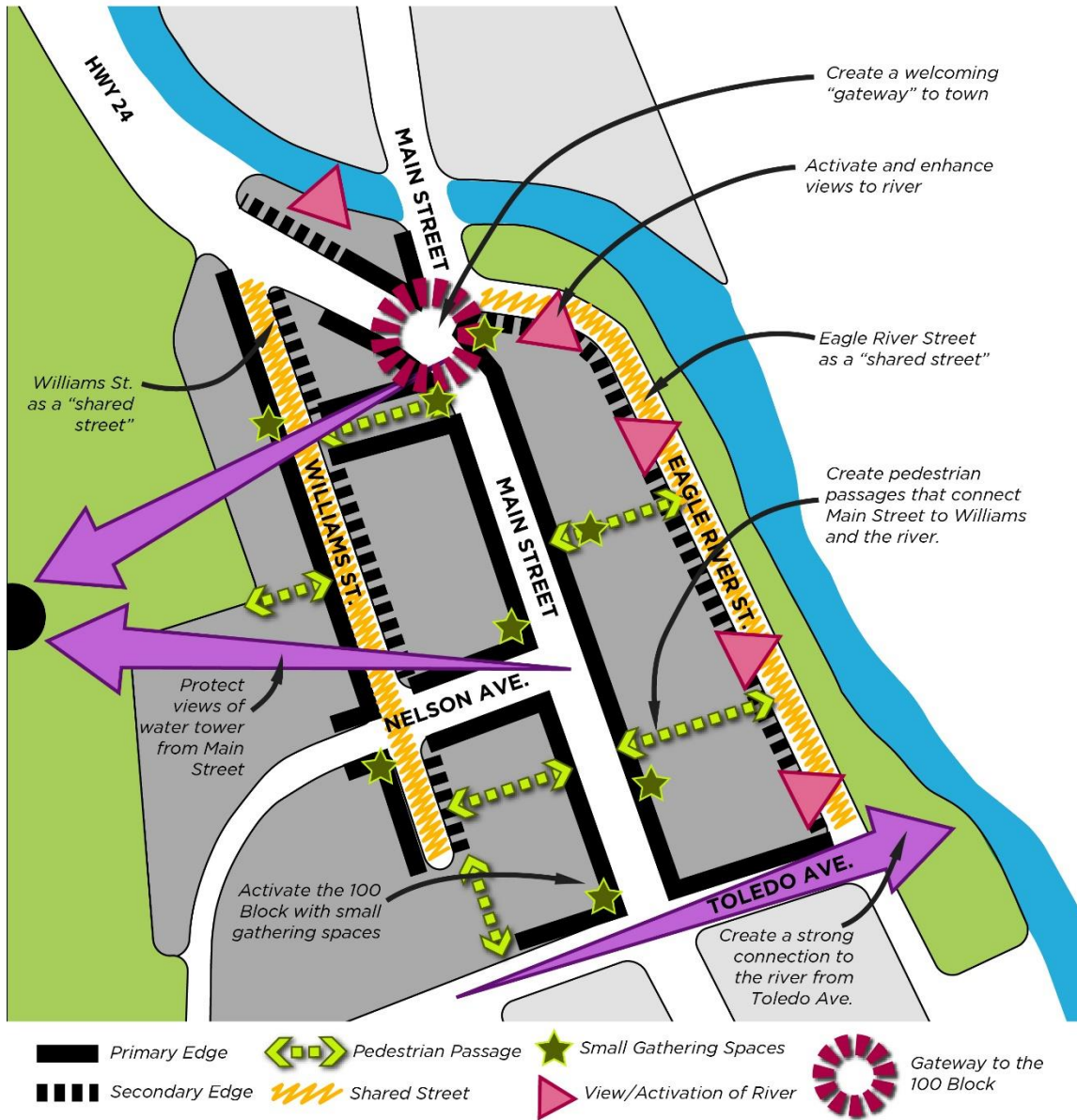


d. Rendering of Williams Street as a woonerf, with unique paving treatments, landscaping, and pedestrian-scaled lighting.

- 2) **Shared Streets**<sup>[GU4]</sup> – Williams and Eagle River Streets have potential to transform into unique, character-rich shared streets known as a “woonerf.” A shared-streetwoonerf is when the street that is designed to be shared by pedestrians and cars. Design features should include a unique paving treatment, such as colored or stamped concrete or unit brick pavers that cause vehicles to slow down, and pedestrian-scaled lighting. It is a goal that

active uses would also orient to these streets to provide more vibrancy and activity along them.

- 3) **Active Edges** – the framework diagram shows primary and secondary edges. Primary edges are desired to be very active – with shopfronts, multiple entrances to businesses, and café dining. Secondary entrances, along the “back” sides of Main Street-facing properties, are also desired to include active ground floor uses to activate the shared streets, but it is also recognized that these areas may be necessary for ~~parking and access~~ other “back of house” needs. Still, these building faces should not be blank walls and should be articulated in a manner similar to the rest of the building to provide visual interest. Unique designs are encouraged to provide activation here.
- 4) **Pedestrian Features** – the experience to walk around the 100 Block should be unique and fun. Pedestrian passages are noted on the Framework Diagram that are intended to be quaint, side-yard passages that connect people from Main Street to the shared streets and river. Also highlighted are “small gathering spaces” which are intended to be small “inlets” along the Main Street sidewalk and shared streets that provide areas to rest – they could include benches or outdoor dining. It should be noted that the diagram’s locations are not required, but rather indicative of the amount number of passages and gathering spaces desired.
- 5) **Views** – enhancing views to the Minturn Water Tower and Eagle River are also desired. Views should be experienced both within and outside of buildings.



## THE CIRCULATION CONCEPT

This plan shows both short-term and long-term considerations for the town to explore (diagrams shown below). In the short-term, pedestrian circulation in the 100 Block can be improved by adding pedestrian passages mentioned in the Framework and making all streets pedestrian-friendly. Auto circulation is more difficult because there are few streets and limited rights-of-way.

Auto circulation would be dramatically improved by reducing the time people spend circulating to find a parking spot and by providing designated areas for deliveries so that delivery trucks are not blocking access for others. It is recommended that Main Street have a few dedicated delivery spots where delivery trucks can park to deliver goods to businesses on either side of the street. This will demand clear signage and enforcement of the rules, as well as working with 100 Block businesses to train their distributors of the new rules. To better direct people to the municipal lot, it is recommended that new signage be added - before arriving in Minturn, and at the gateway intersection - to clearly notify drivers that there is ample parking in the municipal lot north of the 100 Block. It should also be explored with CDOT to add a stop sign at the gateway intersection to allow drivers to "pause" and consider turning to access parking as well as allow for an additional crosswalk to safely get pedestrians from the municipal lot onto Main Street.

Williams and Eagle River Streets are so narrow that they can only serve as one-way streets. The design of both Williams and Eagle River Streets should be as a "shared street" design, or "woonerf," so that cars travel at 10 miles per hour or less and share the street with pedestrians. To address concerns with increasing the load of these narrow streets, the gateway signage to direct people to parking should help.

In the long-term, the town should consider the possibility of reversing the direction of the one-ways to northbound, which would help with the "dead end" conditions that occur on Nelson and Toledo Avenues today. This plan looked at the opportunity to reverse the direction of these one-way streets to northbound, which would allow people another choice once they get further into the 100 Block to turn around and head back to the municipal lot to park. It was ultimately decided, at least for the short-term, to keep these streets as southbound one-ways. This is because CDOT would not allow a left turn from Williams Street onto Hwy 24, as this which could cause additional confusion for people trying to leave Minturn. And for Eagle River Street, the intersection with Main Street is so close to the three-way gateway intersection and Bellm Bridge, that it also would have to be a right-turn only. Once improvements are made to Minturn Road that would allow it to handle additional traffic volumes, this concept could be considered, as Minturn Road also allows a way "out of" Minturn and back to Hwy 24. A Future Circulation Concept was developed for the 100 Block to address key issues for vehicular circulation within the area. The concept proposes to reverse the one-way direction of Williams Street which would alleviate the "dead-end" situation experienced when one turns right onto Nelson. Instead of figuring out how to turn around, which is dangerous and clogs the system, cars could instead turn onto Williams Street to head back to the gateway (right turn only on Highway 24) and municipal lot to park. Eagle River Street was also considered to convert to northbound, but due to the close intersection of Highway 24/Main Street/Eagle River Street near the bridge, it was advised by the town to leave as southbound. The design of both Williams and Eagle River Streets should be as a "shared street" design, or "woonerf," so that cars travel at 10 miles per hour or less and share the street with pedestrians. To address concerns with increasing the load of these narrow streets, the gateway signage to direct people to parking should help.

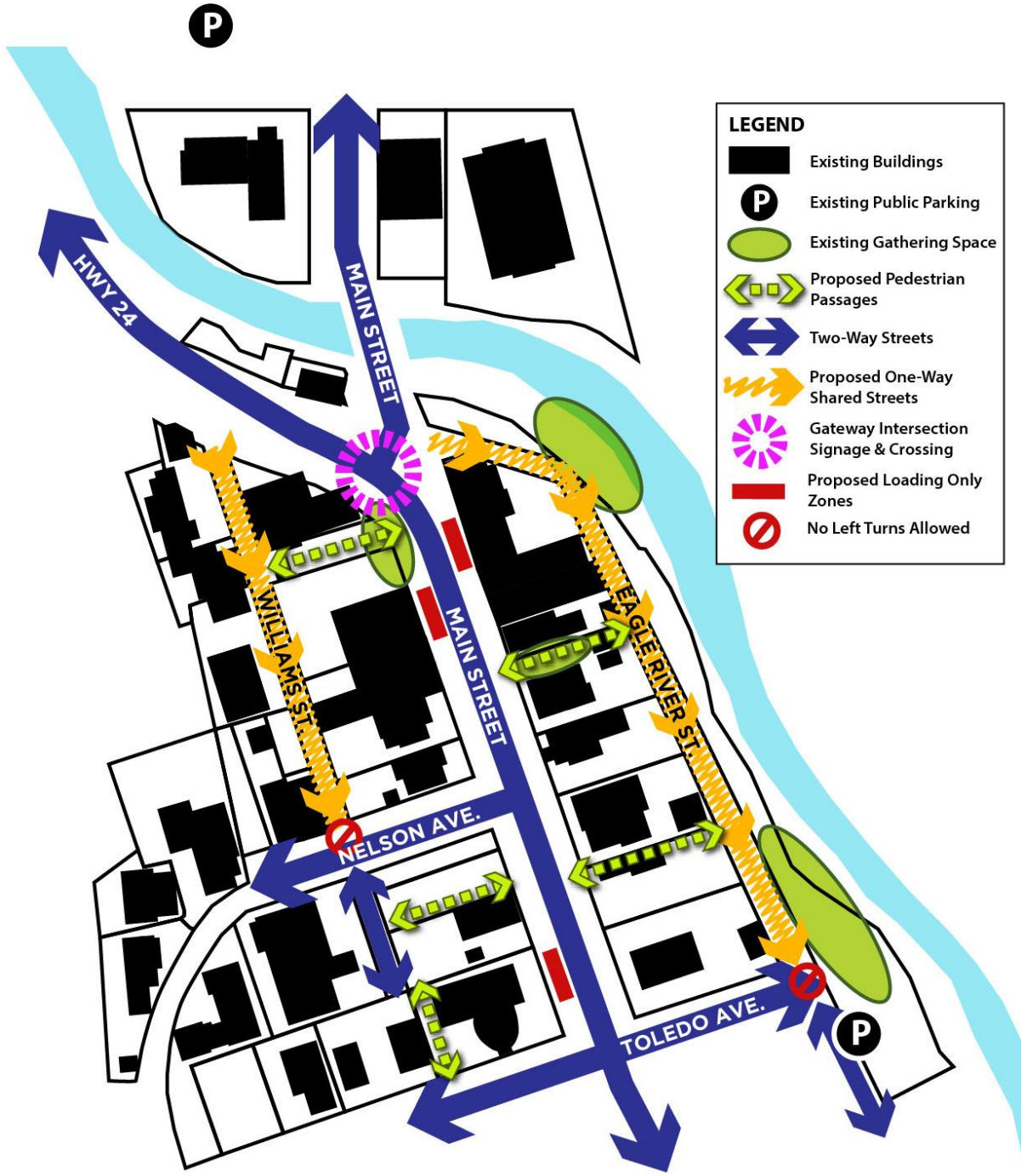
The long-term circulation concept also shows a potential land swap between the town and MR Minturn that would trade the existing easement between Magustos and Minturn Country Club for

a dedicated space along Nelson Avenue. This could be a long-term solution to create a permanent “town square” that could host events and the centerpiece for the Minturn Market. It has a similar view of the water tower as well. ~~During regular days, cars could circulate around it and during events the streets could be partially closed to cars to allow more event space.~~

~~The Circulation Concept preserves vehicular right-of-way on Hwy 24/Main St. Additional placemaking efforts will increase the visual interest and activity of the Main St. area, encouraging drivers to slow down and watch for pedestrians.~~ For additional information regarding the Circulation Concept, see **Chapter 6: Intuitive Mobility, Circulation, and Connectivity.**

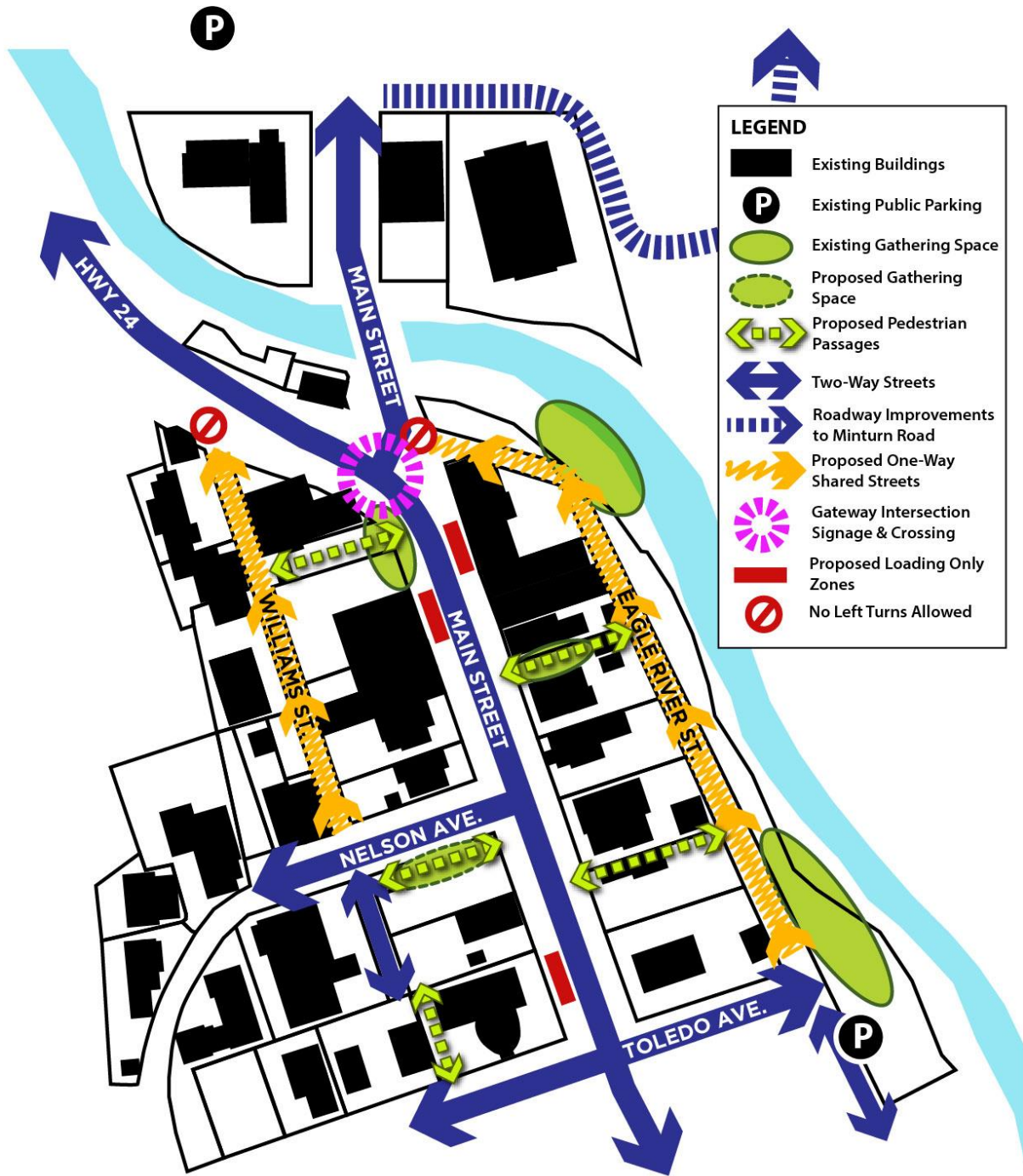
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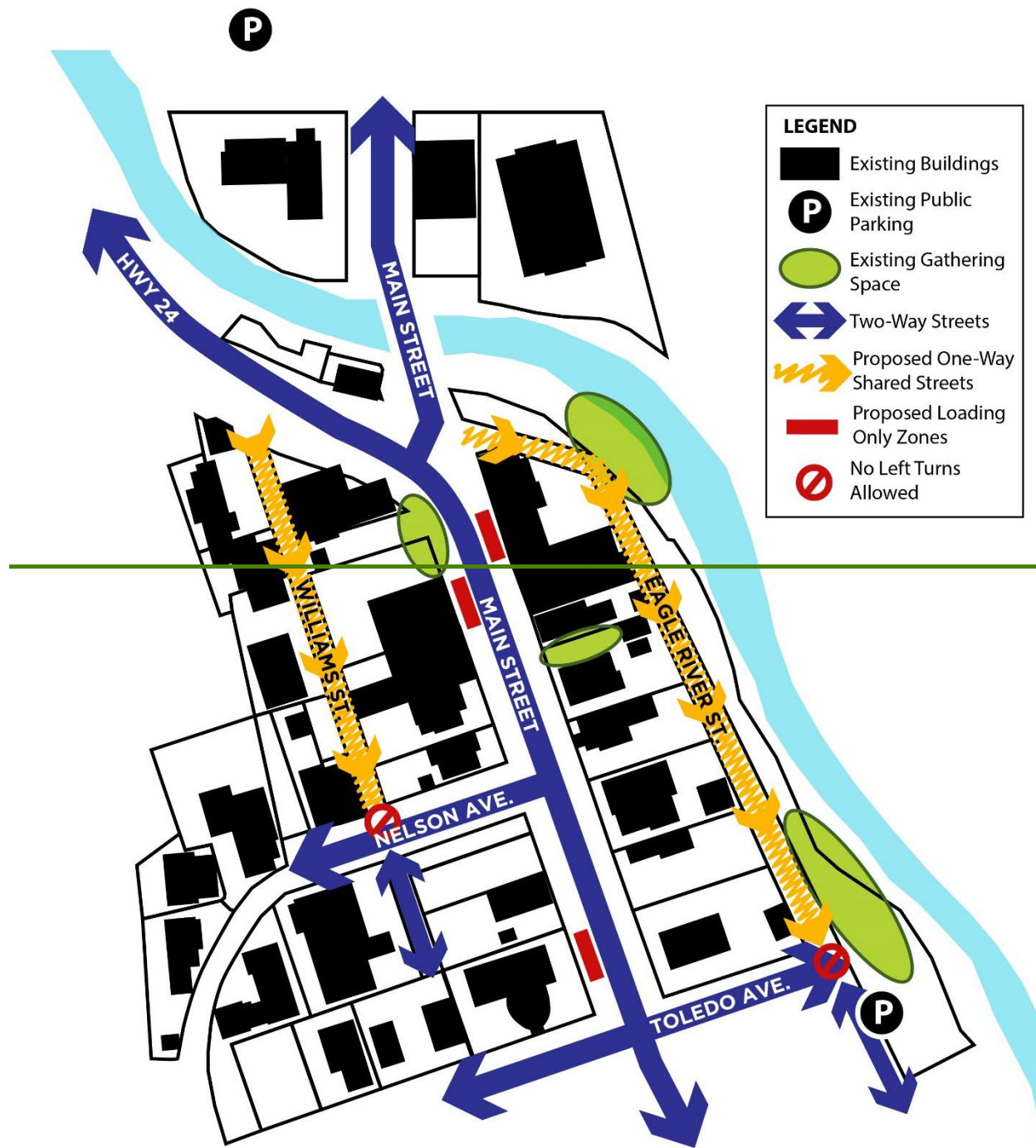
# SHORT-TERM CIRCULATION RECOMMENDATIONS





# LONG-TERM CIRCULATION RECOMMENDATIONS





### THE DESIGN STANDARDS

In addition to the Framework and Circulation concepts, the Design Standards and Guidelines provide guidance on site and building design. The purpose of these guidelines is to encourage

vibrancy and authentic development in Minturn's 100 Block, supporting the economic health and historic charm of the area.

### *Enhancing the Public Realm*

As established in the 100 Block Design Charette events in 2020 and 2022, the public realm is an important facet of the 100 Block area. The design guidelines provide a variety of methods to promote vibrant, active businesses and public spaces within the 100 Block. For example, the guidelines include a series of design considerations to help orient businesses to public spaces [and to provide staggered setbacks that allow for small gathering spaces to front onto Main Street.](#)

### *Encouraging Compatibility with Existing Character*

The design standards describe the desired future development character of the 100 Block as a "pattern quilt." As a "pattern quilt," the 100 Block area is made up of distinct building forms and designs. The design guidelines encourage certain design features already present in the 100 Block, such as vibrant exterior paint colors and the use of natural materials but also encourages each project to be unique. The scale of buildings are also required to fit in with existing structures and design standards ensure that "bulky" buildings will not be allowed. Building heights remain at a pre-established 35-foot height limit, but properties along Main Street can only go up to 35-feet if they meet certain requirements and the 3<sup>rd</sup> floor is also required to be set back on all sides to reduce the perceived mass. Other design standards and guidelines ensure that new designs fit in with old ones, maximize sun access, and add to the eclectic character.

## THE ALLOWED USES & DENSITY

The zoning and land use framework for the 100 Block was developed alongside the 100 Block Standards and Guidelines document. The proposed zoning approach divides the 100 Block into the 100 Block: A and 100 Block: B subareas. The 100 Block: A subarea contains Main St., while the 100 Block: B subarea generally contains areas west of Main St.

As described in **Chapter 1: Smart Land Use & Practical Zoning**, this plan recommends that the 100 Block constitute a distinct zoning district within the Town of Minturn, replacing the 100 Block Commercial Zone district. Allowed uses within this district include retail commercial, lodging, and offices. Residential uses ~~remain conditional uses, mainly due to their parking and access requirements~~ [are allowed on upper floors only](#). This complements the Town's strategic priority to maintain and enhance the 100 Block as the primary commercial core for sales tax-generating uses.

This plan encourages some residential to be mixed in with commercial in the 100 Block. Some residential already exists and more could be added. Mixed-use areas have the added benefit of encouraging interesting, walkable street environments. Residential units also support commercial development and may be necessary for new development to be financially feasible. See **Chapter 3: An Economically Vibrant Community** for a detailed discussion of this topic.

## Recommendations

### OBJECTIVE 2.1: IMPLEMENT A FRAMEWORK FOR REDEVELOPMENT AND GROWTH IN THE 100 BLOCK THAT CONTRIBUTES TO MINTURN'S CHARACTER AND ECONOMIC DEVELOPMENT.

The Design Standards and Guidelines within Appendix B outline a framework for the 100 Block that is both practical and inspiring. To achieve the vision for the 100 Block described in this chapter, the Town of Minturn should use the Design Standards and Guidelines to guide future development/redevelopment efforts.

- **Action 2.1.1** Ensure that future ordinances and land use practices for the 100 Block align with the framework established by the 100 Block Design Standards and Guidelines.
- **Action 2.1.2** Distribute the Design Standards and Guidelines to all developers and property owners pursuing projects within the 100 Block and/or in the recommended Mixed Use 1 zoning district (See **Chapter 1: Smart Land Use & Practical Zoning**).
- **Action 2.1.3** Provide technical assistance in interpreting the 100 Block Design Standards and Guidelines to property owners as needed and appropriate.
- **Action 2.1.4** Continue efforts to organize business and property owners around the formation of a Downtown Development Authority (DDA); see **Chapter 3: An Economically Vibrant Community** for additional details.

#### STAFF PROPOSED NEW ACTION STEP

- **Action 2.1.5** Ensure that ground floor land uses are dedicated to commercial services that generate retail sales and/or lodging tax.

### OBJECTIVE 2.2: IDENTIFY AND PRESERVE KEY HISTORIC RESOURCES AND LANDMARKS IN THE 100 BLOCK AREA.

Future efforts to preserve and promote historic resources in Minturn will no doubt include properties within the 100 Block area. To ensure protection of key historic resources, the Town should establish and codify standards related to historic resources. For further information related to historic character in Minturn, see **Chapter 4: Attainable Housing & Historic Character**.

- **Action 2.2.1** Use the 100 Block Design Standards and Guidelines as a basis for identifying key historic features and qualities in the 100 Block area.
- **Action 2.2.2** Develop an official list of historic properties and landmarks, including those in the 100 Block; collaborate with property owners to highlight these special locations with maps and signage as appropriate.
- **Action 2.2.3** Continue to explore a Historic Preservation Policy for Minturn; ensure that future historic preservation policies remain compatible with the 100 Block Design Standards and Guidelines.
- **Action 2.2.4** Allow modifications to historic and traditional structures that do not conform to code standards; support the preservation of historic facades and modern improvements to properties.

### OBJECTIVE 2.3: COMMUNICATE A VISION FOR THE 100 BLOCK AREA THAT IS INFORMED BY COMMUNITY INPUT AND RESONATES WITH RESIDENTS AND VISITORS ALIKE.

The Town of Minturn has invested in extensive community engagement related to the 100 Block and its design. Following these engagement efforts, the Town has identified a clear vision for the 100 Block area as a well-connected, charming and walkable centerpiece of the community. The Design Standards and Guidelines will enable the implementation of this vision. At this stage, the

Town should enhance the branding and wayfinding of the 100 Block area as part of the implementation of the 100 Block vision. See **Chapter 6: Intuitive Mobility, Circulation, and Connectivity** for a discussion of signage related to parking and vehicle circulation in the 100 to 400 Blocks.

- **Action 2.3.1** Initiate a wayfinding plan for the 100 Block area; ensure that signage and wayfinding elements follow a consistent style that is compatible with the broader area and Minturn’s heritage (e.g. mining and railroad) and highlight key locations and amenities, such as access points to the Eagle River and nearby trailheads.
- **Action 2.3.2** Codify standards for signage in the Old Town to accompany the 100 Block Design Standards and Guidelines document.
- **Action 2.3.3** Create visitor-oriented maps and materials that illustrate the 100 Block area and the businesses and experiences it offers.

#### OBJECTIVE 2.4: ADDRESS PARKING AND CIRCULATION ISSUES AND PROMOTE PEDESTRIAN AND BIKE ACCESS TO AND WITHIN THE 100 BLOCK.

With many businesses, residences, and amenities in the area, the 100 Block is a popular destination within Minturn. Future planning efforts for the 100 Block should account for projected parking and transit needs. Walking and biking should be prioritized as transportation modes within the 100 Block. See **Chapter 6: Intuitive Mobility, Circulation, and Connectivity** for further detail on these topics.

- **Action 2.4.1** Establish one-way shared streets on Williams St. (~~northbound~~southbound) and Eagle River St. (~~keep southbound for now~~northbound); initiate a redesign of these streets to meet the vision including special paving, additional lighting, and install signage that conveys ~~this~~ changes and reduces the vehicle speed limit to promote traffic safety.
- **Action 2.4.2** Provide infrastructure and amenities that support walking and biking, such as sidewalks, bollards or curb extensions, paths, benches, bike racks, and a bike wash/repair station, within the 100 Block area.
- **Action 2.4.3** Collaborate with ECO Transit on future enhancements to transit access from the 100 Block area; facilitate transit trips by publicizing bus schedules in prominent areas.
- **Action 2.4.4** Provide “loading only” areas along Main Street so deliveries to businesses can use Main Street instead of putting large truck travel onto narrow streets.
- **Action 2.4.5** Implement a "Pedestrian Priority Zone" using signage in the 100-Block area to indicate that vehicles should travel cautiously and that pedestrians have the right-of-way.

#### OBJECTIVE 2.5: SUPPORT THE VITALITY AND AFFORDABILITY OF THE 100 BLOCK BY INCENTIVIZING SECOND-STORY RESIDENTIAL DEVELOPMENT, ADUS AND INFILL.

The 100 Block is currently mostly built out and its 25’ lot pattern does limit the footprint of individual property development. There may be future opportunities to encourage second-story residential, Accessory Dwelling Units (ADUs) and infill development in the area. See **Chapter 4: Attainable Housing & Historic Character** for a detailed discussion of Minturn’s projected future housing needs.

- **Action 2.5.1** Allow ADUs as a use-by-right within the 100 Block; discourage garages facing the street.
- **Action 2.5.2** Provide density bonus incentives to encourage upper-floor~~affordable~~ residential development for long-term residents in the 100 Block area.

**OBJECTIVE 2.6: INVEST IN CONTINUOUS PLACEMAKING IMPROVEMENTS TO SUPPORT THE 100 BLOCK AS A VIBRANT, CENTRAL COMMUNITY GATHERING PLACE; IMPROVE CONNECTIVITY TO THE RIVERFRONT AREA AND NEARBY AMENITIES.**

Placemaking efforts can help residents and visitors feel connected to a place and to their broader community. Future enhancements to the 100 Block should include placemaking elements that support social and cultural life in Minturn and craft a distinct, special identity for the 100 Block area.

- **Action 2.6.1** Support resident-led efforts to create inviting spaces, such as courtyards, pedestrian passageways, and gardens on residential and/or commercial properties. Refer project proponents to the design examples provided in the 100 Block Design Standards and Guidelines Appendix B insert (pages 17 and 18).
- **Action 2.6.2** Encourage the development of gathering spaces and community activities as identified in the Framework diagram.
- **Action 2.6.3** Ensure that wayfinding signage highlights the Grouse Creek and Game Creek trailheads to the north of the 100 Block; prioritize pedestrian and bicycle connectivity to these areas from the 100 Block.

DRAFT

# MINTURN



COMMUNITY PLAN  
-Imagine Minturn-

## CHAPTER 3

### An Economically Vibrant Community: Commercial Development & Industry

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#### Introduction

This chapter addresses the Town of Minturn's economy. The chapter contains an overview of trends and conditions and strengths, assets, and challenges derived from public input and the consultant team's analysis. The Central Concepts section addresses the need to incentivize and strategically support commercial development to strengthen the tax base and support Town revenues and community needs.

#### Existing Conditions & Issues<sup>1</sup>

There are about 400 wage and salary jobs in Minturn, a figure that has not changed significantly since 2010. The largest industry is construction with about 156 wage and salary jobs, followed by retail, leisure, and hospitality with about 115 jobs. These figures don't include sole proprietors (self-employed individuals), which can add another 20 to 30% to these numbers especially in construction. Over 90% of the jobs in Minturn are estimated to be filled by people who live outside the Town.

Minturn's economy is notably different than Eagle County's as a whole, particularly in the upper Eagle River Valley, where tourism and retail driven by ski resorts are the dominant sectors. In Minturn, construction and related trades make up almost 40% of the jobs in the Town compared to 10% in Eagle County. In Minturn, retail, leisure, and hospitality make up approximately 30% of jobs compared to approximately 40% in Eagle County.

*Sidebar: The region surrounding Minturn is referenced in a variety of ways. Eagle River diverts from the I-70 corridor at its intersection with Highway 24 just after Eagle-Vail. The I-70 corridor from Edwards to Vail is often referred to as "Vail Valley," particularly within the real estate and resort-tourism fields. However, local Minturn residents refer to this area, more broadly including Minturn as well, as the Eagle River Valley. Thus, this chapter and plan uses the local term of "Eagle River Valley" to reference the area.*

Data from the Town's business licenses show a diverse and entrepreneurial mix of businesses. Construction and trades again represent a large proportion of businesses, ranging from general contracting and construction businesses to heavy construction, as well as high-end custom woodworking, cabinetry, and flooring. The Town is home to local manufacturing businesses,

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<sup>1</sup> Unless otherwise noted, this information is from the Minturn Community Profile Existing Conditions Memorandum, EPS (2022), developed in the analysis phase of this planning process.

including Weston Backcountry, a ski and snowboard manufacturer, and a wooden clog manufacturer. Repair, maintenance, and restoration services also represent a significant sector, such as Steammaster restoration and cleaning services, and other businesses that offer high-end auto repair, motorcycle and snowmobile repair and rentals, and property management. In the retail and leisure sector, there are numerous independent small retailers, restaurants, and lodging businesses mostly located along Main Street and in the 100 Block.

Economic growth is constrained in Minturn despite its location in Eagle County and the Vail Valley, which have a strong economy. The Town contains few vacant commercial development sites that are development ready. Without new commercial development opportunities, it can be challenging for a community to generate new jobs and to strengthen its tax base. In 1982, Colorado's Gallagher Amendment established a 29% assessment rate on commercial property, much higher than the residential assessment rate of 7.15%. Commercial development therefore generates about four times the property tax on the same amount of value. The Town gets about 58% of its revenues from sales tax and 38% from property tax. Without new development, or increasing the yield from existing properties, it is difficult to grow Town revenues without voter-approved increases to tax rates.

The Dowd Junction property could be a significant opportunity for commercial development, but there are numerous issues that will need to be resolved. First, it will take time (several years, potentially) to transfer the property from the US Forest Service to a public entity such as the Town, as it may requires a Congressional action. Summit County acquired the Lake Hill property for workforce housing through federal legislation, a lengthy process. There may be restrictions on what can be developed on the property as well. In addition, the major infrastructure costs that will be necessary to support development at this site are currently unknown. These costs may need some public investment to help make a project attractive and financially possible for developers, which would reduce the net tax generation from commercial development there.

In 2016, the Town of Minturn adopted an Economic Development Strategic Plan that outlined a vision, goals and initiatives for Minturn's economic development. The plan offers recommendations and an action plan concentrating in three main areas: Tourism, Placemaking and Small Business Support. Many of the action items recommended in the plan are still relevant today. The Town has recently engaged with Downtown Colorado, Inc. to start a strategic planning process to support downtown businesses to consider the establishment of a Downtown Development Authority (DDA) or similar entity to convene local businesses around shared needs and goals, generate revenue, and help with execution and implementation. "Downtown Doers," a small local volunteer group that has historically supported downtown business development efforts, is conducting additional engagement on marketing and business support activities and is involved in the strategic planning process. The Central Concepts section of this chapter further describes the role of a DDA and the Recommendations section references relevant actions from the Economic Development Strategic Plan.

### Community Engagement

The following summary of strengths, assets, and challenges was created with input from the Steering Committee, Town Council, and the consultant team.

#### Strengths and Assets

- **Location** – Close to a large national/international visitor market. Minutes' drive to Vail and Beaver Creek.



- **Authentic Place** – Historic railroad and mining town. Distinct from purpose-built resort areas. Offers a different experience than ski area towns to residents and visitors.
- **Year-Round Community** – Most housing is occupied by residents, rather than second homes, which adds to the authenticity of Minturn.
- **Downtown Minturn** – There is no other place in the Vail Valley like it. The historic building stock and eclectic businesses can be a large draw for visitors and residents.
- **Recreation and Outdoor Access** – Eagle River, National Forest, and high-quality hiking and mountain biking trail access. Minturn Bike Park is a draw. Endurance sports races and events draw customers for businesses.
- **Entrepreneurial Population** – Numerous small and independent businesses. Increase in knowledge workers/professionals moving to Minturn and working for remote clients.

### Weaknesses and Challenges

- **Not Well Known** – Despite being so close to major tourism draws, Minturn is not well known to visitors. More exposure is needed for businesses to thrive.
- **Resources** – The Town has limited financial and staff resources to work on opportunities and address challenges. The 2020 General Fund is budgeted for approximately \$2.3 million to fund Town operations and maintenance functions. The Town needs to grow the tax base to increase revenues to sustain Town services.
- **Small Downtown** – Interest in seeing commercial and mixed-use development allowed in a broader area along Main Street. Extending the business locations on Main Street could create more “critical mass.”
- **Water System** – The Town’s water system is nearly at capacity. The Town needs to find additional funding to address repairs to and expansion of the system.
- **Commercial Development** – The existing zoning dates back several decades. Commercial development is not financially feasible under the original zoning. There are few remaining high visibility sites for commercial development. The Dowd Junction property is an opportunity, but development is potentially many years in the future and the infrastructure costs to serve the site are not known.

### Points of Contention

It is important to note that not all Minturn residents, business owners, municipal staff, and elected officials agree unanimously on the strengths and weaknesses listed above. For example, many residents value the fact that Minturn is “off the beaten path” of the I-70 corridor. There is concern that greater visibility and marketing would result in negative impacts associated with increased tourism, such as traffic and parking congestion. Similarly, many residents are concerned about increasing density and impacts related to growing the local economy. However, Minturn’s small local market, and scarce development-ready land limits the viability of attracting new commercial development as well as the sustainability and health of existing businesses. Commercial activity is essential for a community’s overall longevity and success. As discussed in the Existing Conditions section above, adequate commercial tax revenue plays a significant role in supporting the Town’s operations and services. Additionally, local businesses provide jobs as well as essential and recreational goods and services to the community. Without conditions that broadly support the success of those businesses, the Town risks losing those essential assets and the potential to attract new ones, such as a grocery store. Thus, inviting visitors and ensuring supportive land use and development regulations are critical components of sustaining Minturn. With strategic planning – such as recommendations outlined in **Chapter 6: Intuitive Circulation**,

**Mobility & Connectivity** and in **Chapter 1: Smart Land Use & Practical Zoning** – the Town can work to balance these competing interests and mitigate potential negative impacts that concern residents.

The Strengths and Weaknesses analysis above shares several similarities with the Strengths, Weaknesses, Opportunities and Threats (SWOT) analysis that was completed as part of the Economic Development Strategic Plan. The analysis contained in that plan also listed authenticity and access to recreational opportunities as strengths. The weaknesses listed included visual blight and limited commercial space. Opportunities included vacant commercial space, the Eagle River and bike path connections.

Minturn residents also shared input on the town's economy at the Plan's two open houses. At the first open house, which asked participants for broad input, residents indicated that they were in favor of additional commercial development further south in Old Town. Some residents favored more dispersed commercial development, while others favored a more concentrated commercial area. Participants expressed that they did not want big box stores or chains to locate in Minturn. At the second open house, participants were invited to provide feedback on draft plan recommendations. At this open house, participants shared a concern that Dowd Junction might take away economic activity from downtown. Participants also shared their desire for a grocery store and to help people working in Minturn afford to live in Minturn.

#### Central Concepts

Minturn will need to use several strategies to sustain and grow its economy. Major strategy areas include land use and zoning, transportation and placemaking investments, organizational strategies, and budget and tax policies.

#### Leverage Zoning, Infill, and Redevelopment

The original Town zoning dates to the 1950s, subsequently revised in the 90's and early 2000's. The commercial districts in Downtown allow for one-story development with required on-site parking. The resulting allowable building sizes on a typical 5,000 square foot lot (or smaller) are not large enough to be financially feasible for two main reasons. First, the on-site parking requirement consumes a large portion of the site. Second, the rent needed to support the construction cost would be more than local tenants can pay. A proposed zoning change in the 100 Block area would allow residential on upper floor(s) of ground floor commercial. This flexibility to include residential development would generate additional value to make a commercial development or redevelopment more feasible, in addition to providing critical housing options in the downtown area. Additional recommendations in **Chapter 1: Smart Land Use & Practical Zoning** expand upon this concept in other areas of town to offer moderate flexibility to increase development opportunities.

During this planning process, analysis conducted related to this issue shows that a conceptual two-story mixed-use development could generate a nearly 9% return on investment and a three-story mixed-use development could generate a return on investment of over 20%. In addition, the two- and three-story development concepts generate more tax revenue than development under the baseline zoning (**Table 1**). Development or redevelopment under existing zoning is not likely to be financially feasible because a typical site cannot create enough value within Minturn.

**Table 1. Summary of Mixed Use Zoning Feasibility Testing**

Scenario	Annual Sales and Property Tax			Development ROI
	Residential	Commercial	Total	
<b>Baseline Zoning</b>	\$0	\$10,125	<b>\$10,125</b>	<b>-69.2%</b>
<b>2 Stories</b>				
1: ground floor mixed use	\$5,336	\$15,335	<b>\$20,671</b>	<b>8.9%</b>
2: upper floor residential				
<b>3 Stories</b>				
1: ground floor mixed use	\$9,358	\$15,335	<b>\$24,692</b>	<b>25.3%</b>
2: upper floor residential				
3: upper floor residential				

Source: Economic & Planning Systems

Z:\Shared\Projects\DENV2\13101-Minturn Community Plan\Models\1213101- MODEL\_Minturn Test Scenarios\_6-9-22.xlsx\T-Summ

These scenarios illustrate the importance of moderately increasing density to create incentives for redevelopment that can maximize the economic utilization of property. The more commercial and residential units, the more opportunities to host more jobs, generate more tax revenue, and house residents. Given that Minturn is largely built-out, strategic infill development opportunities, such as encouraging mixed-use and live-work redevelopment of low-density commercial space, would help to incrementally expand and diversify the economy for long-term sustainability. A range of land uses should be included to retain the light industrial and service commercial land uses in Minturn and to concentrate them in areas with lower real estate costs (e.g. outside of Old Town).



Live-work development near Basalt, Colorado allows light industrial, professional, and residential uses.

## Invest in Transportation & Placemaking

One of the constraints noted through this community planning process was the perceived distance between business areas in Minturn. This perceived distance deters residents and visitors from staying longer and visiting the different areas of Main Street from north to south. While development density along Main Street from the 100 Block down to King Ranch Road is consistent, the mix of residential and commercial development breaks up businesses just enough that the distance between them can seem greater than reality from a pedestrian scale. Additionally, more noticeable breaks in development between the Meadow Mountain Business Park, central downtown area, and southern Main Street area create spatial separations between these areas of activity. In particular, the lack of pedestrian and bicycle infrastructure between these three areas requires residents and visitors to drive between them and find parking at each location that they wish to visit. If they only have an intention to visit one business on a specific trip, they are unlikely to spend time in the other two areas. These factors dictate economic habits that involve short trips to specific destinations driven by particular needs.

Alternatively, when infrastructure and development patterns encourage people to park once and walk to their target location (or walk/bike from home), they are more likely to pass by other establishments that spark impromptu visitation. Thus, improving pedestrian and bicycle infrastructure, community gathering amenities and clear wayfinding can encourage more non-vehicular circulation. This type of pedestrian activity supports a lively street environment, which adds to the draw of businesses and sense of conviviality – if people see others walking around and making use of public areas, they will be more likely to stop and explore. Likewise, this type of environment will encourage residents to visit local businesses a few blocks away by walking or biking instead of driving farther distances to commercial areas elsewhere in the Eagle River Valley. . A connected, cohesive, and thoughtfully designed streetscape and wayfinding infrastructure would unite Minturn’s business areas and public spaces within a common aesthetic to improve user experience and drive economic activity locally

## Organize Improvement Efforts

Moving forward, Minturn’s economy and businesses will benefit from an organized approach to marketing and business development. The Town does not currently have the staffing capacity to take on this leadership role. Accordingly, the Town is actively considering the strongest option for a third-party entity to organize and lead ongoing efforts.

Based on progress thus far of the current strategic planning process, it is most likely that a Downtown Development Authority (DDA) will be the best option for spearheading economic development efforts in Minturn. A DDA can use tax increment financing (TIF) and/or establish a Central Business District (CBD) to generate funding/financing for infrastructure and other public improvements.

A Downtown Development Authority (DDA) is quasi-municipal corporation created to assist in the development or redevelopment of central business districts (CBDs). DDAs are established to promote development and redevelopment, and to finance improvements. Forming a DDA requires approval by a majority of electors residing, owning, or leasing property within the district. A DDA can collect and levy ad valorem tax on property (up to 5 mills) for the cost of operations. It can also impose fees or charges for services and special assessments. DDAs can also use TIF which is a powerful financing tool. TIF redirects the incremental property taxes from all taxing entities back into the DDA to help build and pay for public infrastructure. Once the

infrastructure has been paid for, the property taxes then go back to the property taxing entities affected by the development. There are existing DDAs throughout Colorado, including in Colorado Springs, Glenwood Springs, Loveland, Grand Junction, and nearby Eagle.

Although the boundaries of a new DDA are yet to be determined, the community will benefit from broad inclusion and participation of local businesses to produce a cohesive economic strategy as described above regarding transportation and placemaking.

### Build Fiscal Health

The ultimate goals of downtown revitalization measures are to improve the financial status of the Town and to bolster the local economy. However, these changes can require upfront costs. Pending public appetite for change, the Town could consider increases to taxes and fees to create more funding for infrastructure and other town services. As described above, some of the financing strategies for downtown improvements can lessen the cost burden of infrastructure and/or maintenance activities associated with construction and investment.

### Recommendations

#### OBJECTIVE 3.1) LEVERAGE THE TOWN'S NATURAL AND RECREATIONAL AMENITIES, HISTORY, AND ARTS AND CULTURAL ASSETS FOR ECONOMIC DEVELOPMENT.

The Town has several local and immediately adjacent recreational opportunities including the Eagle River, hiking and mountain biking trails on surrounding public lands, the Minturn Mile backcountry ski run, Maloit Park, and new addition of the Minturn Bike Park as well as anticipated extension of the paved regional ECO trail. The Town hosts regular endurance sports events and has a popular river frontage for fishing and other water recreation (kayaking, tubing). All of these amenities position Minturn as a recreation hub – potentially for a unique recreational community that is not interested in the busier, more developed resort options. These assets can attract people who will spend money in local businesses, and create the quality of life that attracts businesses and entrepreneurs.

- **Action 3.1.1.** Promote Minturn as a recreation destination by continuing to develop recreational assets and promoting events. Continue to improve pedestrian and bicycle connectivity between recreational assets (trailheads, facilities) and the Town's business districts.
- **Action 3.1.2** Sponsor "arts crawls" or other community events in the Old Town area that encourage residents and visitors to support local craftspeople and Main Street businesses; coordinate with Vail Valley Art Guild as a regional resource for collaboration and support.
- **Action 3.1.3** Continue to support the development of lodging concentrated in the 100 Block and Old Town areas as well as for future development in the Dowd Junction area.
- **Action 3.1.4** Continue to assess Minturn's industries and demographics relative to regional economic trends; conduct an annual strategic call with the Vail Valley Partnership.

See **Chapter 7: Quality of Life, Recreation & An Inviting Public Realm** for additional strategies that can support and broaden Minturn's recreational opportunities. The marketing strategies outlined below under Objective 3.2 are also relevant to this objective. Also see additional strategies in **Chapter 1: Smart Zoning and Practical Land Use** regarding the continued provision of light industrial opportunities in Minturn.

### OBJECTIVE 3.2) IMPROVE THE VISIBILITY OF MINTURN TO VISITORS THROUGH MARKETING AND EXPANDED TRANSIT SERVICE.

Minturn offers a unique visitor experience in the Eagle River Valley that differs from the resort-centric settings of Vail and Beaver Creek. Strategic and targeted marketing on social media and in major lodging businesses and concierge services could generate more exposure and visitation for Minturn, capitalizing on Minturn's unique assets. In addition, direct frequent transit service would enable more guests to visit Minturn for restaurants and shopping. Transit service would also benefit locals by offering other options for accessing the ski areas and surrounding communities.

- **Action 3.2.1** Actively participate in future planning within the Regional Transit Authority to advocate for increased and flexible service options to current and future retail centers within Minturn, including the 100 Block and Old Town and Dowd Junction.
- **Action 3.2.2** Prioritize future expansions and improvements for bicycle and pedestrian infrastructure and amenities to connect current and future retail centers of the community, including the 100 Block, Old Town, and Dowd Junction; encourage multimodal connectivity between destinations in Minturn.
- **Action 3.2.3** As recommended in the Town's Economic Development Strategic Plan, develop a half-day itinerary for visitors to Minturn. Highlight multimodal transit options on this itinerary.
- **Action 3.2.4** Support the development and implementation of a clear and cohesive marketing strategy that capitalizes on Minturn as a unique and authentic community that serves as a gateway to a variety of recreational opportunities throughout the region. This effort can be an initiative of a new DDA.

See **Chapter 6: Intuitive Mobility, Circulation, & Connectivity** for additional information regarding transportation improvements.

### OBJECTIVE 3.3) PURSUE LAND USE CHANGES THAT SUPPORT MINTURN'S TAX BASE AND ECONOMIC GROWTH; PLAN FUTURE DEVELOPMENT ON IMPORTANT CATALYST SITES, INCLUDING DOWD JUNCTION.

The approximately 15-acre Dowd Junction property is currently owned by the US Forest Service (USFS), which is interested in exchanging the property with a local government as they want to consolidate administrative and maintenance facilities. The Town has been in discussions with USFS on this possibility. The site's location at the intersection of I-70 and Hwy 24 contributes to its high visibility, making it a good location for Transit-Oriented Development (TOD), including anchor retail and lodging. The site is also a gateway to nearby recreational attractions, such as the Meadow Mountain trails, which connect to other trails within the Eagle River Valley. Future development could combine retail, hospitality, and recreation elements as well as key transportation improvements to create a hub connecting Minturn and the broader region. See **Chapter 6: Chapter 6: Intuitive Mobility, Circulation, & Connectivity** for additional information regarding transportation improvements at Dowd Junction.

*Sidebar: An "anchor business" is a central establishment within a commercial district or area. The business is typically large in square footage and well known. The broader commercial area is sometimes named for the anchor business. Anchor businesses increase visitation to smaller adjacent*

*businesses within the area as they tend to be a focal draw due to their size, reputation, and/or broader array of offerings.*

The Union Pacific Railroad owns property surrounding its right-of-way off North Main Street. If a strategic partnership or deal with U.P. can be reached in the future, this area represents a significant redevelopment opportunity. The Town should continue to engage in discussions with U.P. on these properties. It needs to be recognized that the Town has made continuous efforts to work with U.P. on this topic. Railroads are often reluctant to give up property as they value the potential for future use.

- **Action 3.3.1** Continue to pursue ownership of the Dowd Junction property and planned development of the area as Transit-Oriented Development.
- **Action 3.3.2** Consider pursuing the development of a Sub-Area Master Plan to establish the highest and best use of U.P. property for the Minturn community, particularly the area adjacent to the Minturn North PUD and current municipal lot which is leased from U.P. Seek out U.P.'s involvement in this process.

#### OBJECTIVE 3.4) IMPLEMENT STRATEGIES TO ESTABLISH ORGANIZED ECONOMIC DEVELOPMENT CAPACITY, ADDRESS PLACEMAKING AND INFRASTRUCTURE INVESTMENT COSTS, AND SUPPORT GRASSROOTS ECONOMIC DEVELOPMENT.

The Town of Minturn has a critical role to play in ensuring the viability and sustainability of a healthy economy. Some strategies, such as supportive zoning and land use regulations, are more obvious and feasible for municipal responsibility. Other strategies are harder to accommodate within the municipality's constrained resources and staffing capacity. Acknowledging this challenge and need, the Town can play an active role in establishing creative solutions to expand capacity and funding for broader economic development initiatives and outcomes.

- **Action 3.4.1** Support ongoing efforts to create a Downtown Development Authority (DDA) to help fund public infrastructure and placemaking projects.
- **Action 3.4.2** Consider establishing and supporting small business grant programs to help with startup or maintenance costs; even small capital grants or low/no interest loans for items like signs, equipment or fixtures can be helpful. This could be an initiative of a new DDA with the potential for supplemental support from the Town.
- **Action 3.4.3** Build on the recommendations and conclusions of the Economic Development Strategic Plan – including an update to the Plan – to more thoroughly address the role of various economic partners in the Town and in Eagle River Valley, and to establish specific and up-to-date business and organizational strategies. This could be a joint effort of the Town and a new DDA.

#### OBJECTIVE 3.5) EVALUATE OPTIONS TO INCREASE THE TOWN'S FINANCIAL RESOURCES AND SPECIFICALLY TO PROVIDE STABLE, ATTAINABLE HOUSING FOR MINTURN'S RESIDENTS AND WORKFORCE.

The Town's ability to follow through on important initiatives and offer community services depends on its fiscal health. Being able to keep up with infrastructure maintenance and to build new projects or amenities that businesses and residents need and would like requires funding. The Town has limited financial resources and it is difficult to keep up with these demands. In addition, the availability of housing that is attainable to town residents and workers is critical to

support economic and community vitality. During the Open House conducted during this planning process, participants noted that some Minturn residents have been relocating down-valley because they cannot find housing they can afford in Minturn. This results in critical loss to the social and economic fabric of the Minturn community Bolstering the Town's tax base through a variety of mechanisms will ensure that all these important facets of the community can adequately support one another. Additional strategy ideas for creative revenue streams can be found in other chapters in this Plan, such as impact fees, short term rental fees, parking fees, local sustainability gift card programs, and others. See **Chapter 4: Attainable Housing & Historic Character** for additional housing-specific strategies.

- **Action 3.5.1** Review all user fees to ensure they are capturing the cost of the service provided, including utility rates.
- **Action 3.5.2** Consider targeted tax increases (property and/or sales tax) to be dedicated to specific functions such as housing, economic development, sustainability, or infrastructure.

*Side Bar: By the Numbers*

*The Town has approximately \$31.0 million in assessed value. Each 1.000 mill of property tax would generate about \$30,000 per year for the purpose identified. On approximately \$1.1 million in sales tax, a 0.25 percent dedicated sales tax would raise about \$70,000 per year.*

**OBJECTIVE 3.6) ENSURE THAT THE TOWN'S ZONING AND DEVELOPMENT CODE SUPPORT AND ENCOURAGE COMMERCIAL DEVELOPMENT AND REDEVELOPMENT IN KEY COMMERCIAL DISTRICTS WHILE PRESERVING HISTORIC CHARACTER.**

The Town's zoning code has been identified as an impediment to re-investment and development in Minturn. There are also concerns that too much incentive for development will compromise the small town and historic character – an asset of its own – that distinguishes Minturn from other places. The Town should continue working on zoning and design options that balance development incentives with community character.

The Town should also consider broadening zoning to allow commercial and mixed-use development further south along Main Street, as described in the zoning chapter of this plan. This could help create more space for attracting businesses and a larger critical mass of business activity along Main Street over time.

See **Chapter 1: Smart Land Use & Practical Zoning** for action items.

**OBJECTIVE 3.7) CONTINUE INVESTING IN STREET DESIGN, STREETScape, AND SAFETY PROJECTS THAT CONTRIBUTE TO THE APPEAL OF COMMERCIAL AREAS IN MINTURN.**

As discussed in the Central Concepts section above and in **Chapter 2: A Thriving 100 Block**, streetscape improvements and novel ideas such as shared streets can improve an area's overall appeal, sense of place, and business activity. Increasing pedestrian activity and safety in the Old Town area and throughout town – a desirable goal in and of itself – will help encourage foot traffic and spending at local businesses. Planned multi-use path connections, wayfinding signage, and additional bicycle infrastructure will help to reduce the perceived distances between



neighborhoods and businesses. The resulting system will foster economic and social activity in downtown Minturn, benefitting businesses, residents, and visitors.

See **Chapter 6: Intuitive Mobility, Circulation, & Connectivity** for action items.

**OBJECTIVE 3.8) CONTINUE WORKING TO ADDRESS THE CAPACITY OF THE TOWN'S WATER SYSTEM AND NEED FOR CRITICAL MAINTENANCE.**

As noted in **Chapter 5: Natural Resources, Infrastructure, & Efficiency**, the Town's water system needs more than \$20 million in infrastructure investment to make repairs to the system and increase capacity. The future of development in Minturn (both commercial and residential) hinges on a sustainable supply of water to the Town. At present, the Town has a moratorium on ~~medium and larger scale~~most development due to these needs. Expansions to the water supply ~~will~~may involve the execution of the 2019 Water Capital Improvement Plan combined with the acquisition of new water rights. The Town needs to prioritize these critical investments and continuously manage its water supply to support economic growth.

See **Chapter 5: Natural Resources, Infrastructure, & Efficiency** for action items.

DRAFT



## CHAPTER 4

### ATTAINABLE HOUSING & HISTORIC CHARACTER: HOUSING BARRIERS, TRENDS & POLICIES

#### Introduction

Housing is a foundational component of any community. Many factors impact the amount, price, and quality of housing available in a given community. The housing element of this plan addresses central questions regarding Minturn's housing stock:

- What housing stock already exists?
- How much does housing cost, and how does that compare to local and regional wages?
- What amenities and qualities do current or prospective residents look for in terms of housing?
- What are the available opportunities for new housing development?
- How will the community's current and future housing stock shape the community's aesthetics, character, and quality of life?

A community's development pattern and supply of housing are physical manifestations of that community's history. The housing stock is characterized by the factors that shaped the community's development over time, such as population, industries, and natural resources. These factors can shape the physical and cultural fabric of a community and may or may not remain relevant today. In a small community like Minturn, the addition or removal of a single building can make a notable difference in the community's overall development pattern and supply of housing. In this way, the community's development decisions can be impactful - and contentious - on a community-wide scale.

The following pages will provide an overview of existing conditions and current issues, input from the community on housing, a graphical overview of the central concepts for housing and historic character goals and objectives in Minturn, a detailed description of objectives and recommendations, and an implementation matrix detailing how the Town will achieve the recommendations.

#### Existing Conditions & Issues

##### Population & Household Trends<sup>1</sup>

The Minturn community is holding a relatively steady year-round population of between 1,000 and 1,100 residents. Minturn's population has grown by less than 1 percent annually since 2010. The median age in Minturn is just under 40, having increased slightly since 2010, indicating an aging population.

<sup>1</sup> Unless otherwise noted, this information is from the Minturn Community Profile Existing Conditions Memorandum, EPS (2022), developed in the analysis phase of this planning process.

There are about 450 households in Minturn, correlating to an average household size of 2.43. Just under two-thirds of Minturn households are families (related individuals), with just 8 percent including children – a lower proportion than many surrounding communities in Eagle County. 24 percent of households are roommate groups – a higher proportion than surrounding communities – and 17 percent are single individual households – a lower proportion than many surrounding communities. Larger households and single-person households are more likely to be renters in Minturn, with 23 percent of renter households comprising more than 2 people and 21 percent comprising single-person households. For ownership households these stats are 16 and 12 percent respectively. A slight majority of households in Minturn are owner-occupied (59%).

### Housing Supply

Minturn has approximately 560 housing units, with an average increase of only 3 per year since 2010. 12 new building permits were issued by the Town between 2019 and 2021. The majority (68%) of homes in Minturn are single-family structures. Almost all owned units in Minturn are single-family and about half of rental units are single-family as well. Just under 30 percent of rental units in Minturn are in duplex, triplex, or fourplex structures, with a limited number of larger housing complexes and mobile homes (about 5% each). Compared to nearby communities, Minturn has a larger proportion of older housing stock with 28 percent of units built before 1940. Most other homes were built between 1970 and 2009. The vacancy rate in Minturn hovers around 20 percent, much of which is made up of second/seasonal homes and about 20 short term vacation rentals.

### Housing Costs & Affordability

Minturn's median household income in 2021 was \$87,901 – lower than the County as a whole and with fewer outlier high-income earners compared to nearby communities. The 2021 average home price in Minturn in 2021 was \$767,000. Since 2015, Minturn has seen a drastic increase in the proportion of homes priced above \$1.0 million and the average price per square foot has increased by 33 percent. The Minturn housing market generally sees about 20-30 sales per year with 2021 experiencing minimal sales, another indicator that the market is tightening due to a lack of inventory for-sale or for rent. Median gross rent in Minturn is \$1,818, increasing 58% since 2015.<sup>2</sup> About half of Minturn households are paying 30 percent or more of their income towards housing (both renters and owners), making that group housing “cost burdened.”

### Housing Development Conditions, Constraints, and Opportunities

Due to the Town's small geographic area and location in the Eagle River Valley with steep slopes on either side, available land for development is sparse in Minturn, especially considering the current context of infrastructure and zoning. In the central part of Minturn (excluding Bolts Lake and Maloit Park, Battle Mountain, and Dowd Junction) only 16 lots in Minturn 11 land parcels are vacant, totaling 9.27 acres. Subtracting lots that are in industrial areas in the northern part of Town, there are only 5.72 vacant acres. Of the 7 lots that are currently undeveloped (no structures) and not zoned for as open space. As illustrated in the map below, 6 of these lots are small enough that they could only accommodate a small number of residential use, the largest is 3.89 acres. units. The current zoning for the other six lots have an average size of just under 5000 sq ft. lots does not allow for the scale of residential or commercial development that is needed and desired in Minturn.

Town staff have identified a few other areas (full parcels or portions of larger parcels) that may have potential for development in Minturn, dependent on land swaps or other agreements with

**Commented [sh1]:** Minturn is apparently not the smallest town in the county geographically; it is one of if not the largest - suggest the first sentence be revised to state that the town has constraints based on the narrowness and steepness of surrounding topography.

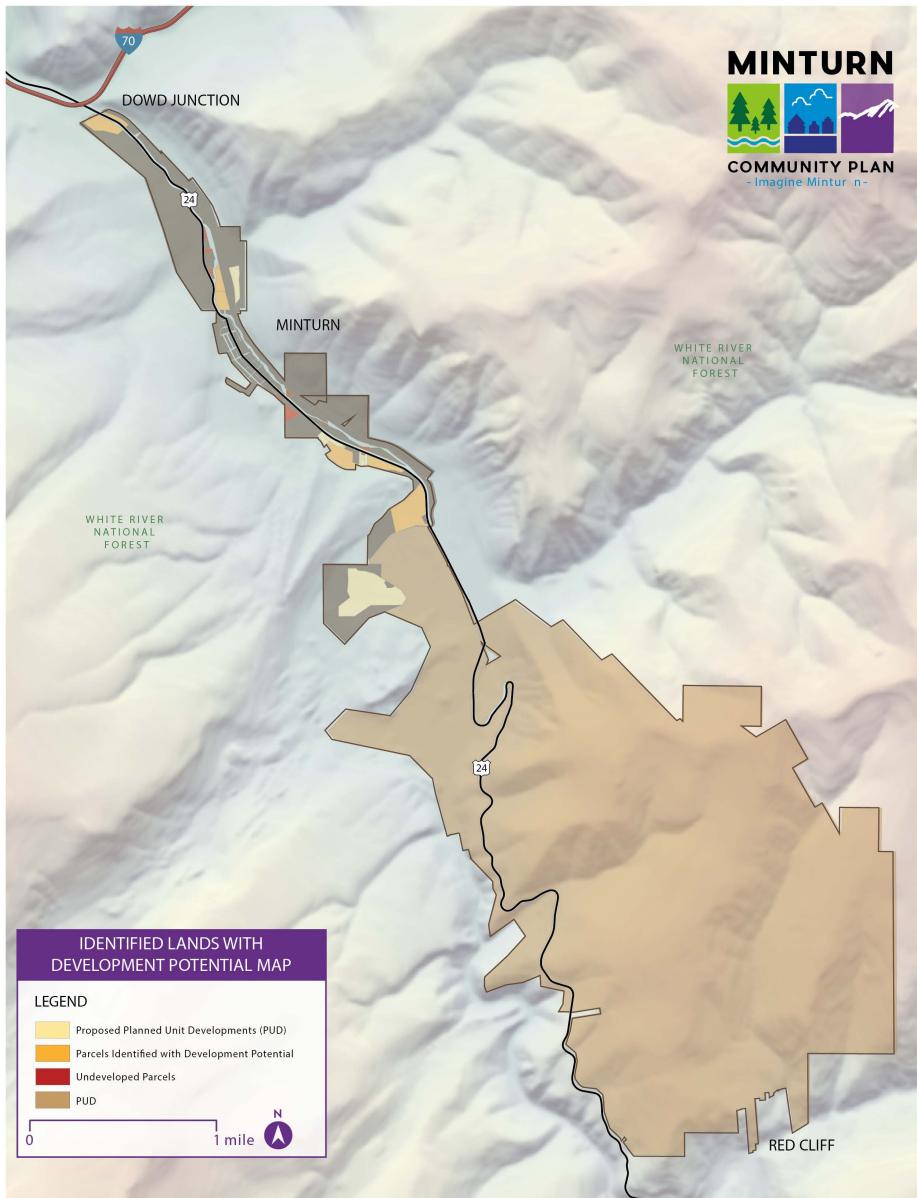
**Commented [WA2]:** Map and updated data will be included in final plan to illustrate various areas with development potential

<sup>2</sup> 2020 ACS Five Year Estimates

current landowners as well as zoning changes. These sites include land surrounding the municipal parking lot currently owned by Union Pacific Railroad; the land between the proposed Midtown Village and Belden Place PUDs in the South Town area, as well as land south of the proposed Belden Place PUD project, both currently owned by the U.S. Forest Service; and land at Dowd's Junction/Meadow Mountain, also currently owned by the U.S. Forest Service. While Bolts Lake & Battle Mountain have previously been explored for development, there are no current plans for development in these areas and any future propositions would require significant planning, infrastructure investment, and detailed consideration. Steep slopes and other environmental conditions in these areas would likely limit development potential for residential and commercial activity.

DRAFT





Water supply constraints also affect future development. To manage limited water supplies, the Town recently implemented a moratorium on any development requiring three or more Single Family Equivalents (SFEs) in annual water use, which will be in place for the foreseeable future. Further discussion on this topic is covered in **Chapter 5: Water & Sustainability: Natural Resources, Infrastructure & Efficiency.**

Minturn's complex zoning code also poses challenges for housing development. Minturn's zoning is oriented toward low-density single-family development, which limits opportunities for different and potentially lower cost housing types, such as duplexes, triplexes, fourplexes, or mixed-use structures. These are considered "missing middle" housing options that provide a variety of size, location, and price variations to meet needs and preferences while also strategically developing under land limitations. Zoning and Land Use are discussed in more detail in **Chapter 1: Smart Land Use & Practical Zoning**.

Minturn has ~~several proposed~~ Planned Unit Development (PUD) sites ~~– three with active developments in progress~~, two that are inactive (~~holding zones previously explored PUD project areas~~), and two that have potential and are in discussion for PUD/~~Master Plan~~ development.<sup>3</sup> ~~These three projects have available water taps to complete a first phase, but the remaining development will be dependent on the uncertain availability of additional water taps. Development of these proposed PUDs is constrained by available water service.~~ Development plans are in flux and subject to change as they move through planning, permitting, and implementation over time.

**Belden Place** would result in 42 units (mix of single-family, duplex, condo, and an ADU as well as ownership and rental). ~~Twenty units would be deed restricted to prioritize year-round Eagle County residents and 4 units would be deed restricted to households earning 200% Area Median Income (AMI) or lower. Various restrictions would be implemented to set aside some units for full-time residents and local workforce.~~ With only 18 available SFE water taps secured, this project would be completed in phases with a timeline of 5-10+ years depending on the timeline for remaining 24 water taps. ~~It is currently in the planning and permitting phase. Land Use approvals for this PUD have stopped due to pending litigation.~~

Plans at **Minturn North** have fluctuated during the review process. As of **November 2022**, ~~discussions between the Town and the developer indicate that the development this project would include 39 units (scaled back from an original plan for 193), units in various styles of multifamily housing. Some restrictions Six units would be deed restricted implemented to prioritize year-round Town of Minturn residents, Eagle County residents and/or low-and-moderate-income residents.~~ The Town has designated 70 available SFEs to this project, which will allow for the first phase of development. ~~The remaining 123 planned units will be dependent on the ability to secure additional taps, should allow for the planned development to occur without further water capacity expansion.~~

**Midtown Village** would result in 42 units (mix of single-family, cottages, townhomes, and apartments as well as ownership and rental) and 3,000 sf of commercial space. ~~Some restrictions would be implemented to prioritize housing for the local workforce. Twenty-seven of these units would be deed restricted to prioritize year-round Town of Minturn residents Eagle County residents.~~ This site has approximately 20 existing SFEs, requiring an additional 23 taps to fulfill the development as planned.

**Battle Mountain and Bolts Lake** are ~~PUD Holding Zones, large land areas in southern Minturn which are land-use distinctions that identify where PUD development has have been previously explored and is anticipated in the future, should development occur. Battle Mountain and Bolts Lake were for development. The area was first explored through a PUD as a PUD for ski resort development in the mid-2000's, but plans were discontinued.~~ More recently, plans for residential development ~~of the Bolts Lake area~~ have circulated but nothing ~~concrete has been proposed has~~

<sup>3</sup> Information from the Status of Minturn's Proposed PUD's Memorandum, SE Group (2022), developed in the analysis phase of this community planning process ~~and from follow-up email communication between SE Group and the Town, dated November 9, 2022.~~

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Commented [sh3]: Just Eagle County, or will they prioritize Minturn residents first; Eagle County residents second?

Commented [MH4R3]: I don't know that we have this info yet

~~been approved. These PUD holding zones do not currently have specific site plans approved and thus do not have dimensional standards associated with them as a traditional zoning district would. Any new development proposition at these locations would require renewed determinations of such standards and detailed site plans per PUD regulations in the Town's code. Battle Mountain is now owned by a single entity, but the owner has not indicated development plans. Other than the old Bolts Lake Reservoir site itself (now owned by the Eagle River Water & Sanitation District), the Bolts Lake PUD holding zone area is also owned by an individual entity with no identified development plans. Thus, the future of both areas is still unclear. The zoning code currently addresses these properties as such follows:~~

~~Except as otherwise provided in Section 16-10-30, uses within the Bolt's Lake, Gilman, Willow Creek, Rock Creek and Holy Cross Character Areas shall be set forth on an approved final development plan for those areas. Until such time as a final development plan is approved, property within the Bolt's Lake, Gilman, Willow Creek, Rock Creek, and Holy Cross Character Areas shall be used only for the following purposes:~~

~~(1) Environmental response activities for superfund sites consistent with EPA and CDPHE process and approvals and this Article.~~

~~(2) Forestry management and maintenance, including without limitation measures to address the infestation of pine beetles and abatement of noxious weeds.~~

~~(3) Passive recreation activities consistent with historic practice.~~

~~(4) Management and maintenance of the property as open space, greenbelt and wildlife habitat.~~

~~(5) Investigation and monitoring of soils, watersheds and other components of the property and other appropriate activities in connection with the management and maintenance of the property and the preparation of the application for the final development plan for the project.~~

~~(6) Operation, maintenance and use of water rights, water resources, water diversion structures, ditches, pipeline structures, ponds, water impoundments and associated facilities consistent with the decreed uses but subject to these restrictions.~~

~~(7) Hunting.~~

The Town is interested in redeveloping the **Dowd Junction** area as one of the Town's most viable and strategic opportunities for development. Town Council sees three main goals at this site:

- To generate sales tax and commercial property tax revenue
- To maintain public access to recreational opportunities at Meadow Mountain; and
- Serve as a regional transit hub and gateway to Minturn.

Additionally, discussions about development potential at this site include workforce housing but visioning and market analysis are still in progress to determine the best use for the site to move forward with proposals. Development at Dowd Junction will need to balance the opportunity for non-residential development with residential development. The Town has limited sites where significant non-residential development can occur to increase the Town's tax base, but the site



represents a unique opportunity to add residential units, commercial space, and transportation infrastructure. Dowd Junction is the current location of a USFS office and is owned by the agency which wants to consolidate administrative operations and properties in the region. Thus, the Town of Minturn has discussed acquiring the site from the agency. Another agency-owned parcel, Martin Creek, may be another opportunity for residential development in the future following a land swap or equivalent agreement.

The Eagle County School District owns property near Maloit Park, where there is housing for District staff. The District is interested in further subdividing the area or using a PUD to expand and improve housing in this area [for District staff. Current concepts envision up to 138 units \(mix of single-family, duplex, townhomes, and multifamily as well as ownership and rental\) in addition to the existing recreational and institutional uses within this broader area. Plans are still developing and could ultimately include public-private partnerships for market-rate or other deed-restricted housing.](#)

Commented [WA5]: Subject to change. Most recent information will be included in final plan.

### Housing Policy Context

In 2019, Minturn adopted a Housing Action Plan in response to increasing pressure on housing from second homeownership, and the need for more long-term rental options for full time residents and workforce. The Housing Action Plan includes eight prioritized tools to implement to manage housing in Minturn. These include:

- An inclusionary housing requirement
- Policy support for ADU development
- Deed-restricted housing requirements in annexation agreements,
- A home buyer assistance program,
- A density bonus program for workforce housing,
- A housing development on vacant Town land (public-private partnership,
- A weatherization/energy efficiency program,
- Management of short-term rentals, and

Residential linkage fees were also recommended for further consideration as part of the prioritized strategies in the Housing Action Plan but were not subsequently supported by Town Council.

In 2020, the Town adopted an inclusionary housing ordinance (No. 7 – Series 2020), which applies to any new housing or mixed-use development with 5 or more units or lots and requires 20% of units for local residents and 10% with initial sales price no greater than 200% AMI/rental no greater than 80% AMI. On a case-by-case basis, the Town Council may agree that a land dedication for community housing development is allowed in lieu of the requirements that otherwise apply within the development. The Town prefers to receive housing units over land, as there are still additional costs and time required to develop housing on any land dedicated in-lieu of a developer building affordable units on-site within the project or off-site.- As it happens, the land dedication option is less likely to be pursued by developers since there is minimal land available in Minturn. The inclusionary housing regulations also prohibit the short-term rental of community housing. Only one development proposal (Midtown Village PUD) has triggered the inclusionary housing ordinance in Minturn since its adoption.

Additionally, in 2017, the Town adopted Ordinance No. 04 – Series 2017 establishing new regulations for short term rentals (STRs). These regulations put in place a cap on STRs of 10% or 50 units of the total housing stock and a two-year ownership requirement prior to licensing application, among other tax and use regulations. In August 2022 the Town amended STR

regulations to remove the 2 year ownership requirement for second-floor rentals above ground floor commercial in the 100-Block.

There is generally a political appetite for expanding ADU allowances within Minturn, but no formal action has yet been implemented.

#### Minturn's Historic Influence<sup>4</sup>

The development of nearby ski resorts such as Vail and Beaver Creek has brought tourism to the area, which Minturn benefits from. However, without its own resort development, constraints on growth, and reduced visibility from I-70, the community has been able to maintain its unique historic character and residential feel. Minturn's 2016 Economic Development Strategic Plan highlights the importance of the community's historic character and friendly atmosphere, tourism, placemaking, and small/local businesses. In particular, residents value Minturn's eclectic architectural character and historic feel that is most clearly present in the 100 Block area. While this specific area is discussed in more detail in **Chapter 2: A Thriving 100 Block: Urban Form, Historic Character & Circulation**, it is important to note the influence and value of this area in establishing and maintaining Minturn's broader community character. Preserving the small-town feel of Minturn is possible, even as the community evolves over time.

Minturn's architectural character is varied, displaying a variety of building periods. One distinctive element is the vibrancy of the colors of residential and commercial buildings, especially in the community's core. The colors of buildings in outlying areas are more subdued. Architectural styles present in Minturn include false-front style, pueblo, milled timber, folk victorian/national folk, gothic revival, neocolonial, modular, minimal traditional, craftsman, modern, and others. Most of the pueblo, milled timber and folk influences are concentrated in the core.

#### *Pueblo-Influenced*

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<sup>4</sup> Minturn Community Plan History & Policy Context, SE Group (2022), developed in the analysis phase of this Community Planning Process.



*Milled Timber*



*False Front Commercial*



## Community Input

During this community plan update, the Town conducted a survey of residents. In this survey, residents of Minturn identified sense of community and appearance of the town as two of the top five attributes of the community.<sup>5</sup> Of the eight “critical areas of focus” determined by the survey, three related to housing and/or character (deed-restricted housing, historic preservation, and the Town’s appearance). 53% of survey respondents indicated strong support for the creation of more attainable housing for workers in the next 5-10 years. This project goal fell below other priorities such as water infrastructure and pedestrian and recreation infrastructure. 64% indicated that there are not enough resident-occupied, deed restricted housing options in Minturn, and 56% said they are very or extremely supportive of adding such housing options.

Participants in the Open House held as part of this planning process expressed concern about losing residents due to high housing costs and more affordable options elsewhere. They also expressed concern about increasing second homeowners as well as tourists interested in renting STRs. Some residents also expressed that Minturn was losing residents to communities down-valley, where housing costs are cheaper. Participants were specifically asked to give feedback on multifamily housing types and locations. Responses indicated support for larger multifamily and duplex development at Dowd Junction and west of Taylor Street. Participants also favored duplexes and townhomes within Old Town and the 700-900 Blocks of southern Minturn. Townhomes were also favored in the 100 Block. Finally, residents were asked to express support for two draft vision statements, one that focused on Minturn’s historic, small-town character, vibrant downtown, and recreational assets. The other focused on adaptability and inclusivity, including affordable housing and sustainability. Respondents favored the first option, which more closely resembles the final vision statement for this plan. Additional input that relates to housing and character is summarized in **Chapter 2: A Thriving 100-Block: Urban Form, Historic Character & Circulation.**

### Competing Sentiments

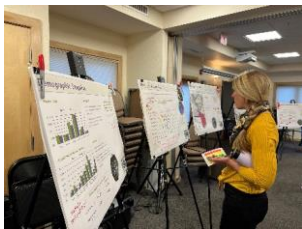
It is important to note that, while residents tend to indicate concern about housing costs and availability to support local residents and workforce as well as support for housing development at the conceptual phase as outlined above, there is also much opposition to proposed housing developments. Thus, there is a conflict between what residents generally say they have an appetite for or are concerned about when asked, and what they will actually support once a more concrete proposal is in play. Anecdotally, much of the opposition is centered around development layout and design including the location of parking and circulation as well as massing and density. Additionally, residents who live adjacent to larger proposed housing projects tend to oppose them while residents who live elsewhere may support a housing concept in that area. For example, residents who participated in the Open House event indicated support for larger multifamily or duplex development west of Taylor Street but residents in this area have expressed strong opposition to this type of development at other events. Additionally some residents have elsewhere expressed opposition to higher density developments at Belden Place and Martin Creek. The Town will need to carefully navigate these local politics to balance competing needs and desires. Town staff, the Planning Commission, and Town Council will need to work closely with developers on creative design and continue to engage in conversations with the public about the importance of housing for the community and circumstances that make moderate density most viable and strategic.

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<sup>5</sup> Town of Minturn Community Survey Report (2022)

The Community Plan Steering Committee also provided valuable feedback as part of this process.<sup>6</sup> During facilitated meetings, the group expressed gratitude for Minturn's neighborliness, strong base of year-round residents (especially compared to some nearby resort communities), generational families, rich history and "funky" architectural character. They reflected concern about increasing short-term rentals and second homes and their effect on housing availability and affordability for year-round residents. They also noted a local tension between the need for more attainable housing and concern over increasing density and development. This group also stressed the importance of including historic character and preservation as key topics within this plan update.

This feedback highlights some key agreements as well as inconsistencies in community perspectives and desires. First and foremost, the community values Minturn's historic and eclectic character and small-town feel. Residents are concerned about the impacts of short-term rentals, second homeowners and increasing tourism pressures, and they want to protect year-round residents and the local workforce but are worried about increasing density and fear change. As illustrated above, support for new housing development, especially for rental housing and affordable unit options, is mixed and complex.



### Central Concepts

This section outlines the important housing policy issues for the Town to continue addressing. The following **Recommendations** section provides more detail on the specific steps to be taken to address housing issues in Minturn.

#### Prioritizing Resident-Occupied Housing

Today, approximately 80% of housing in Minturn is occupied by a full-time resident, compared to approximately 35% in Vail and 65% in Avon. The presence of full-time residents creates a sense of community and year-round vitality: "the lights are on" in Minturn's neighborhoods year-round. This sets Minturn apart from mountain resort towns in which the vast majority of the housing stock is comprised of part-time homes and short-term rentals. The community would like to maintain the current level of full-time occupied housing, long-term rentals and ownership. Because some residential properties in Minturn will naturally deteriorate over time and may be demolished in the future, the Town should institute a broad set of land use regulations and community-based funding approaches to retain existing housing opportunities.

#### Using Short & Long-Term Approaches to Generate Housing

<sup>6</sup> Information from Minturn Community Plan Steering Committee Meeting #1 Memorandum, SE Group (2021)

Many communities rely on land use regulation tools that create affordable housing units and/or fee revenue from new development. Approaches such as inclusionary housing (IH) can be effective when there is a steady pace of development. Combined, Belden Place, Minturn North, and Midtown Village have 108 available water taps for near-term development. At the time this plan was written, these projects have the potential to create 277 total units with additional water capacity. While final numbers are not secured, these projects will produce several deed-restricted units for local year-round residents and labor force as well as some reserved for lower-income households.

Minturn's IH ordinance applies only to projects of 5 or more units. ~~As noted, there are only 5.72 acres of vacant land in Minturn remaining (excluding industrial areas, Bolts Lake and Battle Mountain),~~ so larger projects need to be developed in order for the IH to produce more affordable housing in Minturn. ~~As noted, there is little land available for residential development under current zoning regulations, particularly of parcels large enough to trigger the Town's IH ordinance.~~ Dowd Junction presently offers the greatest potential for larger-scale development that would trigger the IH ordinance. ~~The PUD Holding Areas (, while other properties could be explored. Battle Mountain and Bolts Lake) could create significant free market and deed restricted housing. However, however the timeline for any potential development at these sites is very uncertain.~~ Town Council and subsequent zoning recommendations in Chapter 1 of this Plan identify these areas for low-density development due to concerns about traffic impacts. Even at low densities, Master Plan development at these locations – as opposed to subdivision and incremental one-off development – would trigger the IH ordinance and contribute to the Town's stock of deed-restricted units.

To create more near-term housing opportunities and resources, the Town should use multiple approaches to housing: regulatory and with broader community-based funding. Reliable funding enables the Town to use approaches such as buying market-rate units to be deed-restricted, and purchasing deed restrictions ("buy downs") on existing homes. It can be less expensive to preserve existing housing than to construct new affordable housing.

Options for consideration are outlined below:

#### Reconsider the IH Program Income Limits

The current IH program requires housing to be affordable up to 200 percent of AMI, which is a household income of approximately \$180,000 for a three person household. This income level equates to a home purchase price of over \$750,000. The IH will therefore produce housing that is close to market rate (albeit deed restricted) as the average sale price in 2021 was \$767,000. The Town should consider requiring a lower AMI target such as 120% or a range of AMI levels from below 100 percent to the current 200 percent requirement.

#### Residential and non-residential linkage fees

Linkage fees are applied to new development based on its employee generation and impacts on workforce housing demand. They function like capital impact and tap fees and can be charged at building permit. The 2019 Housing Strategy considered residential and non-residential linkage fees. There was no support for non-residential linkage fees in that effort, and moderate support for residential linkage fees, although neither were ultimately adopted. An advantage of linkage fees over IH is that linkage fees apply to all development; there is not typically a threshold of application (e.g. 5 or more units).

The Town could consider combining linkage fees with the IH program. Linkage fees would apply to small projects under five units and residential projects generating a net increase in gross residential floor area (e.g. expansion or redevelopment of a home into a larger home).

Non-residential linkage fees could be considered but may run counter to the goal of increasing the tax base by attracting more non-residential development. All linkage fee revenue would be restricted to funding housing programs.

*Sidebar: Defining Linkage Fees: Linkage fees are a one-time fee paid by developers for projects of a certain type or size that channels a funding source for community housing development or other specific community needs. Linkage fees are often used instead of an inclusionary zoning (IZ) regulation or to supplement the IZ for smaller projects.*

#### Community-based funding

A revenue source is needed to expand housing opportunities in Minturn. Linkage fees could be one source, but the revenues are still dependent on new development. A more sustainable approach is a voter approved tax or other regulatory fees. A voter-approved mill levy or sales tax would generate an annual regular funding stream. An occupational privilege or “head tax” is another possible source; this is a tax paid by employers on the number of employees. Again, this may run counter to short-term goals to increase (and maintain) Minturn’s tax base.

#### Short-term rental fees

Several mountain and resort communities in Colorado are adopting regulatory fees on short term rentals. The fees are typically higher than annual licensing fees, and the revenues are restricted to funding housing programs. For example, Telluride recently increased their short term rental licensing fees to support the creation of year-round workforce housing, and Breckenridge and Silverthorne have implemented tiered licensing fees based on the number of rooms.

#### Encouraging a Diverse Mix of Housing Types

Approximately 68% of the homes in Minturn are single-family detached structures. This gives the Town a unique character compared to larger, more densely developed resort towns. However, attracting more types of housing and sometimes smaller units can help lower costs and provide more options for renters or people who may prefer an option besides a detached single-family home.

#### Leveraging Opportunities for Housing & Tax Base Expansion in Future Development

Given the limited number of properties with future development potential, the Town should ensure that new development balances creating full-time housing and non-residential development to grow the tax base. These strategies should apply to [the current PUD Holding Areas any potential PUD or Master Plan developments](#) and any future annexations. The Town should employ its land use regulations and development agreements to create – not hinder – full-time housing in new developments.

#### Addressing Water Constraints to Create More Housing Opportunities

The current water moratorium limits the ability to increase housing opportunities and to attract non-residential development. The Town has an ongoing priority to make ongoing improvements to the Town’s water infrastructure, solidify future water sources and encourage the efficient use of existing resources. See **Chapter 5: Water & Sustainability: Natural Resources, Infrastructure & Efficiency** for additional information.

## Recommendations

### OBJECTIVE 4.1) CONTINUE IMPLEMENTING THE RECOMMENDATIONS IN THE 2019 HOUSING ACTION PLAN.

As indicated in the Existing Conditions & Issues Section above, the Town has already accomplished two of the nine prioritized and recommended tools laid out in the 2019 Housing Plan, one of which was identified as a “Tier One” action item. The 2019 Housing Action Plan contains seven remaining priority recommendations that remain relevant today. They are organized into tiers of priority:

- **Action 4.1.1** Implement remaining Tier One recommendations in the 2019 Housing Action Plan.

#### Tier One:

- *Revisit and update current ADU regulations, including:*
  - *Broaden areas where Accessory Dwelling Units (ADUs) are allowed*
  - *Streamline the permitting/review process and consider waiving or deferring fees when there will be a deed restriction*
  - *Loosen parking requirements where applicable and consider creative off-site or public transit alternatives*
  - *Consider expanding or removing the size cap as applicable and revisit dimensional requirements*
  - *Consider waiving water tap fees with local deed restriction on long-term rentals*
- *Develop and adopt policies for negotiating deed restricted housing in annexation agreements. Although future annexation opportunities may be limited, having standards in place if they do occur will ensure that the Town can effectively use these instances to meet community needs and housing goals.*
- **Action 4.1.2** Implement remaining Tier Two recommendations in the 2019 Housing Action Plan.

#### Tier Two:

- *Collaborate with The Valley Home Store (TVHS), which currently offers down-payment and mortgage assistance to Eagle County residents, to ensure that Minturn residents are aware of this resource. Consider establishing a supplementary Minturn local assistance fund.*
- *Consider adopting a density or Floor Area Ratio (FAR) bonus for the inclusion of deed restricted housing in new development.*
- *Consider partnering with private developers to create new community housing on available Town-owned land. An inventory of such parcels is provided in the 2019 Housing Plan but further analysis and consideration is necessary.*
- *Partner with regional organizations such as NWCCOG and Energy Outreach Colorado at Walking Mountains and/or establish a local program to share resources and support local housing rehabilitation, weatherization, and energy efficiency projects.*

### OBJECTIVE 4.2) UPDATE THE 2019 HOUSING ACTION PLAN GOALS REGULARLY AND IN RESPONSE TO COUNTY-LED HOUSING NEEDS ASSESSMENTS AND CHANGES IN AMI.

Economic conditions that affect housing can change quickly and unexpectedly. While Minturn can rely on solid guidance from their 2019 Housing Action Plan and further guidance from this plan, it is important for officials to adapt to circumstances as they change. This adaptable approach will ensure that Minturn can adjust priorities and strategies to align with local and regional housing needs.



- **Action 4.2.1** Monitor Eagle County and Eagle County Housing & Development Authority publications and other publicly available data from sources such as HUD to stay up-to-date on regional housing needs. Use these indicators to inform changes in circumstances that may require adjustments to Minturn’s housing strategy.
- **Action 4.2.2** Update Minturn’s Housing Action Plan by 2025. The 2019 Plan is set to guide housing for 3-5 years.
- **Action 4.2.3** Conduct periodic development code reviews to promote innovative and smaller homes, including ADUs.
- **Action 4.2.4** Continuously support historic preservation to maintain the Town’s unique housing stock.

**OBJECTIVE 4.3) CONSIDER NEW HOUSING STRATEGIES, SUCH AS BUY-DOWN PROGRAMS.**

In a buy-down program, the Town would purchase deed restrictions from current homeowners. The Towns of Breckenridge and Vail are using this approach, and the market for a deed restriction is about one third the market value price of the home. Some homeowners take advantage of the program to get cash to pay off a mortgage, avoid foreclosure, do needed repairs, or to age in-place in their home. The Town then holds a deed restriction on the home, typically limited to full-time residents working, in this case, in Eagle County. This program may be a good match for Minturn with its older housing stock and somewhat older population than the County as a whole. With limited growth potential, a buy down program may be the most effective way to preserve and create opportunities for full-time resident housing.

- **Action 4.3.1** Investigate the viability of a buy-down program in Minturn including identifying a funding source.
- **Action 4.3.2** Broaden the application of a buy-down program to include historic preservation renovations of homes, so long as they are deed restricted to full-time residents.

**OBJECTIVE 4.4) INCREASE THE AMOUNT OF LOCAL FUNDING AVAILABLE FOR FULL-TIME RESIDENT HOUSING THROUGH NEW FEES AND/OR VOTER-APPROVED TAXES.**

The most successful programs for maintaining or increasing the supply of resident housing have a reliable dedicated funding source. Rather than relying on new development, a **tax dedicated to housing** enables a community to proactively plan and fund projects or initiatives. The Town should monitor political support for a modest mill levy or sales tax dedicated to housing. While **linkage fees** received moderate to low support in the 2019 Housing Action Plan, they should be re-considered periodically and especially before major surges in development occur if large projects, sites, or PUDs are expected to come under review. A residential linkage fee may be more appropriate for Minturn, rather than applying linkage fees to commercial development. A **regulatory fee on STRs** should also be considered. This is an emerging tool that several Colorado mountain towns are considering. The Town of Breckenridge adopted a regulatory fee on STRs in 2021. For planning consideration, a fee of \$500 per bedroom per year would generate about \$50,000 per year on the Town’s maximum number of 50 short term rentals.

- **Action 4.4.1** Gauge public appetite for community-funded housing.
- **Action 4.4.2** Consider instituting linkage fees and/or a regulatory fee on STRs.

**OBJECTIVE 4.5) CONTINUOUSLY ADDRESS WATER AND OTHER INFRASTRUCTURE BARRIERS TO HOUSING CONSTRUCTION.**

The Town is nearly at full capacity for its water system, and water may continue to be a constraint on growth for the Town. It should therefore carefully consider how future taps are allocated. The current moratorium on developments that require water taps greater than 3 SFEs is currently a large barrier to housing creation in Minturn. This is particularly limiting given that large-scale projects provide new units quickly and have greater potential to offer “missing middle” and affordable/attainable options such as rental units, apartments, duplexes, townhomes, etc. Multiple such projects are in the pipeline through PUDs but are significantly constrained by these conditions.

~~In 2019 the Council approved for a growth scenario of up to 450 SFE's. This included 120 SFE's for the Eagle County School District as the remainder of the designation from the 2011 Annexation Agreement, as well as 70 SFEs if the Council approves a Minturn North project. These two projects (Minturn North is only in the case of an approval) account for 42% of the 450 SFEs planned for in 2019. While moratoriums are in place, the Town should incentivize small-scale infill housing (re) development such as ADUs, duplexes and triplexes. Current plans will allocate approximately 450 additional SFEs, with 228190 already dedicated to future Eagle County School District development and the Minturn North PUD project planned developments, and an additional 50+ needed to see those plans to completion (277 total). Although not yet secured and with plans in flux, Minturn North and other planned PUD projects will require an additional 170 taps. Thus, these planned projects alone already account for about 4280% of the 450 planned additional SFEs. While these projects could would together increase Minturn's housing stock by more than 60% about 32%, the Town risks facing this same water capacity challenge again in the near future. This initial calculation does not account for any water needs associated with additional commercial development. While moratoriums are in place (and more broadly going forward) the Town should incentivize small-scale infill housing (re)development such as ADUs, duplexes, and triplexes.~~

**Commented [WA6]:** Subject to change. Most recent information will be included in the final plan.

**Commented [WA7R6]:** Updated via discussion with Madison 11/9

- **Action 4.5.1** Prioritize Town funding to support the necessary improvements outlined in Resolution 30 – Series 2020 to increase Minturn's water system capacity.
- **Action 4.5.2** Continue to monitor water capacity and consider expanding the intended capacity improvements beyond what is currently planned.
- ~~**Action 4.5.3** Consider a point system for allocating future water taps that gives higher weight to projects with full-time resident housing. This would function similar to a growth management program that allocates building permits based on defined criteria. For example, the City of Aspen caps market rate new building permits at approximately 19 per year but has no cap on affordable housing. This is an extreme example, and Minturn could develop criteria to address infill, affordability, water conservation, and a variety of other criteria.~~
- **Action 4.5.34** Evaluate an allocation policy along with new rate structures when capacity becomes available.

**OBJECTIVE 4.6) PRIORITIZE HOUSING FOR FULL-TIME RESIDENTS; ENSURE THAT RESIDENTS OF ALL AGES AND INCOME LEVELS ARE ABLE TO FIND HOUSING.**

As described above in the summary of public engagement and feedback related to housing, Minturn residents are interested in preserving and promoting “resident-occupied” housing. A healthy level of full-time residency is critical to maintaining the social and economic fabric of a community. Likewise, it is of the utmost importance to ensure that low- and middle-income

residents of Minturn can afford to live in Minturn. The following regulatory tools aim to support full-time residency and affordability in Minturn:

- **Action 4.6.1** Amend the 200% AMI requirement for inclusionary housing in Minturn to promote more equitable and inclusive access to housing. Further assessment may be necessary to determine the most appropriate thresholds for the community, and many communities require units at a variety of thresholds. A 120% AMI requirement should be available for at least a portion of units.
- **Action 4.6.2** Use deed restrictions to require full-time residency in the community in homes created through housing regulations or preserved with public funding such as a buy-down program.
- **Action 4.6.3** Support a diversity of housing types in the Town code and development guidelines; ensure that updates to PUD guidelines encourage and incentivize affordable housing.

**OBJECTIVE 4.8) CONTINUE TO COLLABORATE WITH REGIONAL JURISDICTIONS AND ENTITIES IN EAGLE COUNTY TO STAY ABREAST OF HOUSING ISSUES, AND POLICIES AND TOOLS BEING USED. ALIGN BEST PRACTICES AND INCOME QUALIFICATION RANGES FOR CONSISTENCY.**

Numerous communities and agencies in Eagle County are working on and administering housing programs. The Town should keep in touch with these other groups and look for areas where Minturn’s policies and regulations may need to be adjusted for consistency.

- **Action 4.8.1** Convene with the housing departments of other jurisdictions and with housing providers annually to review successes and local best practices.
- **Action 4.8.2** Routinely revisit designated AMI ranges for affordable housing to ensure that they correspond with Eagle County housing needs analyses and meaningfully contribute to the generation of affordable housing.
- **Action 4.8.3** Actively engage with regional partners to support the possibility of a new Regional Housing Authority.

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**OBJECTIVE 4.9) ENSURE THAT PUD REGULATIONS AND POLICIES CONTAIN PROVISIONS THAT WILL CREATE FULL-TIME RESIDENT HOUSING, PARTICULARLY AT DOWD JUNCTION AND MARTIN CREEK.**

Since Minturn’s primary development area is built out, PUDs represent a singular opportunity to add substantial additional housing units. Should the potential PUDs in Minturn eventually progress into development review and eventual construction, The Town should ensure that these PUDs will provide an appropriate level of resident housing.

- **Action 4.9.1** Encourage the inclusion of affordable housing in PUDs over and above the current IH requirement; consider offering an expedited review process to incentivize this.
- **Action 4.6.4** Consider implementing “main street” or PUD/Master Plan design regulations for new developments that require active street frontage and rear/hidden parking or other elements to increase the likelihood that development proposals will be palatable to and supported by the community.

**OBJECTIVE 4.10) SUPPORT HISTORIC PRESERVATION WITHIN MINTURN’S BROADER HOUSING STRATEGY.**

Minturn has some of the oldest housing stock in Eagle County which gives the Town its unique character. Many residents are deeply invested in preserving the town’s historic homes. Minturn residents are accustomed to smaller historic houses, creating an opportunity to promote

innovative designs of smaller and less costly homes. These could include duplexes, fourplexes, eightplexes and ADUs. As an added benefit, these smaller units can help address the shortage of rental housing, and/or provide less costly ownership opportunities.

- **Action 4.10.1** Review and update Minturn’s development design guidelines to strengthen and further specify how new residential development, redevelopment, and remodels can contribute to the community’s valued architectural character and vibrancy.
- **Action 4.10.2** To preserve existing buildings to the extent possible, consider instituting a demolition delay ordinance or initiative to support adaptive redevelopment as opposed to teardowns and new builds.
- **Action 4.10.3** Foster discussion around materials and design in the early phases of development review to ensure that developers understand the importance of celebrating Minturn’s existing character.

DRAFT



## CHAPTER 5

### Water Infrastructure, Hazard Mitigation & Sustainability

#### Introduction

One of Minturn's most pressing issues as a community is the current limitations on its water system. These issues threaten Minturn's ability to grow over time, even at its relatively low population growth rate of less than 1 percent per year. The community's water challenges are exacerbated as Colorado continues to experience ongoing drought and other impacts of climate change that could directly affect available water supplies. [Minturn and the surrounding region have been experiencing recent and extended periods of drought and reduced snowpack. Soils in the area are drier than they used to be, and there is less runoff because that water is staying in the soil. Elsewhere in the Colorado River Basin, downriver communities have overallocated water. Minturn and neighboring communities have started to adjust water usage.](#)

It is essential that the Town considers these factors as it strategically and proactively approaches necessary upgrades to the local water system's infrastructure and operations while addressing growth management strategies that balance consumption with community vitality, cost recovery over time, and stewardship. In addition, the Town must consider impacts to wastewater capacity, including stormwater, related to changing development patterns and upward trends of extreme storm events over time.

In addition to the complicated effects of drought and increasing storm events, residents of Minturn and Coloradans more broadly are acutely aware of increasing wildfire threats. Coupled with the discussion of water infrastructure, this chapter addresses how Minturn can proactively prepare for and protect against other potential natural hazards, including wildfire. In addition, this chapter discusses ongoing efforts and additional measures the Town can pursue to promote and increase environmental sustainability throughout the community.

*Sidebar: Defining Sustainability & Resiliency – For the purposes of this chapter, “sustainability” refers to actions, programs, policies, and other initiatives that aim to reduce greenhouse gas emissions, conserve scarce resources, capitalize on natural ecosystem services for environmental management, and reduce waste. Sustainability actions and policies can also support public health outcomes. **This chapter focuses on Minturn's environmental sustainability.** See the **Plan Introduction** for an overview of other dimensions of sustainability, including social and economic sustainability, and how they are covered in this plan document.*

*In the context of this chapter, “resiliency” refers to the ability of the community to anticipate and adapt to environmental hazards, particularly hazards and events associated with climate change. Several portions of the Minturn community are located in fire- and flood-prone areas; achieving resiliency in these areas will require substantial future planning and analysis. In addition to disaster mitigation, disaster event planning is also an important dimension of*

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resiliency. Future planning and resource analysis is required to ensure Minturn's readiness for disaster events. Importantly, "resiliency" applies to slow-moving environmental hazards as well as discrete disaster events. Examples of slow-moving environmental hazards include drought impacts and heat-island effects.

## Existing Conditions & Issues

### MINTURN'S WATER SYSTEMS

The Town of Minturn has its own water utility that provides drinking water for the Town's population of approximately 1100 people. The system includes an aging water treatment plant that is fed from a surface water diversion at Cross Creek. The water system includes two water storage tanks, and approximately 7 miles of water distribution piping.

Based on the Water System Capital Improvement Plan (CIP 2019) prepared by SGM dated September 2019, the Town currently serves approximately 730 Single Family Equivalents (SFEs). The existing water system ~~operates near capacity at times to meet the current water demands, especially during the spring runoff period~~ maintains additional capacity for service.

Sidebar: What are SFEs?

*An SFE (single family equivalent) is the basic unit for determination of water charges and usage for a given development, accounting for water use within the building and on the lot. The Town Code provides that one (1) SFE shall be equal to the water required to serve up to three thousand (3,000) square feet of building area as measured from the exterior dimensions of development and two thousand (2,000) square feet of outside irrigated area.*

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Given the limitation of the water system capacity, the Town of Minturn issued Ordinance No. 05 – Series 2020, which enacted a moratorium on the allocation of water taps for new-build construction projects requiring more than three single family equivalent units (SFEs) above historic use. This moratorium was put into place to restrict new development until improvements could be made to the water system to increase the water system capacity. All other development at this time is limited to historic use plus a maximum of 3 SFEs each until water improvements can be completed to provide additional capacity.

The Town's existing water system does not provide water service to the nearby areas of Dowd Junction or Battle Mountain. If development were to occur in either of those areas, the developer would need to provide their own water rights, water supply and infrastructure to support the development or a fee-in-lieu of supplying water rights. However, in the case of Dowd Junction, it is important to note that the Town may need to consider supporting infrastructure costs as this could be a barrier to the financial feasibility of development. The Town will need to carefully consider priorities and trade-offs and work closely with potential developers to determine the most effective way to support projects at Dowd Junction.

### Water Loss

Based on the 2019 CIP, the water distribution system has significant water loss which means the water treatment plant is producing a higher than average amount of water as compared to the water that is actually used by the customers. The aging water system currently is estimated to experience between 30% to 60% loss. An average municipal water system experiences losses in the range of 10% to 15%. Steps as outlined in the 2019 CIP have been implemented including

~~the installation of a town-wide leak detection system and the replacement of all water meters. With these improvements in place, and minimal leaks to the main distribution system experienced, water loss numbers have remained high. This has led staff to research the water metering and billing system where vast inaccuracies have been detected. With the new WaterSmart meter replacement program nearing completion and going online, the Town hopes to better understand the causes for the apparent water loss and the possibility that the data is based on accounting issues rather than infrastructure shortcomings. are encouraged to further evaluate the water system including leak detection of the water mains and to implement repairs or water main replacement to reduce water loss in the system. Older water meters within the water system shall also be replaced with new meters and more current technology to ensure that accurate water usage measurements are being taken. Improvements to the water system to reduce this water loss could help free up existing water capacity for future development.~~

**Planned Improvements**

September 15, 2021, Resolution 30 – Series 2021 was approved and identified several improvements from the CIP 2019 that are required to improve the existing water system and to serve future development. Option B of the capital improvement plan is included in the resolution outlining several improvements to the Town’s water system that are identified to be completed in near term, mid-term, and long-term timeframes. Some of the main elements of the water system improvements include:

- New membrane water treatment plant
- Additional water source from the Eagle River
- New water storage tanks
- Replacement of water mains\*

\*The town is currently not replacing any water mains, as this infrastructure is in good condition. The CIP includes savings for a reserve fund for the eventual replacement of water mains when necessary. This is done so that the upgrades can be made using cash and not a loan.

The strategy of how best to secure Minturn’s water treatment solution is constantly evolving and the components of the CIP 2019 will likely be amended. The timeframe for completion of these water infrastructure improvements will depend on Town funding, and longer-range improvements may not be complete for 10 years. A new water tank is under construction at the time of writing and is scheduled for completion by the end of 2022, while the design for a new water treatment plant has also begun. ▽

**Future Water Capacity**

~~Once~~ If all of the improvements are completed as identified in Resolution 30 - Series 2021 the Town’s water system will have the capacity to serve an additional 450 SFEs. However, the Town could implement water system improvements that allow for more or fewer than 450 SFEs. Table 1 includes a summary of the Towns water system capacity.

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<i>Description</i>	<i>SFE</i>
Existing Water System	730
Additional Capacity after CIP 2019 Improvements	450
<b>Total Future Capacity</b>	<b>1180</b>

Some of the additional capacity planned for the water system includes SFEs that have been allocated to specific projects including 120 SFEs reserved for the Eagle County School District and ~~additional 70 SFEs allocated to an under review the three active Minturn Crossing (also known as Minturn North) site, PUD projects, if the project is approved by the Town Council should they all be approved.~~ Table 2 includes a summary of SFEs for future development.<sup>1</sup>

<i>Description</i>	<i>SFE</i>
Maloit Park/Eagle County School District	120
Belden Place	42
<u>Allocated</u>	<u>198</u>
<u>Estimate of additional needed per plans</u>	<u>24+22.5</u>
Minturn North	70
Midtown Village	45
<u>Allocated</u>	<u>20</u>
<u>Estimate of additional needed per plans</u>	<u>25+</u>
<del>Total allocated &amp; planned</del>	<del>276.57</del>
Available for other development	173.5
Total additional capacity	450

<i>Description</i>	<i>SFE</i>
Eagle County School District	120
Minturn Crossing Allocation	70
Available for Other Development	260
Total Future Development	450

Based on the improvements planned for the Town’s water system and obligations of some of this future capacity to anticipated developments, future development for other projects is limited to approximately 173260 SFEs (~~222 officially unallocated as of November 2022) within the Town of Minturn. This is not due to a lack of infrastructure or service; planned infrastructure improvements would add capacity to the water system.~~ These calculations do not specify taps needed for development at Dowd Junction or ~~Bolts Lake other potential development areas.~~

**WASTEWATER SERVICE**

The Eagle River Water and Sanitation District (ERWSD) provides wastewater service to the Town of Minturn. During this community planning process, the engineering firm Tetra Tech confirmed wastewater system capacity information for Minturn with Jason Cowles, the Director of Engineering and Water Resources for the ERWSD.

The existing wastewater collection system collects wastewater through a series of sanitary sewer mains within the Town of Minturn, and a single wastewater interceptor line parallels US Highway 24 to Dowd Junction. At Dowd Junction there is a wastewater lift station that pumps

<sup>1</sup> Information from the Status of Minturn’s Proposed PUD’s Memorandum, SE Group (2022), developed in the analysis phase of this community planning process and from follow-up email communication between SE Group and the Town dated November 9, 2022.

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wastewater to the Avon Wastewater Treatment Plant. The Dowd Junction lift station is currently the limiting point for wastewater service to the Town of Minturn. ERWSD will be replacing this lift station with completion scheduled for 2023.

The new lift station has been sized to accommodate up to 1750 SFEs from the Town of Minturn, and this includes the service area of Dowd Junction area, Battle Mountain, or any other future development along the US Highway 24 corridor. Otherwise, the Avon Wastewater Treatment Plant does not have any other limitation to serve the Town of Minturn, and additional SFEs would be purchased with future development.

The ERWSD is currently working on an updated wastewater collection system master plan. The master plan update will review collection system capacity, including if upgrades may be required for the existing interceptor line along US Highway 24. Other smaller sanitary sewer mains in the Town of Minturn may need to be upsized, extended to new development areas, or replaced to reduce Inflow & Infiltration (I&I) within the Town. According to the ERWSD, there are existing I&I issues within the Town, and replacement or repairs to older sanitary sewer mains may be needed to improve capacity in the pipe system. An I&I study is recommended to be completed by the Town to identify problem areas in the wastewater collection system and start prioritizing repairs to reduce I&I. The Town should coordinate with ERWSD to ensure that Minturn's wastewater systems are thoroughly assessed, and needs are addressed.

#### WILDFIRE & OTHER NATURAL HAZARDS

Wildfire is the greatest natural hazard risk in Eagle County, followed by winter storms, avalanches and landslides, high winds and thunderstorms, drought, and flooding. Due to climate change over time, these threats are increasing for Minturn and the broader Eagle River Valley and Eagle County communities. Minturn is serviced by the Eagle River Fire District and is currently informed by Eagle County's Hazard Mitigation Plan.<sup>2</sup>

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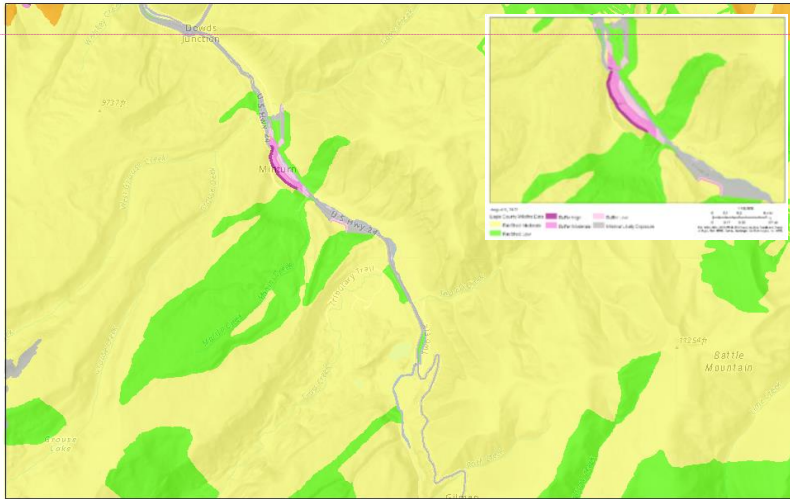
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<sup>2</sup>The Town website offers informational materials for residents regarding strategies to protect homes against wildfire risk, but they are not currently required by code.

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Eagle County Wildfire Map Viewer



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SUSTAINABILITY EFFORTS

Town of Minturn

Several past planning efforts in Minturn have incorporated sustainability topics – a foundation upon which this Plan builds. The 2009 Community Plan includes a section on sustainability and green infrastructure. The 2017 Energy Action Plan lays out goals and action steps for reducing building-related emissions by 5 percent per year and 80 percent total by 2050, among other targets. The 2021-23 Strategic Plan elevates a variety of action items from the 2009 Plan as well as new strategies that address natural hazards and sustainability, including continued cleanup of Eagle Mine Superfund site, implementation of various development practices and regulations, expansion of community composting and waste reduction, capitalizing on sustainable energy opportunities, improvements to water, stormwater, and multi-modal transportation infrastructure, etc. [The Among other achievements which are catalogued in Appendix XX, the](#) Town has successfully supported the implementation of two Electric Vehicle (EV) charging stations – one at Town Hall and one at the Shop & Hop/Shell gas station.

Regional Partners and Non-Profits

Regional entities and non-profits also play a role in supporting sustainability for the Minturn community. The Eagle County Sustainable Communities Department oversees County Environmental Policy and the County Climate Action Plan, and otherwise promotes sustainability and community resilience throughout the region. The Climate Action Plan was recently updated (2020), with overarching goals to reduce GHG emissions by 50% in 2030 and 80% in 2050 from 2014 levels. Minturn can use the planning efforts and resources of this Department to guide local initiatives and align goals. As described in the Recommendations section of this chapter, the Town should continuously monitor its performance in meeting its approved sustainability goals and the effectiveness of these goals.

The Walking Mountain Science Center is another vital asset for Eagle Valley communities. This organization offers a variety of educational sustainability and natural science programming for students and residents of all ages as well as programs and resources for businesses and locally organized events to implement sustainability measures. The Town of Minturn can utilize this resource and amplify the available resources and organization's impact for local residents.

The Eagle River Foundation supports projects that support the "safety, well-being, and environment of the people within Western Eagle County." Since 2000, the Foundation has distributed approximately \$370,000 in grant funding to more than 36 organizations. Projects include Eagle River cleanup and conservation efforts, Eagle Mine Superfund site cleanup, and various educational initiatives including the Walking Mountain Science Center, among others. This is a valuable financial resource that the Town can consider for local projects. Although the Eagle River Foundation primarily serves the western portion of Eagle County, the Town should still explore potential collaborations and support from the group.

The Eagle River Watershed Council – a recipient of Eagle River Foundation grants – "monitors, advocates for, educates, and seeks to restore" the Eagle River watershed. This organization is a vital partner for the river's health and conservation and to promote stewardship of the watershed. The Council hosts a variety of events and organized volunteer initiatives.

The Eagle Summit Wilderness Alliance (ESWA) promotes health and preservation for three regional wilderness areas, most notably Minturn's neighboring Holy Cross Wilderness. The organization oversees volunteer field programs to maintain trails, campsites, monitor invasive species and forest health, and spread awareness of stewardship principles. The organization also conducts outreach and advocacy. Again, this organization can serve as a valuable local partner for engaging in sustainability and conservation initiatives.

The Vail Valley Mountain Trails Alliance (VVMTA) works to balance recreation opportunities with environmental health and conservation. The organization provides sustainable trail planning, design, and building, working closely with local land managers, organizes volunteer events, and oversees a Trails Ambassador Program to enforce trail closures and spread awareness of various stewardship principles and local natural science information. The Town can promote these programs and opportunities for the community to embrace environmental stewardship, including respect for and protection of wildlife and natural ecosystems that surround recreational assets.

Healthy Communities Coalition of Eagle County conducts research and supports initiatives that serve wellness, primarily through nutrition and active living. While the organization's mission is not explicitly about environmental sustainability, the promotion of local food production and active lifestyles have parallel benefits for sustainability. The Coalition developed the Town's 2015 Healthy Eating & Active Living Master Plan Policy Scan.

The Minturn Community Fund also serves as a valuable local resource for community events and projects. The community garden is a popular gathering space and resource for locally-grown food, educational events, and more. Through collaboration and creativity, this resource could support future initiatives related to sustainability.

### Community Input

The Minturn Community Survey conducted during this planning process reveals that Minturn residents recognize the need for proactive planning to protect against potential threats and serve community needs. Protection against wildfire risks, emergency preparedness, and water

conservation were among top focal areas reported by the community.<sup>3</sup> Environmental sustainability was also reported as an important focus area. Repairing and replacing water infrastructure was the most-supported Town project. However, only 55 percent of respondents indicated that they knew about the Town's plans for improving water infrastructure and capacity and about half of respondents reported that they do not feel informed about the Town's water system.

At the Open House event held as part of this planning process, participants had mixed sentiments about a few potential sustainability measures that other mountain communities have implemented. Rebates for solar installation ~~were~~ received the most support, while water use reduction goals (40% reduction in use) were most contentious.

### Central Concepts

#### ACHIEVING WATER SOURCE REDUNDANCY

*Sidebar: What is redundancy? In engineering, "redundancy" refers to the duplication of service systems to ensure that if one infrastructure system fails, there is another system that the community can use and rely on. Redundancy increases a community's resilience to crises and disasters. Water source redundancy means that there are multiple sources of water to draw from should one of them fail.*

Currently, the Town's water system has a single water source, Cross Creek. It would be beneficial to have a secondary water source to provide redundancy to the Town's water supply. Typical risks to surface water sources in the Colorado mountains include reduced stream flows, drought, contamination from a spill, or potential contamination resulting from a wildfire in the watershed. To guard against these risks, it may be wise for the Town to identify and secure a secondary water source.

The 2019 CIP indicates two alternatives to provide redundancy of the Town's water supply. The first is developing the Town's water rights along the Eagle River. The second is to consider an interconnect by pipeline to the Eagle River Water and Sanitation District's (ERWSD) system near Dowd Junction. This second option ~~is not viable due to~~ has experienced public opposition and lack of support from Town Council; recently Eagle River Water and Sanitation stated that an interconnect was no longer an option.

Another ~~alternative to improve redundancy in the~~ potential segment of the Town's water ~~supply system~~ system is Bolts Lake. Bolts Lake is the site of a reservoir located south of Minturn and above the Town. The lake has been dry in recent years, as the dam was determined to be unsafe and breached. Recently, the Eagle River Water and Sanitation District has purchased Bolts Lake and intends to evaluate it for future improvements as a raw water storage site for the District. The Town of Minturn has an intergovernmental agreement with the Eagle River Water and Sanitation District and there could be benefits to the Town of Minturn from raw water storage development at Bolts Lake in the future. At this time, since plans for Bolts Lake are still being developed, the lake is not identified as part of the Town's water system. The lake itself would not be a source of water; this would require substantial permitting and physical upgrades to the site. Although the lake itself would not be a source of water, the use of the lake as a raw water storage facility could provide an alternative water source to Cross Creek or the Eagle River if those sources were ever contaminated by a spill. In this way, the Bolts Lake reservoir could support the redundancy of the Town's water system. By the same reasoning, the reservoir would also increase the Town's resiliency to drought.

<sup>3</sup> Town of Minturn Community Survey Report (2022)

#### INCENTIVIZE WATER CONSERVATION AND EFFICIENCY

The Town of Minturn charges customers for water use on a fixed base rate plus a water usage rate per 1,000 gallons. The Town does not currently have a tiered water rate structure that charges customers at higher rates for high water users, although the Town does have a tiered water rate structure for sprinkler and irrigation accounts. Tiered water rates are a tool that many water providers in Colorado use to help incentivize water conservation, including Eagle River Water and Sanitation District and the Town of Eagle. Implementing a tiered system in Minturn could help to a) promote water conservation and b) help to pay for needed infrastructure improvements.

At the time of writing, the Town is in the process of adopting “waterwise” landscape regulations, scheduled to wrap up in the fall of 2022. These regulations update and replace the Town’s previous landscaping requirements found in the Minturn Municipal Code and are generally applicable to new development and major remodels of existing homes and landscapes. The new regulations would focus on reducing “high water use” areas and water intensive lawn areas on individual properties; designing landscapes with xeriscape and drought tolerant plant species in mind; and water efficient, “smart” irrigation systems.

#### THE ROLE OF WATER AND SUSTAINABILITY IN DEVELOPMENT

As laid out in the Existing Conditions section above, water is essential for future development. ~~Though and the Town’s system has not run into capacity issues yet, it may eventually. Another defining issue for water in Minturn is the Town’s consumptive use limitations. These limitations are at their greatest between August and October. If less outdoor watering took place during those months, more water would be available for additional residential units, does not currently have enough capacity or efficiency to support even incremental growth.~~ Additionally, PUD sites that have the greatest land opportunity for future development are not currently served by the Town’s water and wastewater infrastructure. Thus, many of the recommendations that follow in this chapter are centered around the implementation of the Town’s Capital Improvement Plan and other action steps to improve water system capacity, efficiency, and resiliency to support strategically directed growth and development.

When considering development regulations and incentives, it is important to think about potential trade-offs and goals. Construction and building functions are large contributors to resource use and GHG emissions. In the following section, this plan recommends that the Town promote greater sustainability in buildings and development within the community by adopting incentives (such as density bonuses or fee waivers) or regulations that encourage sustainable building practices, such as more sustainable materials, passive heating and cooling, renewable energy, water and energy efficiency and conservation, green infrastructure, etc. However, in Chapter 4 this plan also recommends similar regulations or incentives for the development of community housing (affordable, workforce, year-round resident-occupied). Development is expensive and complex, and the Town will need to carefully consider all the trade-offs of these potential strategies. What is the highest priority for the Town? Will additional regulations deter developers? Will incentives be attractive enough? How will the loss of revenue or additional massing/density resulting from incentives affect the Town’s operations or community appetite for a given project, and therefore its success? These trade-off considerations also apply to recommendations for dedicated funding streams for community housing or sustainability initiatives, such as impact fees, parking fees, short term rental fees, and others.

#### WILDFIRE & HAZARD MITIGATION

Hazard mitigation planning is a foundational step in assessing potential risks and establishing strategies and programs to prepare for and protect against potential natural hazards as well as coordinate emergency response efforts. Currently, Minturn relies on the Eagle County Hazard

Mitigation Plan, which was most recently updated in 2018. As natural hazard risks increase over time due to climate change, the Town may want to consider developing its own, more focused Hazard Mitigation Plan. Additional resiliency and hazard preparedness recommendations in the following section of this Chapter focus on partnerships, data sharing and monitoring, and strategic land use and development regulation tools to reduce risk impacts of wildfires – including forest fuels and wildland urban-interface (WUI) management, and fire-resistant building codes, landscaping, and development pattern regulations. In future planning and monitoring efforts, the Town should collaborate with relevant partners, including the Colorado Department of Wildlife, on stewardship efforts that support Minturn’s mountain ecosystem, including vegetation management and habitat parameters.



Image source: [planningforwildfire.org](http://planningforwildfire.org)

A SUSTAINABLE COMMUNITY

Environmental sustainability is increasingly becoming a critical factor in any planning initiative and is supported by the Minturn community as an important topical focus. Sustainability can and should be considered and woven into all aspects of the Town's operations and culture wherever possible. From recycling, composting and Zero Waste events such as the Minturn Market, to water conservation, to sustainable development and landscaping practices, to renewable energy viability, to balancing recreation and development with the protection of natural resources, ecosystems, and wildlife, to alternative transportation habits. As described at the outset of this chapter, sustainability includes anything that contributes to the reduction of greenhouse gas emissions, conservation of scarce resources, strategic use of natural ecosystem services for environmental management, or reduction of waste. As a leading entity and instigator of change for the community, the Town should investigate every issue, initiative, and facet of its operations through this lens to see where it can make adjustments that support and promote sustainability. The following concepts and case studies are central to the objectives and action items laid out in this chapter related to sustainability.

### Low-and-No-Carbon Transportation

Transportation is a major contributor to GHG emissions globally, nationally, and locally. In the U.S. this is particularly salient due to the fact that government decisions and funding streams have historically prioritized single-occupancy vehicles (SOV) as the primary mode of transportation. The term SOV refers to the trend of one person per vehicle travel, each of which relies on gas or diesel, and therefore emits carbon and harmful particulates. Mass transit produces less carbon per person than SOVs, and helps to reduce congestion and parking needs, as one vehicle can carry many people. Even better, walking or cycling does not produce any carbon. The increasing viability and production of electric vehicles also promises a lower-emission option for SOV travel. When mass transit, active transportation, and electric vehicle modes are given priority over traditional gasoline SOVs, they become more attractive to users, creating more diverse mobility options, and fostering behavioral shifts that are more sustainable. By improving infrastructure and service to make walking, cycling, public transit, or an electric vehicle more safe, convenient, and affordable, the Town of Minturn can play an active role in progressing toward more sustainable transportation practices.

### Status Quo



## Diversified Mobility Options



Another way to encourage active transportation habits and build support for bike-ped investments in infrastructure is to host organized bike/walk to work events or group rides. These events usually involve booths set up along popular routes with coffee, pastries, music, swag, and positive vibes for participants. Colorado DOT promotes an annual statewide bike to work day each summer. A study in Vancouver, Canada found a variety of strategies to broaden participation in bike to work events through activities such as friendly inter-company competition, multiple related and supportive events rather than one-offs, organized leadership, prizes and incentives, and robust/targeted advertising.<sup>4</sup> Because many Minturn residents work outside of Minturn, this initiative could be broadened to include other green transit options, such as carpooling and taking ECO Transit. This type of event could also be restyled to promote carbon-free trips within Minturn, such as walking or biking to the trailhead or to enjoy a community event in the 100 Block or at Little Beach Park.

Bikes Make Life Better is an organization that helps businesses implement programs and strategies to encourage employees to bike to work more regularly (or take other non-SOV modes). They primarily work with large companies but the same concepts can be implemented on a smaller scale. The following are some potential tactics to support biking within Minturn:<sup>5</sup>

- Provide free, safe, and convenient bike parking infrastructure on site.
- Offer Bike Commute 101 classes or events to encourage and support inexperienced riders. For small-scale businesses this could be a collaborative effort among several businesses.
- Offer paid incentives for employees who bike (or free transit passes); pair this with paid parking for other employees.
- Offer subsidized bike purchases or host a bike swap event.

### Local Sustainability Funding Streams

Establishing a dedicated municipal fund for sustainability projects could provide valuable financial support to implement sustainable practices and projects that both improve Minturn's sustainability and support economic development. The Truckee, CA business community has established a sustainable gift card program where 25% of card value is matched by Visit Truckee-Tahoe, the local tourism agency, to support local sustainability projects. These gift cards can be

<sup>4</sup> [https://www.pedbikeinfo.org/examples/example\\_details.cfm?id=4278](https://www.pedbikeinfo.org/examples/example_details.cfm?id=4278)

<sup>5</sup> <https://actionfigure.ai/blog/conversation-with-anna-walters-from-bikes-make-life-better/>



used at participating businesses who have opted into the program. A similar program could be established in Minturn, particularly if a Downtown Development Association is created. Other potential sources for a sustainability fund could come from visitor parking fees or impact fees. As the Town considers applying these additional fees, there will need to be a broader discussion about the areas of need for additional funding, including community housing. One possible scenario for the Town to consider involves using short-term rental fees to support a community housing fund, parking fees to support transportation-related sustainability initiatives, and impact fees to support both housing and sustainability. [The Town already uses an Exterior Energy Offset Program that charges different fees for heated driveways, spas and hot tubs over 64 square feet, and pools. These fees are intended to subsidize residential solar projects in the Town of Minturn. This program is not well-known or well-utilized, and therefore there is an opportunity for additional sustainability funding mechanisms \(e.g., sustainable gift cards\) that would fund a greater variety of projects.](#)

### Recommendations

#### OBJECTIVE 5.1) IDENTIFY PATHS TO SECURE AND MAINTAIN SUFFICIENT WATER AND WASTEWATER INFRASTRUCTURE TO SUPPORT CURRENT DEVELOPMENT AND GROWTH IN MINTURN.

From a development perspective, Minturn's ability to respond to and accommodate growth is currently limited by the Town's existing water system capacity. The Town has already established a preliminary plan for improving water infrastructure and capacity which it will need to implement as quickly and effectively as possible in the coming years. The following action items focus on this critical step of implementation as well as further steps the Town can take to ensure that Minturn's water and wastewater system will support the community long into the future. While engaging in this work, it is vital that the Town address other potential threats to water supply such as climate-related drought.

- **Action 5.1.1** – Prioritize Town funding and pursue all potential outside funding to support the necessary improvements outlined in Resolution 30 – Series 2021 to increase Minturn's water system capacity, particularly to improve efficiencies and mitigate water loss.
- **Action 5.1.2** – Work with the Eagle River Water and Sanitation District at Bolts Lake and/or on other opportunities to improve water supply resilience through Raw Water Storage and source redundancies.
- **Action 5.1.3** – [Conduct an Inflow & Infiltration \(I&I\) Study and utilize Advocate for wastewater systems upgrades as needed in Minturn through coordination with ERWSD, either through the forthcoming Eagle River Water and Sanitation District ongoing Wastewater Collection System Master Plan update or a separate Inflow & Infiltration \(I&I\) Study specific to inform necessary improvements to wastewater infrastructure in Minturn Minturn, including a prioritized implementation plan.](#)

*An Inflow and Infiltration (I&I) study includes inspections and flow monitoring to identify points where groundwater or stormwater enters the wastewater collection system. Visual inspections and closed circuit television video are completed on the sewer mains, sewer service connections and other infrastructure to identify points of clean water intrusion into the system. Potential reasons for I&I include cracks in pipes, gaps in manholes, root intrusion into service lines or mains, illegal stormwater or groundwater connections to the wastewater collection system.*

**OBJECTIVE 5.2) REFINE DEVELOPMENT REGULATIONS AND STANDARDS AND PURSUE OTHER INITIATIVES TO PROMOTE EFFICIENCY AND SUSTAINABLE PRACTICES IN RESIDENTIAL AND COMMERCIAL BUILDINGS.**

As described in the Central Concepts above, the Town of Minturn can update development regulations, adopt incentives, or establish other programs to promote and ensure sustainability is incorporated into development, landscaping, and building operations. The action items below will serve water and energy conservation and efficiency, support renewable energy production and use, and incentivize other strategies within development and building design that are more sustainable and reduce GHG emissions.

- **Action 5.2.1** – On an annual basis, monitor water and energy use in Minturn to inform efficiency and conservation initiatives.
- **Action 5.2.2** – Establish a tiered water rate system to promote water conservation.
- **Action 5.2.3** – Continue to implement Energy Action Plan strategies to incentivize commercial and residential participation in available energy efficiency and renewable source programs. Conduct a baseline study to assess the Town’s progress on its energy goals and establish whether current policies are working to increase energy efficiency.
- **Action 5.2.4** – Consider implementing a solar rebate or other incentive program to support on-site solar for residential and/or commercial buildings; collaborate with the Walking Mountains Climate Action Collaborative and/or Holy Cross to connect Minturn residents with existing energy incentive programs.
- **Action 5.2.5** – Establish Low Impact Development (LID) standards that promote green infrastructure and nature-based solutions to address stormwater.
- **Action 5.2.6** – Adopt waterwise/xeriscape regulations, incentives, and/or educational tools to promote strategic, responsible and climate-appropriate landscaping practices that reduce water use, improve soil health, and limit maintenance needs.
- **Action 5.2.7** – Consider implementing additional incentives for sustainable design and development practices in PUD regulations.
- **Action 5.2.8** – Consider implementing a Smart Building Incentive Program that allows developers and property owners to waive permit fees if their project conforms to a predetermined set of sustainability standards.
- **Action 5.2.9** – Refine the Town’s Exterior Energy Offset Program to ensure that the program provides an effective means of incentivizing solar energy in Minturn; provide clear guidelines for the fair and productive use of funds collected through the program fees.

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The Town considered the potential for a water reclamation/recycling system but determined it would not be feasible at this time. Such a program requires a separate system with pumping equipment for gray water which would be most feasible for a major user.

**OBJECTIVE 5.3) PRIORITIZE RESILIENCE BY PROACTIVELY PREPARING FOR AND MITIGATING POTENTIAL IMPACTS OF NATURAL HAZARDS**

Wildfires and flooding are the two most critical natural hazard threats for the Minturn community. As climate change contributes to a longer and more severe fire season as well as more frequent storm events, it is important for the Town to proactively prepare for potential hazardous circumstances. Through data monitoring, collaboration and coordination with regional and federal entities, and strategic planning for emergency preparedness and impact mitigation, the Town can bolster the community’s resilience.

- **Action 5.3.1** – Partner with FEMA to update floodplain maps and correct inaccuracies; utilize this information to inform future infrastructure needs, landscaping, development regulations, and other potential flooding preparedness measures.

- **Action 5.3.2** – Continue to reference and implement the Eagle County Hazard Mitigation/Pre-Disaster Mitigation Plan; support its update due in 2024/25 and ensure that Minturn’s particular risks and needs are accounted for in the plan.
- **Action 5.3.3** – Adopt firewise guidelines in building and site planning regulations.
- **Action 5.3.4** – Work with the USFS to determine necessary steps to manage wildfire impacts in Minturn, such as possible forest fuels reduction measures.
- **Action 5.3.5** – Retain the services of an emergency preparedness consultant; consider establishing to assess, bolster, and test Minturn’s emergency response systems; also to establish a natural hazard monitoring system to track local and regional events to accurately assess local risks and changing trends over time.
- **Action 5.3.6** – Continue to support, proactively participate in, and implement Eagle County’s imminent update to the Community Wildfire Protection Plan.
- **Action 5.3.7** – Coordinate with US Forest Service to initiate planning and implement associated strategies for forest fuels health and fire mitigation on public lands surrounding Minturn.
- **Action 5.3.8** – Continue to proactively coordinate with the Eagle River Fire Protection District, and consider reestablishing a volunteer fire safety program with associated training and education.

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**OBJECTIVE 5.4) SUPPORT THE VIABILITY OF LOW-EMISSIONS AND ZERO-EMISSIONS TRANSPORTATION OPTIONS SUCH AS WALKING, BIKING, PUBLIC TRANSIT, AND ELECTRIC VEHICLES.**

Single Occupancy Vehicle (SOV) travel is currently the status quo in Minturn – by and large residents use individual vehicles to get around town and connect to the broader region. Many areas of Minturn do not have safe sidewalks, crosswalks, bike paths, or other infrastructure that makes walking or cycling safe, efficient, convenient, and attractive. While ECO Transit, the regional transportation provider, does operate three bus routes that service Minturn, they run relatively infrequently and/or require residents to drive from town to the Dowd Junction Forest Service Park & Ride. These factors make SOV transportation the most convenient and attractive option for Minturn residents. However, SOV travel significantly contributes to GHG emissions and poor air quality, as well as a perceived parking issue in the Old Town core of Minturn. Mass transit, walking, and cycling are more environmentally friendly forms of travel and offer co-benefits of active lifestyle and health, opportunities for community interaction, fewer vehicles taking up space throughout town, and greater affordability. While electric vehicles are still considered SOV, they can be more environmentally friendly and require charging infrastructure to support their use. As electric vehicles become more common, charging infrastructure will become more critical. These action items promote investments in Minturn infrastructure and partnerships with ECO Transit that will foster a culture of low-emission and active transportation for Minturn residents and visitors.

- **Action 5.4.1** – Monitor the use of Minturn’s two EV charging stations and implement additional stations as needed in strategic locations. Also see Actions under objectives 6.2 – 6.6 in **Chapter 6: Intuitive Mobility, Circulation & Connectivity**. These action items address improved infrastructure and signage for walking and cycling, the expansion of mass transit to connect Minturn with the region and provide local transit options, and strategic integration of land use, mass transit, and bike-ped infrastructure to make non-SOV transportation options more convenient, safe, attractive, and viable.

**OBJECTIVE 5.5) SUPPORT PUBLIC HEALTH BY PROACTIVELY MANAGING AIR QUALITY.**

Public health and environmental health are closely intertwined. GHG emissions contribute to and exacerbate natural air quality issues by releasing and trapping particulates and gases that can be

harmful to human health and contribute to global warming. Through data monitoring, collaboration and coordination with regional entities, and strategic regulations and initiatives, the Town can work to monitor air quality and minimize emissions to protect the health of Minturn residents and visitors.

- **Action 5.5.1** – Collaborate with the Eagle County Environmental Health department to discuss the development of a local air monitoring program.
- **Action 5.5.2** – Consider adopting an Idle-Free resolution to prohibit idling motor vehicles within town. [For example, the Town of Crested Butte adopted an Idle-Free ordinance in 2011 that limits idling to no more than 3 minutes or 5 minutes to remove snow and ice and prohibits any idling for unattended vehicles.<sup>6</sup> Engines Off! Colorado is a collaborative initiative for idling regulation in Colorado and offers additional information and resources related to this issue.](#)
- **Action 5.5.3** – Encourage a community culture of low or zero emissions transportation habits through educational and promotional campaigns and events as well as supportive infrastructure and wayfinding. This could include organized bike/walk-to-work days, gear swaps, or programs with local businesses that incentivize employees or patrons to carpool, walk, bike, or take transit. Also see Objective 5.4 above.

#### OBJECTIVE 5.5.6) PRIORITIZE ENVIRONMENTAL SUSTAINABILITY THROUGH STRATEGIC MONITORING, COLLABORATION, COMMUNICATION, AND CREATIVE FUNDING STREAMS.

Minturn and the broader regional community have established and achieved numerous goals and initiatives related to environmental sustainability and many resources already exist to support future efforts, both at individual and municipal or other collective levels. Tracking successes – and challenges – as well as sharing information and fostering collaboration regarding sustainability will allow the Town and Minturn community to maximize outcomes and improve efficiency. Collaboration and information-sharing will ensure that efforts are not duplicated, that the broadest benefits possible can be reached, and that residents have access to and are aware of resources available to them. For example, the Town can more proactively advertise the valuable resources available through the Walking Mountains Science Center regarding composting, recycling, business and educational programs, and more. Additionally, the Eagle County Climate Action Plan includes a pledge for individual residents to commit to actions that will support the plan's goal to reduce GHG emissions by 80 percent by 2050. The Town can play an active role in promoting this pledge locally.

- **Action 5.6.1** – Implement an internal system to annually monitor town goals and achievements related to environmental sustainability.
- **Action 5.6.2** – Support sustainability initiatives and promote available resources of local and regional groups, non-profits, and agencies as possible; promote partnerships and collaboration to maximize capacity and share resources.
- **Action 5.6.3** – Consider establishing a specific fund for sustainability and resiliency projects. This fund could be sourced from programs such as Sustainable Gift Cards with partnering businesses, parking fees, impact fees, or other creative programs. Otherwise promote available grants, programs, and resources for sustainability initiatives.

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<sup>6</sup> [Ordinance No. 15, Series 2011](#)



## CHAPTER 6: CIRCULATION AND MOBILITY

### CIRCULATION AND MOBILITY

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#### Introduction

This chapter explores one of the main plan themes: Intuitive Mobility, Circulation, & Connectivity. Minturn residents value the ability to easily walk, bike, and drive throughout Minturn. Residents also value the ability to access neighboring communities via public transit. The goal of this chapter is to develop a transportation strategy in Minturn that expands the convenience and safety of multi-modal transportation options within Minturn and regionally. It explores central concepts including parking and curbside management, transit, the bicycle and pedestrian network, the transportation vision for Dowd Junction and the 100 Block of downtown Minturn, and signage and wayfinding. The chapter recommends near-term and long-term strategies for the Town to pursue as part of the long-range planning framework.

#### Existing Conditions & Issues

Minturn is located in a narrow valley; this topography poses spatial constraints and has concentrated the Town's development along a single corridor, US-24. While the northern edge of the Town abuts I-70, the downtown core is offset from the interstate. Residents value the distance from I-70, which helps maintain Minturn's small-town feel. However, residents also rely upon the corridor to reach key destinations around Eagle County such as grocery stores, schools, employment hubs, and healthcare facilities.

One third of commute trips that start in Minturn end in Vail or Avon. Most town residents drive to work, although the regional transit provider, ECO Transit, operates three bus routes that serve Minturn. The two routes that pass through the Town along US-24 operate relatively infrequently. The Highway 6 Route along I-70 that provides connections to Vail and Avon operates more frequently, but serves Minturn only at the Forest Service Park and Ride at Dowd Junction on the north side of town. Enhanced transit connections and service frequency and span could improve regional access.

For people walking, rolling, and biking in Minturn, infrastructure is limited. Sidewalks exist along most of Main Street (US-24) but are largely missing elsewhere. Some sections of Pine Street and Boulder Street have sidewalks, but they are inconsistent along the east and west sides of the street and are missing in sections between Harrison Avenue and Mann Avenue. Main Street

sidewalks have been completed through the south end of town and are planned to extend to Maloit Park Road.

While there are no designated on-street bicycle facilities within the Minturn town boundary, the Town has recommended a paved multiuse trail that will connect Dowd Junction to the north end of the downtown core at the intersection of Minturn Road and Taylor Street. There is also a regional paved multiuse trail, the Eagle Valley Trail, that extends along the Eagle River west of Minturn and continues along the Gore Creek east of Vail. With the implementation of the trail connection from downtown to Dowd Junction, users can bike from Minturn to Vail continuously on trail.

In sum, Minturn has limited existing infrastructure, programs, and policies for people walking, biking, driving, and taking transit. The Town has begun laying the foundation for a comprehensive multi-modal network, with planned infrastructure that will enhance connectivity and transportation options.

#### Sidebar: Growth & Transportation

#### STAFF PROPOSED LANGUAGE

Highway 24 provides the only continuous vehicle transportation connection through Minturn to Interstate 70. During peak times of day (morning and evening commute hours) Highway 24 experiences significant backups particularly in the Old Town area. Level of service for Highway 24 is a constraining factor on future growth. Further, during Town Council meetings, residents have expressed concern about traffic speeds on Highway 24 through South Minturn. Efforts to reduce the speed limit on Highway 24 with CDOT has been frustrating and law enforcement staffing prevents effective policing of the speed limit. Traffic levels and traffic speed are particularly problematic with development in South Minturn, Bolts Lake, Maloit Park, and Battle Mountain because commuters from these areas must travel through the entire length of Town on Highway 24 to reach Interstate 70. As such, traffic management on Highway 24 is a limiting factor on growth particularly in the southern portion of the Town.

As housing units are projected to grow in Minturn, the impacts to Highway 24 will expand. This plan cannot make firm predictions about the associated transportation impacts of proposed development projects (particularly PUDs), as the number of units associated with these projects are in flux. Once the size and nature of these developments are more accurately understood, it will be possible to make more specific conclusions about Minturn's transportation needs and projected impacts. Growth in Minturn, and in particular the southern portion of Minturn, will be required to fully assess impacts to Highway 24 and mitigation may be necessary including off-site traffic improvements and reductions in proposed density. As housing units are projected to grow in Minturn, the Town's transportation needs and conditions may change. This plan cannot make firm predictions about the associated transportation impacts of proposed development projects (particularly PUDs), as the number of units associated with these projects are in flux. Once the size and nature of these developments are more accurately understood, it will be possible to make more specific conclusions about Minturn's transportation needs and projected impacts.

#### Community Input

The Town of Minturn surveyed the community in March of 2020 on a variety of topics, including why residents choose to live in Minturn and what the Town government should address in the future. The survey asked residents their perception of the importance of numerous town attributes, and their satisfaction with the Town's performance on these attributes. These included several transportation-related characteristics, including parking, traffic speed, traffic congestion, and public transit services in town. For each of these transportation attributes, respondents thought that the Town's performance in the area did not meet expectations.

More generally, numerous residents commented that they supported the Town's efforts to provide street improvements, including new crosswalks and sidewalks in recent years. They expressed their desire for additional bike facilities, especially the planned multiuse trail to Dowd Junction and a connection to Maloit Park at the south end of town. Residents also expressed concerns about vehicles speeding through town and their desire for improved public transit service.

As part of the community planning effort for this plan to gather input on plan recommendations, the project team hosted two open houses, two Steering Committee meetings, and a charette for the design of the 100 Block. Open house participants indicated a strong interest in increasing the frequency of transit service and expanding walking and biking trails in Minturn. Steering Committee members noted their perception that traffic has increased in Minturn in the last decade. To address this increase in congestion, they expressed interest in expanding transit service and implementing a bike share program in Minturn. They suggested better connectivity to the Meadow Mountain Business Park and local trails.

Another major theme of community engagement was parking and loading. Parking and loading ranked in the top five priorities of the second 100 Block Design Charette. In the first open house, however, attendees' comments indicated disagreement over whether a parking problem exists in Minturn. This chapter will summarize data collected on parking occupancy in Minturn to provide clarity and recommended parking management strategies based on this data.

Central Concepts

PARKING AND CURBSIDE MANAGEMENT

Parking is a limited resource in Minturn and will affect future development opportunities; therefore management of parking and other curbside demands is critical. The community considered parking a key issue during public engagement, with some residents and stakeholders concerned about insufficient parking supply. As part of this plan, transportation planning firm Fehr & Peers collected parking occupancy data in winter 2021/2022 and summer 2022:

Table 1: Time Periods of Parking Data Collection

	WINTER 2021/2022		SUMMER 2022	
	Dates	Times	Dates	Times
<b>Weekday</b>	Tuesday December 7, 2021 Thursday April 14, 2022	9:00 AM, 12:30 PM, 4:30 PM, 6:00 PM	Friday July 29, 2022	12:30 PM, 6:00 PM
<b>Weekend</b>	Saturday January 15, 2022 Saturday April 16, 2022	11:00 AM, 4:30 PM, 6:00 PM	Saturday July 30, 2022	11:00 AM

The days and times in **Table 1** were selected because they were identified by the project team as being the times of highest parking demand. Peak parking demand on summer weekends was determined by collecting counts during the Minturn Market on July 30th. This parking study focused on public parking areas and excluded privately owned areas within the downtown. The analysis of parking utilization indicated that overall, Minturn has ample parking to meet the needs of local businesses and residents but that certain locations have higher utilization and need increased management.

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Average parking occupancy in the study area in the winter is 23% on weekdays and 27% on weekends, well below 85%, which is a typical industry benchmark for parking that is considered at-capacity. As seen in **Figure 1**, on a block-by-block level, some areas around the 100 Block of Main Street have higher utilization than the rest of town, on average between 20% and 50%. The highest occupancy blocks (between 61% and 93% occupancy) are Eagle River Street south of Toledo Avenue, Main Street between Norman and Harrison Avenues, and Main Street between Bayer and Meek Avenues. This is due in large part to property owners not providing on-site parking for long-term residential units in the 100 and 200 blocks.

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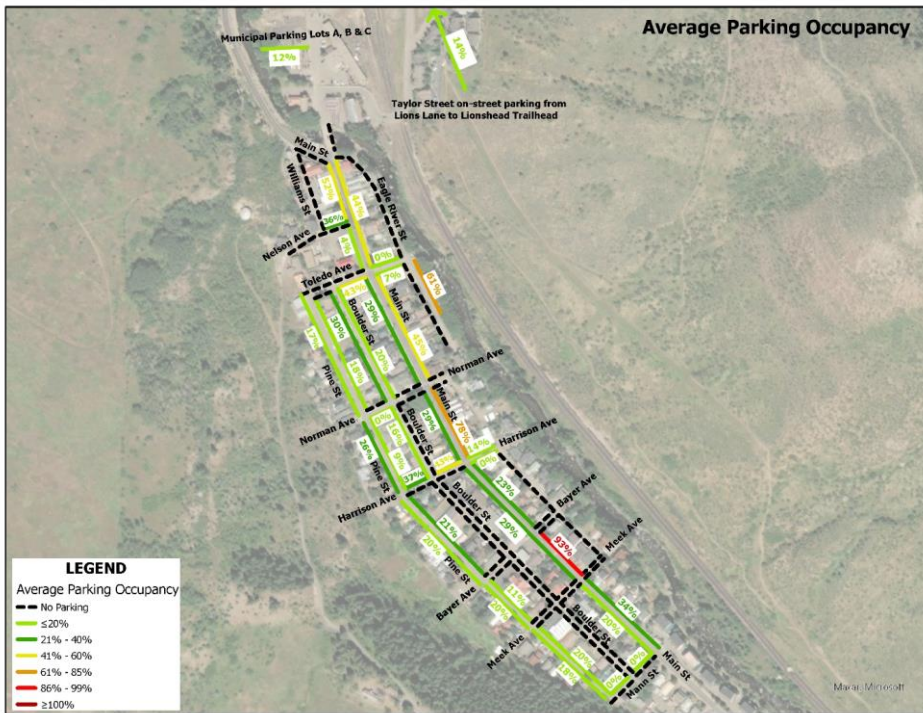


Figure 1: Winter Average Parking Occupancy

The parking study found that winter occupancy was highest in the weekend late afternoon period, when it reached 29% occupancy. As **Figure 2** shows, Eagle River Street south of Toledo Avenue and Main Street between Bayer Avenue and Meek Avenue were both 100% occupied at this time. Other blocks on Main Street were between 61% and 80% occupied. While these blocks experienced higher demand, the Municipal Parking Lot on the north side of town

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remained less than 50% occupied. This indicates that there is parking available within a short distance of blocks that had high utilization.



Figure 2: Winter Weekend Late Afternoon Parking Occupancy

While occupancy was slightly higher, summer parking counts yielded similar results. Average weekday summer parking occupancy in the study area is 38%. As shown in **Figure 3**, on a block-by-block level, parking occupancy exceeds the 85% occupancy threshold along the 100 Block of Main Street and on Main Street between Bayer Avenue and Meek Avenue. During most periods of the summer, parking demand can be accommodated within the Municipal Parking Lot and Taylor Street on-street parking spaces that remain fairly unoccupied.

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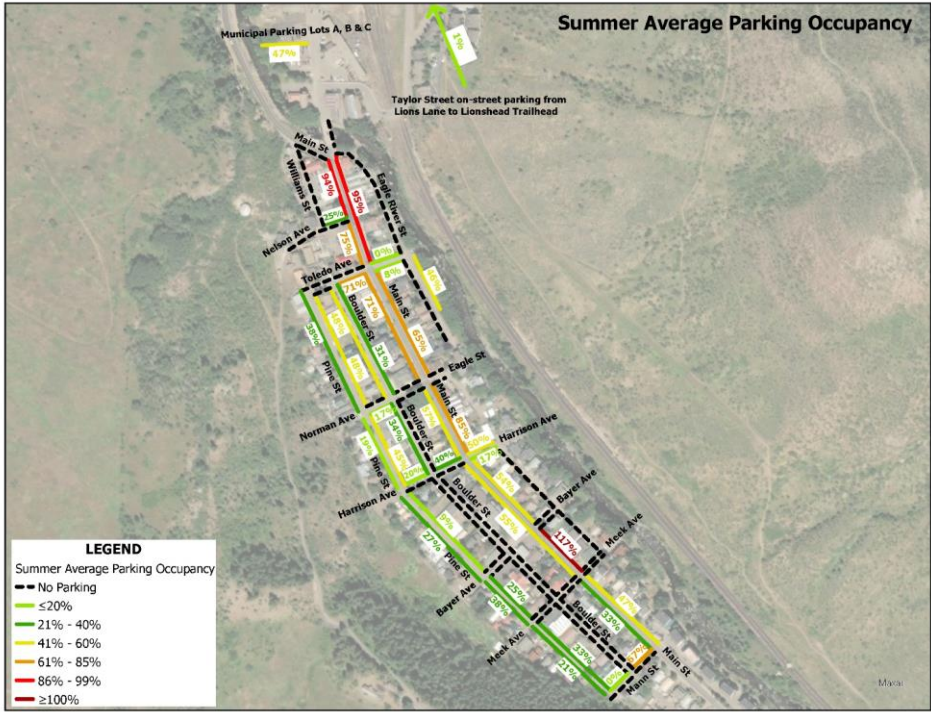


Figure 3: Summer Average Parking Occupancy

Most of the average summer occupancy around the 100 Block can be attributed to people attending the Minturn Market that sits at 100 Main Street on the weekend. During this peak time period, parking approaches or exceeds 85% occupancy on the 100 and 200 blocks and the Municipal Parking Lot (Figure 4). However, the Taylor Street on-street parking spaces remain largely unoccupied, and there is a high amount of available parking within one or two blocks. Better signage and wayfinding can help address parking concerns by directing users to available parking.

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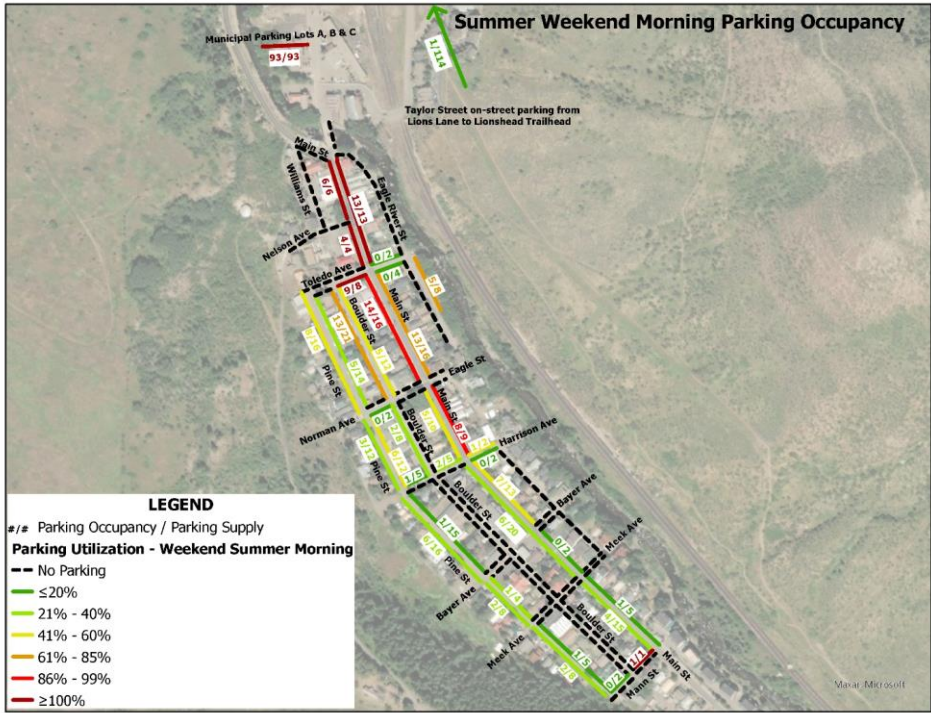


Figure 4: Summer Weekend Morning Parking Occupancy

These findings suggest that no immediate changes in parking policies or parking supply are required to address parking occupancy concerns. There is adequate parking available to meet the current parking needs in the study area. In situations where parking may not be available immediately adjacent to a desired destination, it is likely available within one to two blocks.

Though this analysis concludes that Minturn does not currently have a parking supply problem, this long-range planning effort suggests future parking strategies for the Town to consider as needed. Readers can find more information on these strategies in the full Parking Study ([Appendix X](#)).

The parking study found that parking and loading are not clear and intuitive to users, based on field observations, citizen and business complaints, and staff knowledge. Drivers searching for parking often cannot determine if parking is public, private, or allowed based on signage and striping. Delivery vehicles often load or unload goods in the travel lane, parking lane, or sidewalk, which can be unsafe and inefficient. In the near-term, the Town should consider updating curbside signage to clarify messaging of parking regulations.

Other near-term recommendations for the next five years to improve parking management pertain to density and diversity of land uses, urban design and wayfinding, curbside management, snow maintenance, traffic circulation, shared parking, changes to parking requirements, time restrictions, and increased enforcement. Some of these strategies are parking-adjacent policies that do not directly change parking supply but would influence parking demand.

For example, increasing the mix of land uses within close proximity to the 100 Block can create a more walkable core and allow people to “park once” and explore on foot. Wayfinding signage, discussed later in this chapter, can direct drivers quickly and efficiently to available parking spaces. The Town should ensure the existing parking and snow removal policy retains on-street parking and ADA-accessible parking. The 100 Block circulation concept, also detailed later in the chapter, can improve intuitive and efficient access to parking by reducing cruising for parking.

Curbside management strategies like formalizing locations for passenger and truck loading can reduce instances of loading taking place in on-street parking spaces, the sidewalk, or travel lane. The Town should consider eliminating loading berth requirements for redeveloped properties with a Main Street frontage and identify dedicated locations for loading.

The *Minturn 100 Block Transportation Study* by Stolfus & Associates suggested three possible loading locations which could be further explored – 1) on Main Street, north of Nelson Avenue; 2) on Main Street, south of Nelson Avenue; and 3) on Williams Street, in the public right-of-way. The Williams Street location will be less suitable given this plan’s recommendation to reimagine the area as a one-way shared street, in which pedestrians have the right-of-way. The Town also has an existing 15-minute pick-up and drop-off zone on the northwest side of the 100 Block, adjacent to the Minturn Country Club. This zone should be extended and converted to a 24-hour flexible passenger and goods loading zone.

Minturn currently allows shared parking only between buildings within 100 feet of each other whose operating hours do not overlap. The parking study recommends expanding this shared parking shed to 1,000 feet and allowing shared parking between uses with complementary demand rather than implementing restrictions based on operating hours.

Changing parking requirements to require land uses to adhere to parking maximums rather than minimums will promote a better alignment of parking supply and demand. Parking maximums outline the maximum parking that a developer may build, rather than stipulating the minimum number of spaces required. This change will help limit the number of parking lots that are larger than necessary. The Town ~~should~~ **has** also considered ~~(but rejected)~~ instituting a fee-in-lieu, which allows landowners and developers to pay a fee into a municipal fund instead of providing on-site parking spaces required per the zoning code. **Importantly, this plan does not recommend changing residential parking requirements. See Appendix X: Parking Study for additional discussion of residential and commercial parking in Minturn.**

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As seen in ~~Table 2~~ **Table 2**, similar communities to Minturn including Buena Vista, CO; Jackson, WY; Crested Butte, CO; Breckenridge, CO; Eagle, CO; and Dillon, CO have instituted fee-in-lieu policies. These communities charge a range of one-time rates per required single parking space, which vary based on characteristics of the downtown business core and anticipated revenues to offset costs.

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**Table 2: Peer Community One-Time Parking Fee-in-Lieu Rates for Single Parking Spaces**

COMMUNITY	FEE-IN-LIEU
Buena Vista, CO	\$1,200
Jackson, WY	\$8,500
Crested Butte, CO	\$13,000
Breckenridge, CO	\$19,000
Eagle, CO	\$23,000
Dillon, CO	Space 1-3, \$2,000 Space 4-6 \$5,000

Finally, Minturn can consider expanding the number of blocks with a 2-hour parking time restriction to increase parking turnover in front of businesses. This should be supplemented with increased enforcement of parking restrictions and increasing fines for violations. Minturn should continue to monitor parking occupancy and implement this recommendation when observed parking occupancy is nearing 85%.

In the long-term over the next 8 to 10 years, Minturn can implement additional parking management strategies including improved transit access, paid parking, regular data collection, and evaluation of increasing parking supply. As discussed in the Transit section of this chapter, the Town can work with ECO Transit to develop a shuttle to downtown from Dowd Junction, improve the frequency of existing bus routes, and expand first-and-last mile connections to bus stops.

Eventually, the Town may wish to explore paid parking as a tool to manage demand and encourage the use of alternate transportation modes. Other Colorado towns including Idaho Springs and Manitou Springs have begun charging for parking to manage demand.

Collecting regular data can help the Town determine how these near- and long-term strategies affect parking occupancy over time and demonstrate success to the community. If all of the previously suggested policies fail to effectively manage parking supply or parking demand increases significantly over time, the Town may explore opportunities to increase the parking supply by constructing additional parking. This should be evaluated cautiously, as new parking structures are expensive. Adding supply will also make it more convenient to drive and park in Minturn, which could contribute to additional traffic congestion.

#### TRANSIT

Minturn residents are served by three bus routes (Minturn Route, Leadville Route, and Highway 6 Route) provided by ECO Transit, the regional transit provider. The Minturn Route and the Leadville Route operate north-south through town and serve stops in Minturn’s core.

The Minturn Route operates between Maloit Park in south Minturn and the Vail Transportation Center. During the summer, there are two morning buses from Minturn and two evening buses from Vail. In the winter, there are five morning buses and five evening buses.

The Leadville Route operates between the Leadville Bus Barn and the Vail Transportation Center, with stops in Minturn. During the summer, there is one morning bus from Leadville and one evening bus from Vail. During the winter, there are two buses in the morning and evening. The Highway 6 Route provides more frequent 30-minute service between Edwards and Vail but stops at Dowd Junction on the north side of town and does not route through downtown Minturn.

While the Town of Minturn has limited ability to modify ECO Transit service on its own, there is a concurrent effort to this plan to form a new Regional Transportation Authority (RTA). This is a coordinated effort among the Eagle County business community, local governments, and nonprofit organizations. The proposed Eagle Valley Transportation Authority (EVTA) would enhance and expand ECO Transit service and multimodal infrastructure by forming an intergovernmental agreement and special taxing district. The formation committee have discussed funding opportunities that include a 0.5% sales and use tax, 0.5% transportation sales tax, member contributions, and federal and state grants.

This new EVTA would operate the same ECO Transit routes but would improve service span and frequency on many existing routes and could create new routes. The RTA Formation Committee is evaluating the possibility of developing a fare-free zone that would allow users in some or all of the service area to ride free of cost. This zone will likely include Minturn. The committee is also considering increasing the service span for the Minturn Route and improving service frequency on the Leadville and Highway 6 Routes.

The formation of an EVTA could also include upgrades to transit facilities and infrastructure such as bus stops and first-and-last mile improvements including park and rides, enhanced pedestrian crossings, microtransit, community vanpool, e-bike share, and trail planning, construction, and maintenance. Ultimately, the EVTA could help more riders reach transit and better connect Minturn to the Eagle Valley region.

The Town of Minturn will play a role in the formation of the EVTA and can improve the transit experience for riders through additional strategies at the local level. First, coordinating land use planning with the transportation system by locating new development near bus stops allows more residents and visitors to choose transit as a convenient option. It also increases the efficiency of transit service by improving ridership and making more frequent service cost-effective.

Expansion in transit service should be coordinated with the Town's plans for a mixed-use development at Dowd Junction and expansion of the Forest Service Park and Ride into a new transit center and mobility hub. Making Dowd Junction a transit hub will connect more people in Minturn to the Highway 6 Route, as discussed in detail in the section on Dowd Junction.

At the Dowd Junction transit hub and other bus stops in Minturn, the Town can enhance first-and-last mile connections that improve access to transit. Infrastructure such as wayfinding signage to key destinations, enhanced crossings of US-24, complete sidewalks, trails, bike racks, lighting, landscaping, and other amenities make it easier and more convenient for users to access transit service. At bus stops that lack seating and shelter, Minturn can work with local businesses and the community to install and maintain these amenities, which improve comfort for those waiting for the bus.

#### BICYCLE AND PEDESTRIAN NETWORK

Sidewalks are incomplete in downtown Minturn and missing between neighborhoods on the outskirts of town and downtown. There are no on-street bicycle facilities; bike facilities are limited to soft surface hiking and biking trails around Minturn. The bicycle and pedestrian projects proposed as a part of this plan, shown in **Figure 5** will create a more connected and comfortable environment for Minturn travelers who choose to walk, roll, and bicycle.

Sidewalks on Main Street have been completed through the south end of town to the bus stop at US-24/996 Main as part of CDOT's US-24 Improvements project. The Town has been awarded a \$1.4 million grant to complete Phase 2 of this project, which will extend the sidewalks on both sides of US-24 from where they end at the U-24/996 Main bus stop to the Boneyard

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Open Space. Phase 3 of the project will complete the pedestrian connection between Boneyard Open Space and Maloit Park Road. Completing Phases 2 and 3 of this project should be a near-term priority project for the Town, as the infrastructure will provide a continuous facility for people walking and biking through the core of Minturn and further south along US-24.

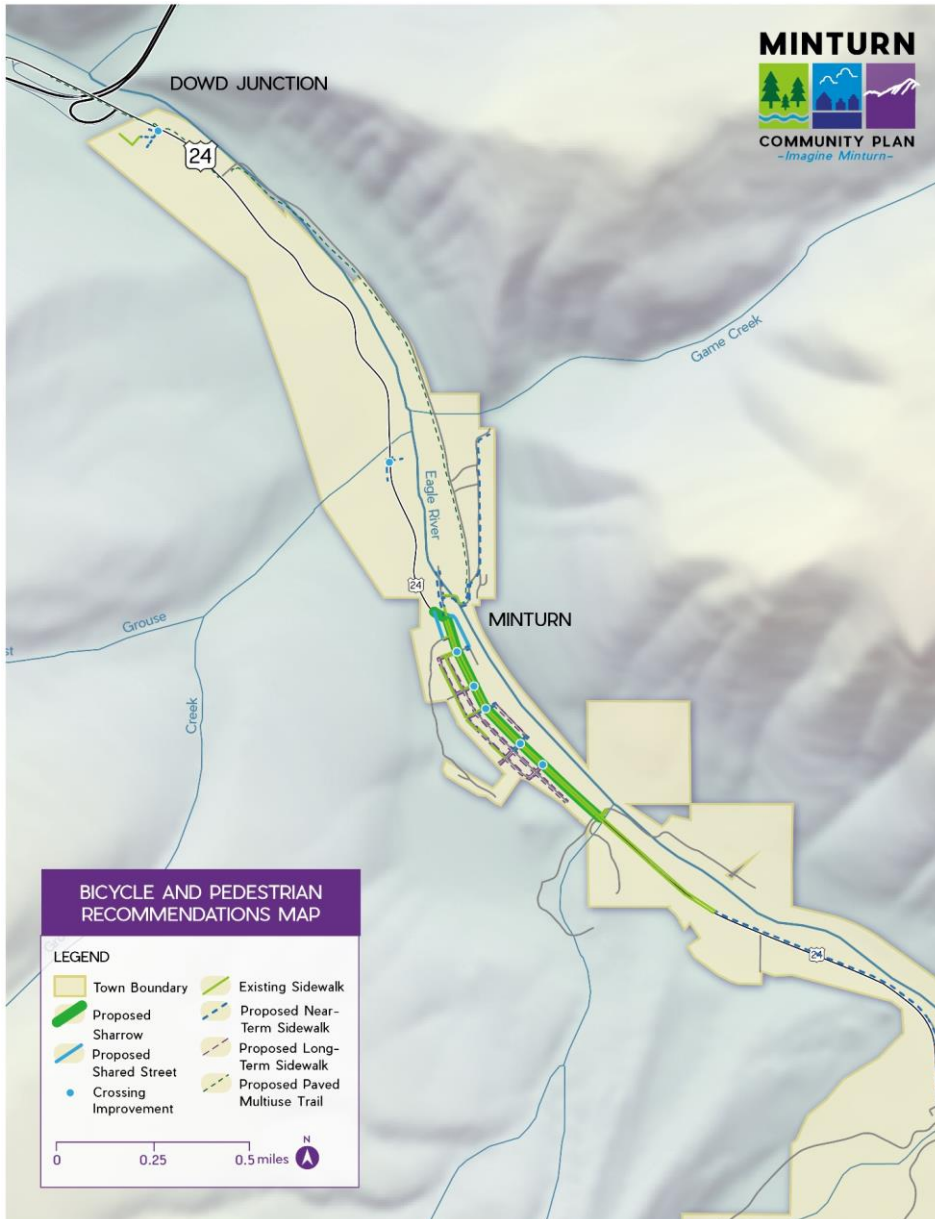


Figure 5: Bicycle and Pedestrian Recommendations

Other near-term priority sidewalk projects should include those that provide missing connections to downtown so that Minturn travelers can comfortably walk to key destinations including bus stops, as shown in **Table 3Table 3**. Projects that complete gaps in the sidewalk network downtown and that are within close proximity of a key destination should also be given additional focus. The sidewalk projects that most satisfy these goals include the following:

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- **Forest Service Park and Ride and Dowd Junction:** completion of sidewalks at this transit hub of Minturn will enhance first-and-last mile connections and comfort for those walking to the bus
- **Harrison Avenue between Boulder Street and Eagle River Street:** completion of sidewalk gap at a key bus stop downtown
- **Main Street by the Municipal Parking Lot:** completion of sidewalk gap between the Municipal Parking Lot and downtown
- **Meek Avenue between Boulder Street and River Street:** completion of sidewalk gap at a bus stop downtown
- **Toledo Avenue between Main Street and Eagle River Street (both sides):** completion of sidewalk gap at a bus stop downtown
- **US-24 and Meadow Mountain Business Park:** completion of sidewalk gaps between key bus stops, commercial area, and Grouse Creek Trailhead
- **Railroad Avenue:** completion of sidewalk gap in area with safety concerns including limited line of sight, poor nighttime lighting, and speeding vehicles
- **Taylor Avenue:** completion of sidewalk gap between Minturn neighborhoods and the town core, improved access between area with on-street parking and downtown

In the longer term, the Town can continue to complete the sidewalk network downtown and fill in gaps between neighborhoods with additional projects, as shown in **Table 3Table 3**. Completing sidewalk projects in the downtown street grid will require collaboration between the Town and downtown residents and businesses and a shared commitment to creating a safe, comfortable pedestrian environment.

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Community feedback indicates broad support for the Town's efforts to improve sidewalks and crosswalks. Dedicating space in the public right-of-way to develop new sidewalks along residential frontages may require tradeoffs with on-street parking. Other long-term sidewalk projects not shown in **Table 3Table 3** will include the completion or upgrade of sidewalks with new development or redevelopment, which the Town should require as a part of the Municipal Code.

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**Table 3: Sidewalk Projects Prioritized**

PRIORITY	LOCATION	MISSING CONNECTION TO DOWNTOWN (WEIGHTED DOUBLE)	WITHIN A BLOCK OF BUS STOP (WEIGHTED DOUBLE)	MISSING CONNECTION WITHIN DOWNTOWN	WITHIN BLOCK OF KEY DESTINATION	MILES
Near-Term	US-24 sidewalks phase II	x	x			0.29
Near-Term	US-24 sidewalks phase III	x	x			0.58
Near-Term	Forest Service Park and Ride		x		x	0.09
Near-Term	Harrison Ave between Boulder St and Eagle River St		x	x		0.06
Near-Term	Main St by Municipal Parking Lot	x			x	1.00
Near-Term	Meek Ave between Boulder St and Eagle River St		x	x		0.08
Near-Term	Toledo Ave between Main St and Eagle River St		x	x		0.06
Near-Term	US-24 and Meadow Mountain Business Park		x		x	0.09
Near-Term	Railroad Ave	x				0.19
Near-Term	Taylor Ave	x				0.40
Long-Term	Boulder St			x	x	0.82
Long-Term	Norman Ave			x	x	0.06
Long-Term	Pine St			x	x	0.51
Long-Term	Harrison Ave west of Boulder St			x		0.02
Long-Term	Mann Ave			x		0.07
Long-Term	Meek Ave west of Boulder St			x		0.08
Long-Term	Nelson Ave west of Boulder St			x		0.02
Long-Term	Eagle River St			x		0.23
Long-Term	Toledo Ave west of Boulder St			x		0.01

In tandem with sidewalk projects, the Town should incorporate crossing improvements that also reduce the speeds of traffic entering and traveling through Minturn. During community engagement, residents expressed frustration with the prevalence of drivers on US-24 speeding through the Town core. The addition of curb extensions at intersections along Main Street (noted in [Table 4](#)) can reduce crossing distances, provide a comfortable place for people walking and shopping to linger while waiting to cross the street, and narrow the roadway at these locations, which has the potential to lower speeds of people driving.

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Figure 6: Example of a Curb Extension – Portland, OR (Source: GreenWorks)

The Main Street intersections at Harrison Avenue and Toledo Avenue are adjacent to bus stops in the Minturn core. The *Eagle County First/Last Mile Strategy Study* identified Harrison Avenue as a high-priority bus stop for bicycle and pedestrian infrastructure improvements based on the nearby population density, income, connectivity within the transportation network, and public input. Marked crosswalks exist on the south side of Harrison Avenue and the north side of Toledo Avenue. Completing crosswalks on the remaining legs of these intersections will improve comfort for pedestrians and highlight these key pedestrian crossings to people driving along Main Street.

Outside of the Minturn core, two other intersections along US-24 will benefit from crossing improvements—at the Forest Service Park and Ride and adjacent to the Meadow Mountain Business Park.

Pairing the addition of sidewalks along US-24 adjacent to the Meadow Mountain Business Park with a formalized curb at the entrance to the commercial area and bus stop improvements would further signal the presence of pedestrians to people driving.

Also, as the Town considers new mixed-use development and a future mobility hub at the Forest Service Park and Ride (Dowd Junction), as well as a new multiuse trail on the east side of US-24, this will become a future crossing location. The crossing, just south of Dowd Junction, will require more significant crossing treatments, given the high speed of vehicles entering and exiting Highway 6 and I-70.

**Table 4: Crossing Improvements Prioritized**

PRIORITY	LOCATION	DESCRIPTION
Near-Term	US-24 and Meadow Mountain Business Park	Formalize curb with bus stops
Near-Term	Main St and Harrison Ave	Complete crosswalk on north/east/west side and add curb extensions to calm traffic entering town
Near-Term	Main St and Toledo Ave	Complete crosswalk on south/east/west side and add curb extensions to calm traffic through town
Near-Term	Main St and Norman Ave	Add curb extensions to calm traffic through town
Near-Term	Main St and Meek Ave	Add curb extensions to calm traffic entering town
Long-Term	US-24 and Forest Service Park and Ride	Future crossing location, treatment determined by PUD
Long-Term	Main St and Mann Ave	Add curb extensions to calm traffic entering town

Minturn currently lacks designated on-street bicycle facilities. During community engagement, residents expressed support for additional bike facilities, especially the planned multiuse trail to Dowd Junction and a connection to Maloit Park at the south end of town. The US-24 sidewalks project will better connect downtown to the fitness center and Vail Ski and Snowboard Academy in Maloit Park.

In the near-term, the planned paved multiuse ECO Trail that will connect Dowd Junction to the north end of Minturn at the intersection of Minturn Road and Taylor Street is an important connection that closes a key gap in the active transportation network. People traveling to the Forest Service Park and Ride to board the Highway 6 Route currently must drive or bike along US-24. The shoulder along US-24 is not comfortable for people biking and does not support riders of all ages and abilities. The addition of the north-south trail connecting the north end of town with the core would improve first-and-last mile connections for transit riders and offer a new recreational amenity for Minturn residents and visitors.

This trail would also close a key gap in the Eagle Valley Trail, which will connect the Vail Pass and Glenwood Canyon trails. Once completed, this system will allow someone to ride a bike over 140 miles from Breckenridge to Aspen. Missing trail segments total 12 miles, with the 1.3 mile connection in Minturn, estimated to cost \$1.5 million, according to ECO Trails. Funds will come from local government agencies, private foundation grants, state and federal programs, individual donors, and a fundraising campaign.

The 2019 *ECO Transit First/Last Mile Strategy Study* explored on-street bicycle facilities on US-24 through Minturn. That study determined that an on-street bicycle lane on US-24/Main Street, which is managed by the Colorado Department of Transportation (CDOT) would not be feasible, but that “sharrow” lane markings and additional bicycle route signage should be added. Sharrow are shared lane markings that demarcate bike routes and alert drivers to people bicycling. The Town should partner with CDOT and continue to advocate for adding sharrow on Main Street through town to draw attention to people bicycling.

### DOWD JUNCTION

Dowd Junction, located at the confluence of US-6, I-70, and US-24 at the north side of town, occupies an important location in Minturn. Currently, the site hosts a US Forest Service (USFS) administrative facility, an ECO Transit bus transfer station, the Meadow Mountain Parking Lot, and a boat ramp for Eagle River recreation across US-24. The Town of Minturn envisions that the bus transfer station and Park and Ride that is currently owned by USFS will likely become town property and developed as a mixed-use Planned Unit Development (PUD). This site is the ideal location for a mobility hub that brings together Minturn's transportation alternatives in one place, with parking, transit, and active transportation connections via the proposed multiuse trail. Through this development, Dowd Junction could also become a gateway signaling the entrance to Minturn.

As suggested in the *Eagle County First/Last Mile Strategy Study*, creating mixed-use development at Dowd Junction and expanding the Forest Service Park and Ride into a new transit center and mobility hub could connect more people in Minturn to the Highway 6 Route. Residents of Dowd Junction would gain convenient access to Avon and Vail via transit service along I-70. The incorporation of features such as wayfinding signage to downtown and other key destinations, an enhanced crosswalk across US-24, complete sidewalks, bike racks, priority carpool/vanpool parking, lighting, and other amenities would create a mobility hub at Dowd Junction. These features would make it possible for users south of Dowd Junction to more easily access frequent east-west transit service by walking, biking, and carpool.

In the future, the Town should explore the potential for the Meadow Mountain Parking Lot to serve as overflow parking for visitors during local events, weekends, and other busy days. During these times, digital signage at Dowd Junction could alert driving visitors that they may park in the lot. The signage could convey in real-time if the Municipal Parking Lot is full. The overflow parking at Dowd Junction should be paired with a microtransit shuttle that connects visitors between Dowd Junction and the Minturn core. The Town could develop such a shuttle in tandem with ECO Transit/the EVTA or independently.

### 100 BLOCK CIRCULATION CONCEPT

As the heart of Town, the 100 Block sees high daily volumes of vehicles and pedestrians. The area also has several circulation challenges; for instance, drivers entering Minturn headed south who miss the left turn toward the Municipal Parking Lot have limited options for turning around. Williams Street and Eagle River Street are both southbound one-ways. Drivers headed south on Main Street attempt to turn right onto Nelson Avenue to make another right on Williams Street to head north back to the Municipal Parking Lot, discover it's a one way, and "get stuck" turning around on Nelson Avenue. The street points up a steep hill and there is little room to perform a U-turn. Drivers who turn left on Toledo Avenue must travel south on Eagle River Street and turn around at the dead end.

Converting Eagle River Street from one-way southbound to one-way northbound would address this issue by allowing drivers to loop north to reach the Municipal Parking Lot. This conversion would also reduce confusion for Minturn visitors. This analysis also considered the possibility of converting the direction of Williams Street. Due to sight distance issues resulting from northbound drivers turning left onto Main Street from Williams Street, this option is recommended only if left turns are prohibited. In the future, the town might consider creating a one-way loop around the lot south of Nelson Avenue, which could eventually serve as a turnaround for cars and trucks.

This plan also envisions that both Eagle River Street and Williams Street will become "shared streets" that give walking and rolling right-of-way, while people driving and bicycling must yield.

Generally, these streets lack curbs to eliminate the distinction between the pedestrian and car realm, signaling to drivers that the area is a shared space. Street features calm traffic so that vehicles operate at speeds low enough to prioritize pedestrian safety.

Given the narrow 15' wide right-of-way on these streets, they are natural candidates for this configuration. The narrow space does not allow enough room for a travel lane, parking lane, and a sidewalk. Alternating parallel parking, staggered on either side of the street, could effectively create chicanes (or offset curb extensions that narrow the street) that slow people driving. The street enclosure, alongside other features such as planters/landscaping, street furniture, and tactile paving, will reduce vehicle speeds and allow people walking, biking, and driving to share the road more comfortably. The Town should update their snow removal policy to plan for winter maintenance of any shared streets. These streets should be plowed frequently. The Town should consider a snowmelt system for these streets and other priority pedestrian corridors.



Figure 7: Example of a Shared Street – Cady's Alley, Georgetown, Washington D.C. (Source: Martinez and Johnson Architecture)

Lastly, the Town should work with Union Pacific Railroad to improve parking in the Municipal Parking Lot, in both the short and long-term. In the near-term, the Town should consider reconfiguring the Municipal Parking Lot to allow for better circulation of vehicles, more parking spaces, and improved safety through additional lighting. In advance of planning and implementing these recommendations, an assessment of the level of investment considering the Town's short-term lease of this lot should be considered. In the near-term, the Town should also continue working with the railroad for the Town of Minturn purchase this land. If it is determined that Town ownership of this land is not feasible, a longer-term lease from the railroad should be

considered and a parking program to fund acquisition and improvements should be initiated. A potential subarea master plan in this area is discussed in the Economy chapter.

#### SIGNAGE AND WAYFINDING

Signage is a practical component of a community's transportation system, directing users to key destinations. However, it also offers an opportunity for the Town to create a sense of place and cohesive, artistic system for orienting visitors and bringing people into the Minturn core to explore shops and restaurants. In this way, wayfinding can simultaneously act as an economic development driver and unite transportation and land use.

Signage plays an important role in facilitating multimodal circulation. While missing or confusing signage may irritate visitors and cause unnecessary circling in the car, clear and simple signage placed correctly enables people to explore and enjoy a downtown area on foot or by bicycle. Additionally, standardized features like light bollards, consistent pedestrian-scale lamp posts, and signs can signal that one has entered the Town. Signage and wayfinding in Minturn should communicate four main messages: 1) when you're entering town; 2) where you're going; 3) where you're allowed to park; and 4) where you can walk, bike, and take transit.

The first type of wayfinding is gateway signage. This type of sign (as shown in **Figure 8**) signals to visitors and residents that they are entering town and highlights the town core. Minturn currently has welcome signs at the north end of town near the Meadow Mountain Business Park and at the south end of town near Notch Mountain Road. Additional gateway signage could be placed entering the historic area of Minturn on Main Street just north of Williams Street and just south of Meek Avenue. With the development of Dowd Junction, this type of signage could also be appropriate north of the mobility hub.

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Figure 8: Example of Gateway Signage in New Haven, CT (Source: Wikimedia Commons)

The second type of signage indicates where to find key destinations, such as public parking lots, downtown Minturn shopping and dining, the Minturn Town Hall and post office, trailheads, and more. Wayfinding signage should be placed throughout the town core between Williams Street and Mann Street, but specifically adjacent to bus stops, parking lots, the Town Hall/post office, trailheads, and hotels, as shown in **Figure 9**. All signs not immediately next to a bus stop should point toward the nearest stop. This will remind residents and visitors of the local transit system and promote ease of use.

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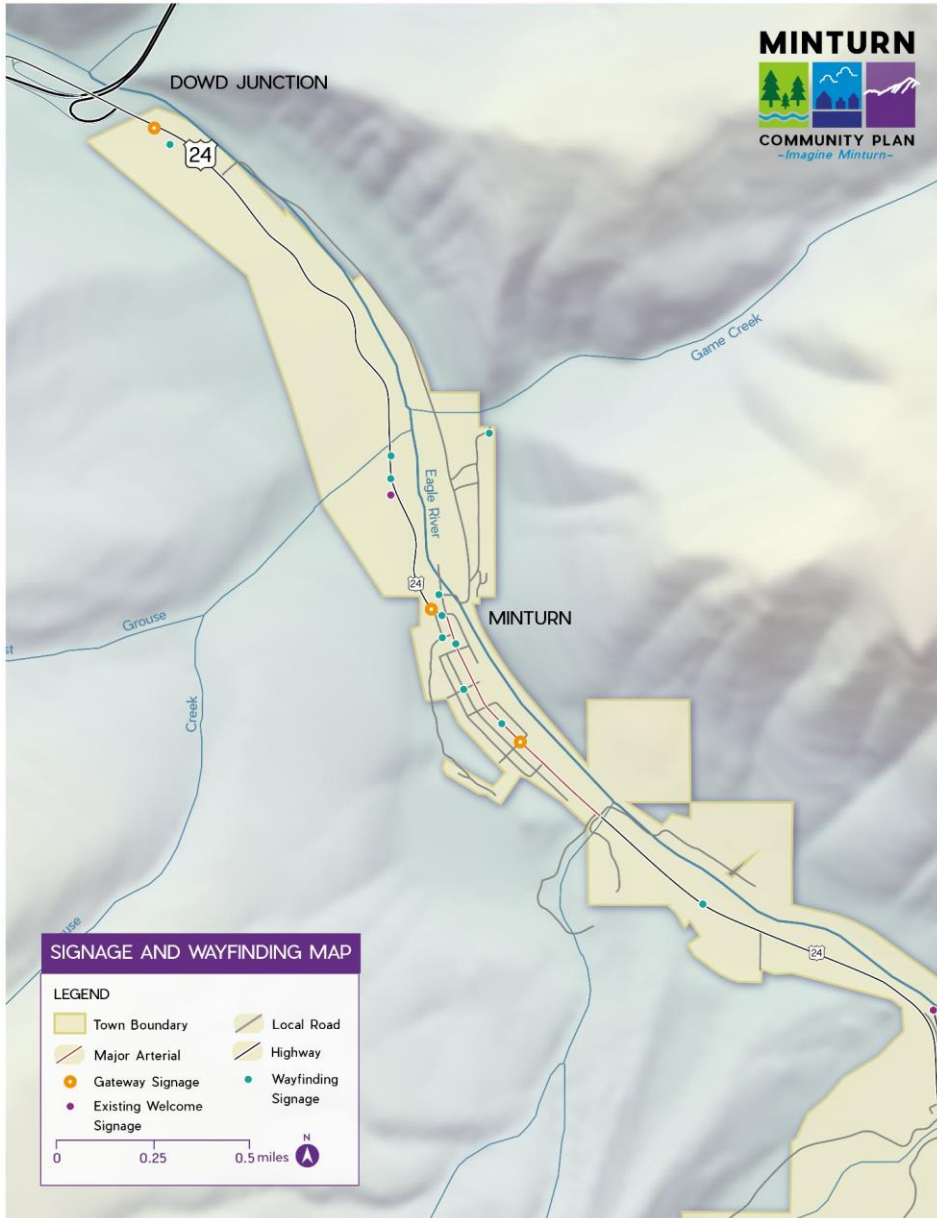


Figure 9: Suggested Wayfinding Locations in Minturn

Thoughtful design and placement of this signage can help visitors and residents orient themselves downtown and easily locate key destinations. **Figure 10** shows how simple this kind of signage can be, while remaining aesthetically pleasing. The pedestrian scale of this

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signage caters to people walking downtown, but it can also be read by those on a bike or in a car. This signage could also include estimated walking time to each destination listed to further highlight ease of pedestrian access.



Figure 10: Example of Wayfinding Signage in South Bend, ID (Source: Corbin Design)

While wayfinding signage will direct visitors to public parking lots, the third type of signage, parking signage, will communicate on-street and lot parking regulations to the public. This type of signage already exists in Minturn but should be updated with any change in parking regulations described previously.

Finally, signage of walking and biking routes can communicate safe places to choose active transportation and recreate. This can include signage on the US-24 corridor, shared streets, and pedestrian passages within the 100 Block.

As suggested in the *Eagle County First/Last Mile Strategy Study*, this plan recommends placing Share the Road signs throughout the US-24 corridor between I-70 and the Minturn core and studying a reduction in the speed limit from 40 mph to 30 mph, alongside the installation of sharrow markings.

The entrances to shared streets on Williams Street and Eagle River Street should also be clearly signed to inform incoming drivers of the street configuration and speed change, similar to the signage shown in [Figure 11](#).

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Figure 11: Example of Shared Street Signage (Source: Planetizen)

Minturn can also use signage to highlight new and future pedestrian passages in the 100 Block that connect the downtown to nearby scenic and recreational amenities like the Eagle River and White River National Forest. As seen in [Figure 12](#), the same kind of signage can concisely show multiple walking routes.



Figure 12: Example of Walking Path Signage (Source: Madison Schmidt, Cincinnati People)

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#### MULTI-MODAL INFRASTRUCTURE

The introduction of multi-modal infrastructure such as electric bike share, bike racks, bike wash/repair stations, and bus shelters and seating would reinforce the availability of transportation alternatives in Minturn.

Neighboring communities in Eagle County including Avon, EagleVail, and Vail launched an e-bike share program called Shift Bike in June 2022. This program offers seasonal, monthly, and pay-as-you-go rentals and maintains 90 bikes and 20 stations. As the ECO Trail connection from Dowd Junction to Minturn is constructed, the town should partner with these neighboring communities to explore expanding the Shift Bike program to Minturn.

The town should collaborate with ECO Transit/the EVTA and commercial businesses to install bike racks near bus stops, the Town Hall/post office, trailheads, and hotels. Installing at least one bike rack on each block in the Minturn core that hosts commercial land uses will support people that choose to bike downtown by providing a safe place to store and lock their bikes. Additionally, during community engagement, the public indicated a desire for a bike wash/repair station in the 100 Block. This should be installed at a central, convenient location.

Currently, the only bus stops in Minturn with seating and shelter are the Forest Service Park and Ride stop and the US-24/472 Main stop. Other stops are only marked with a signpost, and some lack ADA landing pads. Minturn should work with ECO Transit and the eventual EVTA to formalize bus stops, making them ADA accessible and installing seating and shelter.

#### Recommendations

##### OBJECTIVE 6.1: DEVELOP SHARED PARKING AND CURBSIDE MANAGEMENT STRATEGIES IN THE 100 BLOCK AREA BASED ON PARKING MANAGEMENT STUDY.

- **Action 6.1.1:** In the near-term, consider updating curbside signage to clarify messaging of parking regulations.
- **Action 6.1.2:** Increase the mix of land uses within close proximity of the 100 Block to create a more walkable core and allow people to “park once” and explore on foot.
- **Action 6.1.3:** Ensure the existing parking and snow removal policy retains on-street parking and ADA-accessible parking. Update this policy to plan for winter maintenance of shared streets.
- **Action 6.1.4:** Formalize locations for passenger and truck loading downtown.
- **Action 6.1.5:** Expand the shared parking shed from 100 ft to 1,000 ft and consider allowing uses with complementary demand to share parking rather than restricting based on operating hours.
- **Action 6.1.6:** Consider changing parking requirements to require land uses to adhere to parking maximums rather than minimums and instituting a fee-in-lieu policy.
- **Action 6.1.7:** Following an increase in commercial uses or parking occupancy on a block, consider expanding the blocks with a 2-hour time restriction to increase parking turnover in front of businesses
- **Action 6.1.8:** Explore paid parking as a tool to manage demand, encourage the use of alternate transportation modes, and generate revenue.

- **Action 6.1.9:** To increase the effectiveness of parking management strategies and promote compliance, increase enforcement of parking restrictions by increasing fines for violations.
- **Action 6.1.10:** Collect regular parking count data to determine how these near- and long-term strategies affect parking occupancy over time and demonstrate success to the community. Future parking data collection efforts should also focus on early morning and afternoon hours, which were identified by some business owners as the busiest times of day for business and therefore parking.
- **Action 6.1.11:** Work with Union Pacific Railroad to explore opportunities to purchase the land containing the Municipal Parking Lot. If that is not feasible, consider extending the current short-term lease or reconfiguring the Municipal Parking Lot.
- **Action 6.1.12:** If all of the previously suggested policies fail to reduce occupancy to the goal of 85%, explore opportunities to increase the parking supply by constructing additional parking.

**OBJECTIVE 6.2: IDENTIFY MINTURN'S ROLE IN A REGIONAL TRANSIT AUTHORITY AND IN FACILITATING EXPANSIONS TO ECO TRANSIT SERVICE AND/OR TOWN OF VAIL TRANSIT SERVICE. CAPITALIZE ON THE GROWTH OF MASS TRANSIT INFRASTRUCTURE AND RIDERSHIP TO SUPPORT INFILL AND/OR TRANSIT-ORIENTED DEVELOPMENT IN MINTURN.**

- **Action 6.2.1:** Coordinate land use planning with the transportation system by locating new development near bus stops.
- **Action 6.2.2:** Enhance first-and-last mile connections that enable people located outside of walking distance from a bus stop to reach transit by installing features such as printed wayfinding to key destinations, enhanced crossings of US-24, complete sidewalks, trails, bike racks, lighting, landscaping, and other amenities.

**OBJECTIVE 6.3: ENVISION AN INTERCONNECTED NETWORK OF SIDEWALKS AND TRAILS FOR WALKING AND BIKING THAT CONNECTS MINTURN'S NEIGHBORHOODS, COMMERCIAL AREAS, AND RECREATIONAL OPPORTUNITIES.**

- **Action 6.3.1:** Complete near-term priority sidewalk projects that provide missing connections to downtown so that Minturn residents can comfortably walk to key destinations and those within close proximity of a bus stop.
- **Action 6.3.2:** Complete long-term priority projects that fill in the sidewalk network downtown and gaps between neighborhoods.
- **Action 6.3.3:** Update the Municipal Code to require the completion or upgrade of sidewalks with new development or redevelopment.
- **Action 6.3.4:** Install curb extensions at key intersections along Main Street to reduce crossing distances, provide a comfortable place for people walking and shopping to linger while waiting to cross the street, and slow traffic.
- **Action 6.3.5:** Complete crosswalks on the remaining legs of Harrison Avenue and Toledo Avenue to draw attention to crossing pedestrians.
- **Action 6.3.6:** Construct the planned paved multiuse ECO Trail that will connect Dowd Junction to the north end of Minturn at the intersection of Minturn Road and Taylor Street.

- **Action 6.3.7:** Partner with CDOT and advocate for the effort to add sharrows on Main Street through town to draw attention to people bicycling.

**OBJECTIVE 6.4: ADDRESS MULTI-MODAL CONNECTIVITY NEEDS AT DOWD JUNCTION AND ENCOURAGE GREATER CONNECTIVITY TO THE REST OF EAGLE COUNTY.**

- **Action 6.4.1:** Expand the Forest Service Park and Ride into a new transit center and mobility hub to connect more people in Minturn to the ECO Transit Highway 6 Route.
- **Action 6.4.2:** Explore the potential for the Meadow Mountain Parking Lot to serve as overflow parking for visitors during high demand times, paired with digital signage and a shuttle that transports visitors between Dowd Junction and downtown Minturn.

**OBJECTIVE 6.5: ADD SIGNAGE IN KEY LOCATIONS THROUGHOUT TOWN TO CLARIFY PARKING, DESTINATIONS, AND TRANSPORTATION OPTIONS.**

- **Action 6.5.1:** Develop gateway signage on the north and south end of the town core (and eventually at Dowd Junction).
- **Action 6.5.2:** Place wayfinding signage throughout the town core between Williams Street and Mann Street; specifically adjacent to bus stops, parking lots, the town hall/post office, trailheads, and hotels. Design signs not immediately next to a bus stop to point toward the nearest stop to remind residents and visitors of the local transit system and promote ease of use.
- **Action 6.5.3:** Update parking signage with any changes to parking regulations.
- **Action 6.5.4:** Communicate safe places to choose active transportation and recreate by placing Share the Road signage on the US-24 corridor and other wayfinding at the entrances to shared streets and pedestrian passages within the 100 Block.

**OBJECTIVE 6.6: FACILITATE MULTI-MODAL TRIPS BY ADDING INFRASTRUCTURE, SUCH AS BIKE RACKS AND BUS SHELTERS.**

- **Action 6.6.1:** Partner with neighboring communities in Eagle County to expand the Shift Bike program to Minturn.
- **Action 6.6.2:** Collaborate with ECO Transit/the EVTA and commercial businesses to install bike racks near bus stops, the Town Hall/post office, trailheads, and hotels. Place at least one bike rack on each block in the Minturn core that hosts commercial land uses and install a bike wash/repair station at a central, convenient location in the 100 Block.
- **Action 6.6.3:** Work with ECO Transit and the EVTA to formalize bus stops, making them ADA accessible and installing seating and shelter.
- **Action 6.6.4:** Consider participating in a regional or neighboring bike share program, such as the ShiftBike program in Vail, Eagle Vail, and Avon; promote service at Dowd Junction.

**STAFF PROPOSED NEW OBJECTIVE AND ACTION STEPS**

**OBJECTIVE 6.7: MONITOR IMPACTS TO HIGHWAY 24.**

- **Action 6.7.1:** Partner with CDOT to monitor and collect information regarding traffic levels, level of service, and traffic speeds on Highway 24.

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- Action 6.7.2: Require all new major development proposals to include traffic studies with particular focus on potential impacts to Highway 24 resulting from all phases of proposed development.
- Action 6.7.3: Require all new major development proposals to fund off-site traffic improvements, limit density, or both.

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## CHAPTER 7

### QUALITY OF LIFE, RECREATION & AN INVITING PUBLIC REALM: PARKS, TRAILS & COMMUNITY GATHERING SPACES

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#### Introduction

A community with a good quality of life enables residents to live in ways that support their physical, mental, and social wellbeing. Within community planning, “quality of life” refers primarily to spaces and activities that foster public health and vibrancy. Assets that support quality of life, which can include walkable streets, scenic views, parks, and community events, help to build community character and identity. These spaces and events bring people together and provide opportunities for relaxation, rejuvenation, connection to nature, and physical activity.

The purpose of this chapter is to recognize the great amenities, programs, and events that help make Minturn, “Minturn” and to lay out steps the Town can take to preserve and expand upon these opportunities.

#### Existing Conditions & Issues

Minturn already supports a high quality of life, with its myriad of outdoor recreation opportunities, breathtaking vistas, and lively community events.

#### Natural Amenities and Recreation Opportunities<sup>1</sup>

Minturn lies within the striking Eagle River Valley, which is characterized by the steep slopes and impressive cliffs of the White River National Forest and Holy Cross Wilderness that surround the community. The Eagle River itself is a picturesque feature of town, winding past the 100 Block and providing opportunities for fishing, kayaking, rafting, and canoeing. The Town has provided benches, public bathrooms, and a small basketball hoop along the river. There is currently a social soft-surface trail suitable for walking or mountain biking that runs along the east side of the train tracks and river from Lions Lane south to Ballpark Road/Ed6. Another social soft-surface trail connects to this trail, creating a loop farther up the hillside.

Other formal trails and recreation opportunities in the immediate Minturn area include:

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<sup>1</sup> Town of Minturn website, USFS, COTrex, MTB project

EAST SIDE OF THE VALLEY		
NAME	USES & ACTIVITIES	TRAIL CONNECTIONS & ACCESS POINTS
Minturn Mile (winter)/Game Creek Trail (summer)	Backcountry skiing, mountain biking, horseback riding, and hiking	Connects to Vail Mountain trails
Lionshead/Cougar Ridge Trail	Hiking, mountain biking	Connects to additional Vail Mountain trails; accessed via the Game Creek Trail
<a href="#">Eagle River Access Points (3)</a>	<a href="#">Fishing, scenic and wildlife viewing, picnicking</a>	<a href="#">Two are ADA accessible</a>
Little Beach Park	Playground, picnic area, concerts at the amphitheater beside the Eagle River	Connects to Minturn Bike Park and Mini Mile
Minturn Bike Park	Biking skills courses and the Minturn Mini Mile single-track trail (walking, biking)	Connects to Little Beach Park
Boneyard Open Space	Walking, river access	
Two Elk Shooting Area	shooting	Accessed via Ed6 access road (only open to vehicles during the summer & early fall)
Two Elk Trail	hiking, mountain biking, and horseback riding.	This trail connects to the Mill Creek and additional Vail Mountain Trails. Ed6 access road is only open to vehicles during the summer and early fall.

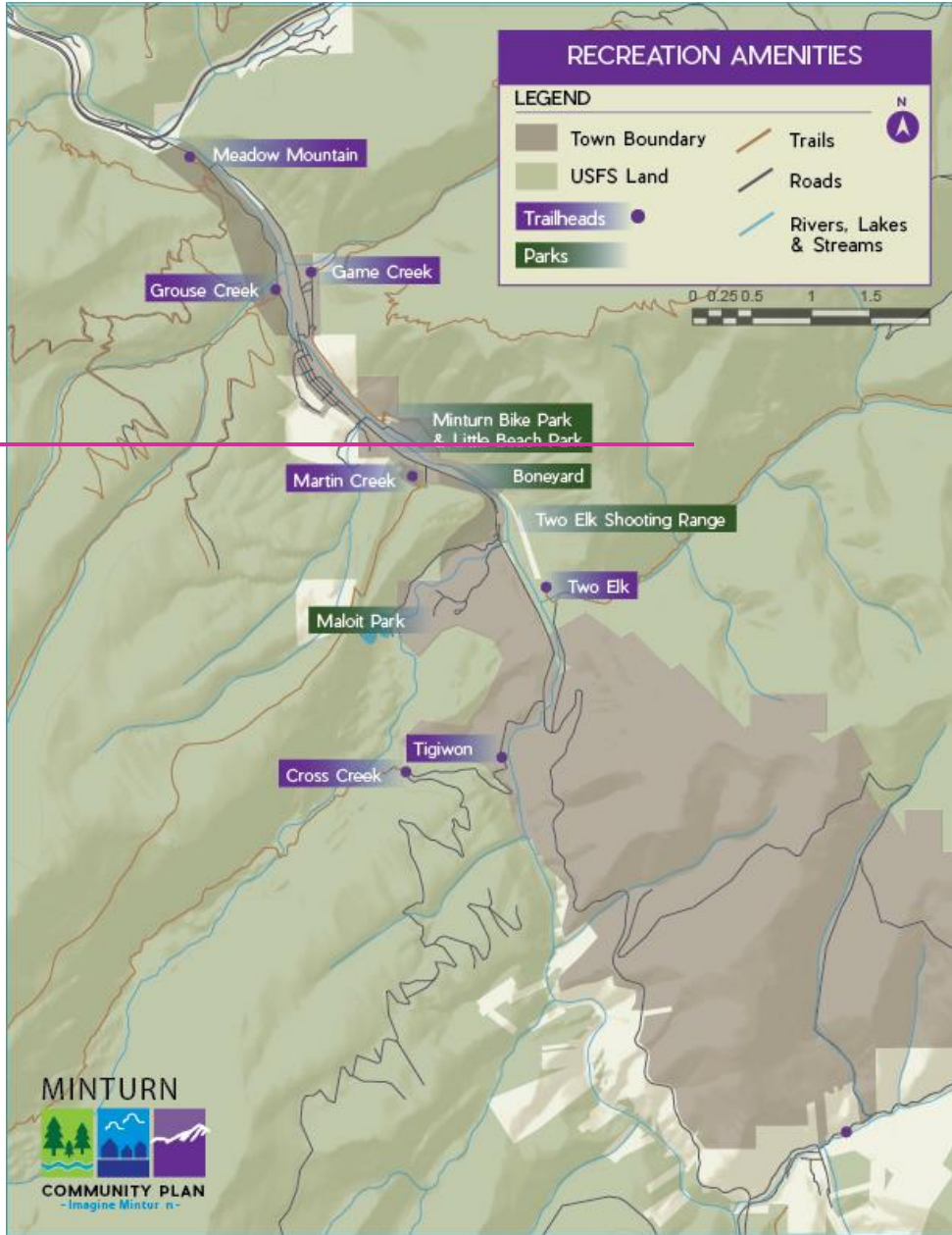
WEST SIDE OF THE VALLEY		
NAME	USES & ACTIVITIES	TRAIL CONNECTIONS & ACCESS POINTS
Meadow Mountain & Everkrisp Trails	Hiking, mountain biking, snowshoeing	Meadow Mountain connects to the Grouse Creek Trail; Everkrisp connects to Whiskey Creek and Paulie's Plunge/Stone Creek trails.
Grouse Creek/Grouse Mountain and Grouse Lake Trails	Hiking, mountain biking, snowshoeing	Connects to the Meadow Mountain Trail and Martin Creek Trail.
Martin Creek Trail	Hiking	



Maloit Park/Vail Ski & Snowboard Academy	Nordic skiing, snowshoeing, walking, and open field for sports or picnicking. Includes an FIS Nordic ski course (only 1 of 2 in North America).	
Cross Creek, Fall Creek, Notch Mountain, and Mountain of the Holy Cross Trails; Half Moon Campground	Hiking, backcountry skiing, tent camping	Notch Mountain Road is groomed in the winter and is closed to motor vehicles through the end of June. The lower Tigiwon Trailhead offers winter access.

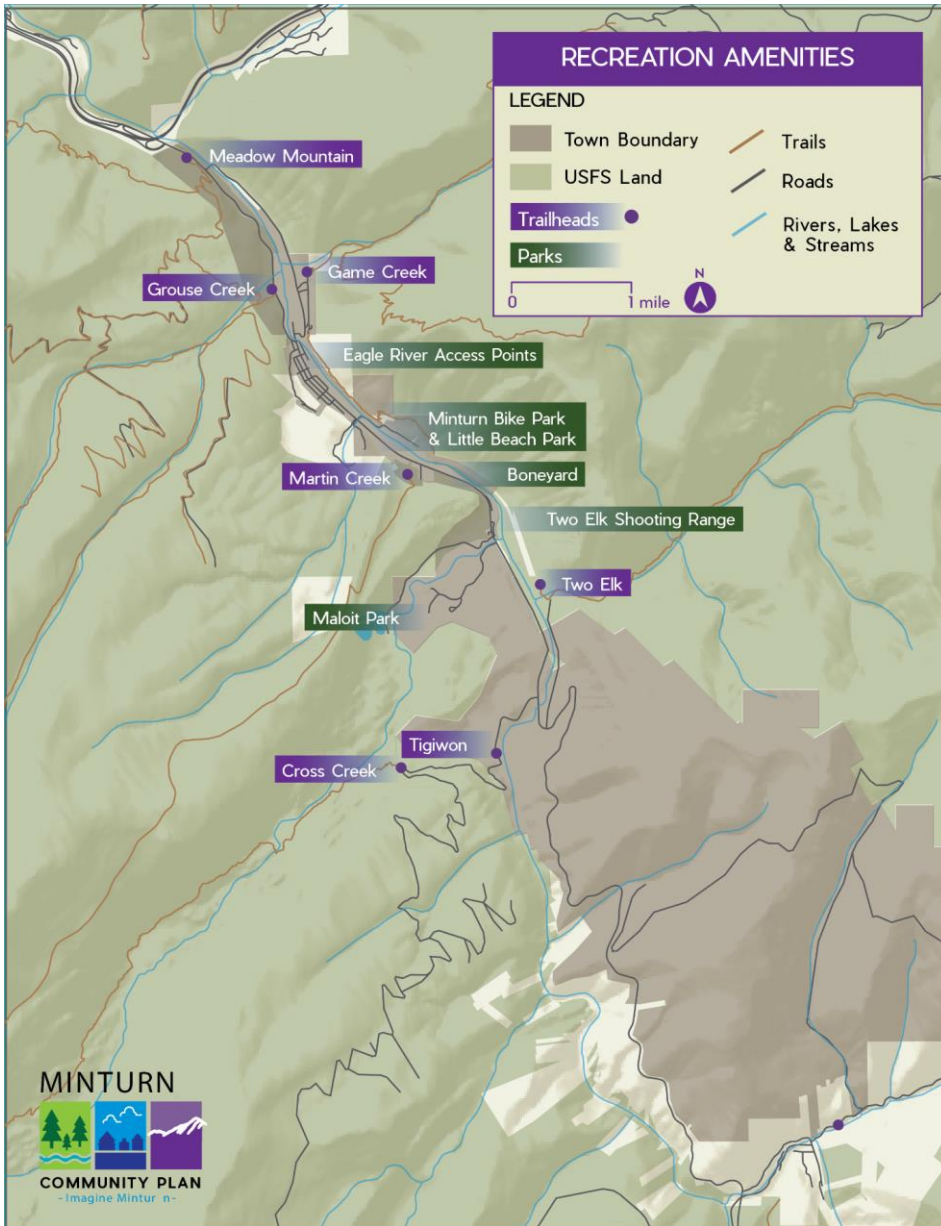
Of course, many other recreation opportunities are available for Minturn residents and visitors in the broader region, including close proximity to Vail and Beaver Creek ski areas. The Minturn Fitness Center, a membership-based gym, also offers opportunities for residents to build their fitness. In addition to an open gym environment, the fitness center offers personal training and classes.

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The Bolts Lake area offers potential opportunity for future recreational development. An asset now owned by ERWSD with intentions to pursue its revival as an acting reservoir, this project

could include recreational amenities such as hiking, boating, and camping, similar to Homestake Reservoir near Leadville.



## Community Events<sup>2</sup>

The Town of Minturn hosts several community events that are valued and attended. These regular events include:

- Minturn Summer Concert Series (free): every Thursday in July and August. Held at the Little Beach Park amphitheater.
- Minturn Market: every Saturday July through Labor Day weekend. Held along Williams Street and adjacent to the Minturn Country Club building (downtown).
- Independence Day Celebration: July 2nd. Held at Little Beach Park.
- Minturn Hometown Throwdown: collection of local music showcases at various locations around town. First Saturday in October.
- Town Tree Lighting Celebration: Includes family-friendly activities. Held in early December at the corner of Toledo Ave and Main Street.
- Christmas Tree Bonfire: community bonfire to celebrate the end of the holiday season and dispose of community Christmas trees. . Held in early January at the Little Beach Park upper parking lot.
- Minturn Halloween: trick-or treating and other family-friendly activities. Town-wide event held annually on October 30th.
- Blue Starlite Drive-In Movies: typically held regularly throughout the summer at the Little Beach Park upper parking lot. These events, hosted by an outside vendor, have been discontinued for the 2022 season.
- Town Cleanup Day: opportunity for residents to properly dispose of/recycle large items and e-waste. Held each spring. Historically, resident groups have also sometimes taken this opportunity to participate in trash pick-up around town. Community dinners: typically organized monthly by the Minturn Community Fund. Held at Town Hall.
- Neighborhood Block Parties: sponsored by the Minturn Community Fund but spearheaded by local residents. These events take place as they arise based on resident interest.

## Quality of Life Policy Context

The 2016 Minturn Economic Development Plan includes three key areas of focus, one of which is placemaking – the act of creating spaces that draw people, encourage chance encounters, and provide opportunities for formal events. This can include infrastructure and service improvements such as bathrooms, concessions, trails, seating, play equipment, parking/bike racks, etc. as well as beautification tactics such as landscaping, art installations, etc. That plan and others, such as the 2015 Plan4Health Policy Scan and 2009 Community Plan encourage new public spaces such as pocket parks and further planning regarding riverfront access and activation as well as trails and active transportation within Minturn. The Plan4Health Policy Scan also encourages more opportunities to bring local produce to the Minturn community.

## Community Input

Residents of Minturn identified sense of community and recreational amenities/programs as two of the top five attributes of the Minturn community.<sup>3</sup> When asked why they choose to live in

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<sup>2</sup> Town of Minturn website

<sup>3</sup> Town of Minturn Community Survey Report (2022)

Minturn, the top two answers by far were quality of life and recreational activities.<sup>4</sup> The small town atmosphere, quality of life, scenery, and recreational amenities were also top things that residents reported enjoying about Minturn. Respondents also reported satisfaction with Town efforts regarding arts and culture amenities as well as programs and events, but that the quality, quantity, and signage for paved pathways could be improved.

Residents strongly supported the idea of adding a bike path from Dowd Junction to Maloit Park, as well as expanding recreational amenities (parks and trails) more generally. Participants also supported expanding paved sidewalks, pathways, and bikeways throughout town and improvements to the restroom facilities at Little Beach Park and amphitheater. Other than improvements to water infrastructure, recreation-related projects were top priorities for survey respondents. 64% of respondents reported that they have been to the Minturn Fitness Center, many of which (45%) indicated that it had been more than a year since they had been, though this lower rate may be a result of COVID-19. 28% said that they went weekly or within the last few months. When asked what would encourage more use, many respondents noted that it is far from the core of the community, that the cost/value is prohibitive, or that they prefer exercising outdoors.

The survey also asked about the positive impact of Town events on quality of life in Minturn. Respondents indicated that the Summer Concert Series, Minturn Market, and Minturn Halloween had the strongest positive impact on quality of life. Town Cleanup Day and neighborhood block parties also received significant support.

Participants in the Open House held as part of this planning process expressed a desire for new walking paths and biking trails, including increased public access to the Eagle River, trails adjacent to the river and pedestrian bridges across the river. Respondents also indicated that existing multi-modal transportation infrastructure is inadequate within Minturn, which affects residents' and visitors' abilities to access recreational amenities and community gathering spaces safely and comfortably without the need for a car. Participants also noted a desire for expanded recreation areas, a formal community center space, and dispersed or organized camping. However, in the past there has been documented opposition to camping in Minturn. There were mixed feelings about the shooting area; some residents believe the shooting range presents safety issues and should be closed. The Keystone Policy Center is facilitating a process to provide recommendations on the shooting range to the Forest Service from a variety of stakeholders. The community garden was noted as an asset to preserve.

The Community Plan Steering Committee also provided valuable feedback as part of this process.<sup>5</sup> During facilitated meetings they expressed gratitude for proximity and access to recreation opportunities, including the ability to walk from your front door to recreate. They noted the value of the Eagle River, the addition of the new Bike Park, Little Beach Park, and the summer concert series. The Committee expressed that working with the railroad is a challenge and limits what the Town can do with property owned by Union Pacific Railroad along Eagle River, including the municipal parking lot which is leased to the Town on an annual basis. They expressed pride in the Town's accomplishments regarding new parks and recreation opportunities since the 2009 Community Plan.

### Central Concepts

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<sup>4</sup> Ibid.

<sup>5</sup> Information from Minturn Community Plan Steering Committee Meeting #1 Memorandum, SE Group (2021)

Many amenities in Minturn already support a high quality of life for residents. This section describes key priorities for increasing quality of life in Minturn. As stated at the beginning of this chapter, increasing quality of life means supporting existing amenities (e.g., recreation areas and trails) and working to provide amenities that are lacking (e.g., a small grocery store).

*Sidebar:*

*Tourism & Quality of Life in Minturn*

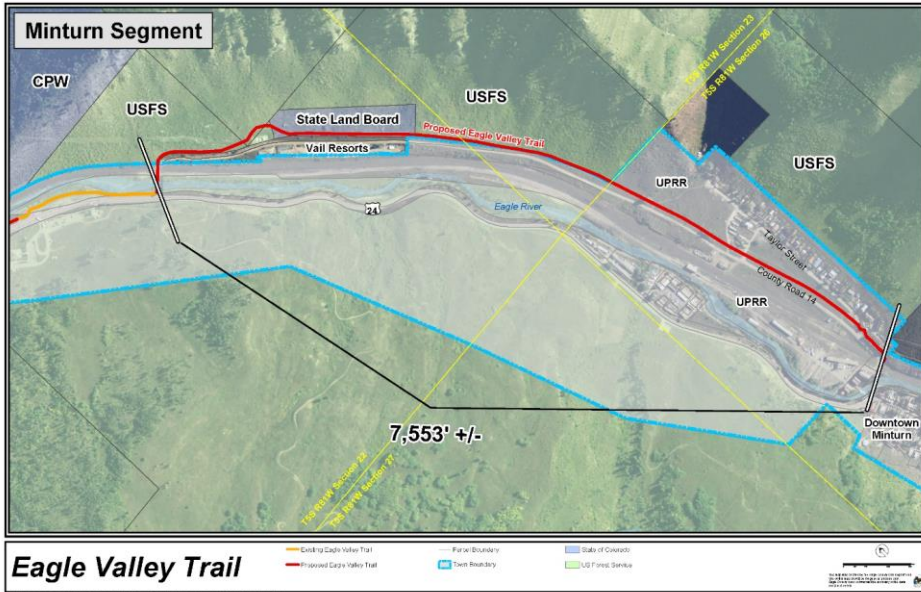
*The same amenities and attributes that support a high quality of life for residents can help create quality experiences for visitors. For example, a walkable downtown area and river access benefit both of these groups.*

*It is important to note that some Minturn residents are concerned about potential negative impacts of increasing tourism activity. As described in **Chapter 3: An Economically Vibrant Community**, Minturn's economy is partially dependent on visitors who supplement the market for local businesses. Without ample visitation, Minturn risks losing businesses that provide essential tax revenue as well as jobs and services for the community. The recommendations in this Plan are intended to acknowledge these concerns and balance them with enhancements to Minturn as a place to live and visit.*

#### Support Recreation Access

Recreation is central to life in Minturn. Visitation to Minturn is also associated with recreation at nearby mountain resorts and increasingly at local recreation sites like Maloit Park and the Minturn Bike Park. To maximize the quality of life benefits of recreation in Minturn, the Town should continue to expand the variety of recreation opportunities that are accessible to residents and visitors of all ages and abilities. The Town should also work to complete pedestrian connections from neighborhoods and Old Town to nearby recreational opportunities.

One example of an ongoing project that will increase recreation opportunities as well as multimodal connections to the broader region is the ongoing effort to extend the regional paved ECO Trail from the I-70 and Hwy 24 intersection to Minturn. Current plans will extend the path to the municipal parking lot just north of the 100 Block, with construction anticipated for 2023. The Town intends to advocate for and support an additional extension to Maloit Park. The figure below illustrates the general trail alignment concept (red line) for planned development in 2023.



The Town should also support the stewardship and ongoing maintenance of its existing recreation opportunities. Sharing and encouraging an ethic of responsible recreation in Minturn has the potential to benefit the Town's trails, parks, and open spaces for generations. The Town is actively pursuing grants related to responsible recreation and stewardship.

**STAFF PROPOSED NEW LANGUAGE**

Much of Minturn's recreation occurs on public lands (e.g. U.S. Forest Service). Often times, public lands are bordered by private lands which can restrict access. In evaluating future developments, the Town should evaluate needs associated with ~~to~~ access public lands as part of a development proposal. Potential public land access opportunities should also be evaluated by the applicable public land agency.

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**Promote Access to Healthy Food & Essential Services**

Access to fresh food is in Minturn's history – the Town was once a landmark producer of iceberg lettuce. At present, Minturn residents have to travel to nearby communities to complete their food shopping. The Steering Committee for this planning effort and participants in the Open House both expressed support for bringing a small grocery store to Minturn. In write-in comments, Open House participants emphasized that they did not support a chain grocery establishment. This type of small grocery store or natural foods store could be located in Minturn's Old Town or could be specifically incorporated into the design of Dowd Junction. Dowd Junction's location directly off I-70 would support the success of this type of business. Other essential services that are currently not present in Minturn include a bank and health clinic. Childcare options could also be expanded locally. The Town should ensure that zoning regulations support the establishment of such services and promote their development.

## Community Events, Arts, & Culture

Minturn's downtown is already a point of pride for the community. As discussed in **Chapter 2: A Thriving 100 Block** and **Chapter 3: An Economically Vibrant Community**, placemaking efforts will help to elevate the 100 Block and Old Town areas as well as the broader community. Amenities such as benches, public art, pedestrian paths, and landscaping can all contribute to the aesthetics and appeal of an area. As discussed in the Existing Conditions and Community Input sections above, Minturn residents value the Town's program of community events. Placemaking improvements will help the Town support more events in the downtown area. Additionally, the Town can actively support and build partnerships with local businesses that contribute to community culture by offering quality gathering spaces, hosting events, supporting the arts community, and more. Chapter 3 acknowledges ongoing discussion and recommends continued efforts to establish a Downtown Development Authority (DDA) – a quasi-governmental entity that could convene local businesses and provide funding for and coordinate placemaking efforts.



*Image source: [www.helenshouseminturn.com](http://www.helenshouseminturn.com)*

### *Sidebar: The Role of Businesses in Building Community*

*The Agora is a great example of a business that actively contributes to building community in Minturn through creative gathering space, arts, events, and food. The community has expressed substantial appreciation and support for the Agora's presence in Minturn and impacts on the community. In addition to serving as a restaurant and creative event space, the business' owner has recently purchased the adjacent property and is working with the Town to provide a pedestrian pathway through the property from Main Street to Eagle River Street – providing essential visibility and access to the river.*



*Image source: [www.helenshouseminturn.com](http://www.helenshouseminturn.com)*

## Quality of Life

## Neighborhood Design that Supports

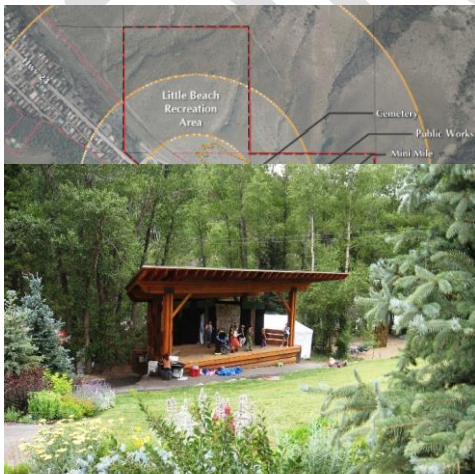


Neighborhood design can play a critical role in a community's quality of life. Thoughtful neighborhood design can support residents in leading healthy lifestyles. For example, in a neighborhood that is well-connected to public transit and multi-use paths, residents may find it easier to commute to work or school, socialize, maintain an active lifestyle, and access the outdoors. A neighborhood or district that is designed around transit opportunities is called "Transit-Oriented Development." Dowd Junction *isis* a significant opportunity for the Town of Minturn to create new housing and commercial opportunities in coordination with multimodal and regional transportation infrastructure. Depending on future investments in bicycle and pedestrian infrastructure as well as public transport services, other areas with potential for TOD-style development include Old Town and the Maloit Park area.

### Quality of Life and Affordability

Quality of life improvements make a place more desirable to live in. When a place becomes more desirable to live in, property values increase. While this is a positive outcome for municipal revenues, it can have the negative consequence by pricing out residents and businesses who can no longer afford rents or ownership costs in the community. When planning and implementing improvements to quality of life, the Town should simultaneously prioritize measures to provide affordable and attainable housing in Minturn to counteract the negative impacts of increasing demand and costs and protect and preserve the local community. Many residents have indicated concern about rising housing costs that are already causing locals to move down valley in search of more affordable options. Every involuntary departure of a resident can disrupt community and social fabric and weaken the local labor pool. (See **Chapter 4: Attainable Housing and Historic Character** for specific recommendations regarding affordability). At the same time, certain quality of life improvements can also make a place more affordable to live in; with enhanced walking, biking, and transit connections, Minturn residents would not have to depend on a personal vehicle to meet their everyday needs.

### Little Beach Park Recreation Area Plan



*Little Beach Park amphitheater*

The Town of Minturn has contracted with Zehren & Associates to develop a recreation-based plan for Little Beach Park and the surrounding area, including the Cemetery, Public Works site, Minturn Bike Park, and future trail connections to other recreation areas. This planning effort is happening concurrently and in coordination with this Community Plan. The purpose of the Plan is to "establish a long-term vision for the area as the Town's major recreation hub" and "unify existing facilities and identify appropriate improvements to support community recreation, trails, and open space uses."

The project also aims to create a sense of arrival at key destinations in the Little Beach Park area, particularly at the Minturn Bike Park. In addition, the project has identified

community desires for new recreational uses that could be appropriate for the area, such as a temporary ice rink and a dog park. Connectivity is also a major theme of the project; the final plan identifies possible trail connections to Two Elk and Maloit Park to expand biking and walking opportunities in Minturn. The project, which will culminate in a conceptual design for the project area, ~~will be scheduled to be completed in September, adopted alongside this plan as of November 2022, (anticipated).~~

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## Recommendations

### OBJECTIVE 7.1) ENCOURAGE A VIBRANT PUBLIC REALM IN MINTURN THAT EMPHASIZES ARTS, STEWARDSHIP, ACCESS TO THE EAGLE RIVER, AND OUTDOOR FUN.

Minturn has a distinct character and strong outdoor recreational influence. As a community surrounded by immense natural resources, Minturn should maintain and foster community responsibility for protecting and preserving local resources. Through new events, infrastructure upgrades, and strategic partnerships, Minturn can become even more colorful, sustainable, beautiful, and community-oriented.

- **Action 7.1.1** Sponsor additional cleanup days to maintain a healthy environment and foster a culture of responsible stewardship. This/these events could capitalize on partnerships locally and more broadly in Eagle County for organization and activities. These events should include elements of fun as well, such as live music, food, art activities, etc.
- **Action 7.1.2** Support/partner with organizations and businesses like the Agora that foster local arts and want to play a role in improving community infrastructure creatively. Support and implement the conceptual pedestrian footpath adjacent to the Agora property to allow access from Main Street to the Eagle River.
- **Action 7.1.3** – To preserve Minturn's stargazing experiences and to support wildlife, consider establishing Minturn as a Dark Sky Community per the International Dark Sky Association standards and designation process. Confer with other Dark Sky communities in Colorado, such as Westcliffe and Silver Cliff as needed.

See **Chapter 3: An Economically Vibrant Community** for additional action items related to placemaking.

### OBJECTIVE 7.2) FACILITATE AND SPONSOR ADDITIONAL PROGRAMMING AND COMMUNITY EVENTS FOR CHILDREN.

While Minturn currently has fewer families than other communities in Eagle County, it is important that the Town supports families with children who do live in the community. In addition, it is important to create a welcoming and supportive environment for any future families that move to the community or current residents who choose to have children down the line. Young adults, youth, and children are the next generation to sustain the community. Especially in small towns, it is important to ensure that young people will want to stay and/or move in to sustain the local labor force, contribute to tax revenue and to the local economy, and preserve social mass.

- **Action 7.2.1** Work with Blue Starlite or other vendors to retain outdoor movie events at Little Beach Park.
- **Action 7.2.2** Support the expansion and improvement of childcare options in Minturn for families with young children.
- **Action 7.2.3** Ensure that popular community events and any new events are family-friendly, considering timing, activities, etc.
- **Action 7.2.4** Support the organization of cultural, recreational, and educational events catered towards children within Minturn.
- **Action 7.2.5** – Promote the inclusion of recreational amenities in redevelopment at Maloit Park, including playgrounds, trails, gathering areas, etc.

**OBJECTIVE 7.3) PROMOTE ACTIVE LIFESTYLES IN MINTURN; COMPLETE BIKE/PED CONNECTIONS AND SUPPORT THE MINTURN FITNESS CENTER AS A COMMUNITY HUB.**

Active lifestyles contribute to good mental and physical health. As a mountain community with many existing recreational assets, Minturn draws residents and visitors who may already enjoy active lifestyles. However, not every resident or visitor fits this bill, and there are a variety of ways that people can or prefer to be physically active. Because Minturn is relatively small geographically, ensuring that people feel comfortable walking or cycling throughout town creates opportunities for people to use active transportation instead of vehicles, which also reduces parking needs and headaches. Additionally, ensuring that people are aware of, have access to, and feel comfortable utilizing the Minturn Fitness Center is essential for providing indoor fitness opportunities.

- **Action 7.3.1** Continue to support ongoing plans and implementation of the Minturn extension to the regional paved ECO Trail; ~~Advocate~~~~advocate~~ for and support further extension ~~all the way~~ to Maloit Park.
- **Action 7.3.2** Formalize/pave the social trail that runs along the east side of the Eagle River. Consider adding additional- bridge access points at the southern end of Minturn. This effort should be incorporated into or in conjunction with the full ECO Trail extension and would likely require coordination with property owners, including Vail Resorts, U.P. Railroad, and the Forest Service.
- **Action 7.3.3** Improve bicycle and pedestrian infrastructure to access to the Fitness Center/Maloit Park Road and work with ECO Transit to increase awareness of and improve service to this area. Consider expanding the Center as a broader community space to accommodate other activities and uses. For example, community members have expressed interest in pickle ball, basketball, a pool, and winter ice rink. ~~Consider~~~~increase community awareness of the \$10 drop in option and consider~~ implementing ~~other~~ rewards programs or ~~other~~ incentives that encourage its use ~~and increase its accessibility to all residents~~.
- **Action 7.3.4** Improve bicycle and pedestrian infrastructure throughout town to improve safety, connectivity, and overall mobility and access for active transportation. Collaborate with neighboring municipalities on an electric bikeshare program. See **Chapter 6: Circulation & Mobility** for further details about infrastructure improvements.
- **Action 7.3.5** Ensure that winter plowing includes pedestrian infrastructure and recreational pathways to allow for and encourage activity in winter months, as possible. Additional information regarding diversified revenue sources to support town services, such as plowing, can be found in other chapters, particularly **Chapter 3: An Economically Vibrant Community**.

**OBJECTIVE 7.4) PROMOTE ACCESS TO QUALITY, LOCALLY GROWN FOOD IN MINTURN; ENCOURAGE COMMUNITY EVENTS RELATED TO LOCAL FOOD.**

The Minturn community currently doesn't have a grocery store, meaning that residents must travel to ~~Avon~~, Vail or Eagle-Vail to get their groceries. Reasonable access to a variety of food choices, including healthy options and fresh produce, is an essential part of quality of life and overall health. This also means that it should not take extensive amounts of time and energy to access ingredients for meals.

- **Action 7.4.1** Support the addition of a grocery store locally in Minturn, likely as part of the potential Dowd Junction PUD development. Ensure that any new grocery option includes fresh produce.

- **Action 7.4.2** Continue to support the Minturn Market. Consider marketing to additional sources of fresh produce within the region.
- **Action 7.4.3** Continue to support the Minturn Community Garden. Work with the Minturn Community Fund or other local and regional partners to expand event offerings at the garden, such as workshops, community dinners, etc. that bring people together to showcase the garden and provide educational opportunities to learn about locally-grown foods and food preparation. Assess demand for and consider other locations that could support additional community garden space, particularly at PUD sites such as Dowd Junction.

**OBJECTIVE 7.5) ENCOURAGE SAFE AND RESPONSIBLE ACCESS TO TRAILS AND RECREATION OPPORTUNITIES ON FOREST SERVICE LAND ADJACENT TO MINTURN.**

Minturn’s access to public lands is unique and special. From trailheads in and around Minturn, residents and visitors can access scenic and remote landscapes. Backcountry adventures can be fun and memorable experiences, but they also come with risks. The Town should help inform residents and visitors of the steps to take in an emergency and ways to minimize human impact on the landscape.

- **Action 7.5.1** Collaborate with the Forest Service, [CPW, and Vail Valley Mountain Trails Alliance](#) on a 1- to 2-page guide to safe and responsible recreation in Minturn; publicize this guide on the Town website, social media channels, and at trailheads. Ensure that this guide includes the number of the relevant parties to call in case of an emergency.
- **Action 7.5.2** Encourage Leave No Trace principles at all parks, trails, and open spaces within the Town of Minturn. Provide stewardship information on town maps, trailhead kiosks, and web materials as appropriate.
- **Action 7.5.3** Collaborate with regional partners such as the Eagle River Watershed Council, Eagle Summit Wilderness Alliance, and [Eagle Valley Land Trust, and Vail Valley Mountain Trails Alliance](#) on stewardship education and enforcement efforts to protect and preserve natural resources and wildlife.
- **Action 7.5.4 – Prepare a Parks, Recreation & Open Space Plan to assess and plan for access and parking needs among other management practices, particularly in the face of potential development such as Dowd Junction.**

**STAFF PROPOSED NEW ACTION STEP**

- **Action 7.5.5 – Require new developments to assess the need and practicality for access to adjacent public lands, if applicable.**

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**OBJECTIVE 7.6) SUPPORT THE GROWTH OF LITTLE BEACH PARK AND THE SURROUNDING AREA AS A HUB FOR RECREATION AND COMMUNITY EVENTS.**

Little Beach Park is one of the Town’s most prized assets and popular venues for community events. As laid out in the previous section, the Town is taking proactive steps to preserve, improve, and expand the opportunities of this great asset and the surrounding area through the Little Beach Park Recreation Area Plan.

- **Action 7.6.1** Implement recommendations from the Little Beach Park Recreation Area Plan.
- **Action 7.6.2** Regularly assess needs, community desires, and new ideas for the Little Beach Park Recreation Area to ensure that the 2022 Plan and Town efforts are on-track.

- **Action 7.6.3** Facilitate future trail connections between recreation assets in the Little Beach Park area (e.g., Minturn Bike Park, Mini Mile) and other trailheads and recreational areas within Minturn.
- **Action 7.6.4** Support the Little Beach Park area as a hub for recreation, arts, performances, and community events in Minturn; support the adaptive reuse of storage yards and underutilized industrial portions of the area for recreation and public enjoyment.

**OBJECTIVE 7.7) CONTINUE WORKING WITH THE EAGLE RIVER WATERSHED COUNCIL AND EAGLE VALLEY LAND TRUST TO SUPPORT STEWARDSHIP AND COMMUNITY EDUCATION INITIATIVES.**

The Eagle River and its broader watershed is a valuable natural resource for the community and surrounding region. The Town should involve the community in efforts to collectively manage and protect this resource and support its longevity. Through signage and educational materials, as well as partnerships and events, the Town can promote a healthy riparian corridor and watershed

- **Action 7.7.1** Expand upon Town Cleanup Day and the Eagle River Watershed Council’s County-wide clean up to maintain a healthy environment and spread a culture of responsible stewardship. This or new events could specifically focus on the health and monitoring of Eagle River and educate residents on regulations and data-research techniques that inform protection of the river’s natural ecosystem, such as watershed mapping, development setbacks, and erosion control measures. Event activities should also include elements of fun, such as live music, food, art activities, etc.
- **Action 7.7.2** Coordinate with the Eagle River Watershed Council to offer citizen science training opportunities, especially for youth in the Minturn community and region.
- **Action 7.7.3** Consider implementing signage and informational materials to educate residents and visitors about stewardship and responsible human-nature interaction.
- **Action 7.7.4** Coordinate with Eagle Valley Land Trust on the continued stewardship and community enjoyment of the Boneyard Open Space area.

**OBJECTIVE 7.8) CONTINUE WORKING WITH THE VAIL VALLEY MOUNTAIN TRAILS ALLIANCE TO SUPPORT STEWARDSHIP AND INITIATIVES OF CURRENT AND FUTURE TRAIL AND RECREATION OPPORTUNITIES**

- **Action 7.8.1** Create and implement a Minturn area recreation and conservation plan that identifies recreation opportunities, connections, trailheads, and prioritized wildlife habitat and migration corridors
- **Action 7.8.2** Coordinate with the VVMTA to expand upon existing Adopt A Trail team to engage the community on additional volunteer trail maintenance or trail building days which increases stewardship of surrounding trails and public lands
- **Action 7.8.3** Continue to partner with the VVMTA to maintain the Minturn Bike Park, host volunteer maintenance events, and community events.

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## CHAPTER 8

### A Collaborative, Resilient Future: Implementation, Governance & Continued Visioning

#### Introduction

This community planning process was a collaborative effort between Town staff, the volunteer Steering Committee, Town Council, the Planning Commission, and Minturn residents. These parties have contributed substantial thought and effort into the vision, objectives and action items laid out in the previous chapters of this plan. To help realize the intended outcomes of this plan, this chapter outlines recommendations for plan implementation. Recommendations in this chapter are designed to promote effective and meaningful governance and identify the level of priority of proposed actions and highlight responsible entities, strategic partnerships, and necessary resources for the initiatives of this plan.

#### Central Concepts

Community development is an iterative process, requiring a constant cycle of reevaluation and implementation, including analysis, community discussion, resource allocation, capacity-building and coordination, and project oversight. This plan is broad and aspirational and is intended to apply for the next decade or more. In that time and following, Minturn may need to revisit and reevaluate the relevancy and efficacy of the plan's goals, priorities, and objectives.

Just as the development of this plan centered on community input and engagement, implementation efforts should likewise involve the Minturn community. Because Town efforts and the objectives of this plan deeply affect the Minturn community, it is essential that residents remain informed of implementation efforts and continue to have the opportunity to share their perspectives and input.

Because Minturn is a small community, it has fewer resources compared to many larger metropolitan areas. Thus, regional collaboration and resource-sharing is an essential strategy to support Minturn's resiliency and vitality.

#### STAFF SUGGESTED ADDITIONAL LANGUAGE

##### Plan Updates and Minor Modifications

This Community Plan is intended to reflect community conditions, values, and desires for a 10- to 15-year period. The Plan captures current conditions in Minturn and the surrounding region as well as trends based on best available information, particularly with regard to potential growth scenarios over the Plan horizon.

That being said, this Plan is a living document that should be reviewed periodically by the Town and affected stakeholders to assure the continued appropriateness, relevancy, and applicability of the Plan's stated vision, goals, policies, implementation strategies and Future Land Use Map. The Community Plan should be updated every 15 years at minimum, or as necessary to reflect new conditions, new community attitudes and/or major new land use opportunities unforeseen at the

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time of the Plan's adoption. Changes to the document at the Plan Update level shall require the implementation of a full public planning process.

Additionally, minor modifications may be necessary over time. These are defined as changes to the document which do not require analysis or community involvement normally associated with an update. Minor modifications typically take the form of factual or technical corrections or adjustments to text or to maps. Minor Modifications may be proposed by Town Staff, public agencies, interest groups, private property owners or business owners. While no formal process is prescribed for minor modifications, this Plan anticipates that written comments describing the nature of the issue and/or the desired change shall be reviewed and either approved or denied by the Town of Minturn Planning Commission at a regularly scheduled and noticed public hearing.

Last, this Plan includes recommendations for consideration by the Town over the next 10-15 years. Some recommendations, particularly zoning and future land use recommendations, may not be implemented or approved by the Town exactly as proposed in this Plan. In such instances, failure to adhere to, approve, or enforce certain recommendations shall not, on its own, cause or necessitate a process to update the Plan or to pursue minor modifications.

## Recommendations

### OBJECTIVE 8.1) IMPLEMENT THE OBJECTIVES OF THIS COMMUNITY PLAN IN A THOUGHTFUL AND COLLABORATIVE MANNER.

This plan contains a variety of proposed actions – some of these are high priority actions meant to be pursued in the short-term. Others are less time-sensitive and are meant to be pursued and implemented over a longer period of time. The Implementation Matrix contained at the end of this chapter is intended to help assist with the realization of high priority and short-term actions in this plan.

- **Action 8.1.1** Use the Implementation Matrix found at the end of this chapter to guide the Implementation phase of this community planning process.
- **Action 8.1.2** As applicable, develop similar implementation matrices for the remaining actions in this plan as they may grow in relevance during the life of this plan.

### OBJECTIVE 8.2) INFORM & ENGAGE RESIDENTS IN COMMUNITY DEVELOPMENT.

The more involved, informed, and engaged that the Minturn community is, the more that community development efforts will reflect their concerns, desires, and needs. Celebrating community achievements, ensuring transparency, and providing frequent and varied opportunities for feedback will reduce unexpected roadblocks and build trust.

- **Action 8.2.1** Continue sharing information with the community via the Town Newsletter, municipal website, and other mechanisms. Consider implementing quarterly mailers to inform residents of key achievements, ongoing projects, engagement opportunities, etc.
- **Action 8.2.2** Work closely with elected officials and Town boards and committees to engage residents in an in-person and virtual capacity.

### OBJECTIVE 8.3) PRIORITIZE PARTNERSHIPS, COLLABORATION, AND RESOURCE-SHARING AS APPLICABLE.



Many recommendations throughout this plan involve partnerships and collaboration. With every initiative, the Town should consider any opportunities that exist to expand capacity and maximize resources through partnerships.

- **Action 8.3.1** Consider a collaborative effort to establish and maintain an inventory of key partners and regular funding sources that can be easily referenced when strategizing for local or regional needs and initiatives.
- **Action 8.3.2** Highlight successful ongoing partnerships with community organizations, other municipalities, Eagle County, and state agencies on the Town website and social media platforms.

**OBJECTIVE 8.4) ORGANIZE AND STREAMLINE INTERNAL PROCESSES TO MAXIMIZE EFFICIENCIES AND CAPACITY, MONITOR PROGRESS, AND COMPILE DATA.**

With limited resources, it is important that the Town regularly evaluates internal processes to improve operations and efficiency.

- **Action 8.4.1** Convene annually with all departments to review operations and strategize improvements for the following year.
- **Action 8.4.2** Establish and maintain an ongoing inventory of completed town projects, initiatives and achievements.
- **Action 8.4.3** Streamline data collection efforts across Town departments to avoid duplication of efforts and promote shared knowledge.

Implementation Matrix

SMART LAND USE & PRACTICAL ZONING						
ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline for implementation (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<b>Action 1.1.1</b> Adopt future zoning map and associated recommended zoning districts and requirements, as stated in this section	Internal Initiative/Policy Change	High	Short-term	\$		Townwide
<b>Action 1.1.2</b> Adopt the <b>100 Block Design Standards and Guidelines Appendix B</b> insert.	Internal Initiative/Policy Change	High	Short-term	\$	Downtown Doers Group	100 Block

A THRIVING 100 BLOCK						
ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<b>Action 2.1.1</b> Ensure that future ordinances and land use practices for the 100 Block align with the framework established by the 100 Block Design Standards and Guidelines.	Internal Initiative/Policy Change	High	Long-term	\$		100 Block
<b>Action 2.2.2</b> Develop an official list of historic properties and landmarks, including those in the 100 Block; collaborate with property owners to highlight these special locations with maps and signage as appropriate.	Internal Initiative/Policy Change	High	Long-term	\$	Minturn Historic Preservation Committee	Townwide
<b>Action 2.2.3</b> Continue to explore a Historic Preservation Policy for Minturn; ensure that future historic preservation policies remain compatible	Internal Initiative/Policy Change, Additional Planning	High	Long-term	\$	Minturn Historic Preservation Committee	Townwide

with the 100 Block Design Standards and Guidelines.						
<b>Action 2.3.1</b> Initiate a wayfinding plan for the 100 Block area; ensure that signage and wayfinding elements follow a consistent style that is compatible with the broader area and highlight key locations and amenities, such as access points to the Eagle River and nearby trailheads.	Internal Initiative, Additional Planning	High	Mid-term	\$\$	ECO Transit, CDOT	100 Block area, Town gateways
<b>Action 2.4.1</b> Establish one-way shared streets on Williams St. and Eagle River St.; install signage that conveys this change and reduces the vehicle speed limit to promote traffic safety.	Internal Initiative, Additional Planning	High	Mid-term	\$\$	CDOT	Williams St. and Eagle River St., 100 Block

AN ECONOMICALLY VIBRANT COMMUNITY						
ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)

<p><b>Action 3.1.3</b> Continue to assess Minturn's industries and demographics relative to regional economic trends; conduct an annual strategic call with the Vail Valley Partnership.</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Long-term</p>	<p>\$</p>	<p>Commercial businesses, Vail Valley Partnership</p>	<p>Townwide</p>
<p><b>Action 3.2.1</b> Actively participate in future planning within the Regional Transit Authority to advocate for increased and flexible service options to current and future retail centers within Minturn, including the 100 Block and Old Town and Dowd Junction.</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Long-term</p>	<p>\$\$</p>	<p>Eagle County, DOT, Public Works</p>	<p>Townwide, 100 Block, Old Town, Dowd Junction</p>
<p><b>Action 3.2.2</b> Prioritize future expansions and improvements for bicycle and pedestrian infrastructure and amenities to connect current and future retail centers of the community, including the 100 Block, Old Town, and Dowd Junction; encourage multimodal connectivity between destinations in Minturn.</p>	<p>Partnership/Additional Planning, Capital Investment</p>	<p>High</p>	<p>Mid-term</p>	<p>\$\$</p>	<p>Commercial Businesses, ECO Transit, Public Works</p>	<p>Townwide, 100 Block, Old Town, Dowd Junction</p>
<p><b>Action 3.3.1</b> Continue to pursue ownership of the Dowd Junction property and planned development of the area as Transit-Oriented Development.</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Mid-term</p>	<p>\$\$</p>	<p>Commercial Businesses, Developers, ECO Transit, Public Works</p>	<p>Dowd Junction</p>

<b>Action 3.4.1</b> Support ongoing efforts to create a Downtown Development Authority (DDA) to help fund public infrastructure and placemaking projects.	Partnership/Additional Planning	High	Short-term	\$	Commercial Businesses, Developers, Public Works	Townwide, 100 Block, Old Town
<b>Action 3.4.3</b> Build on the recommendations and conclusions of the Economic Development Strategic Plan – including an update to the Plan – to more thoroughly address the role of various economic partners in the Town and in Eagle River Valley, and to establish specific and up-to-date business and organizational strategies. This could be a joint effort of the Town and a new DDA.	Partnership/Additional Planning	Medium	Mid-term	\$\$	DDA	Townwide
<b>Action 3.5.1</b> Review all user fees to ensure they are capturing the cost of the service provided, including utility rates.	Internal Initiative/Policy Change	Medium	Short-term	\$		Townwide

ATTAINABLE HOUSING & HISTORIC CHARACTER

ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<b>Action 4.1.1</b> Implement remaining Tier One recommendations in the 2019 Housing Action Plan.	Internal Initiative/Policy Change	High	Short-term	\$\$	Eagle County Housing Authority	Townwide, potential annexation areas
<b>Action 4.2.2</b> Update Minturn's Housing Action Plan by 2025. The 2019 Plan is set to guide housing for 3-5 years.	Additional Planning	High	Mid-term	\$\$	Eagle County Housing Authority	Townwide, potential annexation areas
<b>Action 4.3.1</b> Investigate the viability of a buy-down program in Minturn.	Internal Initiative/Policy Change, Additional Planning	High	Short-term	\$\$		Townwide
<b>Action 4.4.2</b> Consider instituting linkage fees and/or a regulatory fee on STRs.	Internal Initiative/Policy Change, Additional Planning	Medium	Short-term	\$		Townwide
<b>Action 4.6.1</b> Amend the 200% AMI requirement for inclusionary housing in Minturn to promote more equitable and inclusive access to housing. Further assessment may be necessary to determine	Internal Initiative/Policy Change, Additional Planning	High	Short-term	\$	Eagle County Housing Authority	Townwide

the most appropriate thresholds for the community, and many communities require units at a variety of thresholds. A 120% AMI requirement should be available for at least a portion of units.						
<b>Action 4.9.1</b> Encourage the inclusion of affordable housing in PUDs over and above the current IH requirement; consider offering an expedited review process to incentivize this.	Internal Initiative/Policy Change	High	Short-term	\$		PUD Holding Zones, Townwide
<b>Action 4.10.2</b> To preserve existing buildings to the extent possible, consider instituting a demolition delay ordinance or initiative to support adaptive redevelopment as opposed to teardowns and new builds.	Internal Initiative/Policy Change	Medium	Mid-term	\$	Historic Preservation Committee	Townwide

WATER INFRASTRUCTURE, HAZARD MITIGATION, & SUSTAINABILITY



ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<p><b>Action 5.1.1</b> – Prioritize Town funding and pursue all potential outside funding to support the necessary improvements outlined in Resolution 30 – Series 2021 to increase Minturn’s water system capacity, particularly to improve efficiencies and mitigate water loss.</p>	<p>Capital Investment, Partnership/Additional Planning</p>	<p>High</p>	<p>Long-term</p>	<p>\$\$\$</p>	<p>Public Works, Eagle County, State of Colorado</p>	<p>Townwide</p>
<p><b>Action 5.1.2</b> – Work with the Eagle River Water and Sanitation District at Bolts Lake and/or on other opportunities to improve water supply resilience through Raw Water Storage and source redundancies.</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Long-term</p>	<p>\$\$\$</p>	<p>Eagle River Water and Sanitation District</p>	<p>Townwide, Bolts Lake</p>
<p><b>Action 5.1.3</b> – Conduct an Inflow &amp; Infiltration (I&amp;I) Study and utilize the forthcoming Eagle River Water and Sanitation District Wastewater Collection System Master Plan update to inform necessary</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Short-term</p>	<p>\$\$</p>	<p>Eagle River Water and Sanitation District</p>	<p>Townwide</p>

improvements to wastewater infrastructure in Minturn.						
<b>Action 5.2.3</b> – Continue to implement Energy Action Plan strategies to incentivize commercial and residential participation in available energy efficiency and renewable source programs. Conduct a baseline study to assess the Town’s progress on its energy goals and establish whether current policies are working to increase energy efficiency.	Internal Initiative, Partnership/Additional Planning	High	Long-term	\$\$	Xcel Energy, Public Works	Townwide
<b>Action 5.2.4</b> – Consider implementing a solar rebate or other incentive program to support on-site solar for residential and/or commercial buildings; collaborate with the Walking Mountains Climate Action Collaborative and/or Holy Cross to connect Minturn residents with existing energy incentive programs.	Internal Initiative/Policy Change	High	Short-term	\$\$	Walking Mountains Climate Action Collaborative, Holy Cross, Xcel Energy	Townwide
<b>Action 5.2.5</b> – Establish Low Impact Development (LID) standards that promote green infrastructure and nature-based solutions to address stormwater.	Internal Initiative/Policy Change	High	Short-term	\$	Eagle River Watershed Council	Townwide, Intensive Development areas (PUDs, 100 Block, Old Town)

<p><b>Action 5.2.6</b> – Adopt waterwise/xeriscape regulations, incentives, and/or educational tools to promote strategic, responsible and climate-appropriate landscaping practices that reduce water use, improve soil health, and limit maintenance needs.</p>	<p>Internal Initiative/Policy Change</p>	<p>High</p>	<p>Short-term</p>	<p>\$</p>	<p>Eagle River Watershed Council, Public Works</p>	<p>Townwide</p>
<p><b>Action 5.2.7</b> – Consider implementing additional incentives for sustainable design and development practices in PUD regulations.</p>	<p>Internal Initiative/Policy Change</p>	<p>High</p>	<p>Short-term</p>	<p>\$</p>		<p>Townwide, PUD Holding Zones</p>
<p><b>Action 5.2.8</b> – Consider implementing a Smart Building Incentive Program that allows developers and property owners to waive permit fees if their project conforms to a predetermined set of sustainability standards.</p>	<p>Internal Initiative/Policy Change</p>	<p>High</p>	<p>Short-term</p>	<p>\$\$</p>		<p>Townwide</p>
<p><b>Action 5.3.1</b> – Partner with FEMA to update floodplain maps and correct inaccuracies; utilize this information to inform future infrastructure needs, landscaping, development regulations, and other potential flooding preparedness measures.</p>	<p>Partnership/Additional Planning</p>	<p>High</p>	<p>Short-term</p>	<p>\$</p>	<p>FEMA</p>	<p>Townwide</p>

<b>Action 5.3.2</b> – Continue to reference and implement the Eagle County Hazard Mitigation/Pre-Disaster Mitigation Plan; support its update due in 2024/25 and ensure that Minturn’s particular risks and needs are accounted for in the plan.	Partnership/Additional Planning	High	Mid-term	\$	Eagle County	Townwide
<b>Action 5.3.3</b> – Adopt firewise guidelines in building and site planning regulations.	Internal Initiative/Policy Change	High	Short-term	\$	Public Works	Townwide
<b>Action 5.3.4</b> – Work with the USFS to determine necessary steps to manage wildfire impacts in Minturn, such as possible forest fuels reduction measures.	Partnership/Additional Planning	High	Short-term	\$	USFS	Town areas bordering the White River National Forest
<b>Action 5.6.1</b> – Implement an internal system to annually monitor town goals and achievements related to environmental sustainability.	Internal Initiative/Policy Change	High	Short-term	\$	Public Works	Townwide
<b>Action 5.6.3</b> – Consider establishing a specific fund for sustainability and resiliency projects. This fund could be sourced from programs such as Sustainable Gift Cards with partnering businesses, parking fees, impact fees, or other creative programs. Otherwise	Internal Initiative/Policy Change	Medium	Short-term	\$	Commercial Businesses	Townwide

promote available grants, programs, and resources for sustainability initiatives.						
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INTUITIVE MOBILITY, CIRCULATION, & CONNECTIVITY						
ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<b>Action 6.1.1:</b> In the near-term, consider updating curbside signage to clarify messaging of parking regulations.	Capital Investment	High	Short-term	\$	Public Works	Town Core
<b>Action 6.1.2:</b> Increase the mix of land uses within close proximity of the 100 Block to create a more walkable core and allow people to “park once” and explore on foot.	Partnership/Additional Planning and Internal Initiative/Policy Change	High	Mid to long-term	\$	Planning & Zoning	100 Block
<b>Action 6.1.4:</b> Formalize locations for passenger and truck loading downtown.	Capital Investment and Internal Initiative/Policy Change	High	Short-term	\$	Public Works; Commercial Businesses	Town Core

<b>Action 6.1.5:</b> Expand the shared parking shed from 100 ft to 1,000 ft and consider allowing uses with complementary demand to share parking rather than restricting based on operating hours.	Internal Initiative/Policy Change	High	Short-term	\$	Planning & Zoning; Commercial Businesses	Town Core
<b>Action 6.1.11:</b> Work with Union Pacific Railroad to explore opportunities to purchase the land containing the Municipal Parking Lot. If that is not feasible, consider extending the current short-term lease or reconfiguring the Municipal Parking Lot.	Partnership/Additional Planning or Capital Investment	High	Mid-term	\$\$\$	Union Pacific Railroad	Municipal Lot
<b>Action 6.2.1:</b> Coordinate land use planning with the transportation system by locating new development near bus stops.	Partnership/Additional Planning	High	Long-term	\$	Planning & Zoning	Near bus stops
<b>Action 6.3.1:</b> Complete near-term priority sidewalk projects that provide missing connections to downtown so that Minturn residents can comfortably walk to key destinations and those within close proximity of a bus stop.	Capital Investment	High	Short-term	\$\$\$	Public Works	Noted in Table 3 in Chapter 6
<b>Action 6.3.3:</b> Install curb extensions at key intersections along Main	Capital Investment	High	Mid-term	\$	Public Works	Noted in Table 4 in Chapter 6

Street to reduce crossing distances, provide a comfortable place for people walking and shopping to linger while waiting to cross the street, and slow traffic.						
<b>Action 6.3.6:</b> Construct the planned paved multiuse ECO Trail that will connect Dowd Junction to the north end of Minturn at the intersection of Minturn Road and Taylor Street.	Capital Investment	High	Mid-term	\$\$\$	ECO Trails, Public Works	Trail Alignment
<b>Action 6.4.1:</b> Expand the Forest Service Park and Ride into a new transit center and mobility hub to connect more people in Minturn to the ECO Transit Highway 6 Route.	Capital Investment	High	Long-term	\$\$\$	Planning & Zoning, PUD Developer, ECO Transit, EVTA	Forest Service Park and Ride/Dowd Junction
<b>Action 6.5.2:</b> Place wayfinding signage throughout the town core between Williams Street and Mann Street; specifically adjacent to bus stops, parking lots, the town hall/post office, trailheads, and hotels. Design signs not immediately next to a bus stop to point toward the nearest stop to remind residents and visitors of the local transit system and promote ease of use.	Capital Investment	High	Mid-term	\$\$	Public Works, Planning & Zoning	Town Core

<b>Action 6.6.2:</b> Collaborate with ECO Transit/the EVTA and commercial businesses to install bike racks near bus stops, the Town Hall/post office, trailheads, and hotels. Place at least one bike rack on each block in the Minturn core that hosts commercial land uses and install a bike wash/repair station at a central, convenient location in the 100 Block.	Partnership/Additional Planning	High	Mid-term	\$\$	Commercial Businesses, ECO Transit, EVTA, Public Works	Town Core
<b>Action 6.6.3:</b> Work with ECO Transit and the EVTA to formalize bus stops, making them ADA accessible and installing seating and shelter.	Partnership/Additional Planning	High	Mid-term	\$\$	Eco Transit, EVTA, Public Works	Bus stops, along US-24

QUALITY OF LIFE, RECREATION, & AN INVITING PUBLIC REALM						
ACTION ITEM	Type (Internal Initiative/Policy Change, Capital Investment, Partnership/Additional Planning, Other)	Priority (low, med, high)	Timeline (short-term, mid-term, long-term)	Cost (\$, \$\$, \$\$\$)	Identified Collaborators/Partnerships	Key Location(s)
<b>Action 7.1.2</b> Support/partner with organizations and businesses like the Agora that foster local arts and want to play a	Partnership/Additional Planning	Medium	Short-term	\$	The Agora, Commercial Businesses	Townwide, Eagle River, 100 Block



role in improving community infrastructure creatively. Support and implement the conceptual pedestrian footpath adjacent to the Agora property to allow access from Main Street to the Eagle River.						
<b>Action 7.3.1</b> Continue to support ongoing plans and implementation of the Minturn extension to the regional paved ECO Trail; advocate for and support further extension to Maloit Park.	Partnership/Additional Planning	High	Mid-term	\$	ECO Trails, Public Works	Townwide, Maloit Park
<b>Action 7.3.2</b> Formalize/pave the social trail that runs along the east side of the Eagle River. Consider adding additional bridge access points at the southern end of Minturn. This effort should be incorporated into or in conjunction with the full ECO Trail extension and would likely require coordination with property owners, including Vail Resorts, U.P. Railroad, and the Forest Service.	Partnership/Additional Planning	Medium	Long-term	\$\$	Property Owners (Vail Resort, U.P. Railroad, USFS)	Eagle River, eastern portion of Town
<b>Action 7.4.1</b> Support the addition of a grocery store locally in Minturn, likely as part of the potential Dowd	Internal Initiative/Policy Change	High	Mid-term	\$	Healthy Communities Coalition of Eagle County, Developers	Townwide, Dowd Junction

Junction PUD development. Ensure that any new grocery option includes fresh produce.						
<b>Action 7.5.1</b> Collaborate with the Forest Service on a 1- to 2-page guide to safe and responsible recreation in Minturn; publicize this guide on the Town website, social media channels, and at trailheads. Ensure that this guide includes the number of the relevant parties to call in case of an emergency.	Partnership/Additional Planning	High	Mid-term	\$	USFS	Town Trailheads/Areas bordering the White River National Forest
<b>Action 7.6.1</b> Implement recommendations from the Little Beach Park Recreation Area Plan.	Internal Initiative/Policy Change, Capital Investment	High	Long-term	\$\$	Minturn Bike Park, Minturn Cemetery, Public Works, Minturn Community Fund	Little Beach Park & surrounding area

***Michael Boyd***  
***502 1/2***  
***504 Eagle Street***

**DATE: 11/12/22**

**TO: THE MINTURN TOWN COUNCIL**

**I AM WRITING YOU ALL TODAY AS A CITIZEN IN REGARDS TO THE COMMUNITY PLAN UPDATE.**

**I ASK THAT YOU DO NOT RECOMMEND ZONING CHANGES TO MY PROPERTY, IT IS CURRENTLY MIXED USE OR MU1 SHOWN IN EXISTING ZONING MAP AND A ZONING MAP AMENDMENT WAS JUST DONE LAST YEAR FOR MY PROPERTY IN CONJUNCTION WITH MY NEIGHBORS AT 502 MAIN ORDINANCE #10-2021 APPROVED BY COUNCIL.**

**I RESPECTFULLY ASK ONCE YOU DISCUSS THIS, IF ANY FINAL CHANGES MADE IN REGARDS TO THE ZONING MAP CHANGE RECCOMENDATIONS THAT THEY ARE SHOWN IN AN ACCURATE UP TO DATE MAP.**

**THANK YOU TO COUNCIL, STAFF AND THE SE GROUP FOR TIME AND CONSIDERATION ON THIS MATTER.  
PLEASE KEEP ME UPDATED.**

**SINCERELY,**

**MICHAEL BOYD**

October 4, 2022

Michelle Metteer , Minturn Town Manager

In the spring of this year I had a meeting with members of Minturn's staff to discuss what options I may have with my residence at 406 Pine Street.

Among the many issues that have impacted me over the years of living at this address are the changes of the corner lot behind me from being an US Forest Service office office which had only a few people there on a daily basis to a restaurant in the 90s, to an incredibly busy 17 hour per day market and gas station. This generates numerous very loud vehicles from delivery trucks beginning at 6 AM to the many loud motorcycles and cars fueling non stop during the day and evening. Many of the delivery vehicles park adjacent to my back fence with their cooling units running the entire time they are delivering. Loud stereos etc are a constant nearly making the use of my yard impossible. While talking to Minturn staff in the town that the noise levels are really not able to be enforced upon I'm at a loss on how to reconcile these issues. More recently the town allowed a variance in the delivery hours to go from 7 AM in the morning back to 6 AM to allow the market to get their supplies more near opening. I realize these issues along with the Pooh Corner school traffic and lit up into the evening hours, the home across Harrison from my home having been a

short term rental in past years are all encumbrances I may not be able to rectify, This along with the ongoing formidable increase in heavy traffic turning from Main Street to Harrison to enter the market and also the traffic in the alleyways from the Town of Minturn building running directly to my home. During my meeting with the town brought about a suggestion by staff that I may want the status from residential to commercial. to change my property from residential to commercial status. I challenged my residential property taxes with the county on the basis of this and they concurred and lowered the rate.

I believe in the eventuality I need to leave the area as I age. I may find it difficult to find someone who will find the location appealing for residential use.

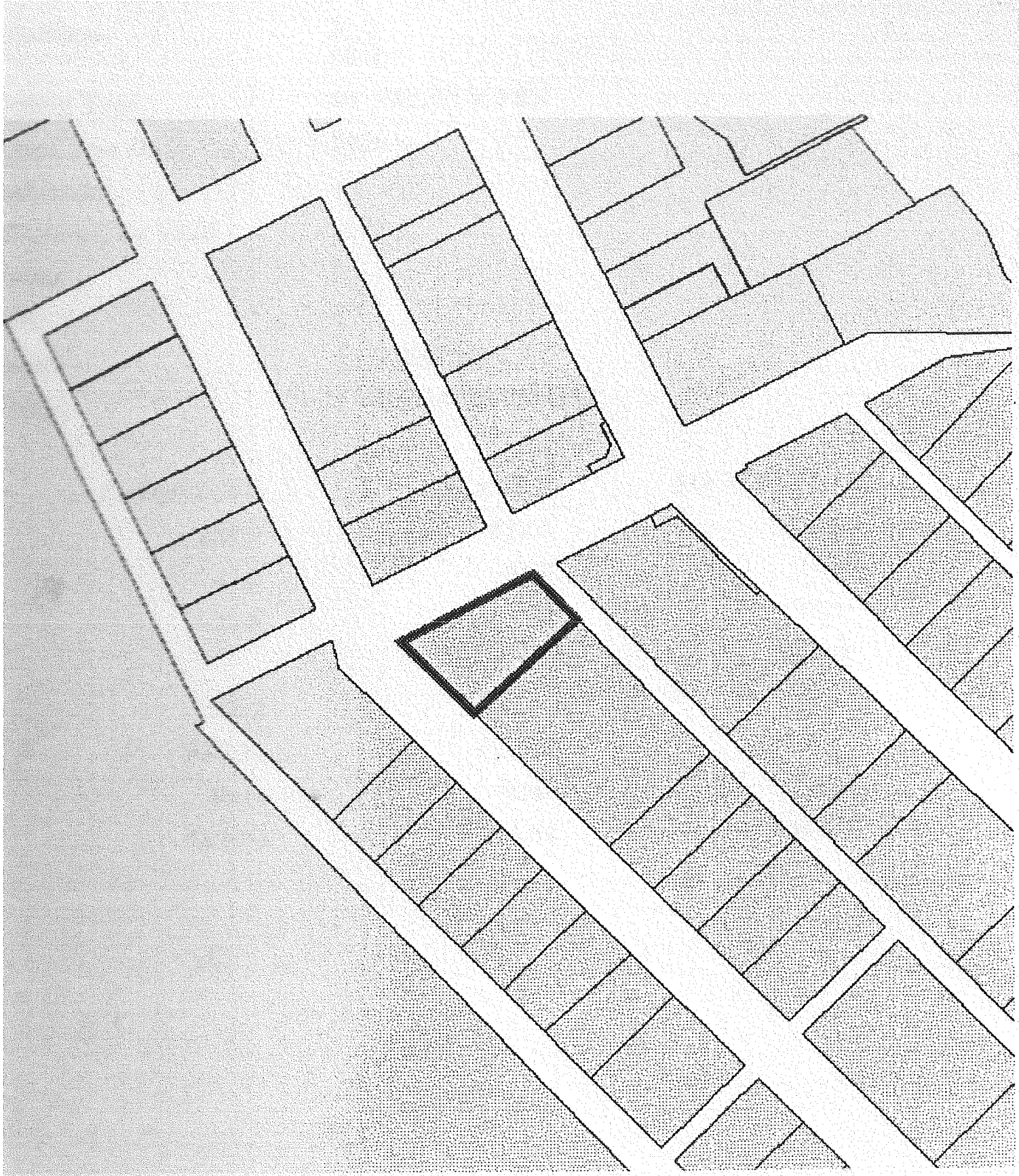
Please find attached some pictures of my property.

Thank you for your attention to these matters.

Respectfully

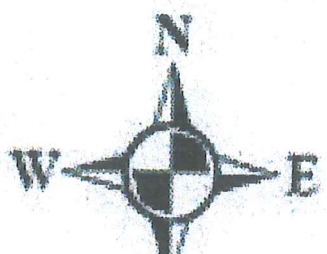
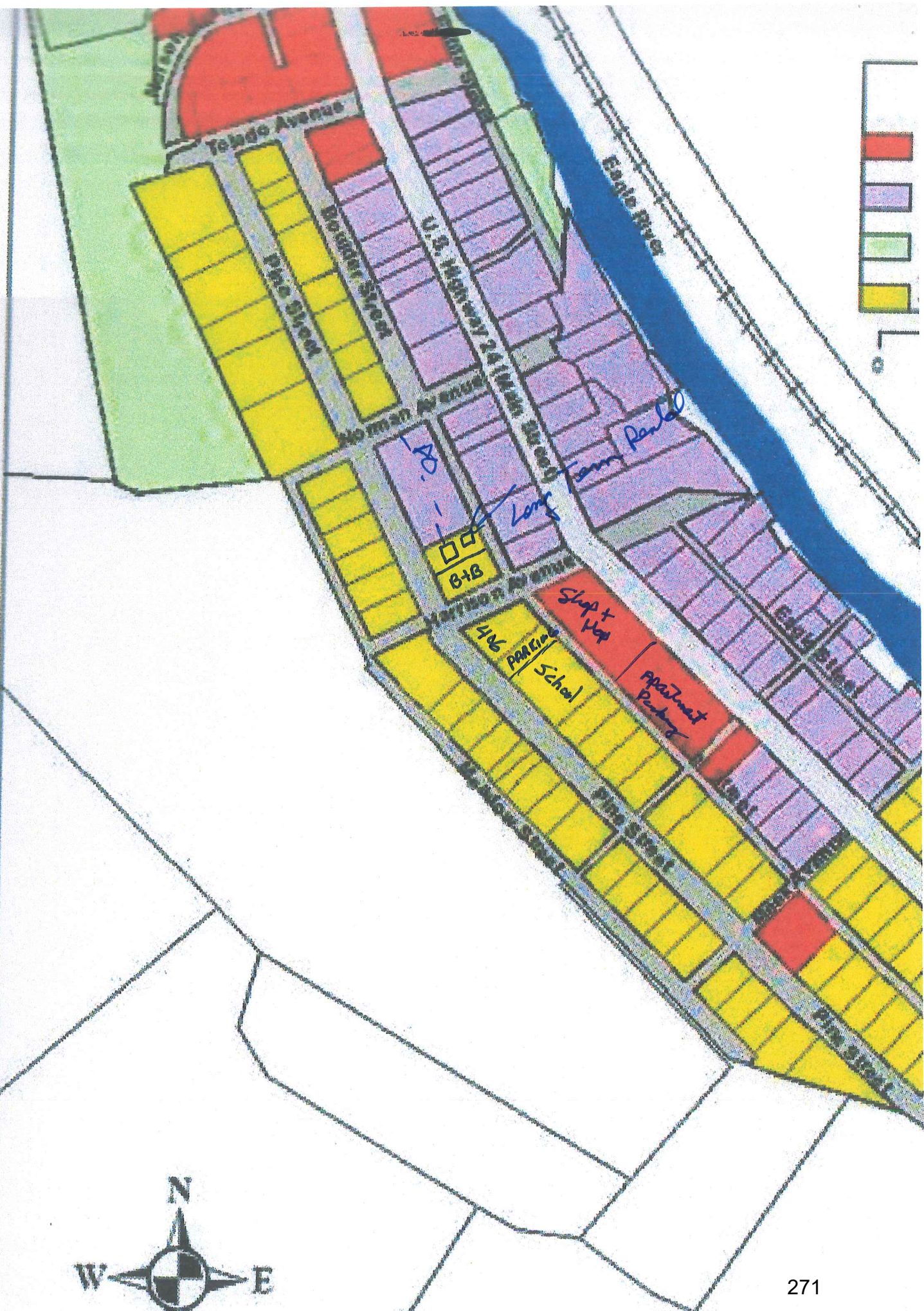
Tim Lahey

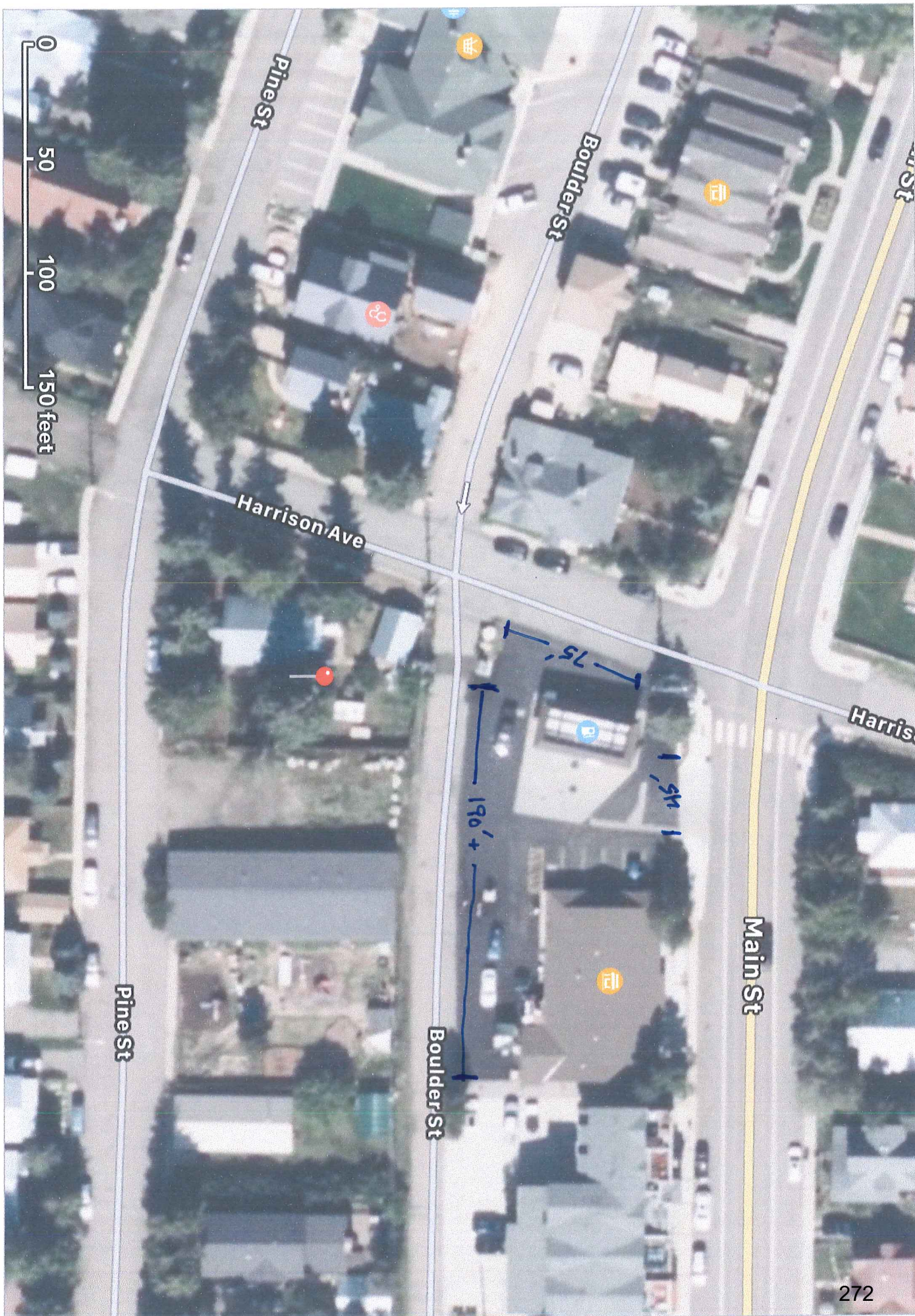
A handwritten signature in blue ink that reads "Tim Lahey". The signature is written in a cursive style with a large initial "T" and a checkmark-like flourish at the end.



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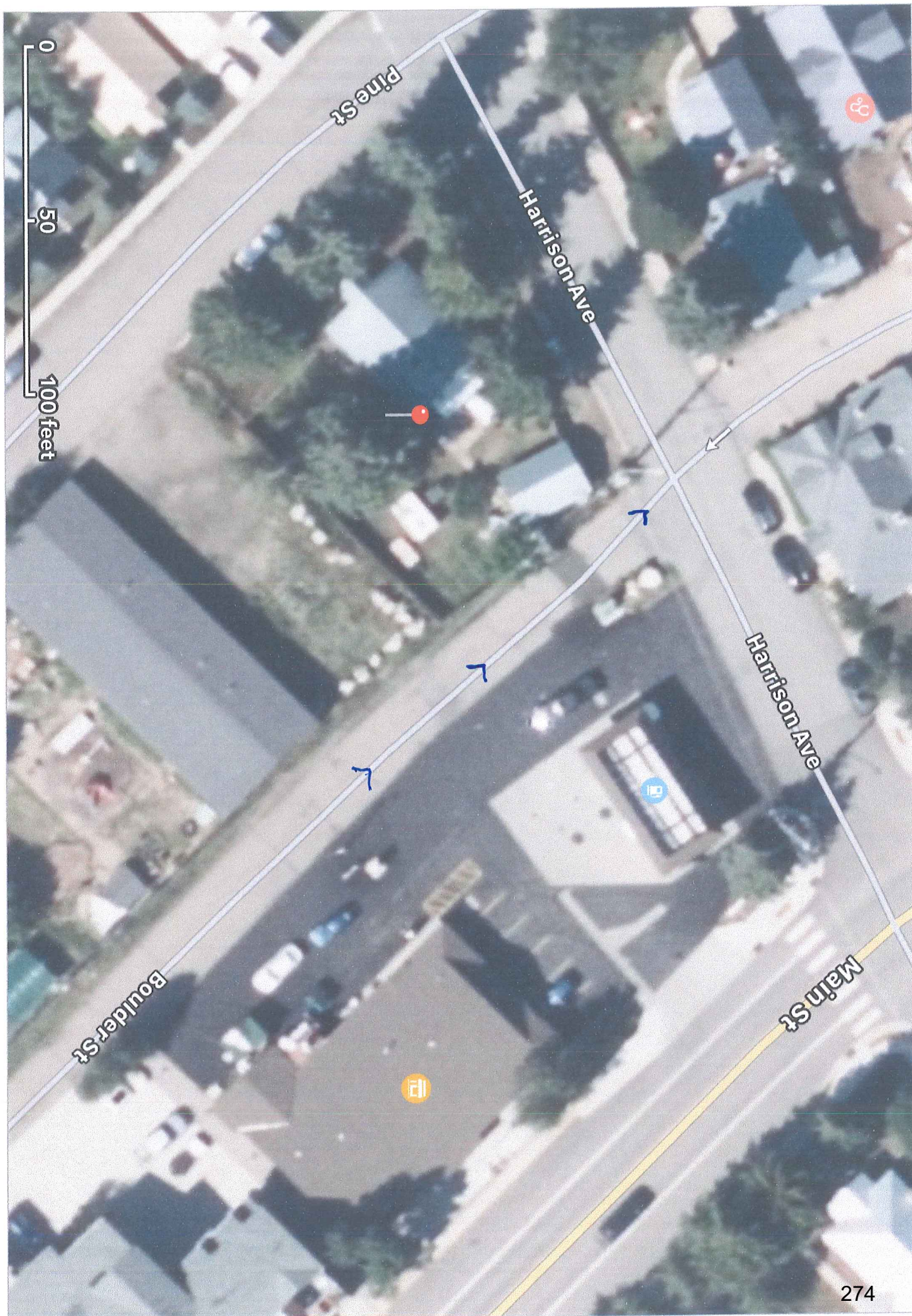
66. GRANTOR - TIMOTHY R. LAHEY, GRANTEE-BENEFICIARY  
CABLE TRUST.







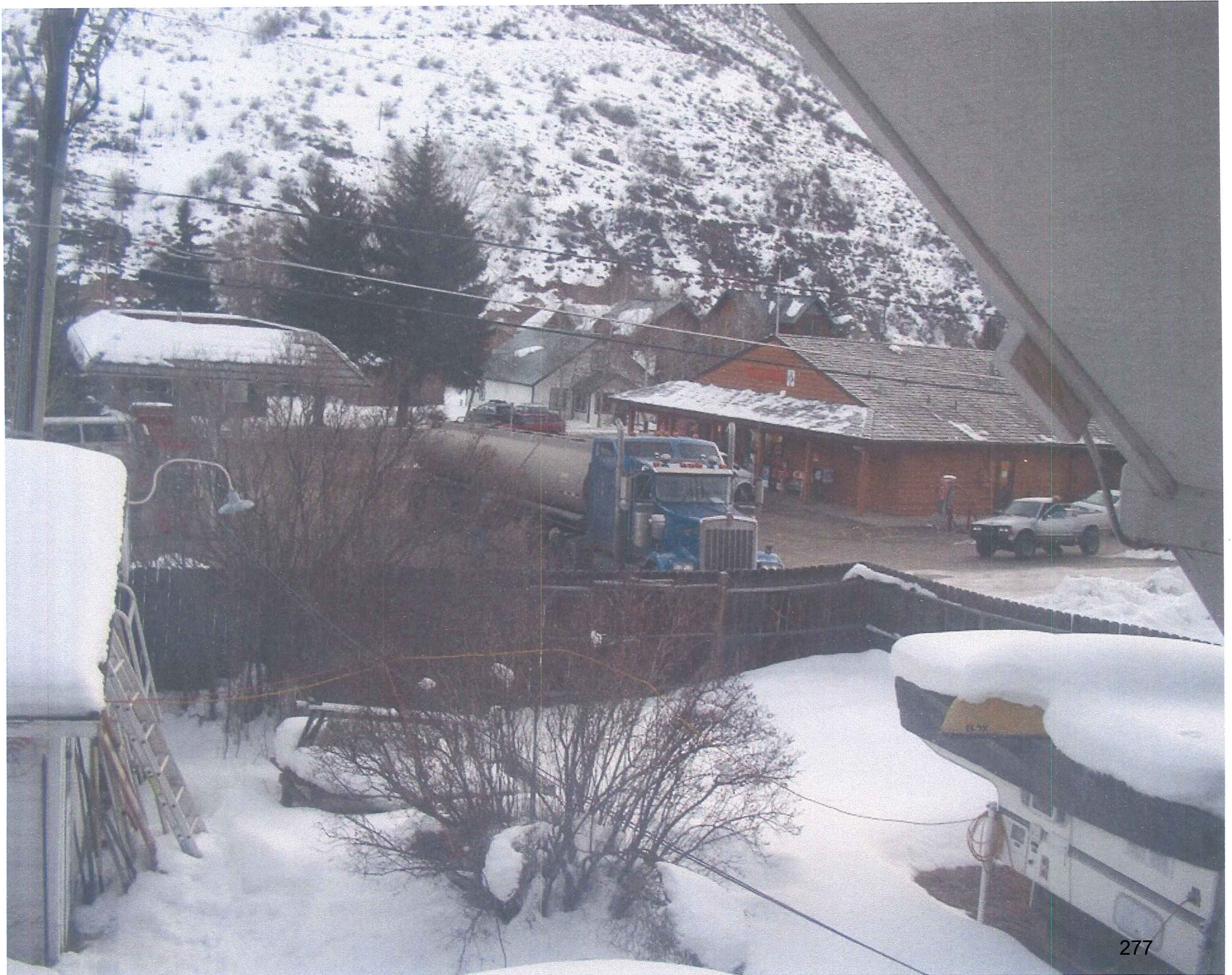




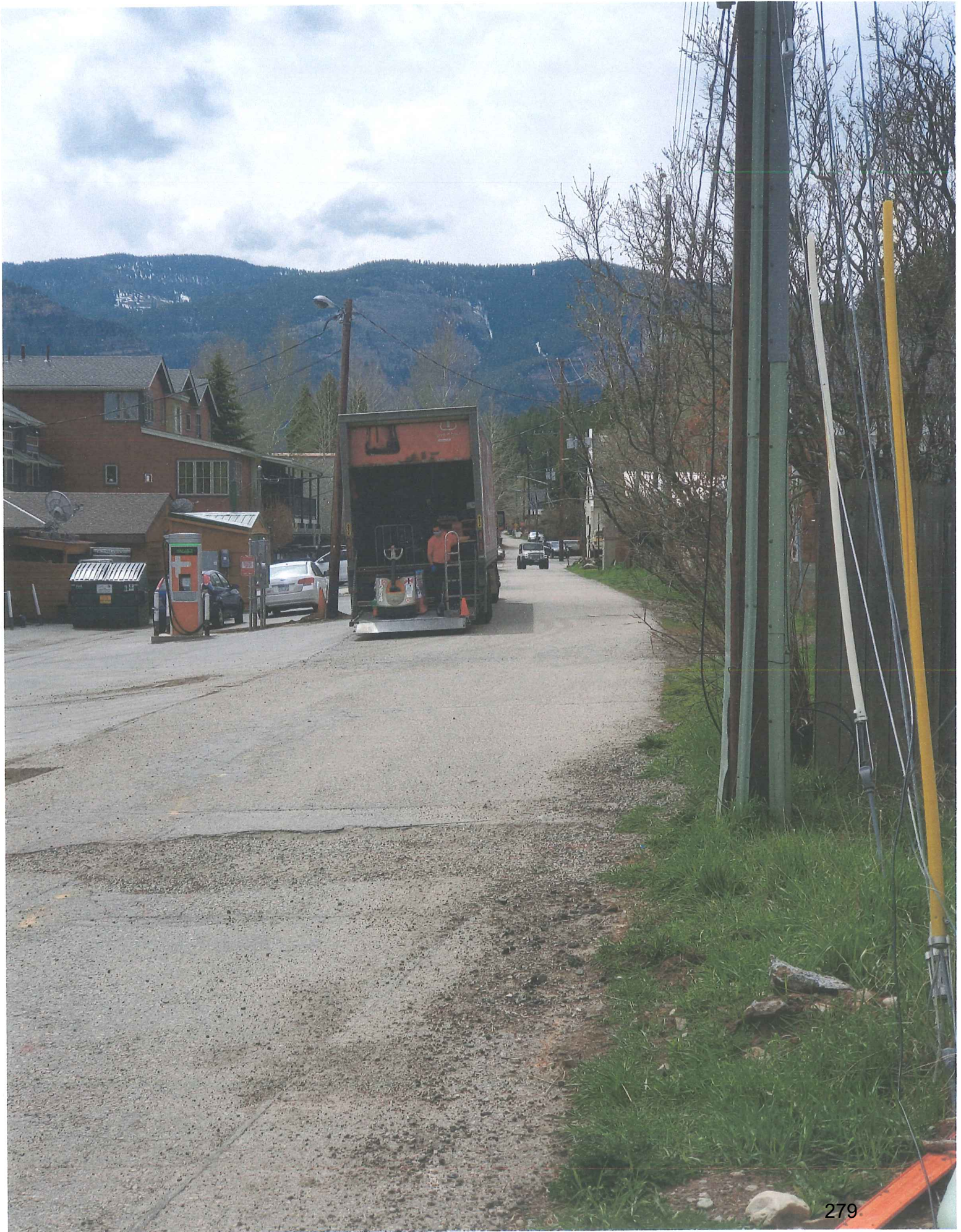


Drill reg ~~drug~~ drilling test holes for  
monitoring Fuel Tanks @ Step + Hop

(Very noisy)

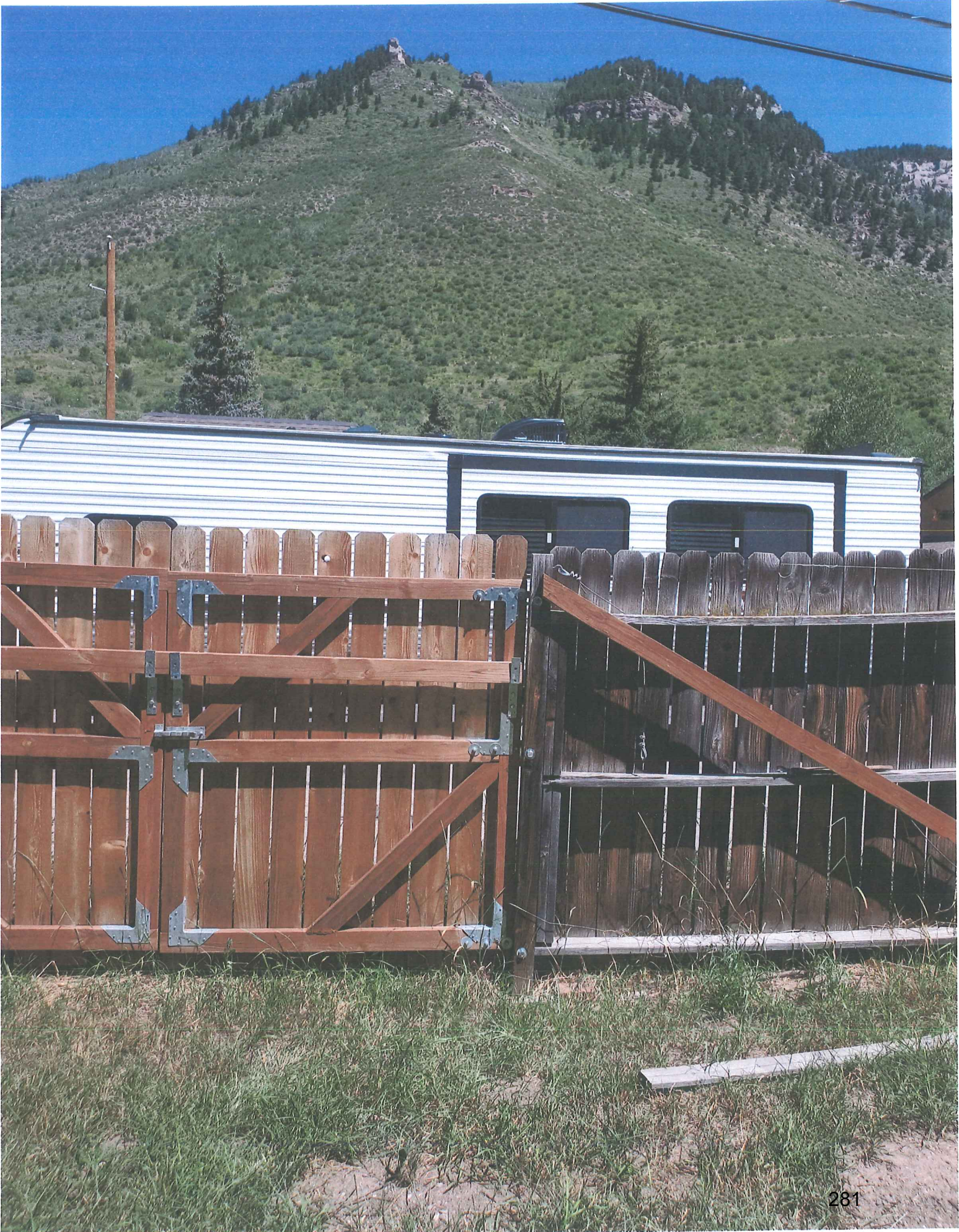














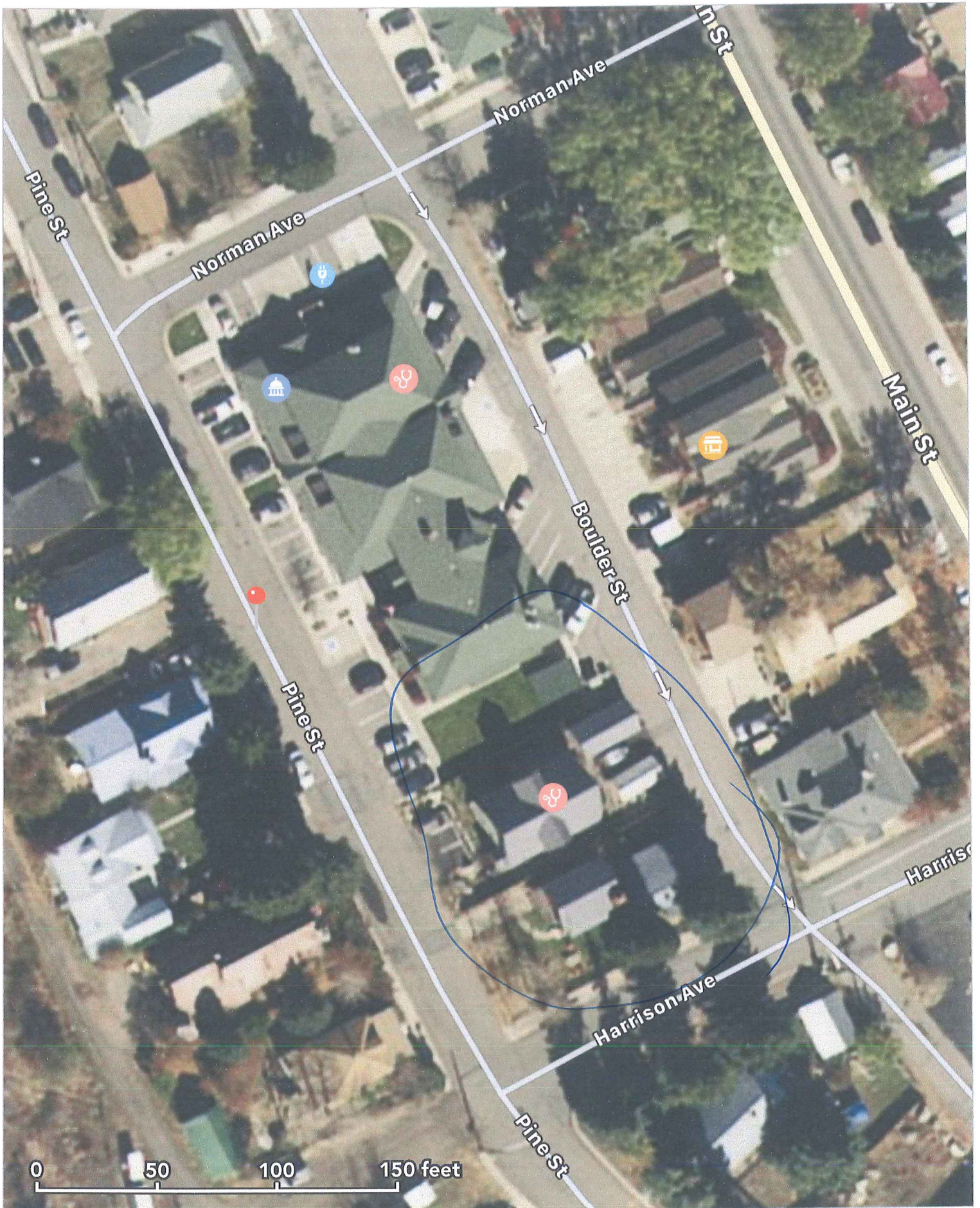
Boulder St

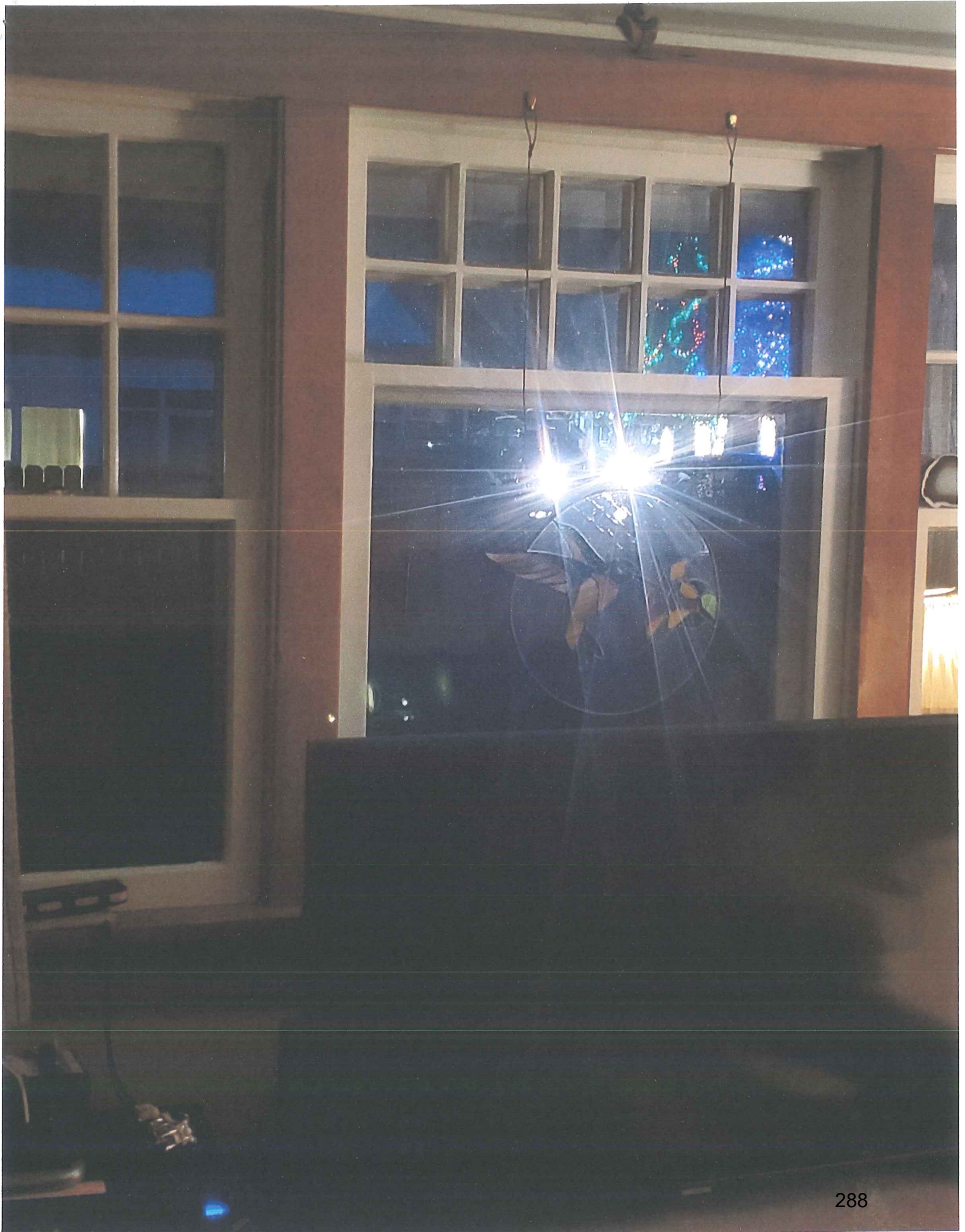
Truck block view of my house  
parked between School + My fence



Parked Truck blocking my walkway  
Picture from my front Door



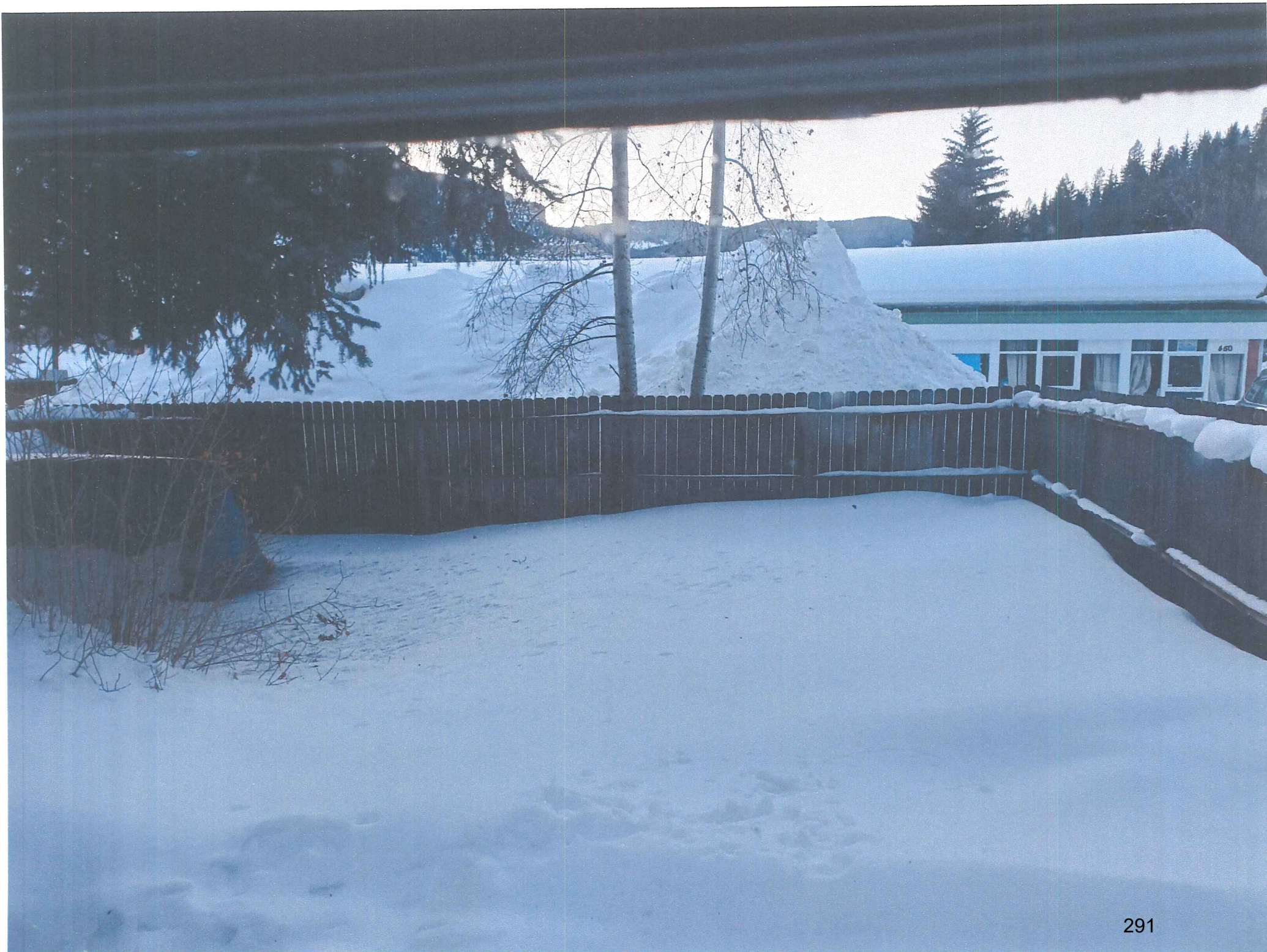






Headlights from car parked in School Lot





School parking Lot / Snow dump  
file from my living room window



School Cars facing my Learning Room

Sunday 10-2-22  
21:00

From my Living Room Window







School parking lot when used as shortcut  
to pine st





School parking lot

Trucks parked in front of my  
Garage to go to Sheep + Hog



Parked on Harrison



This rig was parked on Harrison  
next to my house

NO PARKING  
Sign

# 64 Harrison Street - Minturn Rentals and Property Management

[www.minturnrentals.com](http://www.minturnrentals.com)

*3 bedroom, 3 bath, sleeps 8*

Historic cottage in the rustic railroad and mining town of Minturn. Beautifully remodeled and decorated, preserving all the old charm and adding all the modern conveniences. New bathrooms and kitchen. Bright and cheery, with high ceilings and skylights. Beautiful hardwood floors. Just 10 minutes to Vail or Beaver Creek for world class skiing in the winter. Great golfing, hiking, horseback riding, and mountain biking in the summer. Farmers' market every Saturday in the summer. Walk to the historic Minturn Saloon for awesome margaritas and Mexican food, or to Sunrise Café for some of the best espresso drinks and pastries you've ever had. Located one block off Main Street – quiet, but conveniently located. Fenced yard for kids to play in the summer. Cook burgers on the grill and have a relaxing meal at the picnic table. 2 queen bedrooms, 1 king bedroom and a queen sofa bed in the second living area, 3 bathrooms. Cable TV, high speed internet. Fully equipped kitchen with new appliances. Washer/dryer.

*This property has only 1 onsite parking place*

To: Minturn Town Council  
From: Michelle Metteer  
Date: January 4, 2023  
RE: Town Manager Update



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### **Water Loss Reduction Efforts**

Bringing down Minturn's system water loss continues to be elusive. After installing a leak detection system on mainlines throughout town, (almost) completing a town-wide water meter change-out program, and actively addressing billing and meter read issues, the system loss numbers remain high.

Staff has discussed a revised contract with John Volk, water treatment plant operator, to allow additional time to actively focus on system line leak detection. Currently, there is a leak in the area of the Enclave, and it has yet to be determined if this leak is on a service line before the meter or on a main line. Staff has been busy with the replacement of the mixer in the old tank and therefore has been unable to begin excavation work to determine the leak's source.

### **New Water Tank**

Work has paused on the new water tank while we await the ability for the Minturn team to test out the new pumps. With the replacement work of the old tank's mixer, continual snow requiring almost daily plowing, and the efforts to find and eliminate leaks in the distribution system, along with Arnold and Micah both being unavailable for the foreseeable time, the team has not been able to address this effort. Fortunately, Minturn has contracted with a third party plowing and hauling company to help fill the gap with half of Minturn's public works team out, but other efforts have been paused.

### **Revitalizing Main Streets Grant Award RFQ**

The new CDOT representative has identified Minturn's requirement to execute all new RFQ's for phase two of Main Street's sidewalk efforts. Town staff had been hopeful the RFQ's and original teams from phase one would be approved in an effort to eliminate red tape and additional spending, but that has not been the case. We expect the RFQ for the completion of the phase two design and engineering work to be published later in January.

### **Water Billing & Policy Update**

Minturn residents will soon have online access to their monthly water bills and corresponding daily water use. This gives water users real-time access to their accounts and the ability to detect leaks within the home. With this in mind, **MINTURN WILL NO LONGER WAIVE COSTS ASSOCIATED WITH WATER LEAKS.** It will be the customer's responsibility to address leaks within the home as they arise. This is a change from the prior policy where Minturn would not charge a customer for lost water after the meter (because the customer typically didn't know they had a leak).

### **State & Tribal Assistance Grants (STAG)**

Minturn applied for and has been awarded \$2,000,000 toward the construction of a new water treatment plant. These funds are coming from the same Congressionally Directed Spending funds as the \$1,000,000 award for the water tank construction (the town is applying to have the \$1M deferred to the WTP construction given how late the EPA was to come out with the funds instructions for use and the tank construction almost complete).





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## **FUTURE MEETING AGENDA ITEMS**

Below reflects proposed topics to be scheduled at future Town Council meetings and is informational only. Dates and topics are subject to change.

### **January 4, 2023**

- Groundwater Cleanup update – Shop & Hop
- Consent Agenda: Resolution 01 – Series 2022 a Resolution setting the official posing sites for the Town of Minturn
- Discussion/Direction – Minturn Community Plan
- Ord \_\_\_ - Series 2023 (First Reading) An Ordinance approving the historic landmark designation for the Saloon Building located at 146 N Main St.
- Res \_\_\_ - Series 2023 A Resolution approving the 2023-25 Minturn Strategic Plan
- Res \_\_\_ - Series 2023 A Resolution approving the Little Beach Park sub area Master Plan
- Executive Session – Battle Mountain

### **January 18, 2023**

- Eagle County School District fee in lieu discussion
- Review/Renewal of Ordinance 05 – Series 2020; An Ordinance Renewing the Minturn Water Moratorium
- Ord \_\_\_ - Series 2023 (Second Reading) An Ordinance approving the historic landmark designation for the Saloon Building located at 146 N Main St.
- Res \_\_\_ - Series 2023 A Resolution approving the Community Plan
- Discussion/Direction – Raw water irrigation systems
- Executive Session – Battle Mountain

### **February 1, 2023**

- Discussion/Direction - Enterprise Fund deficit discussion

### **Dates to be Determined**

- Chapter 16 Zoning Amendments
- Assessment of UPRR property